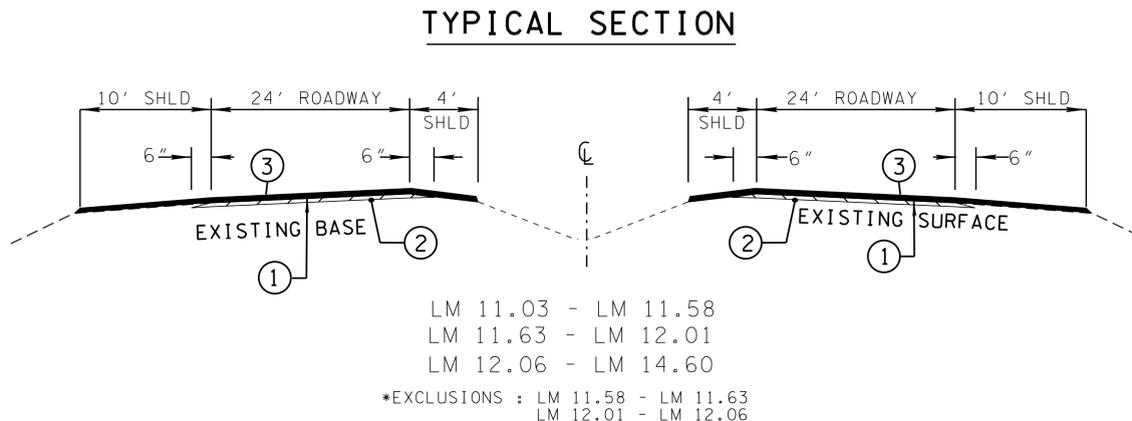


TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2015	STP/HSIP-NH-29(92)	2
		72079-3204-94	
		72079-8204-14	

ESTIMATED ROADWAY QUANTITIES			
ITEM NO.	DESCRIPTION	UNIT	QUANTITY
(1)	303-01 MINERAL AGGREGATE, TYPE A BASE, GRADING D	TON	965
(2)	403-01 BITUMINOUS MATERIAL FOR TACK COAT (TC)	TON	56
(3)(4)	405-01.01 BITUMINOUS MATERIAL (BSC)	TON	208
	405-01.02 MINERAL AGGREGATE (BSC)	TON	1603
(3)(5)(6)	411-03.13 ACS MIX(PG70-22) THIN LIFT D ASPHALT	TON	7800
(7)	415-01.02 COLD PLANING BITUMINOUS PAVEMENT	S.Y.	3378
	712-01 TRAFFIC CONTROL	LS	1
	712-04.02 FLEXIBLE DRUMS (CHANNELIZATION)	EACH	160
	712-05.03 WARNING LIGHTS (TYPE C)	EACH	40
(8)	712-06 SIGNS (CONSTRUCTION)	S.F.	1298
	712-08.03 ARROW BOARD (TYPE C)	EACH	2
	713-16.01 CHANGEABLE MESSAGE SIGN UNIT	EACH	2
(9)	716-05.01 PAINTED PAVEMENT MARKING (4" LINE)	L.M.	18.0
	717-01 MOBILIZATION	LS	1

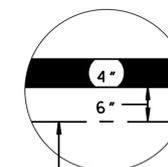
FOOTNOTES

- (1) FOR LOW SHOULDERS WHERE AND AS DIRECTED BY THE TDOT PROJECT SUPERVISOR. QUANTITY BASED ON 3.47 MILES x 4' TOTAL WIDTH x 2" DEPTH
- (2) INCLUDES 6.4 TON FOR CO RDS, CROSS-OVERS, BUS ENTS, ETC. AND SPOT LEVELING.
- (3) A MATERIAL TRANSFER DEVICE (MTD) IS TO BE USED ON ALL MIXES. SEE SPECIAL PROVISION NO. 407G. ALL COST SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THIS ITEM.
- (4) REFER TO SHEET 6 FOR BSC W/HMA OVERLAY NOTES.
- (5) SEE SPECIAL PROVISION 411C FOR RIDEABILITY
- (6) INCLUDES 900 TONS FOR COUNTY ROADS, CROSS-OVERS, BUSINESS ENTRANCES, ETC. AND SPOT LEVELING TO BE PLACED AS DIRECTED BY TDOT PROJECT SUPERVISOR.
- (7) SEE DETAIL THIS SHEET FOR LOCATIONS.
- (8) SEE SIGN TABULATION BLOCK THIS SHEET FOR DETAILS.

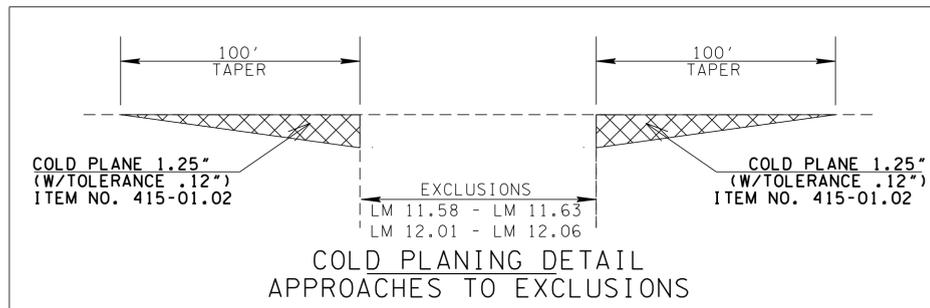


PROPOSED PAVEMENT SCHEDULE

①	TACK COAT (TC) ITEM 403-01 BITUMINOUS MATERIAL FOR TACK COAT (TC) (AT 0.07 GAL./S.Y.)
②	BITUMINOUS SEAL COAT (CHIP SEAL) ITEM 405-01.01 BITUMINOUS MATERIAL FOR BITUMINOUS SEAL COAT (BSC) @ RATE 0.20 - 0.45 GAL/SY ITEM 405-01.02 MINERAL AGGREGATE FOR BITUMINOUS SEAL COAT (BSC) @ RATE 20 - 30 LB/SY (SEE SPECIAL NOTES, SHEET NO. 5, FOR SPECIFICATIONS.)
③	BITUMINOUS SURFACE "THIN LIFT" 0.75" THICK @ 85 LBS/SY ITEM 411-03.13 ACS MIX (PG 70-22) THIN LIFT D ASPHALT @ 85 LBS/S.Y. SEE SPECIAL PROVISION NO. SP411TLD.



LANE JOINT IS TO BE OFF SET 6" FROM CENTER OF ROADWAY.



ESTIMATED SAFETY QUANTITIES			
ITEM NO.	DESCRIPTION	UNIT	QUANTITY
(S1)	411-12.01 SCORING SHOULDERS (CONTINUOUS) (16IN WIDTH)	L.M.	7
(S2)	411-12.02 SCORING SHOULDERS (NON-CONTINUOUS) (16IN WIDTH)	L.M.	5.2
(S3)	706-05 GUARDRAIL ADJUSTMENT	L.F.	4781
(S6)	713-02.14 FLEXIBLE DELINEATOR (WHITE)	EACH	78
	716-01.23 Snwplwble Pvmt Mrkrs (Bi-Dir)(2 Color)	EACH	643
(S4)	716-02.05 PLASTIC PAVEMENT MARKING (STOP LINE)	L.F.	269
	716-02.06 PLASTIC PAVEMENT MARKING (TURN LANE ARROW)	EACH	25
(S5)	716-12.02 ENHANCED FLATLINE THERMO PVMT MRKNG (6IN LINE)	L.M.	17
	716-12.05 ENHANCED FLATLINE THERMO PVMT MRKNG (6IN DOTTED LINE)	L.F.	1100

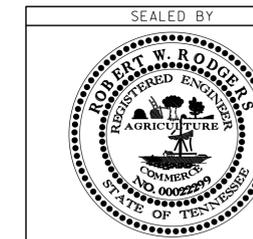
FOOTNOTES

- (S1) FOR INSIDE SHOULDERS ONLY
- (S2) FOR OUTSIDE SHOULDERS ONLY.
- (S3) ADJUSTMENTS TO BE MADE WHERE DIRECTED BY TDOT PROJECT SUPERVISOR.
- (S4) THE CONTRACTOR MAY ELECT TO SUBSTITUTE PREFORMED PLASTIC FOR THE THERMOPLASTIC. PREFORMED PLASTIC SHALL BE PAID FOR AT THE SAME UNIT PRICE AS BID FOR THERMOPLASTIC.
- (S5) FOR PERMANENT PAVEMENT MARKING ONLY.
- (S6) TO BE USED AT CROSSOVERS AND ALONG EDGE OF ROADWAY IN ACCORDANCE WITH STD. DWG. T-S-11.

TABULATED SIGNS (CONSTRUCTION) 712-06

QUANTITY	M.U.T.C.D. NO.	DESCRIPTION	SIZE		ITEM NO.
			IN	IN	712-06 S.F.
4	G20-1	ROAD WORK NEXT 4 MILES	48	24	32
17	G20-2	END ROAD WORK	48	24	136
4	W1-4R	SHIFT RIGHT SYMBOL	48	48	64
4	W3-4	BE PREPARED TO STOP	48	48	64
36	W8-11	UNEVEN LANES	36	36	324
4	W4-2L	MERGE LEFT SYMBOL	48	48	64
13	W20-1	ROAD WORK AHEAD	48	48	208
4	W20-5R	RIGHT LANE CLOSED AHEAD	48	48	64
4	W20-1	ROAD WORK 1500 FT	48	48	64
4	W20-1	ROAD WORK 1000 FT	48	48	64
4	W20-1	ROAD WORK 500 FT	48	48	64
2	W20-4	ONE LANE ROAD AHEAD	36	36	18
2	W20-7a	FLAGGER	48	48	32
4	TN-64	GROOVED PAVEMENT	48	48	64
4	W21-2	FRESH OIL	36	36	36
TOTAL					1298

SEE FIGURE 6F-1 OF FIGURES SHOWN IN CURRENT M.U.T.C.D. THIS CONSTRUCTION SIGNING IS TO BE CONSTRUCTED AS A MINIMUM. OTHER SIGNS AS DIRECTED BY THE ENGINEER MAY BE REQUIRED DURING DIFFERENT PHASES.



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ESTIMATED QUANTITIES, TYPICAL SECTION & DETAILS

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2015	STP/HSIP-NH-29(92)	3
		72079-3204-94	
		72079-8204-14	

PAVEMENT EDGE DROP-OFF TRAFFIC CONTROL NOTES

- A. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES OR TRAFFIC LANE AND SHOULDER WHERE THE TRAFFIC LANE IS BEING USED BY TRAFFIC, CAUSED BY BASE, PAVING OR RESURFACING:
1. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 0.75 INCH AND NOT EXCEEDING 2 INCHES:
 - a. WARNING SIGNS, UNEVEN PAVEMENT (W8-11) AND/OR SHOULDER DROP-OFF (W8-9A), SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.
 - b. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY ADDED PAVEMENT SHALL BE ELIMINATED WITHIN THREE WORKDAYS.
 - c. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY COLD PLANING SHALL BE ELIMINATED WITHIN THREE WORKDAYS.
 - d. WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE TRAFFIC LANE BEING UTILIZED BY TRAFFIC AND SHOULDER THE DIFFERENCE IN ELEVATION SHALL BE ELIMINATED WITHIN SEVEN WORKDAYS AFTER THE CONDITION IS CREATED.

UTILITY OWNERS				
UTILITY	NAME	ADDRESS	CONTACT PERSON	PHONE #
Telephone	Bellsouth dba AT&T	300 E Martin Luther King Boulevard, 5th Floor, Chattanooga TN 37403	Tim Manley	(423) 752-9144
Gas	Middle Tennessee Natural Gas	PO Box 670, Smithville TN 37166	Matt Stennett	(615) 597-0515
Water	North Utility District of Rhea County	PO Box 1089, Spring City TN 37381	Doug MacEwen	(423) 365-2680
Sewer	Spring City, Town of	PO Box 369, Spring City TN 37381	Billy Ray Patton	(423) 365-6441
Electric	Tennessee Valley Authority	1101 Market St MR3F, Chattanooga TN 37402	Missy Davis	(423) 751-7542
Electric	Volunteer Energy Cooperative	PO Box 277, Decatur TN 37322	Rody Blevins	(423) 334-7001

Pre-Construction Ride Numbers

Project Ref. No.	STP/HSIP-NH-29(92)	Contract No.	
Project No.	98023-4217-04	Region	2
Contractor	To Be Awarded	County	Rhea
Weather / Temp.	Cloudy 45°F	Route	SR-29 (US-27)
Standard Specs.	Special Provision SP411C	Test Date	
Equip. Calib. Date	December 10, 2015	Test No.	precon
Milling?	No	Pre-Con Test Date	January 5, 2015
Number of lifts			

Test Performed by TDOT Road Profiler								Average Roughness Pre-Construction HC-IRI Number Per Mile 71.9
Test Limits (mile)		Direction and Measured Roughness (HC-IRI Number Per Mile)						
From	To	NSEW	*Lane	Pre Ride Number	Payment Table	Max. # for Final Ride	Min. Required % Improvement	
0.00	1.00	N	1	96	Table 1	55.0		
1.00	2.00	N	1	96	Table 1	55.0		
2.00	3.00	N	1	86	Table 1	55.0		
3.00	3.52	N	1	82	Table 1	55.0		
0.00	1.00	N	2	63	Table 1	55.0		
1.00	2.00	N	2	56	Table 1	55.0		
2.00	3.00	N	2	57	Table 1	55.0		
3.00	3.52	N	2	69	Table 1	55.0		
0.00	1.00	S	1	59	Table 1	55.0		
1.00	2.00	S	1	59	Table 1	55.0		
2.00	3.00	S	1	87	Table 1	55.0		
3.00	3.43	S	1	61	Table 1	55.0		
0.00	1.00	S	2	56	Table 1	55.0		
1.00	2.00	S	2	49	Table 1	55.0		
2.00	3.00	S	2	92	Table 1	55.0		
3.00	3.43	S	2	62	Table 1	55.0		

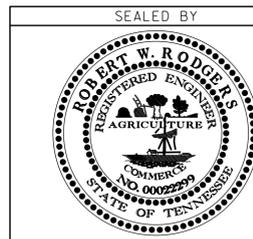
REMARKS

from North of Payne Lane to South of Boofer Lane

Excludes 2 concrete bridges

*Lanes to be Left to Right each Direction - 1,2,3,4, etc. with 1 being the Inside Lane

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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

UTILITY OWNERS,
DROP OFF NOTES
& PRE-CONST
RIDE NOS.

GENERAL NOTES

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2015	STP/HS1P-NH-29(92)	4
		72079-3204-94	
		72079-8204-14	

UTILITIES

- (1) THE LOCATIONS OF UTILITIES SHOWN WITHIN THESE PLANS ARE APPROXIMATE ONLY. EXACT LOCATIONS SHALL BE DETERMINED IN THE FIELD BY CONTACTING THE UTILITY COMPANIES INVOLVED. NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC., AT 1-800-351-1111 AS REQUIRED BY TCA 65-31-106 WILL BE REQUIRED.
- (2) UNLESS OTHERWISE NOTED, ALL UTILITY ADJUSTMENTS WILL BE PERFORMED BY THE UTILITY OR IT'S REPRESENTATIVE. THE CONTRACTOR AND UTILITY OWNERS WILL BE REQUIRED TO COOPERATE WITH EACH OTHER IN ORDER TO EXPEDITE THE WORK REQUIRED BY THIS CONTRACT. ON CONTRACTS WHERE CONSTRUCTION STAKES, LINES, AND GRADES ARE CONTRACT ITEMS, THE CONTRACTOR WILL BE REQUIRED TO PROVIDE RIGHT-OF-WAY OR SLOPE STAKES, DITCH OR STREAM BED GRADES, OR OTHER ESSENTIAL SURVEY STAKING TO PREVENT CONFLICTS WITH THE HIGHWAY CONSTRUCTION. FREQUENTLY, THIS WILL BE REQUIRED AS THE FIRST ITEM OF WORK AND AT ANY LOCATION ON THE PROJECT DIRECTED BY THE ENGINEER.
- (3) THE CONTRACTOR WILL PROVIDE ALL NECESSARY PROTECTIVE MEASURES TO SAFEGUARD EXISTING UTILITIES FROM DAMAGE DURING CONSTRUCTION OF THIS PROJECT. IN THE EVENT THAT SPECIAL EQUIPMENT IS REQUIRED TO WORK OVER AND AROUND THE UTILITIES, THE CONTRACTOR WILL BE REQUIRED TO FURNISH SUCH EQUIPMENT. THE COST OF PROTECTING UTILITIES FROM DAMAGE AND FURNISHING SPECIAL EQUIPMENT WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (4) PRIOR TO SUBMITTING HIS BID, THE CONTRACTOR WILL BE SOLELY RESPONSIBLE FOR CONTACTING OWNERS OF ALL AFFECTED UTILITIES IN ORDER TO DETERMINE THE EXTENT TO WHICH UTILITY RELOCATIONS AND/OR ADJUSTMENTS WILL HAVE UPON THE SCHEDULE OF WORK FOR THE PROJECT. WHILE SOME WORK MAY BE REQUIRED 'AROUND' UTILITY FACILITIES THAT WILL REMAIN IN PLACE, OTHER UTILITY FACILITIES MAY NEED TO BE ADJUSTED CONCURRENTLY WITH THE CONTRACTOR'S OPERATIONS. ADVANCE CLEAR CUTTING MAY BE REQUIRED BY THE ENGINEER AT ANY LOCATION WHERE CLEARING IS CALLED FOR IN THE SPECIFICATIONS AND CLEAR CUTTING IS NECESSARY FOR A UTILITY RELOCATION. ANY ADDITIONAL COST WILL BE INCLUDED IN THE UNIT PRICE BID FOR THE CLEARING ITEM SPECIFIED IN THE PLANS.
- (5) THE CONTRACTOR SHALL NOTIFY EACH INDIVIDUAL UTILITY OWNER OF HIS PLAN OF OPERATION IN THE AREA OF THE UTILITIES. PRIOR TO COMMENCING WORK, THE CONTRACTOR SHALL CONTACT THE UTILITY OWNERS AND REQUEST THEM TO PROPERLY LOCATE THEIR RESPECTIVE UTILITY ON THE GROUND. THIS NOTIFICATION SHALL BE GIVEN AT LEAST THREE (3) BUSINESS DAYS PRIOR TO COMMENCEMENT OF OPERATIONS AROUND THE UTILITY IN ACCORDANCE WITH TCA 65-31-106.

MISCELLANEOUS

- (6) THE CONTRACTOR SHALL BE REQUIRED TO REMOVE AND RESET MAILBOXES WHERE AND AS DIRECTED BY THE ENGINEER.
- (7) NOTHING IN THE GENERAL NOTES OR SPECIAL PROVISIONS SHALL RELIEVE THE CONTRACTOR FROM HIS RESPONSIBILITIES TOWARD THE SAFETY AND CONVENIENCE OF THE GENERAL PUBLIC AND THE RESIDENTS ALONG THE PROPOSED CONSTRUCTION AREA

PAVEMENT MARKINGS

- (8) PERMANENT PAVEMENT LINE MARKINGS SHALL BE 6" ENHANCED FLATLINE THERMOPLASTIC INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-12.02, ENHANCED FLATLINE THERMO PVMT MRKNG (6IN LINE), L.M. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.

PAVEMENT - PAVING

- (9) THE CONTRACTOR SHALL BE REQUIRED TO COLD PLANE AND PAVE IN THE DIRECTION OF TRAFFIC.

RESURFACING

- (10) WHERE DIRECTED BY THE TDOT ENGINEER, THE CONTRACTOR SHALL BE REQUIRED TO SHAPE PUBLIC SIDE ROADS, BUSINESS ENTRANCES, AND

PRIVATE DRIVES, AS WELL AS CLEANING OF EXISTING DRAINS BEFORE PLACING MATERIALS. ALL COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.

- (11) ALL PUBLIC SIDE ROADS SHALL BE PAVED ONE PAVER WIDTH THROUGH THE INTERSECTION AS A MINIMUM. A SATISFACTORY TRANSITION FROM THE NEW PAVEMENT TO THE EXISTING GRADE OF THE INTERSECTING PUBLIC ROAD OR BUSINESS ENTRANCE SHALL BE PROVIDED. SHOULD THE PAVEMENT OF THE INTERSECTING PUBLIC ROAD BE DISTRESSED, THE RESURFACING WIDTH MAY BE INCREASED TO THE NORMAL RIGHT OF WAY LINE.
- (12) IN ALL CASES, THE LENGTH OF THE PAVEMENT TRANSITION, THE THICKNESS AND WIDTH OF THE RESURFACING AND ANY ADDITIONAL PAVEMENT MATERIALS SHALL BE AS DIRECTED BY THE TDOT ENGINEER.

CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

- (13) ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE WEEK BEFORE NEEDED, IF THE SIGN FACE IS FULLY COVERED.
- (14) IF THE CONTRACTOR MOVES OFF THE PROJECT, HE SHALL COVER OR REMOVE ALL UNNEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OF REMOVAL, COVERING, AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO 712-06, SIGNS (CONSTRUCTION) PER SQUARE FOOT.
- (15) A LONG TERM BUT SPORADIC USE WARNING SIGN, SUCH AS A FLAGGER SIGN, MAY REMAIN IN PLACE WHEN NOT REQUIRED PROVIDED THE SIGN FACE IS FULLY COVERED.
- (16) TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.
- (17) USE OF BARRICADES, PORTABLE BARRIER RAILS, VERTICAL PANELS, AND DRUMS SHALL BE LIMITED TO THE IMMEDIATE AREAS OF CONSTRUCTION WHERE A HAZARD IS PRESENT. THESE DEVICES SHALL NOT BE STORED ALONG THE ROADWAY WITHIN THIRTY (30) FEET OF THE EDGE OF THE TRAVELED WAY BEFORE OR AFTER USE UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL INCREASE TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. THESE DEVICES SHALL BE REMOVED FROM THE CONSTRUCTION WORK ZONE WHEN THE ENGINEER DETERMINES THEY ARE NO LONGER NEEDED. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.
- (18) THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY, WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHEN THE LANE IS OPEN TO TRAFFIC UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO PARK WITHIN THIRTY (30) FEET OF AN OPEN TRAFFIC LANE AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE.. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.
- (19) ALL DETOUR AND CONSTRUCTION SIGNING SHALL BE IN STRICT ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

EROSION PREVENTION AND SEDIMENT CONTROL

LITTER, DEBRIS, WASTE, PETROLEUM

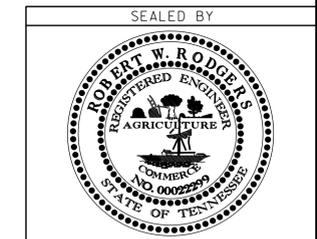
- (20) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT LITTER, CONSTRUCTION DEBRIS, AND CONSTRUCTION

WASTES FROM ENTERING WATERS OF THE STATE/U.S. THESE MATERIALS WILL BE PICKED UP AND REMOVED FROM STORMWATER EXPOSURE PRIOR TO ANTICIPATED STORM EVENTS. AFTER USE, MATERIALS USED FOR EPSC WILL BE REMOVED FROM THE SITE.

- (21) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS TO ENSURE THAT PETROLEUM PRODUCTS OR OTHER CHEMICAL POLLUTANTS ARE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. ALL EQUIPMENT REFUELING, SERVICING, AND STAGING AREAS SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL LAWS, RULES, REGULATIONS, AND ORDINANCES, INCLUDING THOSE OF THE NATIONAL FIRE PROTECTION ASSOCIATION (NFFPA). APPROPRIATE CONTAINMENT MEASURES FOR THESE AREAS SHALL BE USED. ALL SPILLS MUST BE REPORTED TO THE APPROPRIATE AGENCY, AND MEASURES SHALL BE TAKEN IMMEDIATELY TO PREVENT THE POLLUTION OF WATERS OF THE STATE/U.S., INCLUDING GROUNDWATER, SHOULD A SPILL OCCUR.
- (22) IF PORTABLE SANITARY FACILITIES ARE PROVIDED ON CONSTRUCTION SITES, SANITARY WASTE WILL BE COLLECTED FROM THE PORTABLE UNITS IN A TIMELY MANNER BY A LICENSED WASTE MANAGEMENT CONTRACTOR OR AS REQUIRED BY ANY LOCAL REGULATIONS. THE CONTRACTOR WILL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF SANITARY WASTE.
- (23) ALL HAZARDOUS WASTE MATERIALS WILL BE DISPOSED OF IN A MANNER WHICH IS COMPLIANT WITH LOCAL OR STATE REGULATIONS. SITE PERSONNEL WILL BE INSTRUCTED IN THESE PRACTICES, AND THE INDIVIDUAL DESIGNATED AS THE CONTRACTOR'S ON-SITE REPRESENTATIVE WILL BE RESPONSIBLE FOR SEEING THAT THESE PRACTICES ARE FOLLOWED. THE CONTRACTOR WILL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF HAZARDOUS MATERIAL.
- (24) WASTE MATERIAL (EARTH, ROCK, ASPHALT, CONCRETE, ETC.) NOT REQUIRED FOR THE CONSTRUCTION OF THE PROJECT WILL BE DISPOSED OF BY THE CONTRACTOR. THE CONTRACTOR WILL OBTAIN ANY AND ALL NECESSARY PERMITS INCLUDING, BUT NOT LIMITED TO NPDES, AQUATIC RESOURCES ALTERATION PERMIT(S) CORPS OF ENGINEERS SECTION 404 PERMITS, AND TVA SECTION 26A PERMITS TO DISPOSE OF WASTE MATERIALS.

SPILL PREVENTION, MANAGEMENT AND NOTIFICATION

- (25) ONLY NEEDED PRODUCTS WILL BE STORED ON-SITE BY THE CONTRACTOR. THE CONTRACTOR WILL STORE ALL MATERIALS UNDER COVER AND IN APPROPRIATE CONTAINERS, PRODUCTS MUST BE STORED IN ORIGINAL CONTAINERS AND LABELED. MATERIAL MIXING WILL BE CONDUCTED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS. THE CONTRACTOR'S SITE SUPERINTENDENT WILL INSPECT MATERIAL STORAGE AREA REGULARLY TO ENSURE PROPER USE AND DISPOSAL.
- (26) WHEN POSSIBLE, ALL PRODUCTS WILL BE USED COMPLETELY BEFORE PROPERLY DISPOSING OF THE CONTAINER OFF SITE, THE MANUFACTURER'S DIRECTIONS FOR DISPOSAL OF MATERIALS AND CONTAINERS WILL BE FOLLOWED.
- (27) ALL ON-SITE VEHICLES WILL BE MONITORED FOR LEAKS AND RECEIVE REGULAR PREVENTIVE MAINTENANCE TO REDUCE THE CHANCE OF LEAKAGE.
- (28) ALL PAINT CONTAINERS WILL BE TIGHTLY SEALED AND STORED WHEN NOT REQUIRED FOR USE. EXCESS PAINT WILL BE DISPOSED OF ACCORDING TO THE MANUFACTURER'S INSTRUCTIONS AND APPLICABLE STATE AND LOCAL REGULATIONS.
- (29) FOR ALL HAZARDOUS MATERIALS STORED ON SITE, THE MANUFACTURER'S RECOMMENDED METHODS FOR SPILL CLEAN UP WILL BE CLEARLY POSTED, SITE PERSONNEL WILL BE MADE AWARE OF THE PROCEDURES AND THE LOCATIONS OF THE INFORMATION AND CLEANUP SUPPLIES.
- (30) APPROPRIATE CLEANUP MATERIALS AND EQUIPMENT WILL BE MAINTAINED BY THE CONTRACTOR IN THE MATERIALS STORAGE AREA ON-SITE AND UNDER COVER. SPILL RESPONSE EQUIPMENT WILL BE INSPECTED AND MAINTAINED BY THE CONTRACTOR AS NECESSARY TO REPLACE ANY MATERIALS USED IN SPILL RESPONSE ACTIVITIES.
- (31) ALL SPILLS WILL BE CLEANED IMMEDIATELY AFTER DISCOVERY AND THE MATERIALS DISPOSED OF PROPERLY, THE SPILL AREA WILL BE KEPT WELL VENTILATED AND PERSONNEL WILL WEAR APPROPRIATE PROTECTIVE CLOTHING TO PREVENT INJURY FROM CONTACT WITH A HAZARDOUS SUBSTANCE.



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

GENERAL
NOTES

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2015	STP/HS1P-NH-29(92)	5
		72079-3204-94	
		72079-8204-14	

GENERAL NOTES -continued

- (1) THE CONTRACTOR'S SITE SUPERINTENDENT WILL BE THE SPILL PREVENTION AND CLEANUP COORDINATOR. THE CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT THE SITE SUPERINTENDENT HAS HAD APPROPRIATE TRAINING FOR HAZARDOUS MATERIALS HANDLING, SPILL MANAGEMENT, AND CLEANUP.
- (2) WHERE A RELEASE CONTAINING A HAZARDOUS SUBSTANCE IN AN AMOUNT EQUAL TO OR IN EXCESS OF A REPORTABLE QUANTITY ESTABLISHED UNDER EITHER 40 CFR 117 OR 40 CFR 302 OCCURS DURING A 24 HOUR PERIOD, SEE THE LATEST TENNESSEE GENERAL PERMIT NO. TNR100000 STORMWATER DISCHARGES FROM CONSTRUCTION ACTGIVITIES SECTION 5.1 FOR REPORTING REQUIREMENTS.

ENVIRONMENTAL

- (3) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION COMPREHENSIVE INSPECTION OFFICE SHALL BE INVITED TO ALL PRE-CONSTRUCTION MEETINGS.

SPECIAL NOTES

TRAFFIC CONTROL

- (1) THE CONTRACTOR SHALL COMPLY WITH SECTION 712 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION REGARDING TEMPORARY TRAFFIC CONTROL AND THE CURRENT EDITION OF THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES".
- (2) SHOULD A FLAGGING OPERATION FOR TRAFFIC CONTROL BE UTILIZED FOR RESURFACING, THE CONTRACTOR SHALL MAKE PROVISIONS TO SAFELY CONTROL TRAFFIC INGRESS AT ALL ROADWAY ENTRANCES WITHIN THE FLAGGED WORK ZONE TO THE SATISFACTION OF THE ENGINEER. ALL COST ASSOCIATED WITH THE CONTROLS WILL BE INCLUDED IN THE PRICE BID FOR TRAFFIC CONTROL.
- (3) EXISTING CONSTRUCTION, REGULATORY AND WARNING SIGNS WHICH CONFLICT WITH THE CONSTRUCTION SIGNING SHALL BE REMOVED DURING CONSTRUCTION AND REINSTALLED AS DIRECTED BY THE ENGINEER. ALL COSTS TO BE INCLUDED IN THE PRICE BID FOR ITEM NO. 712-01, TRAFFIC CONTROL, PER LUMP SUM.
- (4) THE CONTRACTOR SHALL GIVE THE TDOT PROJECT SUPERVISOR A MINIMUM OF SEVEN (7) DAYS NOTICE PRIOR TO STARTING WORK SO THAT SUFFICIENT NOTICE CAN BE PREPARED AND DISTRIBUTED TO THE MEDIA.
- (5) IMMEDIATELY UPON COMPLETION OF EACH CONSTRUCTION PHASE, ALL TRAFFIC CONTROL ITEMS THAT ARE NOT NECESSARY FOR THE SUCCEEDING PHASE SHALL BE REMOVED, COVERED OR TURNED TO FACE AWAY FROM TRAFFIC.
- (6) THE CONTRACTOR SHALL PROVIDE FLAGGER AT EACH SIDEROAD WITHIN THE PAVING OPERATION.
- (7) A MINIMUM OF TWO (2) TYPE "C" ARROW BOARDS WILL BE REQUIRED. IF THE CONTRACTOR SCHEDULES HIS OPERATIONS SUCH THAT MORE THAN TWO (2) ARROW BOARDS ARE REQUIRED, THE COST OF ALL ADDITIONAL ARROW BOARDS WILL BE AT THE CONTRACTOR'S EXPENSE.
- (8) TWO (2) CHANGEABLE MESSEGE SIGNS UNITS (ITEM NO. 713-16.01) WILL BE REQUIRED ON THIS PROJECT. THE SIGNS WILL BE USED FOR ADVANCED WARNING OR AS DIRECTED BY THE ENGINEER.
- (9) ACCESS TO THE PROJECT WILL BE PERMITTED AT THE INTERCHANGES ONLY. THE CONTRACTOR WILL BE PERMITTED TO CROSS MEDIAN AT EXISTING CROSS-OVERS ONLY.
- (10) ALL COSTS ASSOCIATED WITH STAKING SIGNS SHALL BE INCLUDED IN THE PRICE BID FOR 712-06, SIGNS (CONSTRUCTION).

LANE CLOSURES

- (11) THE RESTRICTION OF TRAFFIC TO ONE LANE SHALL NOT EXCEED ONE DAY'S PAVING OPERATION.
- (12) LANE CLOSURE WITH LEFT LANE MERGE AND LANE SHIFT TRAFFIC CONTROL IS TO BE USED ON ALL LANE CLOSURES.
- (13) THE CONTRACTOR SHALL KEEP ALL TRAFFIC LANES OPEN TO TRAFFIC DURING NON-WORKING HOURS AND/OR NON-WORK DAYS.

- (14) OVERNIGHT LANE CLOSURES WILL NOT BE ALLOWED UNLESS DIRECTED BY THE TDOT PROJECT SUPERVISOR.
- (15) THE CONTRACTOR SHALL SCHEDULE PAVING OPERATIONS TO PREVENT INTERRUPTION OF SCHOOL TRAFFIC DURING MORNING START AND AFTERNOON DISMISSAL.
- (16) THE CONTRACTOR SHALL NOT BE ALLOWED TO INTERRUPT TRAFFIC FLOW AND SHALL MAINTAIN ALL LANES OF TRAFFIC IN EACH DIRECTION ON THE FOLLOWING DAYS:
 - A. OFFICIAL STATE HOLIDAYS.
 - B. FRIDAY AT 6:00 PM UNTIL TUESDAY AT 7:00 AM, IF A STATE HOLIDAY OCCURS OR IS OBSERVED ON MONDAY.
 - C. THURSDAY AT 6:00 PM UNTIL MONDAY AT 7:00 AM, IF A STATE HOLIDAY OCCURS OR IS OBSERVED ON FRIDAY.
 - D. DURING LOCAL FESTIVALS, GAMES OR EVENTS THAT COULD BE IMPEDED BY THE PAVING OPERATIONS WHERE AND AS DIRECTED BY THE TDOT PROJECT SUPERVISOR.

PAVING

- (17) ALL SURPLUS PATCHING AND JOINT MATERIAL IS TO BE REMOVED BEFORE PLACING TACK COAT AND SURFACE. THIS WORK WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (18) MATERIAL TRANSFER DEVICE (MTD) TO BE USED ON ALL MIXES. COST TO BE INCLUDED IN THE UNIT PRICE BID ON ASPHALT PAVEMENT ITEMS. SEE SPECIAL PROVISION 407G.
- (19) THE FOUR FOOT INSIDE SHOULDER SHALL BE PAVED CONCURRENTLY WITH THE INSIDE TRAFFIC LANE.
- (20) THE CONTRACTOR WILL BE REQUIRED TO SHAPE PUBLIC SIDE ROADS, BUSINESS ENTRANCES, PRIVATE DRIVES AND CLEAN EXISTING SIDE DRAINS AS DIRECTED, WHERE NECESSARY, BEFORE PLACING TACK COAT AND SURFACE. THIS WORK WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (21) FREQUENTLY USED PRIVATE DRIVES ARE TO BE PAVED AS DIRECTED BY THE TDOT PROJECT SUPERVISOR WITH SURFACE MATERIAL NOT TO EXCEED THE NORMAL PAVERS WIDTH ALONG THE EDGE OF THE PROPOSED ROADWAY PAVING, THEN FEATHERED TO THE EXISTING GRADE WITH EITHER CRUSHED STONE, TACK COAT, LEVELING OR SURFACE MATERIAL.
- (22) ALL PUBLIC SIDE ROADS AND BUSINESS ENTRANCES ARE TO BE SURFACED ONE (1) PAVERS WIDTH OR AS DIRECTED WITH TACK AND SURFACE MATERIAL. THICKNESS AND WIDTH WILL BE AS DIRECTED BY THE TDOT PROJECT SUPERVISOR.
- (23) ANY QUANTITY REMAINING ON ITEMS COMPLETED PRIOR TO THE PAVING OPERATION WILL NOT BE CONVERTED TO ADDITIONAL ASPHALT FOR THE ROADWAY.
- (24) ITEM NO. 303-01, CRUSHED STONE FOR SHOULDERS, IS TO BE PLACED TO EXISTING SHOULDER WIDTH OR AS DIRECTED BY THE TDOT PROJECT SUPERVISOR. ALL COSTS OF CONSTRUCTING SHOULDERS, SHAPING OR CLIPPING OF EXISTING SHOULDERS AND DISPOSAL OF ANY DEBRIS FROM SHOULDER CLIPPING ARE TO BE INCLUDED IN THE UNIT BID PRICE FOR ITEM NO. 303-01.

COLD PLANING

- (25) THE CONTRACTOR WILL BE REQUIRED TO PAVE THE COLD PLANED PAVEMENT WITHIN A PERIOD OF 48 HOURS.
- (26) PROVIDED THAT THE CONTRACTOR USES A COLD PLANED PLANNING MACHINE WITH THE MILLING DRUM SPECIFICATIONS OF CLOSELY SPACED FINE TOOTH (5/16 INCHES OR SMALLER) MILLING DRUM, MOVING FORWARD AT MODERATE SPEED (50 TO 65 FPM.) AND THE UNDER SURFACE REMAINS INTACT (HAVING NO LOOSE MATERIAL OR GOUGES,) THEN THE CONTRACTOR MAY GET PERMISSION FROM REGION 2 CONSTRUCTION TO MILL ALL SURFACES BEFORE COVERING WITH BITUMINOUS MATERIAL.
- (27) ALL COLD PLANED MATERIAL IS TO BECOME THE PROPERTY OF THE CONTRACTOR.
- (28) IF SUBSURFACE PAVEMENT FAILURE IS EXPOSED AFTER MILLING, REMOVE AREA OF FAILURE TO SOUND PAVEMENT AND REPLACE WITH "D" MIX PRIOR TO PROPOSED OVERLAY.
- (29) AT THE COMPLETION OF COLD PLANING OPERATION, ANY DEPRESSIONS WILL BE CLEANED AND BROUGHT TO PROPER GRADE WITH "D" MIX

MATERIAL. THE COST OF THE CLEAN UP WILL BE INCLUDED IN THE UNIT PRICE FOR "D" MIX MATERIAL.

PAVEMENT MARKING

- (30) THE CONTRACTOR IS RESPONSIBLE FOR THE LAYOUT OF ALL PAVEMENT MARKING. ANY CHANGE FROM THE EXISTING PAVEMENT MARKINGS MUST BE APPROVED BY THE REGION 2 TRAFFIC MANAGER.
- (31) EXISTING PAVEMENT MARKINGS IN THE EXCLUSION AREAS AND SIDE ROAD RADII SHALL BE RE-TRACED AS DIRECTED BY THE TDOT PROJECT SUPERVISOR DURING FINAL STRIPING.
- (32) TEMPORARY PAVEMENT LINE MARKINGS ON INTERMEDIATE LAYERS OF PAVEMENT AND TAPERED JOINTS SHALL BE REFLECTIVE TAPE OR REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS BEFORE DARK HOURS, SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. THESE MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-05.01, PAINTED PAVEMENT MARKING (4") LINE.
- (33) THE CONTRACTOR WILL BE REQUIRED TO REMOVE EXISTING SNOWPLOWABLE MARKERS AND RAISED PAVEMENT MARKERS INCLUDING THOSE ON SHOULDERS AND RAMPS PRIOR TO PLACING SURFACE. ANY HOLES OR DIVOTS RESULTING FROM REMOVAL OF A MARKER THAT IN THE OPINION FO THE TDOT PROJECT SUPERVISOR PRESENTS A PROBLEM FOR TRAFFIC SHALL BE PATCHED WITH THE APPROPRIATE ASPHALT MATERIALS. THIS PATCH WORK WILL NOT BE PAID FOR DIRECTLY, BUT WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.

CLEANING DITCHES

- (34) DITCHES MAY NEED TO BE CLEANED OUT AS DIRECTED BY THE TDOT PROJECT SUPERVISOR. ALL COST ASSOCIATED WITH CLEANING DITCHES TO BE INCLUDED IN OTHER ITEMS.
- (35) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS PRIOR TO CLEANING OR REMOVAL/REPLACEMENT OF DITCHES, SIDE DRAINS, AND CROSS DRAINS TO ENSURE THAT ENVIRONMENTAL FEATURES (E.G., STREAMS, WETLANDS, AND SPRINGS ARE NOT IMPACTED. IF THE CONTRACTOR/TDOT INSPECTOR IS UNSURE OF THE IDENTITY OF AN ENVIRONMENTAL FEATURE, THE INSPECTOR SHALL CONTACT THE TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION IMMEDIATELY.

GUARDRAIL

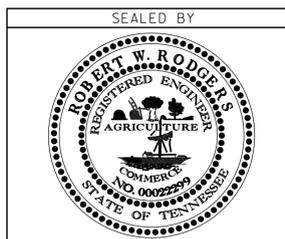
- (36) ALL EXISTING GUARDRAIL REMOVED FROM THIS PROJECT IS TO BECOME THE PROPERTY OF THE CONTRACTOR.

NPDES

- (37) IT IS OUR UDERSTANDING THAT THE PROJECT WILL NOT DISTURB GREATER THAN 1.0 ACRE. IF IT IS DEEMED NECESSARY TO DISTURB GREATER THAT 1.0 ACRE, THE CONTRACTOR/TDOT INSPECTOR SHALL CONTACT THE TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION IMMEDIATELY.

MISCELLANEOUS

- (38) TIME ON THE PROJECT WILL START THE DAY THAT CONSTRUCTION SIGNS ARE PUT UP OR IN ACCORDANCE WITH SP108B IF A MAXIMUM AMOUNT OF CALENDAR DAYS IS SPECIFIED.



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

SPECIAL
NOTES

GENERAL NOTES FOR BITUMINOUS SEAL COAT (BSC) WITH HMA APPLICATION

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2015	STP/HSIP-NH-29(92)	6
		72079-3204-94	
		72079-8204-14	

DESCRIPTION:

This work shall consist of the placement of Bituminous Seal Coat (BSC) followed with a HMA overlay in accordance with the 2006 Standard Specifications and these general notes :

EQUIPMENT (Chip Seal):

- A. Distributor**
Use a distributor that has a monitoring system and a method for controlling the width of the spray bar and application rate of emulsion.
- B. Aggregate Spreader**
Use a self-propelled mechanical type aggregate spreader capable of distributing the aggregate uniformly to the required width and at the designed rate. Use a self-propelled type mounted on pneumatic-tired wheels.
- C. Pneumatic-Tired Rollers**
Provide a minimum of two (2) self-propelled pneumatic-tired rollers as specified by the project manager.
- D. Brooms**
Provide motorized brooms with a positive means of controlling vertical pressure and capable of cleaning the road surface prior to spraying bituminous material and removing loose aggregate after chip sealing.

CONSTRUCTION REQUIREMENTS:

The contractor shall submit a plan of work to the Project Manager for approval. The Project Manager may revise or recommend modifications until approval is acknowledged. All work shall be in accordance with Section 405 of the 2006 Standard Specifications for Road and Bridge Construction except as modified within these general notes.

- A. Weather Limitations**
 - (1) Complete by September 30, 2015. (Entire Process)
 - (2) Work only during daylight hours unless otherwise specified.
 - (3) Start when the pavement and air temperature are 60 degrees Fahrenheit and rising.
 - (4) The road surface shall be dry at all times during the construction operations.
 - (5) No construction is allowed in foggy weather.
- B. Road Surface Preparation**
Clean all pavements to be chip sealed. Sweep the pavement with a motorized broom to remove loose material. Clean depressions and cracks not reached by the power broom, using hand brooming or pressurized air.

Cover any utility installations to prevent adherence of the bituminous material. Suitable covering includes plywood disks, sand, Kraft paper, roofing felt or other approved methods. Remove the protective coverings before opening the road to traffic. The cost for these adjustments is to be included in the bid price for other items.
- C. Application of Material (Chip Seal)**
The Contractor shall have all equipment calibrated properly prior to start of work. The TDOT Inspector shall be present during time of calibration to determine aggregate spread rate and distributor rates. The Mechanical Chip Spreader shall have proper calibration to assure proper and accurate coverage. The Distributor Truck shall have proper calibration of spray nozzles and shall have proper and clean nozzles. The Engineer shall stop work if distributor is not applying material properly (such as gaps or streaking).

The Contractor shall place a 500 ft. test strip at the beginning of the project to assure proper coverage and proper equipment calibration. The test section is to verify a quick breaking emulsion. The test strip shall be able to carry normal traffic within 3 hours. If normal traffic cannot be carried, the emulsion shall be adjusted and another test strip will be required.

The emulsion material to be utilized for the chip seal application procedure shall be CRS2-P. The CRS2-P shall be placed at a shot rate as described in table 405-1 shown below or as determined by the Project Manager by a test section. The distributor shall provide uniform triple-lap coverage of emulsion.

Apply the bituminous material at the recommended temperature limits specified by the manufacturer.

(REVISED) The aggregate shall be #8 or #89 limestone meeting TDOT requirements as referenced in section 903.11. In addition, the aggregate shall meet the requirements of section 903.13 and 903.22 for the specified aggregate. The aggregate shall be washed so the amount of material finer than the No. 200 mesh shall not exceed 1%.

Uniformly moistened aggregates, which are visually damp at the time of placement, shall be provided at a rate as described in table 405-1 shown below. Immediately (within one minute) after the bituminous material has been sprayed, apply the aggregates. The speed of the spreader shall be such that the aggregates are not rolling over, and starting and stopping of the spreader is minimized. The edges of the aggregate applications shall be sharply defined. Previously used (swept) aggregates will not be allowed.

Table 405-1

Aggregate Size (903.22)	Aggregate Spread Rate (lb/yd2)	Emulsion Shot Rate (gal/yd2)
7	25 - 30	0.30 - 0.45
78	22 - 28	0.28 - 0.38
8	20 - 25	0.20 - 0.35
89	17 - 23	0.17 - 0.28

Water used in the process shall be potable and compatible with the chip seal.

- D. Rolling Operations (Chip Seal)**
Complete the initial rolling within 2 minutes after applying the aggregate. Proceed at a recommended speed less than or equal to 5 miles per hour, to prevent turning over aggregate. Make a minimum of three complete passes over the aggregate. Roll the aggregate so the entire width of the treatment area is covered in one pass of all the rollers.
- E. Sweeping**
Sweep off the surplus aggregate on the same day of the chip seal rolling. Sweep with care so as not to dislodge any aggregate that is setting. Re-sweep the same areas the day after the initial sweeping. The Contractor will dispose of the surplus seal coat aggregate in a manner satisfactory to the Project Manager.
- F. Protection of the Surface**
No traffic is permitted on the chip sealed road surface until after the specified rolling and initial sweeping has been completed and the bituminous material has set so it will not pick up on vehicle tires.
- G. Progress of Work**
The chip seal will usually cure within 24 hours under dry conditions and temperatures above 60 degrees Fahrenheit. Allow the chip seal to cure a minimum of one day before placement of the HMA material.

Commencement of the placement of the wearing surface material shall begin within 48 hours upon completion of the bituminous seal coat.

The Contractor shall be responsible for notifying each utility owner of his plan of operation by calling 1-800-351-1111, and have them locate their utilities within the project limits prior to construction beginning.

TRAFFIC CONTROL:

All traffic control shall be furnished by the Contractor and shall be in accordance with the Manual on Uniformed Traffic Control Devices.

During the execution of work, the Contractor shall keep one travel lane open at all times. A pilot vehicle will be required on this project. The pilot vehicle will not be paid for directly, but will be included in the price of other items. Detours will be allowed as approved by the Project Manager. All lane closures shall be approved prior to closure. Homeowner access to their property shall be coordinated prior to work beginning.

Compensation for a maximum of two arrow boards will be allowed for the duration of the project.

BASIS OF PAYMENT:

Any and all quantities that appear herein are strictly estimates used for bidding purposes only and are neither implied nor inferred as guarantees. The accepted quantities of work will be paid for at the contract unit price bid for each item shown in the proposal. The price shall be full compensation for all work as directed by the Project Manager, including the furnishing of all labor, tools, equipment and incidentals to satisfactorily complete the work as specified.



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

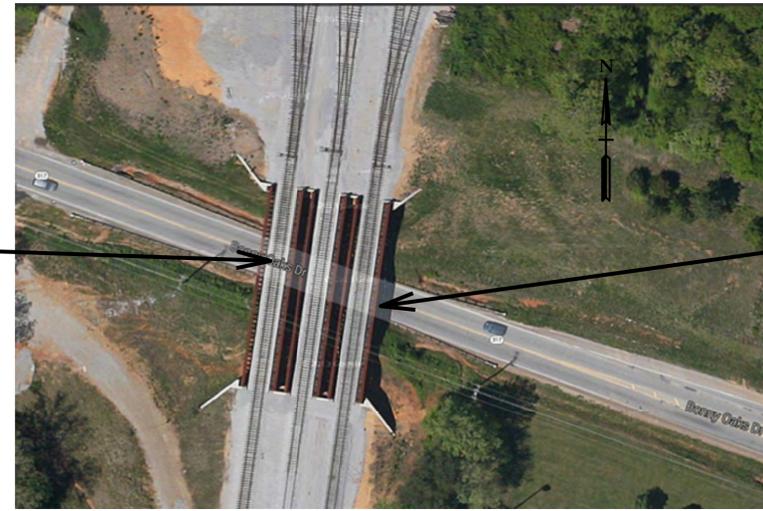
BSC W/HMA OVERLAY NOTES

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2015	NH/HSIP-31 7(1 8)	7
		33070-3234-94	
		33070-8234-14	

**NSRR
UNDERPASS
#736672R
Q LM 3.067**



SR-317
FACING EAST



SR-317
FACING WEST

**NORFOLK SOUTHERN RAILWAY
SPECIAL NOTES**

- (1) THE CONTRACTOR SHALL NOT, FOR ANY REASON, STORE CONSTRUCTION EQUIPMENT UPON NOT STORE OR DUMP WASTE CONSTRUCTION MATERIALS UPON THE RAILROAD'S RIGHTS-OF-WAY.
- (2) THE CONTRACTOR IS PROHIBITED FROM INSTALLING ANY CONSTRUCTION SIGNS, MESSAGE BOARDS, ARROW BOARDS AND/OR OTHER TEMPORARY TRAFFIC CONTROL SIGNS OR DEVICES WITHIN THE RAILROAD'S RIGHTS-OF-WAY, AND IN ALL CASES, THESE SIGNS MUST NEVER BE PLACED IN A POSITION SO AS TO OBSTRUCT THE VIEW OF ANY ADVANCE RAILROAD WARNING SIGNS OR OF ANY OTHER RAILROAD GRADE CROSSING WARNING DEVICES SUCH AS CROSS-BUCK SIGNS, CROSSING SIGNALS, ETC.
- (3) THE CONTRACTOR SHALL CONDUCT HIS WORK SO AS TO PROTECT THE RAILROAD'S TRACK FACILITIES AND PROPERTY FROM DAMAGE AND ANY WORK BY THE CONTRACTOR WITHIN 25 FEET OF THE RAILROAD'S R.O.W. WILL REQUIRE THE PRESENCE AND PERMISSION OF AN AUTHORIZED RAILROAD WATCHMAN.
- (4) SEE SPECIAL PROVISION 105C(SL)(R) TO SET UP FLAGGING SERVICES AND SUBMIT FOR INSURANCE REQUIRED FOR WORK AT OR NEAR THE RAILROAD GRADE CROSSINGS ON SR-62, SR-84 AND SR-164.
- (5) THE CONTRACTOR SHALL COORDINATE HIS WORK AT THE RAILROAD CROSSING IN ACCORD WITH SUPPLEMENTAL SPECIFICATION 100SS, SUBSECTION 107.16 OF THE STATE CONTRACT WITH THE FOLLOWING REPRESENTATIVE OF THE RAILROAD:

Mr. Don Sutherland – Division Engineer
Norfolk Southern Corporation
Central Division Office
7208 Old Rutledge Pike
Knoxville, TN 37914
Phone: (865) 521-1407
E-Mail: donald.sutherland@nscorp.com

AND

Mr. Daniel Parker, Engineer–Public Improvements
Norfolk Southern Corporation
1200 Peachtree Street, NE
Atlanta, GA 30309
Phone: (404) 529-1256
E-Mail: daniel.parker@nscorp.com



STATE OF TENNESSEE
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DETAILS**