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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF ENGINEERING

SHELBY COUNTY

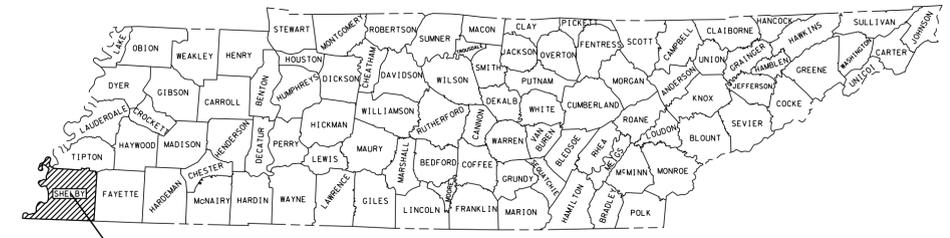
S.R. 14

FROM: L.M. 15.66 (TREZEVANT ST.)
TO: L.M. 20.04 (I-40 RAMP)

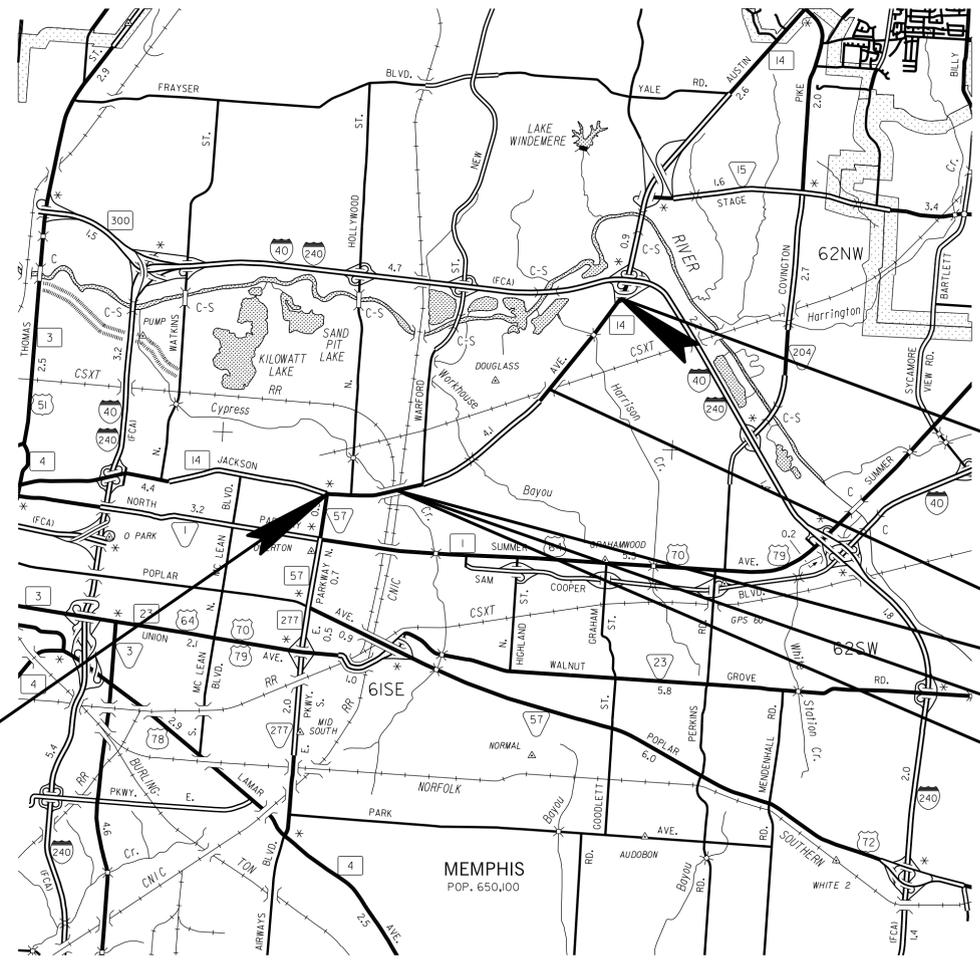
RESURFACING

STATE HIGHWAY NO. 14 F.A.H.S. NO. N.A.

TENN.	YEAR	SHEET NO.
	2015	1
FED. AID PROJ. NO.	STP/HSIP-NH-14(58)	
STATE PROJ. NO.	79224-8203-14	



PROJECT SITE



**BEGIN PROJECT L.M. 15.66
(TREZEVANT ST.)**

**END PROJECT L.M. 20.04
(I-40 RAMP)**

UTILITY OWNERS	
UTILITY	UTILITY CONTACT
TELEPHONE	A T & T DON ROE (731) 423-5037 315 EAST COLLEGE ST. JACKSON, TN. 38301
MLG&W	MEMPHIS LIGHT GAS AND WATER TOM WORD (901) 528-4186 220 S. MAIN ST. MEMPHIS, TN. 38101
SEWER	CITY OF MEMPHIS GARY W. VADEN P.E. (901) 576-6725 125 N. MAIN ST. SUITE 639 MEMPHIS, TN. 38103



RAILROAD NOTE	
DOT# 348511A @ L.M. 18.27	CSXT RAILROAD OVERHEAD CROSSING
DOT# 348548P @ L.M. 16.28	CSXT RAILROAD OVERHEAD CROSSING
DOT# 438081R @ L.M. 16.28	UP RAILROAD OVERHEAD CROSSING
DOT# UNKNOWN @ L.M. 16.28	IC RR OVERHEAD CROSSING

**UNOFFICIAL
SET
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BIDDING**



PROJECT LENGTH 4.38 MILES
TOTAL LANE MILES RESURFACED 17.80 MILES

TRAFFIC DATA	
ADT (2015)	21660

SPECIAL NOTES

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED JANUARY 1, 2015 AND ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS AND IN THE PROPOSAL CONTRACT

TDOT C.E. MANAGER 1 TABITHA CAVANESS, P.E., REG. 4
DESIGNER RYAN PHILPOTT, P.E., REG. 4 CHECKED BY DEREK LINK, REG. 4
P.E. NO. 98043-4283-04
PIN: 082052.01

SEAL BY

APPROVED: Paul D. Decker
CHIEF ENGINEER

DATE: _____

APPROVED: [Signature]
JOHN SCHROER, COMMISSIONER

U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION

APPROVED: _____
DIVISION ADMINISTRATOR DATE

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2015	STP-HSIP-NH-14(58)	2

FOOTNOTES

ESTIMATED ROADWAY QUANTITIES					
ITEM NO.	DESCRIPTION	UNIT	79224-8203-14 QUANTITY	STP-HSIP-NH-14(58) 79224-3203-94 QUANTITY	TOTAL QUANTITY
202-03.01	REMOVAL OF ASPHALT PAVEMENT	S.Y.	2660		2660
203-06	WATER	M.G.	14		14
① 303-02	MINERAL AGGREGATE, TYPE B BASE, GRADING (TYPE D)	TON	1800		1800
② 307-01.01	ASPHALT CONCRETE MIX (PG64-22) (BPMB-HM) GRADING A	TON	1224		1224
③ 403-01	BITUMINOUS MATERIAL FOR TACK COAT (TC)	TON	145		145
③④ 411-03.09	ACS MIX(PG76-22) THIN LIFT CS ASPHALT	TON	3900		3900
③⑤⑥ 411-03.10	ACS MIX(PG76-22) GRADING D	TON	11100		11100
⑦ 415-01.02	COLD PLANING BITUMINOUS PAVEMENT	S.Y.	166800		166800
611-09.01	ADJUSTMENT OF EXISTING CATCHBASIN	EACH	10		10
⑧ 701-01.01	CONCRETE SIDEWALK (4")	S.F.	11000		11000
⑨ 701-02.01	CONCRETE CURB RAMP (RETROFIT)	S.F.	11400		11400
⑧⑩ 702-03	CONCRETE COMBINED CURB & GUTTER	C.Y.	130		130
⑧⑪ 705-08.13	PERMANENT IMPACT ATTENUATOR NCHRP350 TL-3	EACH		4	4
712-01	TRAFFIC CONTROL	LS	1		1
⑫ 712-06	SIGNS (CONSTRUCTION)	S.F.	1696		1696
712-08.03	ARROW BOARD (TYPE C)	EACH	4		4
716-01.23	Snowplwble Pvmt Mrkrs (Bi-Dir)(2 Color)	EACH		1000	1000
716-02.03	PLASTIC PAVEMENT MARKING (CROSS-WALK)	L.F.		3700	3700
716-02.04	PLASTIC PAVEMENT MARKING(CHANNELIZATION STRIPING)	S.Y.		500	500
⑬⑭ 716-02.05	PLASTIC PAVEMENT MARKING (STOP LINE)	L.F.		2400	2400
716-02.06	PLASTIC PAVEMENT MARKING (TURN LANE ARROW)	EACH		50	50
716-03.01	PLASTIC WORD PAVEMENT MARKING (ONLY)	EACH		25	25
⑮ 716-05.01	PAINTED PAVEMENT MARKING (4" LINE)	L.M.	34.050		34.05
⑬ 716-13.01	SPRAY THERMO PVMT MRKNG (60 mil) (4IN LINE)	L.M.		17.025	17.025
⑬⑭ 716-13.05	SPRAY THERMO PVMT MRKNG (60 mil) (6IN DOTTED LINE)	L.F.		685	685
717-01	MOBILIZATION	LS	1		1

- ① FOR BREAKOUT ONLY.
- ② FOR REPLACEMENT OF QUANTITY REMOVED UNDER ITEM 202-03.01 ONLY.
- ③ THE CONTRACTOR SHALL USE ANY MEASURE TO PREVENT TRACKING OF TACK COAT AND BITUMINOUS MATERIAL ACROSS THE STRUCTURES AT L.M. 16.28 AND L.M. 18.27. THE COST OF THIS MEASURE SHALL BE INCLUDED WITHIN THIS ITEM.
- ④ INCLUDES 889 TONS FOR GORE AREAS, INTERSECTIONS, SIDE ROADS, CROSS-OVERS, AND TURN LANES.
- ⑤ INCLUDES 2413 TONS FOR GORE AREAS, INTERSECTIONS, SIDE ROADS, CROSS-OVERS, AND TURN LANES.
- ⑥ SEE SPECIAL PROVISION NO. 407G FOR MATERIAL TRANSFER DEVICE AND SPECIAL PROVISION NO. 411C FOR RIDEABILITY SPECIFICATIONS.
- ⑦ INCLUDES 34900 S.Y. FOR GORE AREAS, INTERSECTIONS, SIDE ROADS, CROSS-OVERS, AND TURN LANES.
- ⑧ THE COST OF REMOVAL TO BE INCLUDED IN THIS ITEM.
- ⑨ HANDICAP RAMPS MUST BE CONSTRUCTED USING CITY OF MEMPHIS STANDARDS.
- ⑩ SECTIONS OF CURB AND GUTTER THAT ARE POURED MONOLITHIC WITH THE RAMP ARE INCIDENTAL AND SHOULD BE A PART OF THE HANDICAP RETROFIT ITEM.
- ⑪ 2 IMPACT ATTENUATORS TO BE PLACED AT BRIDGE OVER SCOTT STREET, 1 TO BE PLACED AT BRIDGE OVER CHELSEA AVENUE, AND 1 TO BE PLACED AT AT MEDIAN WALL AT L.M. 20.04.
- ⑫ QUANTITY INCLUDES
- | | | | |
|----|-----------------------------|----------|-------------|
| 2 | ROAD WORK NEXT 2 MI. | (G20-1) | (48" X 24") |
| 2 | END ROAD WORK | (G20-2) | (48" X 24") |
| 16 | UNEVEN LANES | (W8-11) | (48" X 48") |
| 46 | ROAD WORK AHEAD | (W20-1) | (48" X 48") |
| 2 | ROAD WORK 1500 FT. | (W20-1) | (48" X 48") |
| 2 | ROAD WORK 1000 FT. | (W20-1) | (48" X 48") |
| 2 | ROAD WORK 500 FT. | (W20-1) | (48" X 48") |
| 4 | RIGHT LANE CLOSED 1,500 FT. | (W20-5) | (48" X 48") |
| 4 | RIGHT LANE CLOSED 1,000 FT. | (W20-5) | (48" X 48") |
| 4 | RIGHT LANE CLOSED 500 FT. | (W20-5) | (48" X 48") |
| 4 | LEFT LANE CLOSED 1,500 FT. | (W20-5) | (48" X 48") |
| 4 | LEFT LANE CLOSED 1,000 FT. | (W20-5) | (48" X 48") |
| 4 | LEFT LANE CLOSED 500 FT. | (W20-5) | (48" X 48") |
| 4 | FLAGGER AHEAD | (W20-7a) | (48" X 48") |
| 2 | FRESH OIL | (W21-2) | (48" X 48") |
| 6 | MOTORCYCLE GROOVE SIGN | (TN-64) | (48" X 48") |

THIS CONSTRUCTION SIGNING IS TO BE AS A MINIMUM. OTHER SIGNS AS DIRECTED BY THE ENGINEER MAY BE REQUIRED DURING DIFFERENT PHASES.

- ⑬ FOR FINAL PAVEMENT MARKING ONLY.
- ⑭ THE CONTRACTOR MAY ELECT TO SUBSTITUTE PERFORMED PLASTIC FOR THERMOPLASTIC. PERFORMED PLASTIC SHALL BE PAID FOR AT THE SAME UNIT PRICE BED FOR THERMOPLASTIC.
- ⑮ FOR USE AS TEMPORARY LINE MARKINGS.

SIGNALIZATION TABULATION
REQUESTED

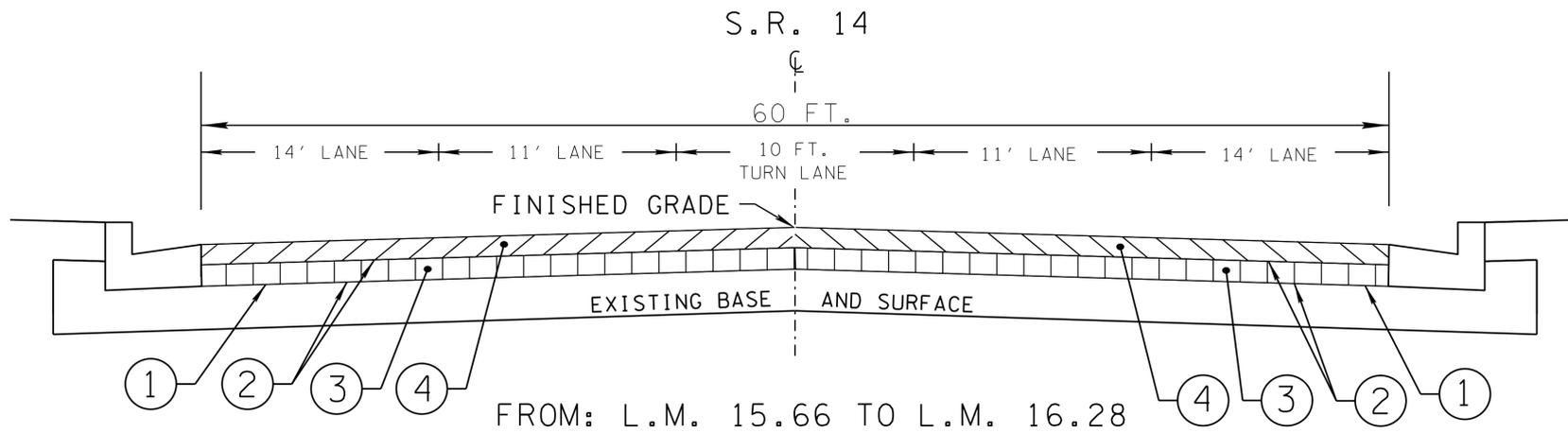
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DEPARTMENT OF TRANSPORTATION

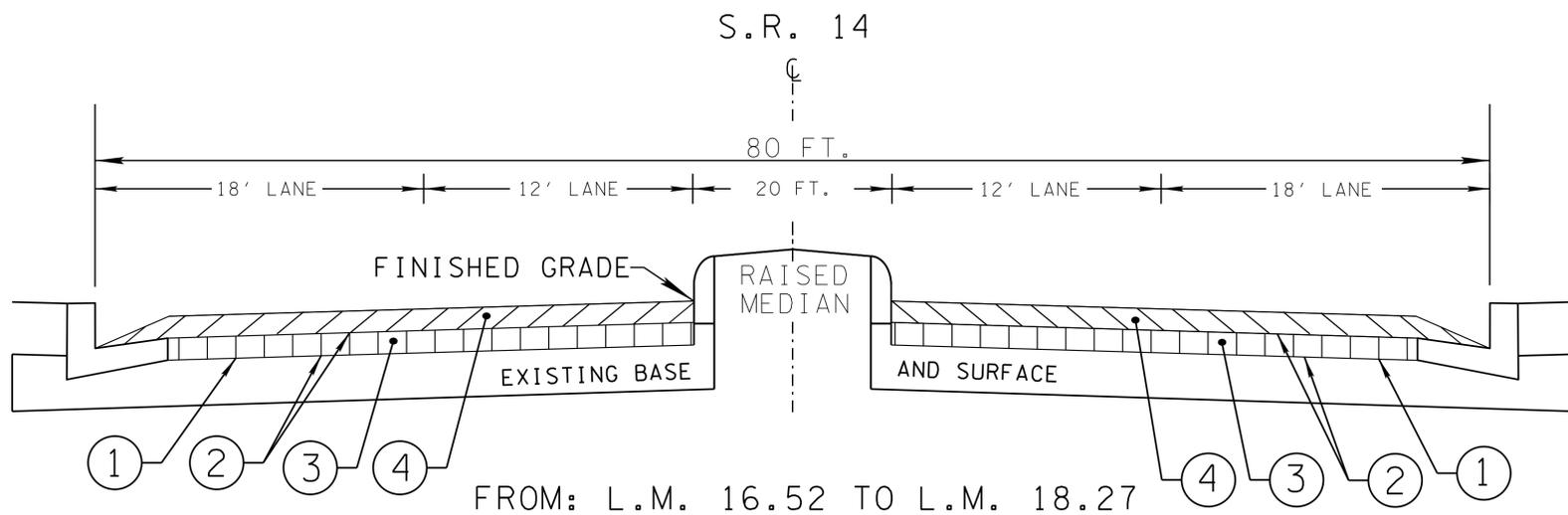
ESTIMATED
ROADWAY
QUANTITIES

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2015	STP/HSIP-NH-14(58)	3



TYPICAL SECTION NOTE

* CONCRETE PAVEMENT, NO OVERLAY BETWEEN L.M. 19.60 AND L.M. 20.04.



BRIDGE NOTE

THE CONTRACTOR SHALL:

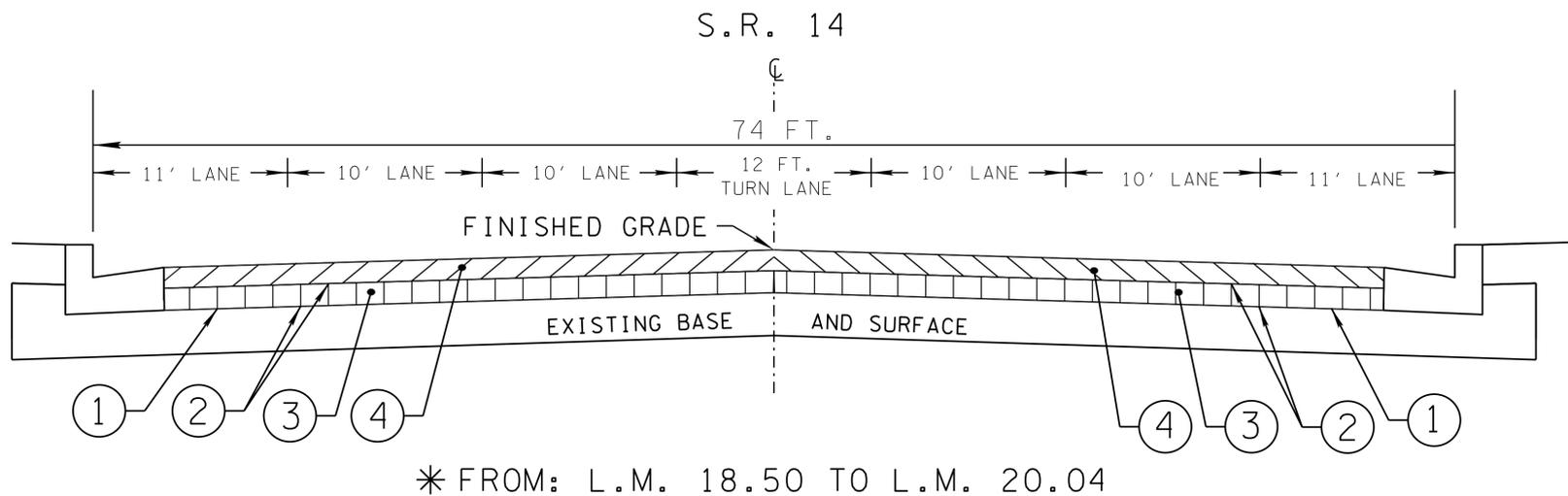
"LEAVE AS IS"
 BRIDGES AT L.M. 16.28 (1247' LT. & RT.)
 AND AT L.M. 18.27 (1185.5' LT. & RT.)

COLD PLANE 1.8" DEPTH AND PAVE ACROSS EXISTING STRUCTURES WITH PLANS QUANTITIES ONLY @ L.M. 17.27 (42' LT. & RT.)

COLD PLANE 1.8" DEPTH ACROSS EXISTING STRUCTURE AND PAVE WITH TOP COAT ONLY @ L.M. 19.12 (85.5' LT. & RT.)

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SEE PROPOSED PAVEMENT SCHEDULE ON SHEET 3A

STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

**TYPICAL
 SECTIONS**
 (NOT TO SCALE)

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2015	STP/HSIP-NH-14(58)	3A

VERTICAL CLEARANCES

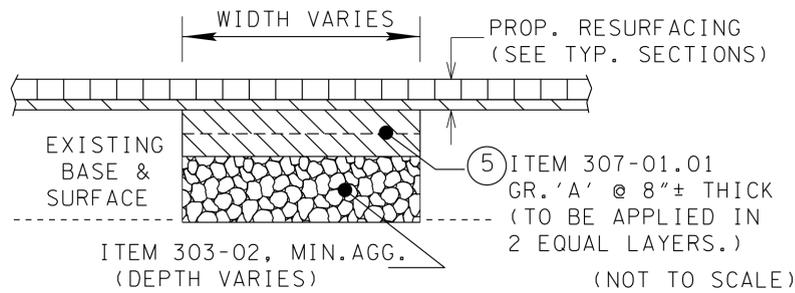
NOTICE TO CONTRACTOR:

NBL/SBL

@ L.M. 19.72 (16.50' RT. & 16.42' LT.)

AND L.M. 19.82 (17.42' RT. & 17.00' LT.)

DETAIL OF BREAKOUT



PROPOSED PAVEMENT SCHEDULE

- | | |
|---|---|
| ① | COLD PLANING @ 1.80"± THICK (APPROX. 189.00 LBS./S.Y.)
ITEM 415-01.02 COLD PLANING BITUMINOUS PAVEMENT |
| ② | TACK COAT (TC) FOR MILLING - COLD PLANE
ITEM 403-01 BITUMINOUS MATERIAL FOR TACK COAT (TC) (AT 0.10 GAL./S.Y.) |
| ③ | ASPHALTIC CONCRETE SURFACE (ACS) @ 0.40"± THICK (APPROX. 46.00 LBS./S.Y.)
ITEM 411-03.09 ACS MIX (PG76-22) THIN LIFT CS ASPHALT |
| ④ | ASPHALTIC CONCRETE SURFACE (ACS) @ 1.25"± THICK (APPROX. 132.50 LBS./S.Y.)
ITEM 411-03.10 ACS MIX (PG76-22) GRADING "D" |
| ⑤ | BITUMINOUS BASE COURSE (BLACK BASE) @ 8"± THICK (APPROX. 920.0 LBS./S.Y.)
ITEM 307-01.01 ASPHALT CONCRETE MIX (PG64-22) (BPMB-HM) GRADING "A"
(THIS ITEM IS TO BE USED FOR BREAKOUT ONLY) |

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TYPICAL
SECTIONS
(NOT TO SCALE)

GENERAL NOTES

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2015	STP/HSIP-NH-14(58)	4

GRADING

ANY AREA THAT IS DISTURBED OUTSIDE LIMITS OF CONSTRUCTION DURING THE LIFE OF THIS PROJECT SHALL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE.

THE CONTRACTOR SHALL NOT DISPOSE OF ANY MATERIAL EITHER ON OR OFF STATE-OWNED R.O.W. IN A REGULATORY FLOOD WAY AS DEFINED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY WITHOUT APPROVAL BY SAME. ALL MATERIAL SHALL BE DISPOSED OF IN UPLAND (NON-WETLAND) AREAS AND ABOVE ORDINARY HIGH WATER OF ANY ADJACENT WATERCOURSE. THIS DOES NOT ELIMINATE THE NEED TO OBTAIN ANY OTHER LICENSES OR PERMITS THAT MAY BE REQUIRED BY ANY OTHER FEDERAL, STATE, OR LOCAL AGENCY.

UTILITIES

THE LOCATIONS OF UTILITIES SHOWN WITHIN THESE PLANS ARE APPROXIMATE ONLY. EXACT LOCATIONS SHALL BE DETERMINED IN THE FIELD BY CONTACTING THE UTILITY COMPANIES INVOLVED. NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC., AT 1-800-351-1111 AS REQUIRED BY TCA 65-31-106 WILL BE REQUIRED.

UNLESS OTHERWISE NOTED, ALL UTILITY ADJUSTMENTS WILL BE PERFORMED BY THE UTILITY OR IT'S REPRESENTATIVE. THE CONTRACTOR AND UTILITY OWNERS WILL BE REQUIRED TO COOPERATE WITH EACH OTHER IN ORDER TO EXPEDITE THE WORK REQUIRED BY THIS CONTRACT. ON CONTRACTS WHERE CONSTRUCTION STAKES, LINES, AND GRADES ARE CONTRACT ITEMS, THE CONTRACTOR WILL BE REQUIRED TO PROVIDE RIGHT-OF-WAY OR SLOPE STAKES, DITCH OR STREAM BED GRADES, OR OTHER ESSENTIAL SURVEY STAKING TO PREVENT CONFLICTS WITH THE HIGHWAY CONSTRUCTION. FREQUENTLY, THIS WILL BE REQUIRED AS THE FIRST ITEM OF WORK AND AT ANY LOCATION ON THE PROJECT DIRECTED BY THE ENGINEER.

THE CONTRACTOR WILL PROVIDE ALL NECESSARY PROTECTIVE MEASURES TO SAFEGUARD EXISTING UTILITIES FROM DAMAGE DURING CONSTRUCTION OF THIS PROJECT. IN THE EVENT THAT SPECIAL EQUIPMENT IS REQUIRED TO WORK OVER AND AROUND THE UTILITIES, THE CONTRACTOR WILL BE REQUIRED TO FURNISH SUCH EQUIPMENT. THE COST OF PROTECTING UTILITIES FROM DAMAGE AND FURNISHING SPECIAL EQUIPMENT WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.

PRIOR TO SUBMITTING HIS BID, THE CONTRACTOR WILL BE SOLELY RESPONSIBLE FOR CONTACTING OWNERS OF ALL AFFECTED UTILITIES IN ORDER TO DETERMINE THE EXTENT TO WHICH UTILITY RELOCATIONS AND/OR ADJUSTMENTS WILL HAVE UPON THE SCHEDULE OF WORK FOR THE PROJECT. WHILE SOME WORK MAY BE REQUIRED 'AROUND' UTILITY FACILITIES THAT WILL REMAIN IN PLACE, OTHER UTILITY FACILITIES MAY NEED TO BE ADJUSTED CONCURRENTLY WITH THE CONTRACTOR'S OPERATIONS. ADVANCE CLEAR CUTTING MAY BE REQUIRED BY THE ENGINEER AT ANY LOCATION WHERE CLEARING IS CALLED FOR IN THE SPECIFICATIONS AND CLEAR CUTTING IS NECESSARY FOR A UTILITY RELOCATION. ANY ADDITIONAL COST WILL BE INCLUDED IN THE UNIT PRICE BID FOR THE CLEARING ITEM SPECIFIED IN THE PLANS.

THE CONTRACTOR SHALL NOTIFY EACH INDIVIDUAL UTILITY OWNER OF HIS PLAN OF OPERATION IN THE AREA OF THE UTILITIES. PRIOR TO COMMENCING WORK, THE CONTRACTOR SHALL CONTACT THE UTILITY OWNERS AND REQUEST THEM TO PROPERLY LOCATE THEIR RESPECTIVE UTILITY ON THE GROUND. THIS NOTIFICATION SHALL BE GIVEN AT LEAST THREE (3) BUSINESS DAYS PRIOR TO COMMENCEMENT OF OPERATIONS AROUND THE UTILITY IN ACCORDANCE WITH TCA 65-31-106.

MISCELLANEOUS

THE CONTRACTOR SHALL BE REQUIRED TO REMOVE AND RESET MAILBOXES WHERE AND AS DIRECTED BY THE ENGINEER.

NOTHING IN THE GENERAL NOTES OR SPECIAL PROVISIONS SHALL RELIEVE THE CONTRACTOR FROM HIS RESPONSIBILITIES TOWARD THE SAFETY AND CONVENIENCE OF THE GENERAL PUBLIC AND THE RESIDENTS ALONG THE PROPOSED CONSTRUCTION AREA.

PAVEMENT MARKINGS

TEMPORARY PAVEMENT LINE MARKINGS ON INTERMEDIATE LAYERS OF PAVEMENT SHALL BE REFLECTIVE TAPE OR REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAYS WORK. SHORT, UNMARKED SECTIONS SHALL NOT BE ALLOWED. THESE MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-05.01 PAINTED PAVEMENT MARKINGS (4" LINE), L.M.

PERMANENT PAVEMENT LINE MARKINGS SHALL BE 4" SPRAY THERMOPLASTIC (60 mil) INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-13.01, SPRAY THERMO PVMT MRKNG (60 mil) (4IN LINE), L.M. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATIONS IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.

THE CONTRACTOR MAY ELECT TO SUBSTITUTE PREFORMED PLASTIC FOR THERMOPLASTIC. PREFORMED PLASTIC SHALL BE PAID FOR AT THE SAME UNIT PRICE AS BID FOR THERMOPLASTIC.

PAVING

THE CONTRACTOR SHALL BE REQUIRED TO COLD PLANE AND PAVE IN THE DIRECTION OF TRAFFIC.

RESURFACING

WHERE DIRECTED BY THE TDOT ENGINEER, THE CONTRACTOR SHALL BE REQUIRED TO SHAPE PUBLIC SIDE ROADS, BUSINESS ENTRANCES, AND PRIVATE DRIVES, AS WELL AS CLEANING OF EXISTING DRAINS BEFORE PLACING MATERIALS. ALL COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTIONS.

ALL PUBLIC SIDE ROADS SHALL BE PAVED ONE PAVER WIDTH THROUGH THE INTERSECTION AS A MINIMUM. A SATISFACTORY TRANSITION FROM THE NEW PAVEMENT TO THE EXISTING GRADE OF THE INTERSECTING PUBLIC ROAD OR BUSINESS ENTRANCE SHALL BE PROVIDED. SHOULD THE PAVEMENT OF THE INTERSECTING PUBLIC ROAD BE DISTRESSED, THE RESURFACING WIDTH MAY BE INCREASED TO THE NORMAL RIGHT OF WAY LINE.

IN ALL CASES, THE LENGTH OF THE PAVEMENT TRANSITION, THE THICKNESS AND WIDTH OF THE RESURFACING AND ANY ADDITIONAL PAVEMENT MATERIALS SHALL BE AS DIRECTED BY THE TDOT ENGINEER.

SIGNALIZATION

ANY SIGNAL HEADS, WHEN VISIBLE TO DRIVERS BUT NOT OPERATIONAL, SHALL BE COMPLETELY COVERED.

THE PROJECT ENGINEER SHALL NOTIFY THE LOCAL GOVERNMENTAL AGENCY RESPONSIBLE FOR TRAFFIC CONTROL MAINTENANCE AT LEAST ONE DAY IN ADVANCE OF THE COLD PLANING ACTIVITY AT SIGNALIZED INTERSECTIONS WHERE DETECTOR LOOPS ARE ON THE PAVEMENT. THE MAINTAINING AGENCY WILL THEN BE RESPONSIBLE FOR DISCONNECTING THE LOOP DETECTORS AND MARKING ANY NECESSARY TIMING ADJUSTMENTS IN THE SIGNAL CONTROLLER PRIOR TO THE CONSTRUCTION.

THE PROJECT ENGINEER SHALL BE RESPONSIBLE FOR SUPPLYING THE CONTRACTOR WITH AS BUILT SIGNAL PLANS AT THE PRE-CONSTRUCTION CONFERENCE. THESE PLANS WILL PROVIDE THE CONTRACTOR WITH THE DESIRED LOCATION FOR DETECTOR LOOP REPLACEMENT.

LOOPS SHALL BE INSTALLED IN THE LEVELING COURSE IF LEVELING COURSE IS PROVIDED.

LOOPS REPLACEMENT SHALL BE IN ACCORDANCE WITH SECTION 730 OF THE STANDARD SPECIFICATIONS.

GUARDRAIL

THE CONTRACTOR SHALL NOT REMOVE ANY SECTIONS OF EXISTING GUARDRAIL TO REWORK SHOULDERS OR FLATTEN SLOPE UNTIL THE ENGINEER CONCURS IN THE NECESSITY OF REMOVAL DUE TO CONSTRUCTION REQUIREMENTS AND THE APPROPRIATE WARNING DEVICES ARE INSTALLED. THE PROPOSED GUARDRAIL, INCLUDING ANY ANCHOR SYSTEM, SHALL BE INSTALLED QUICKLY TO MINIMIZE TRAFFIC EXPOSURE TO ANY HAZARD. NO PAYMENT WILL BE MADE FOR A SECTION OF PROPOSED GUARDRAIL, INCLUDING ANCHORS, UNTIL IT IS COMPLETE IN PLACE.

IF ANY APPROACH END OF A SECTION OF GUARDRAIL OR BRIDGE RAIL MUST TEMPORARILY BE LEFT INCOMPLETE AND EXPOSED TO TRAFFIC, THE CONTRACTOR SHALL USE TWO (2) TEMPORARY BARRICADES OR DRUMS WITH TYPE A LIGHTS AND ROUNDED END ELEMENTS AS MINIMUM MEASURES TO PROTECT TRAFFIC FROM THE HAZARD OF AN EXPOSED END. ALL COST OF FURNISHING AND INSTALLING A TEMPORARY ROUNDED END ELEMENT SHALL BE INCLUDED IN THE COST OF THE PROPOSED GUARDRAIL.

CONSTRUCTION WORK ZONE & TRAFFIC CONTROL NOTES

ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE WEEK BEFORE NEEDED, IF THE SIGN FACE IS FULLY COVERED.

IF THE CONTRACTOR MOVES OFF THE PROJECT, HE SHALL COVER OR REMOVE ALL UNNEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OF REMOVAL, COVERING, AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION) PER SQUARE FOOT.

A LONG TERM BUT SPORADIC USE WARNING SIGN, SUCH AS A FLAGGER SIGN, MAY REMAIN IN PLACE WHEN NOT REQUIRED PROVIDED THE SIGN FACE IS FULLY COVERED.

TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.

THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY, WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHEN THE LANE IS OPEN TO TRAFFIC UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO BE PARKED WITHIN THIRTY (30) FEET OF AN OPEN TRAFFIC LANE AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.

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SEALED BY

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES

SPECIAL NOTES

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2015	STP/HSIP-NH-14(58)	4A

SURFACE IS TO BE CROWNED AS DIRECTED BY THE ENGINEER.

ALL SIGNS WHICH INTERFERE WITH CONSTRUCTION WILL BE RELOCATED OUTSIDE LIMITS OF CONSTRUCTION BY THE CONTRACTOR. UPON COMPLETION OF CONSTRUCTION, THE CONTRACTOR WILL RESTORE THE SIGNS TO ORIGINAL LOCATION. COST TO BE INCLUDED IN ITEM NO. 712-06.

IF THE CONTRACTOR ELECTS TO UTILIZE SIGN POST ANCHORS (STUBS) FOR SIGN ERECTION, THESE SHALL BE REMOVED WHEN THE SIGNS ARE REMOVED TO AVOID FUTURE DAMAGE TO MOWERS OR OTHER MACHINERY.

UNDER THE DIRECTION OF THE ENGINEER, THE CONTRACTOR MAY BE REQUIRED TO APPLY PAINTED MARKINGS IN THE PAVEMENT AREAS NOT SPECIFICALLY DETAILED IN THE PLANS. PAYMENT WILL BE MADE AT THE UNIT PRICE BID FOR ITEM NO. 716-05.01.

UNDER THE DIRECTION OF THE ENGINEER, THE CONTRACTOR MAY BE REQUIRED TO APPLY PLASTIC MARKINGS IN THE PAVEMENT AREAS NOT SPECIFICALLY DETAILED IN THE PLANS. PAYMENT WILL BE MADE AT THE UNIT PRICE BID FOR ITEM NO. 716-13.06.

ITEM 303-02 TO BE PLACED BEFORE PLACING SURFACING MATERIAL.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ANY SIGNS AND MAILBOXES DURING THE OPERATION, ANY SIGNS OR MAILBOXES DAMAGED AS A RESULT OF THE OPERATIONS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.

THE CONTRACTOR SHALL PROPERLY ADJUST ALL MANHOLES, ALL UTILITY VALVE COVERS AND LIKE STRUCTURES TO THE FINISHED GRADE OF THE PAVEMENT. IF EXTENSION RINGS ARE USED, THEY WILL BE SPOT WELDED A MINIMUM OF FIVE TIMES. COST OF ADJUSTMENT IS TO BE INCLUDED IN ITEM 411-03.10, ADJUSTMENT IS TO BE MADE PRIOR TO THE LAYING OF ITEM 411-03.10.

TAPER AROUND ALL CATCH BASINS.

MATERIAL TRANSFER DEVICE SHALL BE USED FOR D MIX.

THE CONTRACTOR SHALL KEEP TWO TRAFFIC LANES, ONE IN EACH DIRECTION, OPEN TO TRAFFIC DURING NON-WORK HOURS / OR NON-WORK DAYS.

ONLY 4 ARROW BOARDS WILL BE PAID FOR DURING THE LIFE OF THIS PROJECT. IF THE CONTRACTOR'S WORK PHASES REQUIRED ADDITIONAL ARROW BOARDS, THEIR COST WILL BE INCLUDED IN THE COST OF OTHER ITEMS.

THE INSIDE SHOULDERS WILL BE PAVED CONCURRENTLY WITH THE INSIDE TRAFFIC LANE.

THE CONSTRUCTION OFFICE SHOULD CONTACT THE CITY ENGINEER FOR ADVICE ON LOCATING THE ADVANCE DETECTION LOOPS

THE CONTRACTOR SHALL NOT DISTURB EXISTING LOOP DETECTORS WHILE BREAKING OUT AND REMOVING EXISTING PAVEMENTS.

THE CONTRACTOR WILL BE REQUIRED TO PAVE IMMEDIATELY BEHIND COLD PLANING OPERATIONS TO ENSURE THAT THE MILLED SURFACE WILL BE COVERED WITH BITUMINOUS/CEMENT THE SAME WORK DAY, AND ABIDE BY DROP-OFF POLICY.

THE RAISED REFLECTOR SHALL BE REMOVED BEFORE COLD PLANING.

BETWEEN L.M. 15.66 AND L.M. 20.04:
THE CONTRACTOR SHALL PROPERLY ADJUST ALL CATCH BASINS TO THE FINISHED GRADE OF THE PAVEMENT. EXTENSION RINGS SHALL BE REMOVED FROM EXISTING CATCH BASINS TO LOWER GRATE TO PAVEMENT ELEVATION. ADJUSTMENTS ARE TO BE MADE PRIOR TO LAYING OF ITEM 411-03.10. PAYMENT FOR THESE ADJUSTMENT WILL BE MADE AT THE UNIT PRICE BID FOR ITEM 611-09.01.

PAVEMENT EDGE DROP-OFF TRAFFIC CONTROL

A. DIFFERENCE IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES OR TRAFFIC LANE AND SHOULDER WHERE THE TRAFFIC LANE IS BEING USED BY TRAFFIC. CAUSED BY BASE, PAVING OR RESURFACING:

1. DIFFERENCE IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 0.75 INCH AND NOT EXCEEDING 2 INCHES.

a. WARNING SIGNS, UNEVEN PAVEMENT (W8-11) AND/OR SHOULDER DROP-OFF (W8-9A), SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

b. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY ADDED PAVEMENT SHALL BE ELIMINATED WITHIN THREE WORKDAYS.

c. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY COLD PLANING SHALL BE ELIMINATED WITHIN THREE WORKDAYS.

d. WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE TRAFFIC LANE BEING UTILIZED BY TRAFFIC AND SHOULDER, THE DIFFERENCE IN ELEVATION SHALL BE ELIMINATED WITHIN SEVEN WORKDAYS AFTER THE CONDITION IS CREATED.

SPECIAL EROSION PREVENTION AND SEDIMENT CONTROL

THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS, BEFORE CLEANING OUT OR REMOVAL/REPLACEMENT OF DITCHES, SIDE DRAINS, AND CROSS DRAINS, TO ENSURE THAT THE DRAINAGE FEATURES/ WATERCOURSES ARE NOT STREAMS OR WETLANDS. IF THE CONTRACTOR/ TDOT INSPECTOR IS UNSURE WHETHER THE DRAINAGE FEATURES ARE STREAMS OR WETLANDS, THE INSPECTOR SHALL CONTACT THE TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION TO OBTAIN THE APPROPRIATE PERMITS.

THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS BEFORE CONSTRUCTING EARTH PADS FOR THE TYPE 38 TERMINALS TO ENSURE THAT THE ENVIRONMENTAL FEATURES (STREAMS, WETLANDS, OR SPRINGS) ARE NOT IMPACTED DURING CONSTRUCTION. IF THE CONTRACTOR/TDOT INSPECTOR IS UNSURE WHETHER THE ENVIRONMENTAL FEATURES ARE STREAMS, SPRINGS OR WETLANDS, THE INSPECTOR SHALL CONTACT THE TDOT ENVIRONMENTAL DIVISION ECOLOGY OR PERMITS SECTION SO AN ACCURATE DETERMINATION CAN BE ASSESSED. AND DURING CONSTRUCTION OF THE EARTH PADS FOR THE TYPE 38 GUARDRAIL TERMINALS, EXISTING VEGETATION WILL BE REQUIRED TO BE REMOVED FOR THE PROPOSED PAD AND SLOPES. IF DURING THE VEGETATION REMOVAL, ANY TREES GREATER THAN 3 INCHES IN DIAMETER ARE REQUIRED TO BE REMOVED, CONTACT THE TDOT ENVIRONMENTAL DIVISION ECOLOGY SECTION IMMEDIATELY. DUE TO RECENT MODIFICATION OF THE USFWS STANCE REGARDING THE INDIANA BAT NO TREES >3" DBH SHOULD BE CUT WITHOUT CLEARANCE FROM THE TDOT ENVIRONMENTAL DIVISION ECOLOGY SECTION.

DUE TO THE TYPE OF WORK IT IS OUR UNDERSTANDING THAT THE PROJECT WILL NOT DISTURB MORE THAN ONE ACRE. IF THE DISTURBED AREA IS MORE THAN ONE ACRE, PLEASE CONTACT THE TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION BECAUSE A NPDES PERMIT WILL BE REQUIRED.

RAILROAD CROSSING NOTES

THE CONTRACTOR SHALL CONDUCT HIS WORK SO AS TO PROTECT THE RAILROADS' TRACKS AND PROPERTIES FROM ANY DAMAGE. THE WORK SHALL BE DONE IN ACCORDANCE WITH REGULATIONS STIPULATED BY EACH INDIVIDUAL RAILROAD BEING AFFECTED SO AS TO MAINTAIN CLEARANCE AND NOT INTERRUPT TRAIN TRAFFIC IN ANY MANNER.

THE CONTRACTOR SHALL NOT, FOR ANY REASON, STORE CONSTRUCTION EQUIPMENT UPON NOR STORE OR DUMP WASTE CONSTRUCTION MATERIALS UPON THE RAILROADS' RIGHT OF WAY.

EXCLUDE RESURFACING OF BRIDGES OVER CSX RAILROAD AT L.M. 16.28 AND L.M. 18.27 AND OVER UNION PACIFIC RAILROAD AND ILLINOIS CENTRAL RAILROAD AT L.M. 16.28.

THE CONTRACTOR SHALL USE EXTREME CARE AND TAKE ANY MEASURE NECESSARY TO PREVENT DEBRIS FROM FALLING ON TO THE RAILROADS' RIGHT OF WAY. THE COST OF REMOVING AND DISPOSING OF DEBRIS AND THE COMPLETE AND FULL PROTECTION OF THE CONTRACTOR'S WORK AREA FOR COLD PLANING AND RESURFACING SHALL BE INCLUDED IN UNIT ITEM BID ON.

SEE AGREEMENT/STATE CONTRACT SPECIAL PROVISION 105C(R) REGARDING INSURANCE AND RAILROAD FLAGGING SERVICES REQUIREMENTS. FOR WORK UNDER AND/OR ADJACENT TO THE RAILROADS' RIGHT OF WAY.

THE CONTRACTOR SHALL BE REQUIRED TO SUBMIT A DETAILED AND COMPREHENSIVE PLAN, PROCEDURE AND SCHEDULE FOR REVIEW AND APPROVAL BY THE PUBLIC ENGINEERS/MANAGERS FOR THE RAILROADS BEING AFFECTED BY THIS PROJECT FOR THE FOLLOWING TASK IN THE RAILROAD'S RIGHT OF WAY:

RE-STRIPING ON HIGHWAY OVER CSXT, UNION PACIFIC RR, AND IC RAILROAD AT L.M. 16.28, AND OVER CSXT AT L.M. 18.27.

THE CONTRACTOR SHALL NOTIFY AND COORDINATE HIS WORK AT THE RAILROAD CROSSING WITH THE FOLLOWING REPRESENTATIVES OF THE RAILROAD.

FOR CSX RAILROAD

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GENERAL ENGINEERING CONSULTANT DESIGNATE
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RAILROAD'S PUBLIC WORKS ENGINEER

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**SPECIAL
NOTES**