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SEE SHEET IA

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
BUREAU OF ENGINEERING

**STEWART COUNTY**

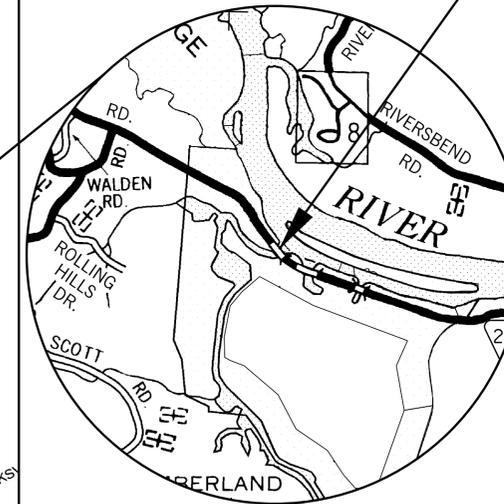
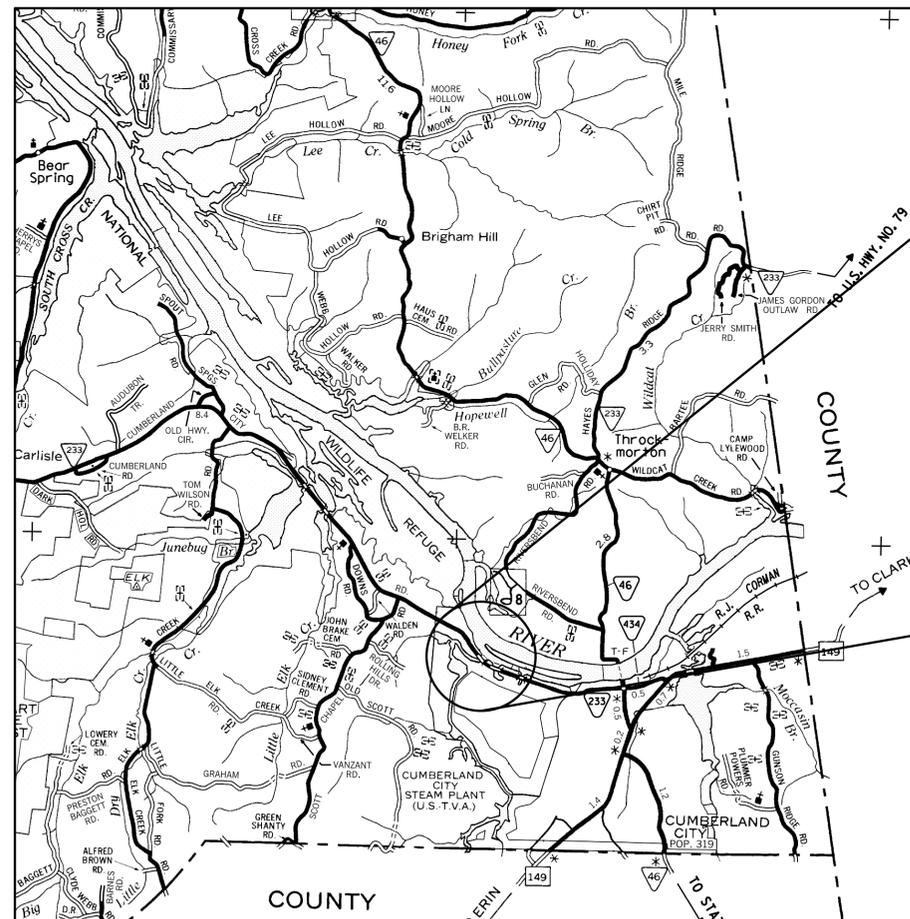
STATE ROUTE 233 OVER WELLS CREEK  
BRIDGE NO. 81-SR233-06.67

STATE HIGHWAY NUMBER 233 F.A.H.S. NO. NONE

**BRIDGE REPAIR**

TENN.	YEAR	SHEET NO.
	2015	1
FED. AID PROJ. NO.		
STATE PROJ. NO.	81010-4215-04	

PROJECT NO. 81010-4215-04



BRIDGE NO. 81-SR233-06.67  
OVER  
WELLS CREEK

**UNOFFICIAL SET**  
NOT FOR BIDDING

**SPECIAL NOTES**

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED JANUARY 1, 2015 AND ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS AND IN THE PROPOSAL CONTRACT.

TDOT C.E. MANAGER 1 OR  
TDOT DESIGN MANAGER 1 BRIAN EGLI

TDOT PROJECT MANAGER TERRY MACKIE

DESIGNED BY JAMES + ASSOCIATES, INC.

DESIGNER DAVID THOMPSON, P.E. CHECKED BY JAMIE GILLESPIE, P.E.

P.E. NO. 81010-4215-04

PIN NO. 120407.00

SCALE: 1" = 5,280'

TRAFFIC DATA	
ADT (2015)	2,600
ADT (2035)	2,950
DHV (2015)	383
D	65 - 35
T (ADT)	8 %
T (DHV)	12 %
V	30 MPH

ROADWAY LENGTH 0.060 MILES  
BRIDGE LENGTH 0.038 MILES  
PROJECT LENGTH 0.098 MILES

APPROVED: Paul D. Degges  
PAUL D. DEGGES, CHIEF ENGINEER

DATE: \_\_\_\_\_

APPROVED: John Schroer  
JOHN SCHROER, COMMISSIONER

U.S. DEPARTMENT OF TRANSPORTATION  
FEDERAL HIGHWAY ADMINISTRATION

APPROVED: \_\_\_\_\_  
DIVISION ADMINISTRATOR DATE

\*\*\*\*\*SYTIME\*\*\*\*\*  
\*\*\*\*\*DGN5PEC\*\*\*\*\*

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## ROADWAY DESIGN STANDARDS

DWG. NO.	REVISION DATE	DESCRIPTION
RD-A-1	12-18-99	STANDARD ABBREVIATIONS
RD-L-1	10-26-94	STANDARD LEGEND
RD-L-2	09-05-01	STANDARD LEGEND FOR UTILITY INSTALLATIONS
RD-L-3	04-15-04	STANDARD LEGEND FOR SIGNALIZATION AND LIGHTING
RD-L-4	04-15-04	STANDARD LEGEND FOR SIGNALIZATION AND LIGHTING
RD-L-5	05-01-08	STANDARD LEGEND FOR EROSION PREVENTION AND SEDIMENT CONTROL
RD-L-6	03-30-10	STANDARD LEGEND FOR EROSION PREVENTION AND SEDIMENT CONTROL
RD-L-7	05-24-12	STANDARD LEGEND FOR EROSION PREVENTION AND SEDIMENT CONTROL

## TRAFFIC CONTROL APPURTENANCES

T-M-1	07-24-14	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS AND MARKING ABBREVIATIONS
T-M-2	07-24-14	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS
T-PBR-1	06-30-09	INTERCONNECTED PORTABLE BARRIER RAIL
T-PBR-2	11-01-11	DETAIL FOR VERTICAL PANELS AND FLEXIBLE DELINEATORS
T-S-9	06-10-14	STANDARD LAYOUT GROUND MOUNTED SIGNS
T-S-11	06-06-11	DELINEATOR AND MILEPOST DETAILS
T-S-16	06-05-14	GROUND MOUNTED ROADSIDE SIGN AND DETAILS
T-S-20	11-01-11	SIGN DETAILS
T-SG-1	11-01-11	WOOD POLE, DETAILS FOR SPAN MOUNTED SIGNALS
T-SG-2	07-29-04	LOOP LEAD-INS, CONDUIT AND PULL BOXES
T-SG-3	11-11-04	NOTES AND DETAILS OF INDUCTIVE LOOPS
T-SG-3A		ALTERNATE DETECTION DETAILS
T-SG-4		SPAN WIRE AND MESSENGER CABLE DETAILS
T-SG-5	12-04-13	CONTROLLER CABINET DETAILS
T-SG-7A	11-01-11	TYPICAL SIGNAL HEAD PLACEMENT
T-SG-8	12-04-13	STRAIN POLE DETAILS FOR SPAN MOUNTED SIGNALS
T-SG-9A	05-01-14	MISCELLANEOUS SIGNAL DETAILS
T-SG-12	11-01-11	TYPICAL WIRING FOR SIGNAL HEADS AND DETECTION LOOPS
T-WZ-10	04-02-12	ADVANCE ROAD WORK SIGNING ON HIGHWAYS AND FREEWAYS
T-WZ-32	10-29-13	TRAFFIC CONTROL PLAN SIGNAL LAYOUT FOR TRAFFIC SIGNAL AT TWO LANE BRIDGE RECONSTRUCTION SITE
T-WZ-33	05-27-98	TRAFFIC CONTROL PLAN FOR CLOSE INTERSECTION CONDITIONS USING TRAFFIC SIGNAL AT TWO LANE BRIDGE RECONSTRUCTION SITE
T-WZ-34	09-01-05	TRAFFIC CONTROL PLAN GENERAL NOTES FOR TRAFFIC SIGNAL AT TWO LANE BRIDGE RECONSTRUCTION SITE
T-WZ-35	04-02-12	TRAFFIC CONTROL PLAN PAY ITEM AND SIGN DETAILS FOR TRAFFIC SIGNAL AT TWO LANE BRIDGE RECONSTRUCTION SITE

## EROSION PREVENTION AND SEDIMENT CONTROL

EC-STR-3C	08-01-12	SILT FENCE WITH WIRE BACKING
EC-STR-3E	04-01-08	SILT FENCE FABRIC JOINING DETAILS
EC-STR-25	08-01-12	TEMPORARY CULVERT CROSSING, CONSTRUCTION EXIT, CONSTRUCTION FORD

## BRIDGE APPURTENANCES (NEW STRUCTURES)

STD-9-1	10-07-08	REINFORCING BAR SUPPORT DETAILS FOR CONCRETE SLABS
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TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	2015	81010-4215-04	1A

1 7-31-15 JRG REMOVED SAFETY APPURTENANCES AND FENCE STANDARD DRAWINGS AND EDITED BRIDGE APPURTENANCES (NEW STRUCTURES) STD. DWGS. AND EDITED LIST OF DRAWINGS

UNOFFICIAL SET

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STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

BRIDGE NO. 81-SR233-06.67  
STATE ROUTE 233  
OVER WELLS CREEK  
STEWART COUNTY  
2015

INDEX AND STANDARD DRAWINGS

TENNESSEE D.O.T.  
DESIGN DIVISION

FILE NO.

TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	2015	81010-4215-04	1B

**PROJECT COMMITMENTS**

COMMITMENT TO	SOURCE DIVISION	DESCRIPTION	STA./LOCATION
-----	ENVIRONMENTAL DIVISION, ENVIRONMENT	COMMITMENT ID: EDHZ001 - AN ASBESTOS-CONTAINING MATERIAL (ACM) SURVEY WAS CONDUCTED ON BRIDGE# 81561140007, SR-233 OVER WELLS CREEK, LM 6.67 (81-SR233-6.67). NO ACM WAS DETECTED. NO SPECIAL ACCOMMODATIONS FOR DEMOLITION AND WASTE DISPOSAL ARE ANTICIPATED FOR THIS BRIDGE AND THE MATERIAL CAN BE DEPOSITED IN A C&D LANDFILL. PLEASE NOTE THAT EVEN THOUGH THE SAMPLES WERE FOUND TO CONTAIN NO ASBESTOS, THE DEMOLITION CONTRACTOR IS REQUIRED TO SUBMIT THE NATIONAL EMISSION STANDARDS FOR HAZARDOUS AIR POLLUTANTS STANDARD 10-DAY NOTICE OF DEMOLITION TO THE TENNESSEE DIVISION OF AIR POLLUTION CONTROL.	LM 6.67

**UNOFFICIAL SET**  
  
NOT FOR BIDDING

BRIDGE NO. 81-SR233-06.67  
STATE ROUTE 233  
OVER WELLS CREEK  
STEWART COUNTY  
2015

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

**PROJECT COMMITMENTS**





## ESTIMATED ROADWAY QUANTITIES <sup>1</sup>

ITEM NO.	DESCRIPTION	UNIT	QUANTITY
① 105-01	CONSTRUCTION STAKES, LINES AND GRADES	LS	1
202-03.01	REMOVAL OF ASPHALT PAVEMENT	S.Y.	161
203-01	ROAD AND DRAINAGE EXCAVATION (UNCLASSIFIED)	C.Y.	25
② 209-05	SEDIMENT REMOVAL	C.Y.	1
② 209-08.02	TEMPORARY SILT FENCE (WITH BACKING)	L.F.	132
303-01	MINERAL AGGREGATE, TYPE A BASE, GRADING D	TON	102
307-01.01	ASPHALT CONCRETE MIX (PG64-22) (BPMB-HM) GRADING A	TON	29
307-01.08	ASPHALT CONCRETE MIX (PG64-22) (BPMB-HM) GRADING B-M2	TON	19
402-01	BITUMINOUS MATERIAL FOR PRIME COAT (PC)	TON	1
403-01	BITUMINOUS MATERIAL FOR TACK COAT (TC)	TON	1
411-01.10	ACS MIX (PG64-22) GRADING D	TON	47
415-01.02	COLD PLANING BITUMINOUS PAVEMENT	S.Y.	718
709-05.05	MACHINED RIP-RAP (CLASS A-3)	TON	40
④ 712-01	TRAFFIC CONTROL	LS	1
712-02.02	INTERCONNECTED PORTABLE BARRIER RAIL	L.F.	410
⑤ 712-04.01	FLEXIBLE DRUMS (CHANNELIZING)	EACH	16
712-04.50	PORTABLE BARRIER RAIL DELINEATOR	EACH	22
712-05.01	WARNING LIGHTS (TYPE A)	EACH	4
712-05.03	WARNING LIGHTS (TYPE C)	EACH	16
⑥ 712-06	SIGNS (CONSTRUCTION)	S.F.	795
712-07.03	TEMPORARY BARRICADES (TYPE III)	L.F.	48
712-09.01	REMOVABLE PAVEMENT MARKING LINE	L.F.	7,000
712-09.04	REMOVABLE PAVEMENT MARKING (STOP LINE)	L.F.	52
712-09.05	REMOVABLE PAVEMENT MARKING (ARROW)	EACH	2
713-02.14	FLEXIBLE DELINEATOR (WHITE)	EACH	36
⑦ 713-16.01	CHANGEABLE MESSAGE SIGN UNIT	EACH	2
713-16.20	SIGNS (OM3-L)	EACH	2
713-16.21	SIGNS (OM3-R)	EACH	2
716-01.21	SNWPLWBLE PVMT MRKRS (BI-DIR) (1 COLOR)	EACH	6
716-13.01	SPRAY THERMO PVMT MRKNG (60 MIL) (4" LINE)	L.M.	0.4
717-01	MOBILIZATION	LS	1
⑧ 730-40	TEMPORARY TRAFFIC SIGNAL SYSTEM	EACH	1
⑨ 801-01	SEEDING (WITH MULCH)	UNIT	1
⑩ 801-03	WATER (SEEDING AND SODDING)	M.G.	1

### FOOTNOTES <sup>1</sup>

- ① ALL DIMENSIONAL DETAILS SHOWN ON PLANS, INCLUDING ELEVATIONS, SHALL BE CHECKED BY THE CONTRACTOR TO ASSURE ACCURACY OF THE LAYOUT PRIOR TO CONSTRUCTION. ALL BRIDGE SUBSTRUCTURES SHALL BE CHECKED AS TO LOCATION, DIMENSIONAL LAYOUTS AND ELEVATIONS, BY MEANS OF TWO INDEPENDENT LAYOUT METHODS. ANY ERRORS AND APPARENT DISCREPANCIES FOUND IN PREVIOUS SURVEYS, OR IN EITHER THE SPECIFICATIONS OR SPECIAL PROVISIONS, SHALL BE CALLED TO THE ENGINEER'S ATTENTION BY THE CONTRACTOR IMMEDIATELY AND PRIOR TO PROCEEDING WITH WORK.
- ② SEE SUBSECTION 209.07 OF THE STANDARD SPECIFICATIONS FOR MAINTENANCE REPLACEMENT.
- ③ REMOVED NOTE
- ④ INCLUDES COST FOR REMOVAL OF EXISTING OR CONFLICTING MARKINGS.
- ⑤ INCLUDES RELOCATION AND INSTALLATION FOR EACH PHASE OF THE CONSTRUCTION SEQUENCE: PHASE I = 16 AND PHASE II = 16 ON STATE ROUTE 233.
- ⑥ BASED ON SECTION 712.10 OF STANDARD SPECIFICATIONS, 509 S.F. FOR WIDE LOAD DETOUR.
- ⑦ COORDINATE WITH T.D.O.T. CONSTRUCTION DIVISION FOR LOCATION AND MESSAGE FOR CHANGEABLE MESSAGE SIGN.
- ⑧ THE COST ASSOCIATED WITH COORDINATING THE TRAFFIC SIGNAL PHASING AND TIMING SHALL BE INCLUDED IN THE UNIT PRICE.
- ⑨ ITEM MAY BE INCREASED, DECREASED OR ELIMINATED AS DIRECTED BY THE ENGINEER.
- ⑩ INCLUDES 1 THOUSAND GALLONS FOR EROSION PREVENTION AND SEDIMENT CONTROL.
- ⑪ REMOVED NOTE

TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	2015	81010-4215-04	2B

1 7-31-15 JRG REMOVED ITEM NOS. 705-01.01, 705-02.02, 705-04.05, 705-04.07, 705-08.51 AND 706-01, AND REMOVED NOTES 3 AND 11

**UNOFFICIAL SET**

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STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

BRIDGE NO. 81-SR233-06.67  
STATE ROUTE 233  
OVER WELLS CREEK  
STEWART COUNTY  
2015

**ESTIMATED ROADWAY QUANTITIES**

# ROADWAY GENERAL NOTES

## GRADING

- (1) ANY AREA THAT IS DISTURBED OUTSIDE LIMITS OF CONSTRUCTION DURING THE LIFE OF THIS PROJECT SHALL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE.
- (2) THE CONTRACTOR SHALL NOT DISPOSE OF ANY MATERIAL EITHER ON OR OFF STATE-OWNED R.O.W. IN A REGULATORY FLOODWAY AS DEFINED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY WITHOUT APPROVAL BY SAME. ALL MATERIAL SHALL BE DISPOSED OF IN UPLAND (NON-WETLAND) AREAS AND ABOVE ORDINARY HIGH WATER OF ANY ADJACENT WATERCOURSE. THIS DOES NOT ELIMINATE THE NEED TO OBTAIN ANY OTHER LICENSES OR PERMITS THAT MAY BE REQUIRED BY ANY OTHER FEDERAL, STATE OR LOCAL AGENCY.

## SEEDING AND SODDING

- (3) ITEM NO. 801-01, SEEDING (WITH MULCH), SHALL BE USED WHERE EROSION CONTROL BLANKET OR SOD ARE NOT APPLIED.

## GUARDRAIL

- (4) THE CONTRACTOR SHALL NOT REMOVE ANY SECTIONS OF EXISTING GUARDRAIL TO REWORK SHOULDERS OR FLATTEN SLOPES UNTIL THE ENGINEER CONCURS IN THE NECESSITY OF REMOVAL DUE TO CONSTRUCTION REQUIREMENTS AND THE APPROPRIATE WARNING DEVICES ARE INSTALLED. THE PROPOSED GUARDRAIL, INCLUDING ANY ANCHOR SYSTEM, SHALL BE INSTALLED QUICKLY TO MINIMIZE TRAFFIC EXPOSURE TO ANY HAZARD. NO PAYMENT WILL BE MADE FOR A SECTION OF PROPOSED GUARDRAIL, INCLUDING ANCHORS, UNTIL IT IS COMPLETELY IN PLACE.
- (5) IF ANY APPROACH END OF A SECTION OF GUARDRAIL OR BRIDGE RAIL MUST TEMPORARILY BE LEFT INCOMPLETE AND EXPOSED TO TRAFFIC, THE CONTRACTOR SHALL USE TWO (2) TEMPORARY BARRICADES OR DRUMS WITH TYPE "A" LIGHTS AND ROUNDED END ELEMENTS AS MINIMUM MEASURES TO PROTECT TRAFFIC FROM THE HAZARD OF AN EXPOSED END. ALL COST OF FURNISHING AND INSTALLING A TEMPORARY ROUNDED END ELEMENT SHALL BE INCLUDED IN THE COST OF THE PROPOSED GUARDRAIL.

## DRAINAGE

- (6) THE CONTRACTOR SHALL SHAPE DITCHES TO THE SPECIFIED DESIGN. THIS WORK WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COST WILL BE INCLUDED IN THE COST OF OTHER ITEMS.

## UTILITIES

- (7) THE LOCATIONS OF UTILITIES SHOWN WITHIN THESE PLANS ARE APPROXIMATE ONLY. EXACT LOCATIONS SHALL BE DETERMINED IN THE FIELD BY CONTACTING THE UTILITY COMPANIES INVOLVED. NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC., AT 1-800-351-1111 AS REQUIRED BY TCA 65-61-106 WILL BE REQUIRED.
- (8) UNLESS OTHERWISE NOTED, ALL UTILITY ADJUSTMENTS WILL BE PERFORMED BY THE UTILITY OR IT'S REPRESENTATIVE. THE CONTRACTOR AND UTILITY OWNERS WILL BE REQUIRED TO COOPERATE WITH EACH OTHER IN ORDER TO EXPEDITE THE WORK REQUIRED BY THIS CONTRACT. ON CONTRACTS WHERE CONSTRUCTION STAKES, LINES, AND GRADES ARE CONTRACT ITEMS, THE CONTRACTOR WILL BE REQUIRED TO PROVIDE RIGHT-OF-WAY OR SLOPE STAKES, DITCH OR STREAM BED GRADES, OR OTHER ESSENTIAL SURVEY STAKING TO PREVENT CONFLICTS WITH THE HIGHWAY CONSTRUCTION. FREQUENTLY, THIS WILL BE REQUIRED AS THE FIRST ITEM OF WORK AND AT ANY LOCATION ON THE PROJECT DIRECTED BY THE ENGINEER.
- (9) THE CONTRACTOR WILL PROVIDE ALL NECESSARY PROTECTIVE MEASURES TO SAFEGUARD EXISTING UTILITIES FROM DAMAGE DURING CONSTRUCTION OF THIS PROJECT. IN THE EVENT THAT SPECIAL EQUIPMENT IS REQUIRED TO WORK OVER AND AROUND THE UTILITIES, THE CONTRACTOR WILL BE REQUIRED TO FURNISH SUCH EQUIPMENT. THE COST OF PROTECTING UTILITIES FROM DAMAGE AND FURNISHING SPECIAL EQUIPMENT WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (10) THE CONTRACTOR SHALL NOTIFY EACH INDIVIDUAL UTILITY OWNER OF HIS PLAN OF OPERATION IN THE AREA OF THE UTILITIES. PRIOR TO COMMENCING WORK, THE CONTRACTOR SHALL CONTACT THE UTILITY OWNERS AND REQUEST THEM TO PROPERLY LOCATE THEIR RESPECTIVE UTILITY ON THE GROUND. THIS NOTIFICATION SHALL BE GIVEN AT LEAST THREE (3) BUSINESS DAYS PRIOR TO COMMENCEMENT OF OPERATIONS AROUND THE UTILITY IN ACCORDANCE WITH TCA 65-31-106.

## MISCELLANEOUS

- (11) ALL DETOUR, ACCESS, SERVICE AND FRONTAGE ROADS SHALL BE CONSTRUCTED WITH A MINIMUM OF ONE (1) COURSE OF BASE MATERIAL BEFORE TRAFFIC IS INTERRUPTED ON EXISTING ROADS.
- (12) THE CONTRACTOR SHALL BE REQUIRED TO REMOVE AND RESET MAILBOXES WHERE AND AS DIRECTED BY THE ENGINEER.

- (13) NOTHING IN THE GENERAL NOTES OR SPECIAL PROVISIONS SHALL RELIEVE THE CONTRACTOR FROM HIS RESPONSIBILITIES TOWARD THE SAFETY AND CONVENIENCE OF THE GENERAL PUBLIC AND THE RESIDENTS ALONG THE PROPOSED CONSTRUCTION AREA.

## PAVEMENT MARKINGS

- (14) PERMANENT PAVEMENT LINE MARKINGS SHALL BE 4" SPRAY THERMOPLASTIC (60 MIL) INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-13.01, SPRAY THERMO PVMT MRKNG (60 MIL) (4IN LINE), L.M. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.

## DETOURS, LANE SHIFTS AND MEDIAN CROSS-OVERS

- (15) BEFORE OPENING THE LANE SHIFT TO TRAFFIC, THE TRANSITIONAL MARKINGS ON THE EXISTING ROADWAY MUST BE IN PLACE. THESE MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM 712-09.01 REMOVABLE PAVEMENT MARKING LINE, LIN. FT. ALL EXISTING MARKINGS IN THE AREA OF THESE TRANSITIONAL MARKINGS SHALL BE OBLITERATED AND ALL EXISTING RAISED PAVEMENT MARKERS SHALL BE REMOVED TO ELIMINATE CONFLICTING MARKINGS. REMOVAL OF THE EXISTING CONFLICTING MARKINGS AND RAISED PAVEMENT MARKERS WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COST WILL BE INCLUDED IN ITEM NO. 712-01 TRAFFIC CONTROL, LUMP SUM.

## PAVING

- (16) THE CONTRACTOR SHALL BE REQUIRED TO COLD PLANE AND PAVE IN THE DIRECTION OF TRAFFIC.

## RESURFACING

- (17) WHERE DIRECTED BY THE TDOT ENGINEER, THE CONTRACTOR SHALL BE REQUIRED TO SHAPE PUBLIC SIDE ROADS, BUSINESS ENTRANCES, AND PRIVATE DRIVES, AS WELL AS CLEANING OF EXISTING DRAINS BEFORE PLACING MATERIALS. ALL COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (18) ALL PUBLIC SIDE ROADS SHALL BE PAVED ONE PAVEMENT WIDTH THROUGH THE INTERSECTION AS A MINIMUM. A SATISFACTORY TRANSITION FROM THE NEW PAVEMENT TO THE EXISTING GRADE OF THE INTERSECTING PUBLIC ROAD OR BUSINESS ENTRANCE SHALL BE PROVIDED. SHOULD THE PAVEMENT OF THE INTERSECTING PUBLIC ROAD BE DISTRESSED, THE RESURFACING WIDTH MAY BE INCREASED TO THE NORMAL RIGHT OF WAY LINE.
- (19) PRIVATE DRIVEWAYS, FIELD ENTRANCES, AND BUSINESS ENTRANCES WILL BE RESURFACED A PAVEMENT WIDTH (LANE WIDTH) AS A MINIMUM. A PAVEMENT TAPER TO TRANSITION THE NEW PAVEMENT SHALL BE REQUIRED. IT SHALL BE BASED ON AN ADDITIONAL ONE FOOT OF WIDTH PER ONE INCH DEPTH OF PAVEMENT. IF THE SHOULDER IS NARROW ENOUGH THAT THE SUM OF THE SHOULDER AND THE TRANSITION ARE LESS THAN A PAVEMENT WIDTH, THE TRANSITION SHALL OCCUR WITHIN THE PAVEMENT WIDTH. IF THE SUM OF THE SHOULDER AND THE TRANSITION IS GREATER THAN A PAVEMENT WIDTH (LANE WIDTH), THE TRANSITION SHALL OCCUR OUTSIDE OF THE PAVEMENT WIDTH.
- (20) IN ALL CASES, THE LENGTH OF THE PAVEMENT TRANSITION, THE THICKNESS AND WIDTH OF THE RESURFACING AND ANY ADDITIONAL PAVEMENT MATERIALS SHALL BE AS DIRECTED BY THE TDOT ENGINEER.

## CONSTRUCTION WORK ZONE AND TRAFFIC CONTROL NOTES

- (21) ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE WEEK BEFORE NEEDED, IF THE SIGN FACE IS FULLY COVERED.
- (22) IF THE CONTRACTOR MOVES OFF THE PROJECT, HE SHALL COVER OR REMOVE ALL UNNEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OF REMOVAL, COVERING, AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION) PER SQUARE FOOT.
- (23) A LONG TERM BUT SPORADIC USE WARNING SIGN, SUCH AS A FLAGGER SIGN, MAY REMAIN IN PLACE WHEN NOT REQUIRED PROVIDED THE SIGN FACE IS FULLY COVERED.
- (24) TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.

- (25) USE OF BARRICADES, PORTABLE BARRIER RAILS, VERTICAL PANELS, AND DRUMS SHALL BE LIMITED TO THE IMMEDIATE AREAS OF CONSTRUCTION WHERE A HAZARD IS PRESENT. THESE DEVICES SHALL NOT BE STORED ALONG THE ROADWAY WITHIN THIRTY (30) FEET OF THE EDGE OF THE TRAVELED WAY BEFORE OR AFTER USE UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL INCREASE TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. THESE DEVICES SHALL BE REMOVED FROM THE CONSTRUCTION WORK ZONE WHEN THE ENGINEER DETERMINES THEY ARE NO LONGER NEEDED. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.
- (26) THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY, WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHEN THE LANE IS OPEN TO TRAFFIC UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO PARK WITHIN THIRTY (30) FEET OF A OPEN TRAFFIC LANE AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.
- (27) ALL DETOUR AND CONSTRUCTION SIGNING SHALL BE IN STRICT ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

## EROSION PREVENTION AND SEDIMENT CONTROL

### DISTURBED AREA

- (28) AREAS TO BE UNDISTURBED SHALL BE CLEARLY MARKED IN THE FIELD BEFORE CONSTRUCTION ACTIVITIES BEGIN. THE TOTAL AREA TO BE DISTURBED NOT INCLUDING EXCLUSIVE BORROW/WASTE AREAS AND STAGING AREAS IS 0.074 ACRES. IF THE TOTAL DISTURBED AREA FOR THE PROJECT SITE (INCLUDING STAGING AND EXCLUSIVE WASTE/BORROW AREAS) INCREASES TO MORE THAN 1 ACRE, THEN THE NATURAL RESOURCES OFFICE OF THE ENVIRONMENTAL DIVISION MUST BE NOTIFIED SO THAT A SWPPP CAN BE PREPARED.
- (29) PRE-CONSTRUCTION VEGETATIVE GROUND COVER SHALL NOT BE DESTROYED, REMOVED OR DISTURBED (I.E. CLEARING AND GRUBBING INITIATED) MORE THAN 15 CALENDAR DAYS (10 CALENDAR DAYS FOR SITES WITH AN ACTIVE ARAP) PRIOR TO GRADING OR EARTH MOVING ACTIVITIES UNLESS THE AREA IS MULCHED, SEEDED WITH MULCH, OR OTHER TEMPORARY COVER IS INSTALLED.
- (30) CLEARING, GRUBBING, AND OTHER DISTURBANCE TO RIPARIAN VEGETATION SHALL BE LIMITED TO THE MINIMUM NECESSARY FOR SLOPE CONSTRUCTION AND EQUIPMENT OPERATIONS. EXISTING VEGETATION SHOULD BE PRESERVED TO THE MAXIMUM EXTENT POSSIBLE. UNNECESSARY VEGETATION REMOVAL IS PROHIBITED.
- (31) ALL DISTURBED AREAS SHALL BE PROPERLY STABILIZED AS SOON AS PRACTICABLE. PRIORITY SHALL BE GIVEN TO FINISHING OPERATIONS AND PERMANENT EPSC MEASURES OVER TEMPORARY EPSC MEASURES ON ALL PROJECTS.
- (32) CONSTRUCTION SHALL BE SEQUENCED AND STAGED TO MINIMIZE THE EXPOSURE TIME OF GRADED OR DENUDED SOIL AREAS, PRESERVE TOPSOIL, AND MINIMIZE SOIL COMPACTION.
- (33) IF OFFSITE BORROW AND WASTE AREAS BECOME NECESSARY DURING THE LIFE OF THE PROJECT, THIS SUPPORT ACTIVITY SHALL BE ADDRESSED PER THE TDOT WASTE AND BORROW MANUAL.

### SEDIMENT CONTROL

- (34) EPSC MEASURES SHALL BE INSTALLED AND FUNCTIONAL PRIOR TO ANY EARTH MOVING OPERATIONS, AND SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD.

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UNOFFICIAL SET

NOT FOR BIDDING

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

ROADWAY  
GENERAL NOTES

BRIDGE NO. 81-SR233-06.67  
STATE ROUTE 233  
OVER WELLS CREEK  
STEWART COUNTY  
2015

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# ROADWAY GENERAL NOTES (CONT'D)

## EROSION PREVENTION AND SEDIMENT CONTROL SEDIMENT CONTROL (CONT'D)

- (35) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT THE OFF-SITE MIGRATION OR DEPOSIT OF SEDIMENT ON ROADWAYS USED BY THE GENERAL PUBLIC. IF SEDIMENT ESCAPES THE CONSTRUCTION SITE, OFF-SITE ACCUMULATIONS OF SEDIMENT THAT HAVE NOT REACHED A STREAM MUST BE REMOVED AT A FREQUENCY SUFFICIENT TO MINIMIZE OFF-SITE IMPACTS (E.G., FUGITIVE SEDIMENT THAT HAS ESCAPED THE CONSTRUCTION SITE AND HAS COLLECTED IN A STREET MUST BE REMOVED SO THAT IT IS NOT SUBSEQUENTLY WASHED INTO STORM SEWERS AND STREAMS BY THE NEXT RAIN AND/OR SO THAT IT DOES NOT POSE A SAFETY HAZARD TO USERS OF PUBLIC STREETS). ARRANGEMENTS CONCERNING REMOVAL OF SEDIMENT ON ADJOINING PROPERTY MUST BE SETTLED WITH THE ADJOINING PROPERTY OWNER BEFORE REMOVAL OF SEDIMENT.
  - (36) WATER PUMPED FROM WORK AREAS AND EXCAVATION MUST BE HELD IN SETTLING BASINS OR TREATED BY FILTRATION OR CHEMICAL TREATMENT PRIOR TO ITS DISCHARGE INTO SURFACE WATERS. ALL PHYSICAL AND/OR CHEMICAL TREATMENT WILL BE APPLIED IN ACCORDANCE WITH MANUFACTURER'S GUIDELINES AND FULLY DESCRIBED IN THE EPSC PLANS. WATER MUST BE HELD IN SETTLING BASINS UNTIL AT LEAST AS CLEAR AS THE RECEIVING WATERS. SETTLING BASINS SHALL NOT BE LOCATED CLOSER THAN 20 FEET FROM THE TOP BANK OF A STREAM. SETTLING BASINS AND SEDIMENT TRAPS SHALL BE PROPERLY DESIGNED ACCORDING TO THE SIZE OF THE DRAINAGE AREAS OR VOLUME OF WATER TO BE TREATED. TREATED WATER MUST BE DISCHARGED THROUGH A PIPE OR WELL-VEGETATED OR LINED CHANNEL, SO THAT THE DISCHARGE DOES NOT CAUSE EROSION OR SEDIMENT TRANSPORT. DISCHARGES FROM BASINS AND IMPOUNDMENTS SHALL UTILIZE OUTLET STRUCTURES THAT ONLY WITHDRAW WATER FROM NEAR THE SURFACE OF THE BASIN OR IMPOUNDMENT. DISCHARGES MUST NOT CAUSE AN OBJECTIONABLE COLOR CONTRAST WITH THE RECEIVING STREAM.
  - (37) CHECK DAMS SHALL BE USED WHERE RUNOFF IS CONCENTRATED. CLEAN ROCK, BRUSH, GABION, OR SANDBAG CHECK DAMS SHALL BE PROPERLY CONSTRUCTED TO REDUCE VELOCITY AND CONTROL EROSION.
  - (38) DELAYING PLANTING OF PERMANENT COVER VEGETATION UNTIL WINTER MONTHS OR DRY MONTHS SHOULD BE AVOIDED, IF POSSIBLE.
  - (39) OFFSITE VEHICLE TRACKING OF SEDIMENTS AND THE GENERATION OF DUST SHALL BE MINIMIZED. A STABILIZED CONSTRUCTION ACCESS (A POINT OF ENTRANCE/EXIT TO THE CONSTRUCTION PROJECT) SHALL BE PROVIDED, AS NEEDED, TO REDUCE THE TRACKING OF MUD AND DIRT ONTO PUBLIC ROADS BY CONSTRUCTION VEHICLES.
  - (40) TEMPORARY EPSC MEASURES MAY BE REMOVED AT THE BEGINNING OF THE WORKDAY, BUT MUST BE REPLACED AT THE END OF THE WORKDAY.
  - (41) ALL PHYSICAL AND/OR CHEMICAL TREATMENT WILL BE APPLIED IN ACCORDANCE WITH MANUFACTURER'S GUIDELINES AND DESCRIBED ON THE EPSC PLANS FOR ALL PROJECTS REQUIRING ADDITIONAL PHYSICAL OR CHEMICAL TREATMENT OF STORMWATER RUNOFF.
- ### STREAM/WETLAND
- (42) SOIL MATERIALS MUST BE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. EPSC MEASURES TO PROTECT WATER QUALITY MUST BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD. APPROPRIATE EPSC MEASURES MUST BE INSTALLED ALONG THE BASE OF ALL FILLS AND CUTS, ON THE DOWNHILL SIDE OF STOCKPILED SOIL, AND ALONG STREAM BANKS IN CLEARED AREAS TO PREVENT SEDIMENT MIGRATION INTO STREAMS IN ACCORDANCE WITH TDOT STANDARDS. THEY MUST BE INSTALLED ON THE CONTOUR, ENTRENCHED AND STAKED, AND EXTEND THE WIDTH OF THE AREA TO BE CLEARED.
  - (43) INSTREAM EPSC DEVICES REQUIRE THE ENVIRONMENTAL DIVISION'S PERMITS SECTION REVIEW AND MUST BE PROCESSED BY THE PERMITS SECTION TO OBTAIN TDEC, USACE, AND TVA PERMITS.
  - (44) THE OPERATION OF EQUIPMENT IN WATERS OF THE STATE/U.S., INCLUDING WETLANDS, SHALL BE ONLY AS SHOWN ON THE PROJECT PLANS AND/OR AS SO SPECIFIED IN THE ARAP/401, SECTION 404 PERMITS(S) AND/OR TVA26(A), IF APPLICABLE. ANY ADDITIONAL PERMITS REQUIRED BY THE CONTRACTOR'S METHOD OF OPERATION SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO OBTAIN, AFTER RECEIVING THE APPROVAL OF TDOT ENVIRONMENTAL DIVISION.
  - (45) THE WIDTH OF THE FILL ASSOCIATED WITH TEMPORARY CROSSINGS SHALL BE LIMITED TO THE MINIMUM NECESSARY FOR THE ACTUAL CROSSING.
  - (46) STREAM BEDS SHALL NOT BE USED AS TRANSPORTATION ROUTES FOR CONSTRUCTION EQUIPMENT. TEMPORARY CROSSING MUST BE LIMITED TO ONE POINT PER STREAM AND EPSC MEASURES MUST BE USED WHERE THE STREAM BANKS ARE DISTURBED. WHERE THE STREAMBED IS NOT COMPOSED OF BEDROCK, A PAD OF CLEAN ROCK MUST BE USED AT THE CROSSING POINT AND CULVERTED TO PREVENT THE IMPOUNDMENT OF WATER FLOW. CLEAN ROCK IS ROCK OF VARIOUS TYPE AND SIZE, DEPENDING UPON APPLICATION, WHICH CONTAINS NO FINES, SOILS, OR OTHER WASTES OR CONTAMINANTS. OTHER MATERIALS USED FOR ALL TEMPORARY FILLS MUST BE COMPLETELY REMOVED IN THEIR ENTIRETY AFTER THE WORK IS COMPLETED AND THE AFFECTED AREAS RETURNED TO THEIR PREEXISTING ELEVATION. ALL TEMPORARY CROSSINGS MUST BE CONSTRUCTED IN ACCORDANCE WITH STD. DWG. EC-STR-25 UNLESS SPECIFICALLY ADDRESSED IN THE EPSC PLANS. ALTERNATIVELY, PLACING A TEMPORARY BRIDGE (BAILEY BRIDGE OR EQUIVALENT, TIMBERS, ETC.) FROM TOP OF BANK TO TOP OF BANK OR THE APPROPRIATE USE OF BARGES AT THE CROSSING TO AVOID DISTURBANCE OF THE STREAMBED IS AN ACCEPTABLE OPTION.
  - (47) HEAVY EQUIPMENT WORKING IN WETLANDS MUST BE PLACED ON MATS, OR OTHER MEASURES MUST BE TAKEN TO MINIMIZE SOIL DISTURBANCE UNLESS SPECIFICALLY ADDRESSED IN THE EPSC PLANS. ANY MATS AND OTHER MEASURES USED FOR HEAVY EQUIPMENT MUST BE REMOVED IN THEIR ENTIRETY AFTER THE WORK IS COMPLETED.
  - (48) WETLANDS SHALL NOT BE USED AS EQUIPMENT STORAGE, STAGING, OR TRANSPORTATION AREAS, UNLESS PROVIDED FOR IN THE PLANS.

## SPECIES

- (49) NO ACTIVITY MAY SUBSTANTIALLY DISRUPT THE MOVEMENT OF THOSE SPECIES OF AQUATIC LIFE INDIGENOUS TO THE WATER BODY, INCLUDING THOSE SPECIES THAT NORMALLY MIGRATE THROUGH THE AREA. THE SWPPP SHALL BE MODIFIED TO INCLUDE EPSC MEASURES TO PREVENT NEGATIVE IMPACTS TO LEGALLY PROTECTED STATE OR FEDERAL FAUNA OR FLORA OR AS INDICATED IN THE ECOLOGICAL STUDIES OR ON THE PERMIT(S).

## INSPECTION, MAINTENANCE, REPAIR

- (50) EPSC CONTROLS WILL BE INSTALLED AND MAINTAINED IN ACCORDANCE WITH TDOT STANDARD DRAWINGS AND GOOD ENGINEERING PRACTICES.
- (51) INSPECTION, REPAIR, AND MAINTENANCE OF EPSC MEASURES/STRUCTURES IS TO BE PERFORMED ON A REGULAR BASIS. SEDIMENT SHALL BE REMOVED FROM SEDIMENT CONTROL STRUCTURES WHEN THE DESIGN CAPACITY HAS BEEN REDUCED BY FIFTY PERCENT (50%). DURING SEDIMENT REMOVAL, THE CONTRACTOR SHALL TAKE CARE TO ENSURE THAT STRUCTURAL COMPONENTS OF EPSC MEASURES ARE NOT DAMAGED AND THUS MADE INEFFECTIVE. IF DAMAGE DOES OCCUR, THE CONTRACTOR SHALL REPAIR THE STRUCTURES AT THE CONTRACTOR'S OWN EXPENSE.
- (52) SEDIMENT REMOVED FROM SEDIMENT CONTROL STRUCTURES SHALL BE PLACED AND BE TREATED IN A MANNER SO THAT THE SEDIMENT IS CONTAINED WITHIN THE PROJECT LIMITS AND DOES NOT MIGRATE INTO WATERS OF THE STATE/U.S. COST FOR THIS TREATMENT IS TO BE INCLUDED IN PRICE BID FOR ITEM NO. 209-05 SEDIMENT REMOVAL, C.Y.
- (53) DISCHARGE POINTS SHALL BE INSPECTED TO ASCERTAIN WHETHER EPSC MEASURES ARE EFFECTIVE IN PREVENTING SIGNIFICANT IMPACTS TO SURROUNDING WATERS. WHERE DISCHARGE LOCATIONS ARE INACCESSIBLE, NEARBY DOWNSTREAM LOCATIONS SHALL BE INSPECTED. LOCATIONS WHERE VEHICLES ENTER AND EXIT THE SITE SHALL BE INSPECTED FOR EVIDENCE OF OFF-SITE ROADWAY SEDIMENT TRACKING.
- (54) THE TDOT PROJECT SUPERVISOR (OR THEIR DESIGNEE) AND THE CONTRACTOR'S SITE SUPERINTENDENT ARE RESPONSIBLE FOR INSPECTIONS, MAINTENANCE AND REPAIR ACTIVITIES ARE THE RESPONSIBILITY OF THE CONTRACTOR.
- (55) EPSC MEASURES FOUND TO BE INEFFECTIVE SHALL BE REPAIRED, REPLACED, OR MODIFIED BEFORE THE NEXT RAIN EVENT, IF POSSIBLE.

## MATERIALS AND STAGING

- (56) WASTE AND BORROW AREAS SHALL BE LOCATED IN NON-WETLAND AREAS AND ABOVE THE 100-YEAR, FEDERAL EMERGENCY MANAGEMENT AGENCY FLOODPLAIN. BORROW AND WASTE DISPOSAL AREAS SHALL NOT AFFECT ANY WATERS OF THE STATE/U.S. UNLESS THESE AREAS ARE SPECIFICALLY COVERED BY AN ARAP, 404, OR NPDES PERMIT, OBTAINED SOLELY BY THE CONTRACTOR.
- (57) THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR AND OBTAIN ANY NECESSARY ENVIRONMENTAL PERMITS OR APPROVALS, INCLUDING BUT NOT LIMITED TO ARCHAEOLOGY, ECOLOGY, HISTORICAL, HAZARDOUS MATERIALS, AIR AND NOISE, TDEC ARAP/401, USACE SECTION 404, TVA SECTION 26A, AND TDEC NPDES PERMITS, FROM FEDERAL, STATE AND/OR LOCAL AGENCIES REGARDING ANY STAGING AREAS TO BE USED. ANY SUCH PERMITS SHALL BE SUPPLIED TO THE TDOT PROJECT RESPONSIBLE PARTY PRIOR TO THE USE OF THE PERMITTED AREAS.

## PERMITS, PLANS, RECORDS

- (58) THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR AND OBTAIN ANY NECESSARY ENVIRONMENTAL PERMITS OR APPROVALS, INCLUDING BUT NOT LIMITED TO TDEC ARAP/401, USACE SECTION 404, TVA SECTION 26A, AND TDEC NPDES PERMITS, FROM FEDERAL, STATE AND/OR LOCAL AGENCIES REGARDING THE OPERATION OF ANY PROJECT-DEDICATED ASPHALT AND/OR CONCRETE PLANTS.
- (59) ANY DISAGREEMENT BETWEEN THE PROJECT PLANS, THE PROJECT AS CONSTRUCTED, AND THE PERMIT(S) ISSUED FOR THE PROJECT, SHALL BE BROUGHT TO THE ATTENTION OF THE TDOT PROJECT ENGINEER, THE ENVIRONMENTAL DIVISION, ROADWAY DESIGN DIVISION, AND HEADQUARTERS CONSTRUCTION OFFICE SHALL BE CONTACTED IN THESE INSTANCES AND DECIDE WHICH HAS PRECEDENCE AND WHETHER PERMIT OR PLANS REVISIONS ARE NEEDED. IN GENERAL, PERMIT CONDITIONS WILL PREVAIL.
- (60) ALL PROJECT RELATED ENVIRONMENTAL PERMITS SHALL BE MAINTAINED AT OR NEAR THE PROJECT SITE.
- (61) IF A CHANGE IN PROJECT SCOPE OCCURS DURING CONSTRUCTION, INCLUDING VALUE ENGINEERING, THE ENVIRONMENTAL DIVISION SHALL BE CONTACTED TO DETERMINE WHETHER PERMIT REVISIONS ARE NEEDED. THE ROADWAY DESIGN DIVISION SHALL BE CONTACTED TO DETERMINE IF ANY PLAN REVISIONS ARE NEEDED.
- (62) THE EPSC PLAN SHALL BE UPDATED WHENEVER EPSC INSPECTIONS INDICATE, OR WHERE STATE OR FEDERAL OFFICIALS DETERMINE EPSC MEASURES ARE PROVING INEFFECTIVE IN ELIMINATING OR SIGNIFICANTLY MINIMIZING POLLUTANT SOURCES OR ARE OTHERWISE NOT ACHIEVING THE GENERAL OBJECTIVES OF CONTROLLING POLLUTANTS IN STORM WATER DISCHARGES ASSOCIATED WITH THE CONSTRUCTION ACTIVITY.
- (63) THE TDOT PROJECT RESPONSIBLE PARTY (INCLUDING TDOT STAFF, CONSULTANTS AND CONTRACTOR STAFF) RESPONSIBLE FOR THE IMPLEMENTATION AND MAINTENANCE OF EPSC PLANS SHALL SUCCESSFULLY COMPLETE THE TDEC "LEVEL 1 - FUNDAMENTALS OF EROSION PREVENTION AND SEDIMENT CONTROL FOR CONSTRUCTION SITES" COURSE AND ANY REFRESHER COURSES AS REQUIRED TO MAINTAIN CERTIFICATION. A COPY OF CERTIFICATION RECORDS FOR THE COURSES SHALL BE KEPT ON SITE AND AVAILABLE UPON REQUEST.

- (64) THE EPSC PLAN IS TO SERVE AS AN INITIAL GUIDE FOR SITE PERSONNEL AS THE CONSTRUCTION PROCESS DEVELOPS. IT MUST BE AMENDED, MODIFIED, AND UPDATED WHENEVER A CHANGE IN THE DESIGN OR CONSTRUCTION OF THE PROJECT OCCURS. THE PHASES DEPICTED IN THE EPSC PLANS MAY NOT COINCIDE WITH THE ACTUAL PHASES OF CONSTRUCTION ESTABLISHED BY THE CONTRACTOR DURING CONSTRUCTION. THUS MODIFICATIONS WILL BE REQUIRED TO ENSURE THE EPSC PLAN IS MAINTAINED TO DEPICT CURRENT SITE CONDITIONS. IT SHOULD BE MAINTAINED SUCH THAT IT WILL ALWAYS REFLECT THE MEASURES THAT ARE INSTALLED DURING THE VARIOUS STAGES OF CONSTRUCTION. IT IS IMPRACTICAL TO DETERMINE ALL THE INTERMEDIATE PHASES OF CONSTRUCTION THAT WILL OCCUR. THUS THESE DOCUMENTS WILL HAVE TO BE UPDATED THROUGHOUT THE LIFE OF THE CONSTRUCTION PROJECT.

## LITTER, DEBRIS, WASTE, PETROLEUM

- (65) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT LITTER, CONSTRUCTION DEBRIS, AND CONSTRUCTION WASTES FROM ENTERING WATERS OF THE STATE/U.S. THESE MATERIALS WILL BE PICKED UP AND REMOVED FROM STORMWATER EXPOSURE PRIOR TO ANTICIPATED STORM EVENTS. AFTER USE, MATERIALS USED FOR EPSC WILL BE REMOVED FROM THE SITE.
- (66) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS TO ENSURE THAT PETROLEUM PRODUCTS OR OTHER CHEMICAL POLLUTANTS ARE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. ALL EQUIPMENT REFUELING, SERVICING, AND STAGING AREAS SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL LAWS, RULES, REGULATIONS, AND ORDINANCES, INCLUDING THOSE OF THE NATIONAL FIRE PROTECTION ASSOCIATION (NFPA). APPROPRIATE CONTAINMENT MEASURES FOR THESE AREAS SHALL BE USED. ALL SPILLS MUST BE REPORTED TO THE APPROPRIATE AGENCY, AND MEASURES SHALL BE TAKEN IMMEDIATELY TO PREVENT THE POLLUTION OF WATERS OF THE STATE/U.S., INCLUDING GROUNDWATER, SHOULD A SPILL OCCUR.
- (67) IF PORTABLE SANITARY FACILITIES ARE PROVIDED ON CONSTRUCTION SITES, SANITARY WASTE WILL BE COLLECTED FROM THE PORTABLE UNITS IN A TIMELY MANNER BY A LICENSED WASTE MANAGEMENT CONTRACTOR OR AS REQUIRED BY ANY LOCAL REGULATIONS. THE CONTRACTOR WILL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF SANITARY WASTE.
- (68) ALL HAZARDOUS WASTE MATERIALS WILL BE DISPOSED OF IN A MANNER WHICH IS COMPLIANT WITH LOCAL OR STATE REGULATIONS. SITE PERSONNEL WILL BE INSTRUCTED IN THESE PRACTICES, AND THE INDIVIDUAL DESIGNATED AS THE CONTRACTOR'S ON-SITE REPRESENTATIVE WILL BE RESPONSIBLE FOR SEEING THAT THESE PRACTICES ARE FOLLOWED. THE CONTRACTOR WILL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF HAZARDOUS MATERIAL.
- (69) WASTE MATERIAL (EARTH, ROCK, ASPHALT, CONCRETE, ETC.) NOT REQUIRED FOR THE CONSTRUCTION OF THE PROJECT WILL BE DISPOSED OF BY THE CONTRACTOR. THE CONTRACTOR WILL OBTAIN ANY AND ALL NECESSARY PERMITS INCLUDING, BUT NOT LIMITED TO NPDES, AQUATIC RESOURCES ALTERATION PERMIT(S) CORPS OF ENGINEERS SECTION 404 PERMITS, AND TVA SECTION 26A PERMITS TO DISPOSE OF WASTE MATERIALS.

## SPILL PREVENTION, MANAGEMENT AND NOTIFICATION

- (70) ONLY NEEDED PRODUCTS WILL BE STORED ON-SITE BY THE CONTRACTOR. THE CONTRACTOR WILL STORE ALL MATERIALS UNDER COVER AND IN APPROPRIATE CONTAINERS. PRODUCTS MUST BE STORED IN ORIGINAL CONTAINERS AND LABELED. MATERIAL MIXING WILL BE CONDUCTED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS. THE CONTRACTOR'S SITE SUPERINTENDENT WILL INSPECT MATERIALS STORAGE AREAS REGULARLY TO ENSURE PROPER USE AND DISPOSAL.
- (71) WHEN POSSIBLE, ALL PRODUCTS WILL BE USED COMPLETELY BEFORE PROPERLY DISPOSING OF THE CONTAINER OFF SITE. THE MANUFACTURER'S DIRECTIONS FOR DISPOSAL OF MATERIALS AND CONTAINERS WILL BE FOLLOWED.
- (72) WHEEL WASH WATER WILL BE COLLECTED AND ALLOWED TO SETTLE OUT SUSPENDED SOLIDS PRIOR TO DISCHARGE. WHEEL WASH WATER WILL NOT BE DISCHARGED DIRECTLY INTO ANY STORMWATER SYSTEM OR STORMWATER TREATMENT SYSTEM.
- (73) ALL ON-SITE VEHICLES WILL BE MONITORED FOR LEAKS AND RECEIVE MAINTENANCE TO REDUCE THE CHANCE OF LEAKAGE.

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ROADWAY  
GENERAL  
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# ROADWAY GENERAL AND SPECIAL NOTES (CONT'D)

## EROSION PREVENTION AND SEDIMENT CONTROL

### SPILL PREVENTION, MANAGEMENT AND NOTIFICATION (CONT'D)

- (74) FERTILIZERS WILL BE APPLIED ONLY IN THE AMOUNTS SPECIFIED BY TDOT. ONCE APPLIED, FERTILIZERS WILL BE WORKED INTO THE SOIL TO LIMIT THE EXPOSURE TO STORMWATER.
- (75) ALL PAINT CONTAINERS WILL BE TIGHTLY SEALED AND STORED WHEN NOT REQUIRED FOR USE. EXCESS PAINT WILL BE DISPOSED OF ACCORDING TO THE MANUFACTURER'S INSTRUCTIONS AND APPLICABLE STATE AND LOCAL REGULATIONS.
- (76) CONTRACTORS WILL PROVIDE DESIGNATED TRUCK WASHOUT AREAS ON THE SITE. THESE AREAS MUST BE SELF CONTAINED AND NOT CONNECTED TO ANY STORMWATER OUTLET OF THE SITE. UPON COMPLETION OF CONSTRUCTION WASHOUT AREAS WILL BE PROPERLY STABILIZED. WASH DOWN OR WASTE DISCHARGE OF CONCRETE TRUCKS WILL NOT BE PERMITTED ON-SITE UNLESS PROPER SETTLEMENT AREAS HAVE BEEN PROVIDED IN ACCORDANCE WITH BOTH STATE AND FEDERAL REGULATIONS.
- (77) APPROPRIATE CLEANUP MATERIALS AND EQUIPMENT WILL BE MAINTAINED BY THE CONTRACTOR IN THE MATERIALS STORAGE AREA ON-SITE AND UNDER COVER. SPILL RESPONSE EQUIPMENT WILL BE INSPECTED AND MAINTAINED BY THE CONTRACTOR AS NECESSARY TO REPLACE ANY MATERIALS USED IN SPILL RESPONSE ACTIVITIES.
- (78) ALL SPILLS WILL BE CLEANED IMMEDIATELY AFTER DISCOVERY AND THE MATERIALS DISPOSED OF PROPERLY. THE SPILL AREA WILL BE KEPT WELL VENTILATED AND PERSONNEL WILL WEAR APPROPRIATE PROTECTIVE CLOTHING TO PREVENT INJURY FROM CONTACT WITH A HAZARDOUS SUBSTANCE.
- (79) THE CONTRACTOR'S SITE SUPERINTENDENT WILL BE THE SPILL PREVENTION AND CLEANUP COORDINATOR. THE CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT THE SITE SUPERINTENDENT HAS HAD APPROPRIATE TRAINING FOR HAZARDOUS MATERIALS HANDLING, SPILL MANAGEMENT, AND CLEANUP.
- (80) IF OIL SHEEN IS OBSERVED ON SURFACE WATER (E.G. SETTLING PONDS, DETENTION PONDS, SWALES), ACTION WILL BE TAKEN IMMEDIATELY TO REMOVE THE MATERIAL CAUSING THE SHEEN. THE CONTRACTOR WILL USE APPROPRIATE MATERIALS TO CONTAIN AND ABSORB THE SPILL. THE SOURCE OF THE OIL SHEEN WILL ALSO BE IDENTIFIED AND REMOVED OR REPAIRED AS NECESSARY TO PREVENT FURTHER RELEASES.
- (81) WHERE A RELEASE CONTAINING A HAZARDOUS SUBSTANCE IN AN AMOUNT EQUAL TO OR IN EXCESS OF A REPORTABLE QUANTITY ESTABLISHED UNDER EITHER 40 CFR 117 OR 40 CFR 302 OCCURS DURING A 24 HOUR PERIOD, SEE THE LATEST TENNESSEE GENERAL PERMIT NO. TNRI00000 STORMWATER DISCHARGES FROM CONSTRUCTION ACTIVITIES SECTION 5.1 FOR REPORTING REQUIREMENTS.

## SPECIAL NOTES

- (82) NO LESS THAN SEVEN (7) DAYS PRIOR TO THE CLOSURE OF LANES, THE CONTRACTOR SHALL NOTIFY THE FOLLOWING INDIVIDUALS OR AGENCIES COMPLETELY DESCRIBING THE AFFECTED ROADS AND THE APPROXIMATE DURATION OF THE CONSTRUCTION; THESE PARTIES INCLUDE, BUT ARE NOT LIMITED TO: (1) STEWART COUNTY SHERIFF'S DEPARTMENT, (2) STEWART COUNTY FIRE DEPARTMENT, (3) LOCAL AMBULANCE SERVICE, (4) STEWART COUNTY SCHOOL SUPERINTENDENT, (5) LOCAL POSTAL SERVICE, (6) TDOT REGION 3 TRAFFIC ENGINEERING OFFICE.

### UTILITIES

- (83) PRIOR TO SUBMITTING HIS BID, THE CONTRACTOR WILL BE SOLELY RESPONSIBLE FOR CONTACTING OWNERS OF ALL AFFECTED UTILITIES IN ORDER TO DETERMINE THE EXTENT TO WHICH UTILITY RELOCATIONS AND/OR ADJUSTMENTS WILL HAVE UPON THE SCHEDULE OF WORK FOR THE PROJECT. WHILE SOME WORK MAY BE REQUIRED "AROUND" UTILITY FACILITIES THAT WILL REMAIN IN PLACE, OTHER UTILITY FACILITIES MAY NEED TO BE ADJUSTED CONCURRENTLY WITH THE CONTRACTOR'S OPERATIONS. ADVANCE CLEAR CUTTING MAY BE REQUIRED BY THE ENGINEER AT ANY LOCATION WHERE CLEARING IS CALLED FOR IN THE SPECIFICATIONS AND CLEAR CUTTING IS NECESSARY FOR A UTILITY RELOCATION. ANY ADDITIONAL COST WILL BE INCLUDED IN OTHER ITEMS BID ON.

### DEMOLITION, REPAIR, OR REHABILITATION OF BRIDGES

- (84) THE CONTRACTOR SHALL VERIFY THAT AN ASBESTOS SURVEY HAS BEEN COMPLETED PRIOR TO ANY DEMOLITION, REPAIR OR REHABILITATIONS ACTIVITIES (NOT INCLUDING ASPHALT MILLING OR OVERLAY).
- (85) ASBESTOS-CONTAINING MATERIALS (ACM) ABATEMENT IS THE RESPONSIBILITY OF THE CONTRACTOR AND SHALL BE COMPLETED PRIOR TO ANY DEMOLITION, REPAIR OR REHABILITATION OF BRIDGE(S). ABATEMENT SHOULD BE ACCOMPLISHED PER SP202ACM SPECIAL PROVISION REGARDING REMOVAL OF ASBESTOS-CONTAINING MATERIALS. STATE OF TENNESSEE ASBESTOS ACCREDITATION REQUIREMENTS (TCA 1200-01-20) MANDATE THAT ACM ABATEMENT WORK BE PERFORMED BY AN ACCREDITED FIRM (CONTRACTOR) USING ACCREDITED ABATEMENT WORKERS AND SUPERVISORS.
- (86) THE CONTRACTOR SHALL BE RESPONSIBLE FOR SUBMITTING A NOTICE TO THE TDEC, DIVISION OF AIR POLLUTION CONTROL TEN (10) DAYS IN ADVANCE OF ANY ACM ABATEMENT, DEMOLITION, OR MAJOR REPAIR INVOLVING THE REMOVAL/REPLACEMENT OF A STRUCTURAL COMPONENT.

### ENVIRONMENTAL

- (87) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION COMPREHENSIVE INSPECTION OFFICE SHALL BE INVITED TO ALL PRE-CONSTRUCTION MEETINGS.

### ECOLOGY

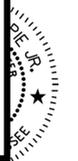
- (88) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION OR A DESIGNEE WILL ADVISE THE CONTRACTOR DURING THE PRE-CONSTRUCTION MEETING CONCERNING WHEN ENVIRONMENTAL DIVISION PERSONNEL OR DESIGNATED CONSULTANT WILL NEED TO BE ON-SITE FOR WORK BEING DONE WHICH COULD AFFECT THE STREAM OR SPECIES.
- (89) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION OR A DESIGNEE WILL ATTEND THE PRE-CONSTRUCTION MEETING FOR ALL PROJECTS WHICH HAVE THREATENED OR ENDANGERED SPECIES OR CRITICAL HABITAT PROXIMAL TO SCHEDULED BRIDGE WORK. THIS WILL PROVIDE THE OPPORTUNITY TO ENSURE THAT PERSONNEL INCLUDING THE CONTRACTOR'S PERSONNEL AND SUBCONTRACTORS ARE MADE AWARE OF THE NECESSARY PRECAUTIONS WHICH MUST BE FOLLOWED.
- (90) ALL BRIDGE PROJECTS WITH THREATENED OR ENDANGERED SPECIES OR CRITICAL HABITAT IDENTIFIED MUST HAVE MEASURES IN PLACE TO CONTAIN CONCRETE DUST, CEMENT DUST AND ALL OTHER MATERIALS. THESE MATERIALS ARE NOT ALLOWED TO ENTER THE STREAM.

### PROJECT COMMITMENTS

- (91) SEE PROJECT COMMITMENTS, SHEET 1B, FOR DETAILS RELATING TO SPECIAL ENVIRONMENTAL COMMITMENTS REQUIRED BY THIS PROJECT.

TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	2015	81010-4215-04	2E

**UNOFFICIAL SET**  
 NOT FOR BIDDING



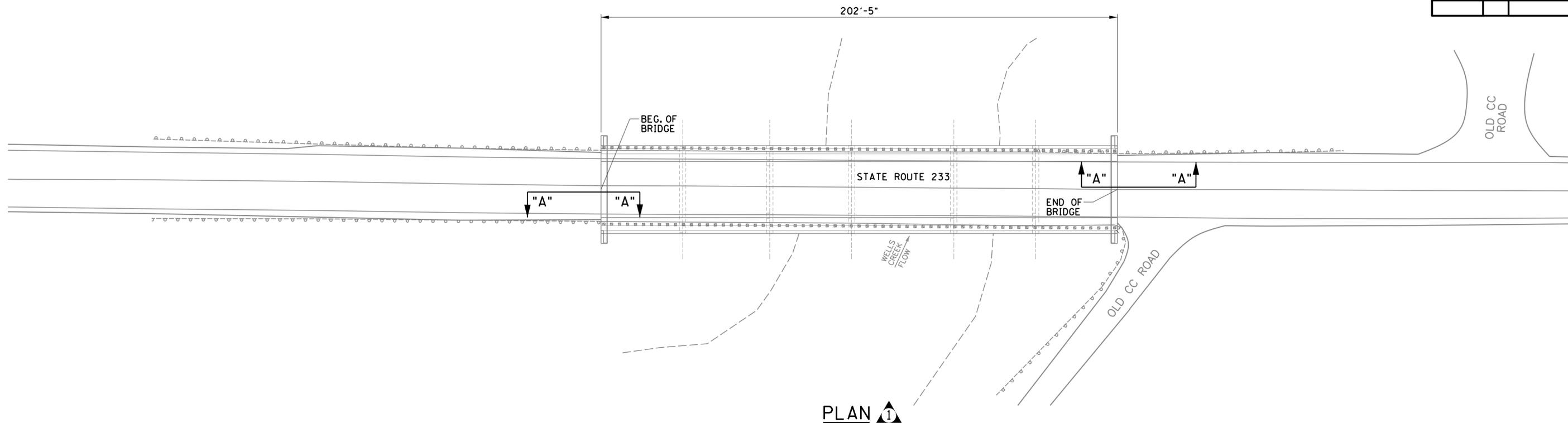
STATE OF TENNESSEE  
 DEPARTMENT OF TRANSPORTATION

BRIDGE NO. 81-SR233-06.67  
 STATE ROUTE 233  
 OVER WELLS CREEK  
 STEWART COUNTY  
 2015

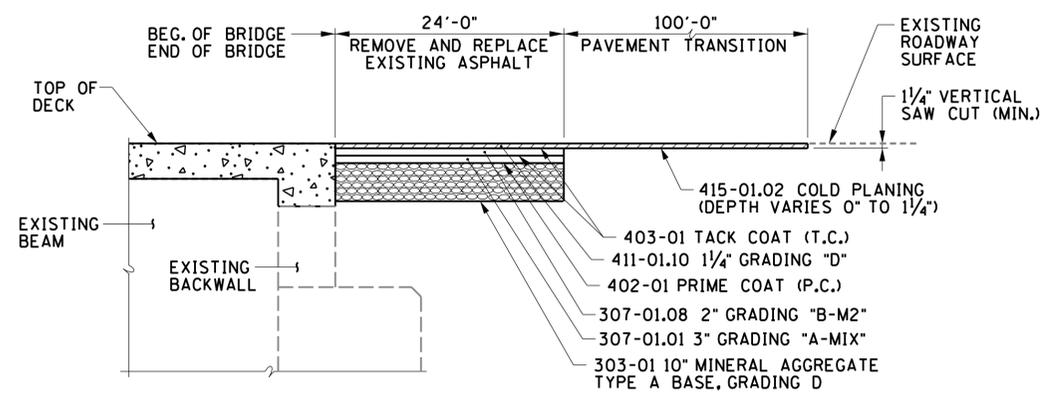
**ROADWAY  
 GENERAL AND  
 SPECIAL NOTES**

1 7-31-15 JRG REMOVED ALL REFERENCES TO GUARDRAIL AND BRIDGE PARAPET DRAINS IN PLAN VIEW AND REMOVED FILL SLOPE DETAIL

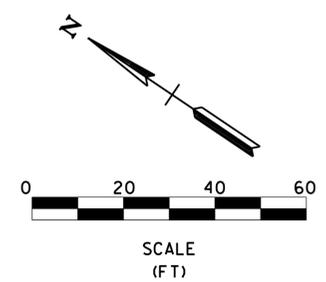
TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	2015	81010-4215-04	2F



PLAN 1



**SECTION "A-A"**  
(ASPHALT TRANSITION)  
(AT ABUTMENT NO. 1 AND NO. 2)  
N.T.S.



BRIDGE NO. 81-SR233-06.67  
STATE ROUTE 233  
OVER WELLS CREEK  
STEWART COUNTY  
2015

**UNOFFICIAL SET**  
NOT FOR BIDDING



STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

**ROADWAY DETAILS**

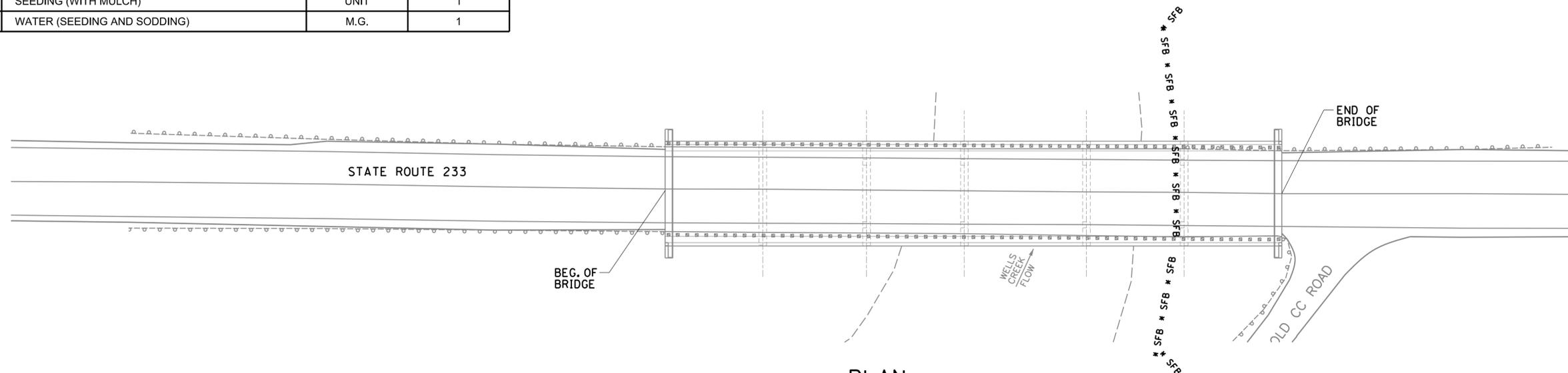
**EROSION CONTROL QUANTITIES**

ITEM NO.	DESCRIPTION	UNIT	QUANTITY
209-05	SEDIMENT REMOVAL	C.Y.	1
209-08.02	TEMPORARY SILT FENCE (WITH BACKING)	L.F.	132
801-01	SEEDING (WITH MULCH)	UNIT	1
801-03	WATER (SEEDING AND SODDING)	M.G.	1

**EPSC LEGEND**

•SFB•SFB•	SILT FENCE WITH BACKING (SFB)	EC-STR-3C
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TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	2015	81010-4215-04	26



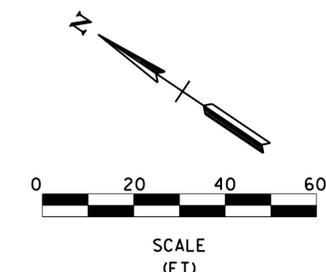
**PLAN**

AREA OF DISTURBANCE = 3,245 S.F. (0.074 ACRES)

- NOTE: SILT FENCE SHALL BE INSTALLED ON EXISTING GROUND CONTOURS WITH A 3' MINIMUM UPHILL TURN ON EACH END.
- NOTE: SEEDING (WITH MULCH) SHALL BE PLACED WITH A 4' MINIMUM WIDTH AFTER REMOVAL OF THE SILT FENCE.
- NOTE: ALL WORK AND EQUIPMENT TO REMAIN INSIDE EXISTING RIGHT OF WAY.
- NOTE: SEE SHEET NO. 2B (ESTIMATED ROADWAY QUANTITIES) FOR FOOTNOTES TO EACH ITEM.

- NOTE: THE CONTRACTOR SHALL TAKE APPROPRIATE MEASURES BELOW THE BRIDGE DURING THE REMOVAL OF CONCRETE FROM SUBSTRUCTURES AND DECK TO PREVENT ANY DEBRIS FROM FALLING INTO THE CREEK. COST FOR CONTAINMENT WILL NOT BE PAID DIRECTLY BUT SHALL BE INCLUDED IN ITEMS BID ON.
- NOTE: THE CONTRACTOR SHALL REMOVE ANY DEBRIS THAT FALLS ONTO THE AREAS BELOW THE BRIDGE, AND ANY PAINT CHIPS SHALL BE HANDLED AND DISPOSED OF PER THE SPECIFICATIONS SET FORTH IN THE GENERAL NOTES.

UNOFFICIAL SET  
  
NOT FOR BIDDING



BRIDGE NO. 81-SR233-06.67  
STATE ROUTE 233  
OVER WELLS CREEK  
STEWART COUNTY  
2015

STATE OF TENNESSEE  
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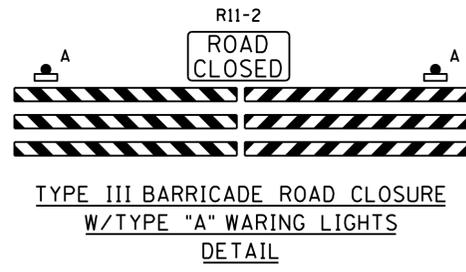
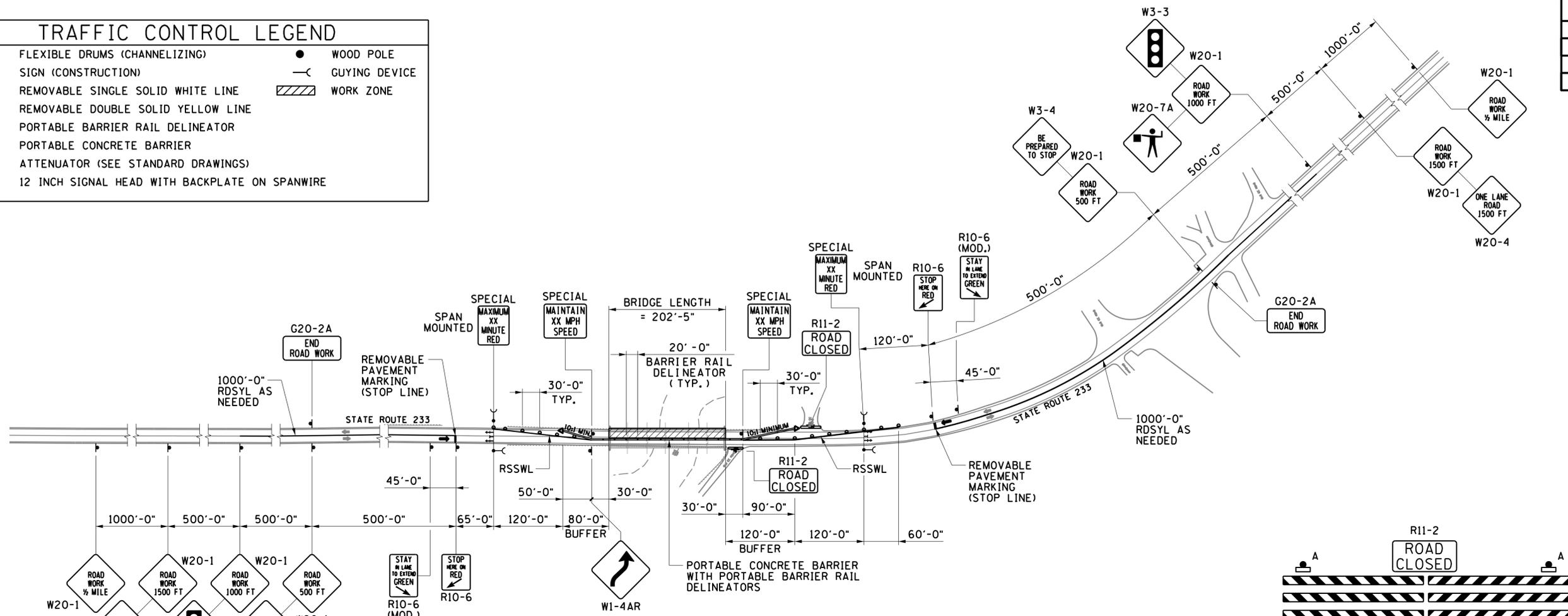
EROSION CONTROL  
PLAN  
AND  
SPECIAL NOTES



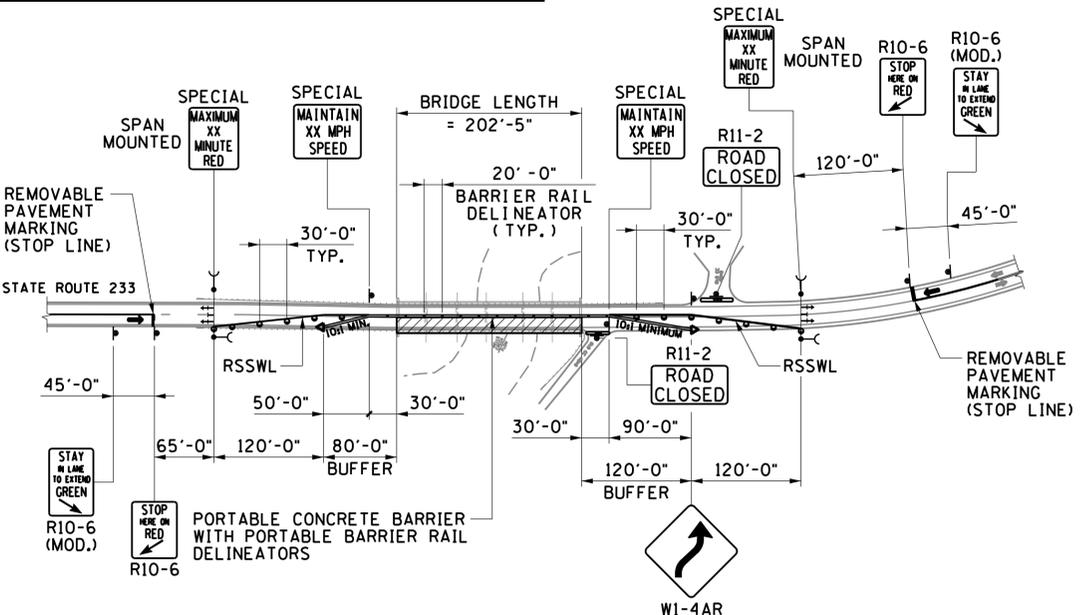
TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	2015	81010-4215-04	2J

### TRAFFIC CONTROL LEGEND

	FLEXIBLE DRUMS (CHANNELIZING)		WOOD POLE
	SIGN (CONSTRUCTION)		GUYING DEVICE
	REMOVABLE SINGLE SOLID WHITE LINE		WORK ZONE
	REMOVABLE DOUBLE SOLID YELLOW LINE		
	PORTABLE BARRIER RAIL DELINEATOR		
	PORTABLE CONCRETE BARRIER		
	ATTENUATOR (SEE STANDARD DRAWINGS)		
	12 INCH SIGNAL HEAD WITH BACKPLATE ON SPANWIRE		

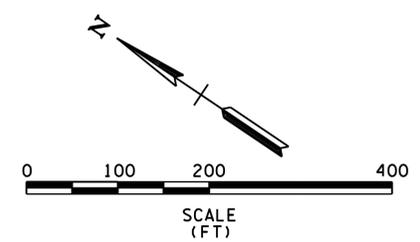


ADVANCE WARNING SIGNING  
TYPICAL FOR ALL PHASES  
OF CONSTRUCTION



**SPECIAL NOTES**

- CONTRACTOR SHALL REMOVE OR COVER ANY EXISTING STRIPING THAT CONFLICTS WITH TEMPORARY TRAFFIC CONTROL STRIPING. CONTRACTOR TO REPLACE STRIPING TO PRECONSTRUCTION DISPOSITION PRIOR TO OPENING ROAD TO NORMAL TRAFFIC.
- COORDINATE WITH T.D.O.T. CONSTRUCTION DIVISION FOR LOCATION AND MESSAGE FOR CHANGEABLE MESSAGE SIGN.



BRIDGE NO. 81-SR233-06.67  
STATE ROUTE 233  
OVER WELLS CREEK  
STEWART COUNTY  
2015

**UNOFFICIAL  
SET**  
NOT FOR  
BIDDING

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

**TRAFFIC CONTROL  
STATE ROUTE 233**

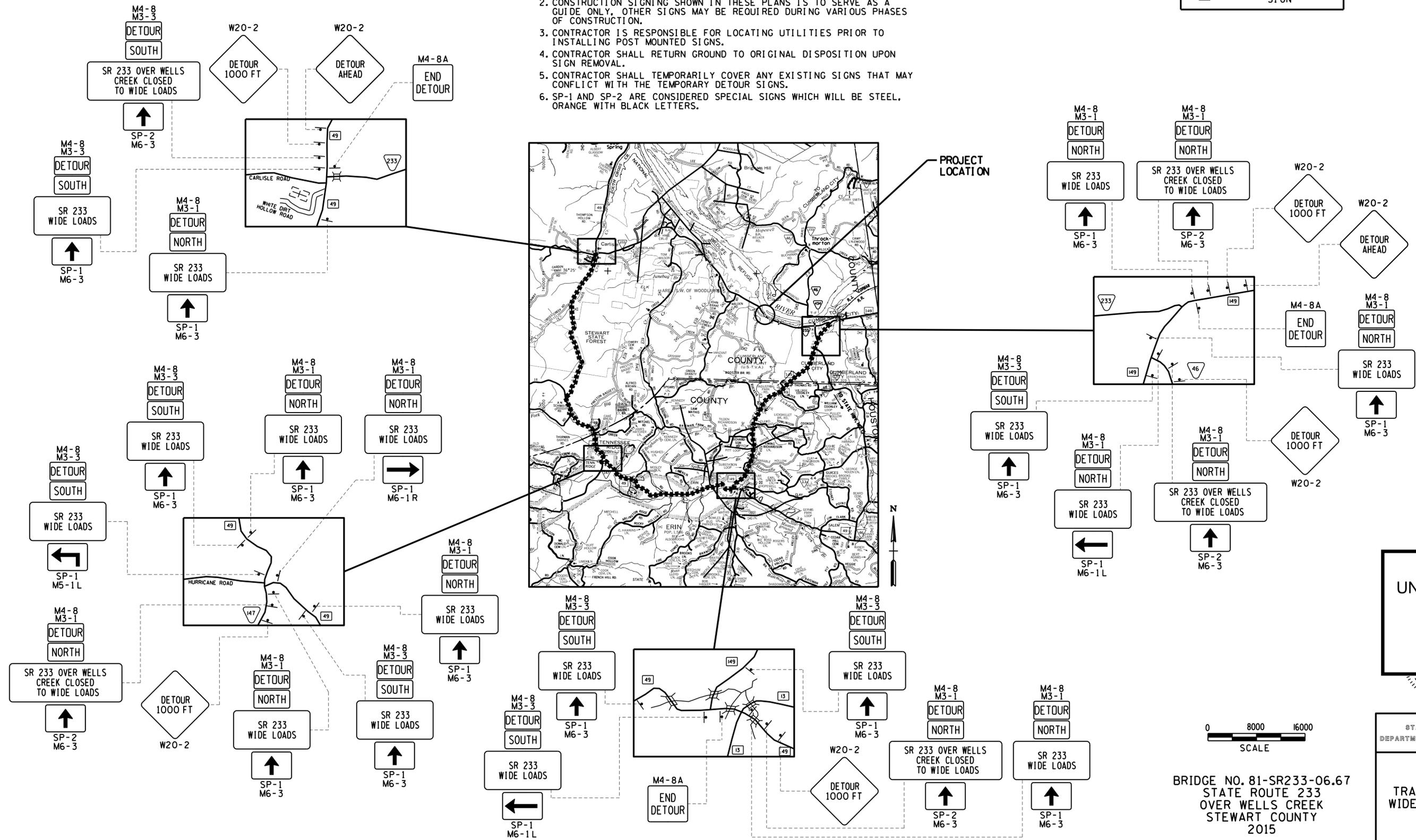
### SPECIAL NOTES

1. THESE TRAFFIC CONTROL PLANS DO NOT RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY OF INSTALLING TRAFFIC CONTROL DEVICES IN ACCORDANCE WITH THE CURRENT EDITION OF THE "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES."
2. CONSTRUCTION SIGNING SHOWN IN THESE PLANS IS TO SERVE AS A GUIDE ONLY. OTHER SIGNS MAY BE REQUIRED DURING VARIOUS PHASES OF CONSTRUCTION.
3. CONTRACTOR IS RESPONSIBLE FOR LOCATING UTILITIES PRIOR TO INSTALLING POST MOUNTED SIGNS.
4. CONTRACTOR SHALL RETURN GROUND TO ORIGINAL DISPOSITION UPON SIGN REMOVAL.
5. CONTRACTOR SHALL TEMPORARILY COVER ANY EXISTING SIGNS THAT MAY CONFLICT WITH THE TEMPORARY DETOUR SIGNS.
6. SP-1 AND SP-2 ARE CONSIDERED SPECIAL SIGNS WHICH WILL BE STEEL, ORANGE WITH BLACK LETTERS.

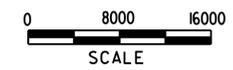
TOTAL DETOUR DISTANCE: 19 MILES

TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	2015	81010-4215-04	2K

### LEGEND



UNOFFICIAL SET  
NOT FOR BIDDING



BRIDGE NO. 81-SR233-06.67  
STATE ROUTE 233  
OVER WELLS CREEK  
STEWART COUNTY  
2015

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL  
WIDE LOAD DETOUR

**REQUIRED LOCATION REQUEST INFORMATION**

NAME OF CALLER  
 TELEPHONE NUMBER  
 BEST TIME TO CALL  
 COUNTY  
 TOWN  
 STREET ADDRESS  
 START DATE START TIME  
 TYPE OF WORK  
 BLASTING ?  
 WORK BEING DONE BY  
 WORK BEING DONE FOR

**UTILITY CONTACTS**

AT&T 333 COMMERCE ST. 23C142 NASHVILLE, TN 37201 DAVID HUFFAKER 615-214-4871	CUMBERLAND ELECTRIC MEMBERSHIP CORPORATION 420 SPRING STREET DOVER, TN 37058
WATER DEPARTMENT 121 MAIN STREET CUMBERLAND CITY, TN 37050 MR. MIKE PERRIGO	

NOTE: UTILITY INFORMATION SHOWN WAS DERIVED FROM FIELD MEASUREMENT AND OBSERVATION. THE CONTRACTOR SHALL FIELD VERIFY ALL EXISTING UTILITY LOCATIONS AND SERVICES PRIOR TO ROADWAY AND BRIDGE CONSTRUCTION.

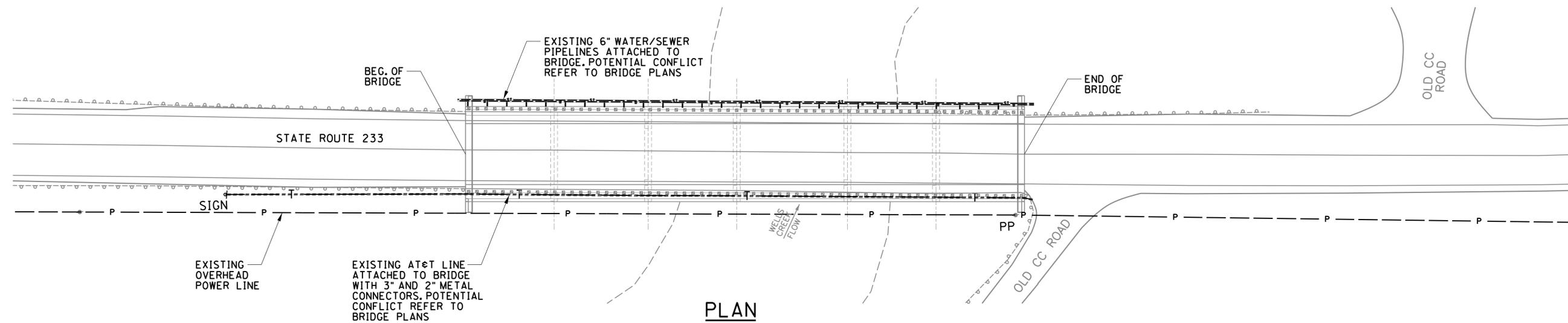
NOTE: CONTRACTOR SHALL USE EXTREME CAUTION WHEN OPERATING IN ANY AREA WHERE UTILITIES ARE PRESENT AND AVOID WORKING IN THESE AREAS AS MUCH AS PRACTICALLY POSSIBLE. DAMAGES TO ANY UTILITIES ARE TO BE REPAIRED BY THE CONTRACTOR AT HIS OWN EXPENSE.

NOTE: THE CONTRACTOR SHALL VERIFY THE LOCATION OF EXISTING POLES, GUY WIRES AND ROADWAY SIGNS PRIOR TO COMMENCEMENT OF CONSTRUCTION. THE CONTRACTOR SHALL COORDINATE THE MOVEMENT OF SUCH SIGNS, POLES AND GUY WIRES WITH THEIR RESPECTIVE OWNERS. THE CONTRACTOR SHALL RE-SET EXISTING SIGNAGE IN ITS ORIGINAL LOCATION. THE COST OF RE-SETTING SIGNAGE WILL NOT BE PAID FOR DIRECTLY BUT WILL BE INCLUDED IN OTHER ITEMS OF CONSTRUCTION.

PORTIONS OF THIS DRAWING DEPICTS EXISTING CONDITIONS. LIMITS OF PROPOSED BRIDGE CONSTRUCTION MAY VARY FROM EXISTING CONDITIONS.

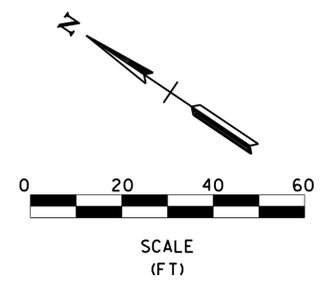
TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	2015	81010-4215-04	2L

CALL THREE WORKING DAYS BEFORE YOU DIG  
 1-800-351-1111  
 IN NASHVILLE : 366-1987



**PLAN**

LEGEND		LINE STYLES	
<b>MAPPING SYMBOLS AND CODES</b>		CABLE LINE	— C —
[Symbol] CATCH BASIN (CB)	[Symbol] MAN HOLE (MH)	CENTER LINE	— C —
[Symbol] CATCH BASIN DOUBLE (DCB)	[Symbol] MONUMENT CONCRETE OLD (MON(O))	CONCRETE AREA	[Symbol]
[Symbol] CATCH BASIN TRIPLE (TCB)	[Symbol] MONUMENT CONCRETE NEW (MON(N))	EDGE OF WOODS LINE	[Symbol]
[Symbol] ELECTRIC JUNCTION BOX (EJB)	[Symbol] P.K. NAIL OLD (PK(O))	FENCE LINE	— X — X — X —
[Symbol] ELECTRIC METER (EM)	[Symbol] P.K. NAIL NEW (PK(N))	GAS LINE	— X* — G —
[Symbol] ELECTRIC TRANSFORMER (ETF)	[Symbol] LIGHT POLE WITH POWER (PP)	OVERHEAD POWER LINE	— P —
[Symbol] FIBER OPTIC MARKER (FOM)	[Symbol] LIGHT POLE WITH TELEPHONE (T)	OVERHEAD TELEPHONE LINE	— T —
[Symbol] FIRE HYDRANT (FH)	[Symbol] POWER MAN HOLE (PMH)	OVERHEAD POWER AND TELEPHONE LINE	— P — T —
[Symbol] FLOOD LIGHT (FLT)	[Symbol] POWER POLE (P)	PROPERTY LINE	— R —
[Symbol] FLAG POLE (FP)	[Symbol] POWER AND TELEPHONE POLE (P/T)	SANITARY SEWER	— X* — SA —
[Symbol] GAS METER (GM)	[Symbol] RIGHT-OF-WAY MONUMENT (RWM)	STORM WATER	— X* — ST —
[Symbol] GUY POLE (GP)	[Symbol] SPRINKLER HEAD/VALVE (SPH,SPV)	FIBER OPTIC CABLE	— F —
[Symbol] GAS VALVE (GV)	[Symbol] SANITARY MAN HOLE (SSMH)	UNDERGROUND FIBER OPTIC CABLE	— F(UG) —
[Symbol] GUY WIRE (GW)	[Symbol] SIGN (SN)	UNDERGROUND POWER LINE	— P(UG) —
[Symbol] IRON PIN NEW (IR(N))	[Symbol] STEAM MAN HOLE (STMH)	UNDERGROUND POWER AND TELEPHONE LINE	— UG — P — T —
[Symbol] IRON PIN OLD (IR(O))	[Symbol] STORM WATER MAN HOLE (SWMH)	UNDERGROUND TELEPHONE LINE	— T(UG) —
[Symbol] IRON PIPE OLD (IP(O))	[Symbol] TELEPHONE POLE (T)	WATER LINE	— X* — W —
[Symbol] IRON SPIKE OLD (IS(O))	[Symbol] TREE		
[Symbol] LIGHT STANDARD METAL/WOOD (LS/LP)	[Symbol] WATER METER (WM)		
[Symbol] MAIL BOX (MB)	[Symbol] WATER VALVE (WV)		



BRIDGE NO. 81-SR233-06.67  
 STATE ROUTE 233  
 OVER WELLS CREEK  
 STEWART COUNTY  
 2015

**UNOFFICIAL SET**  
 NOT FOR BIDDING

STATE OF TENNESSEE  
 DEPARTMENT OF TRANSPORTATION

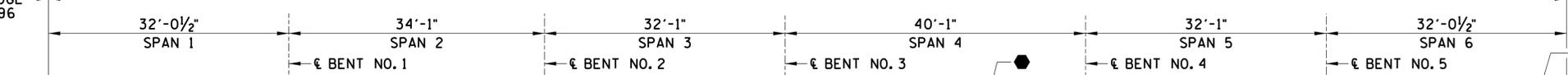
**UTILITIES**

BEG. OF BRIDGE  
STA. 45+68.96

TOTAL LENGTH = 202'-5"

END OF BRIDGE  
STA. 47+71.33

380  
370  
360  
350  
340  
330



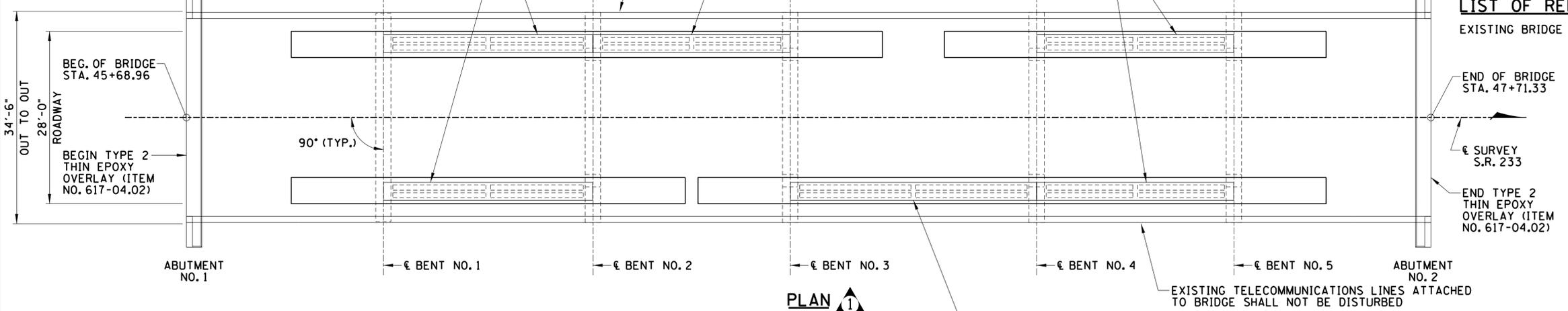
SPALL REPAIR SHALL BE QUANTIFIED AND PERFORMED ON BRIDGE RAIL AND CURBS AFTER HORIZONTAL BRIDGE RAIL MEMBERS HAVE BEEN REMOVED AND ALL SILT, DEBRIS AND VEGETATION HAS BEEN REMOVED FROM THE CURBS. SEE BR-118-129 "CONCRETE REPAIR DETAILS" FOR ESTIMATED REPAIR QUANTITIES. ALL REPAIR AREAS SHALL BE DESIGNATED BY THE BRIDGE REPAIR OFFICE AS STATED IN NOTE 1 ON BR-118-129 AND SHALL BE INCLUDED IN ITEM NO. 604-10.05 OR ITEM NO. 604-10.54.

NOTE: ALL DIMENSIONS AND VERTICAL ELEVATIONS ARE BASED ON EXISTING PLAN INFORMATION AND FIELD MEASUREMENTS. PRIOR TO CONSTRUCTION, IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO FIELD VERIFY ALL DIMENSIONS.

- ▲ DENOTES: EXISTING WATER/SEWER PIPELINE ATTACHED TO BRIDGE SHALL NOT BE DISTURBED.
- DENOTES: FOR EXISTING CONCRETE RAIL TO REMAIN, REMOVE AND DISPOSE OF ALL TOP, HORIZONTAL RAIL MEMBERS. DO NOT DISTURB EXISTING UTILITIES, CONCRETE POSTS, LOWER HORIZONTAL MEMBER OR METAL GUARDRAIL (ITEM NO. 202-04.01).

REMOVE AND DISPOSE OF EXISTING 17" DEEP x 36" WIDE x 34'-0" LONG PRESTRESSED CONCRETE BOX BEAM AND REPLACE WITH A 17" DEEP x 35" WIDE x 34'-0" LONG PRESTRESSED CONCRETE BOX BEAM. SEE BR-118-121 AND BR-118-124 FOR DETAILS.

REMOVE AND DISPOSE OF EXISTING 17" DEEP x 36" WIDE x 32'-0" LONG PRESTRESSED CONCRETE BOX BEAM AND REPLACE WITH A 17" DEEP x 35" WIDE x 32'-0" LONG PRESTRESSED CONCRETE BOX BEAM. SEE BR-118-121 AND BR-118-124A FOR DETAILS.



PLAN 1

1 SCOPE OF WORK

1. CONSTRUCT TEMPORARY ENVIRONMENTAL PROTECTION.
2. INSTALL TRAFFIC CONTROL TO PROVIDE ONE LANE OF TRAFFIC ON SR233 DURING CONSTRUCTION PHASES USING TEMPORARY SIGNALIZATION.
3. REMOVE CONCRETE OVERLAY AS REQUIRED AND REMOVE AND REPLACE THE FOLLOWING CRACKED CONCRETE BOX BEAMS: BEAM NO. 2 IN SPAN NO. 2, BEAM NO. 10 IN SPAN NO. 2, BEAM NO. 2 IN SPAN NO. 3, BEAM NO. 10 IN SPAN NO. 4, BEAM NO. 2 IN SPAN NO. 5 AND BEAM NO. 10 IN SPAN NO. 5.
4. REPLACE ALL OF THE CONCRETE OVERLAY THAT WAS REMOVED TO REPLACE THE 6 BEAMS WITH CLASS D CONCRETE (BRIDGE DECK).
5. PERFORM PARTIAL DEPTH REPAIRS TO THE OVERLAY IN AREAS INDICATED IN THESE PLANS OR DESIGNATED IN THE FIELD BY THE ENGINEER.
6. CONSTRUCT A TYPE 2 THIN EPOXY OVERLAY ON THE ENTIRE BRIDGE.
7. REPAIR AREAS OF CONCRETE SPALLING IN BEAMS, SUBSTRUCTURES AND BRIDGERAILS AND CONCRETE CURBS AS SHOWN IN THESE PLANS OR DESIGNATED IN THE FIELD BY THE ENGINEER.
8. FILL VOID IN EXISTING RIP-RAP ADJACENT TO ABUTMENT NO. 2 WITH NEW RIP-RAP.

REMOVE AND DISPOSE OF EXISTING 21" DEEP x 36" WIDE x 40'-0" LONG PRESTRESSED CONCRETE BOX BEAM AND REPLACE WITH A 21" DEEP x 35" WIDE x 40'-0" LONG PRESTRESSED CONCRETE BOX BEAM. SEE BR-118-121 AND BR-118-124B FOR DETAILS.

LIST OF SPECIAL PROVISIONS

DWG. NO.	LAST REV. DATE	DESCRIPTION
107CS	02-13-2012	NESTING SITES OF CLIFF SWALLOWS AND BARN SWALLOWS

BRIDGE APPURTENANCES 1

STD-9-1	10-07-08	REINFORCING BAR SUPPORT DETAILS FOR CONCRETE SLABS
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LIST OF BRIDGE DRAWINGS 1

DWG. NO.	REV. DATE	BRIEF DESCRIPTION
BR-118-115	07-31-15	LAYOUT OF BRIDGE TO BE REPAIRED
BR-118-116	07-31-15	ESTIMATED BRIDGE QUANTITIES AND MISC. DETAILS
BR-118-117		BRIDGE GENERAL AND SPECIAL NOTES
BR-118-118	07-31-15	BRIDGE DECK THIN EPOXY OVERLAY NOTES
BR-118-119	07-31-15	PHASING (PHASE I)
BR-118-120	07-31-15	PHASING (PHASE II)
BR-118-121	07-31-15	SUPERSTRUCTURE
BR-118-122	07-31-15	SUPERSTRUCTURE DETAILS
BR-118-123	07-31-15	DECK REPAIR DETAILS
BR-118-124	07-31-15	PRESTRESSED CONCRETE BOX BEAM DETAILS
BR-118-124A	07-31-15	PRESTRESSED CONCRETE BOX BEAM DETAILS
BR-118-124B	07-31-15	PRESTRESSED CONCRETE BOX BEAM DETAILS
BR-118-125		CONCRETE REPAIRS (ABUTMENTS)
BR-118-126		CONCRETE REPAIRS (BENT NOS. 1 AND 2)
BR-118-127		CONCRETE REPAIRS (BENT NO. 3)
BR-118-128		CONCRETE REPAIRS (BENT NOS. 4 AND 5)
BR-118-129	07-31-15	CONCRETE REPAIR DETAILS
BR-118-130	07-31-15	BILL OF STEEL

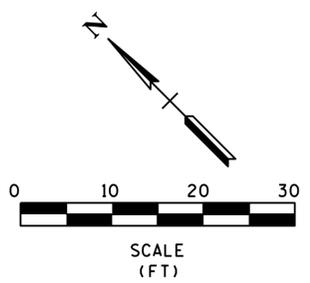
LIST OF REFERENCE DRAWINGS

DWG. NO.	DESCRIPTION
BR-37-09, BR-37-15 THRU BR-37-17, F-10-84, F-10-85 F-10-85A, H-5-114, K-3-33A	EXISTING BRIDGE PLANS

CONST. NO.	PROJECT NO.	YEAR	SHEET NO.
	81010-4215-04	2015	

REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
1	07-31-15	DWT	REVISION DATES ADDED, BR-118-130A ELIMINATED, ADDED BR-118-124A AND BR-118-124B, REVISED SCOPE OF WORK, REVISED PLAN AND ELEVATION VIEWS, REVISED BRIDGE APPURTENANCES



DESIGNED BY DAVID THOMPSON DATE \_\_\_\_\_  
 DRAWN BY ANGELA MOORE DATE \_\_\_\_\_  
 SUPERVISED BY DARRELL JAMES DATE \_\_\_\_\_  
 CHECKED BY JAMIE GILLESPIE DATE \_\_\_\_\_



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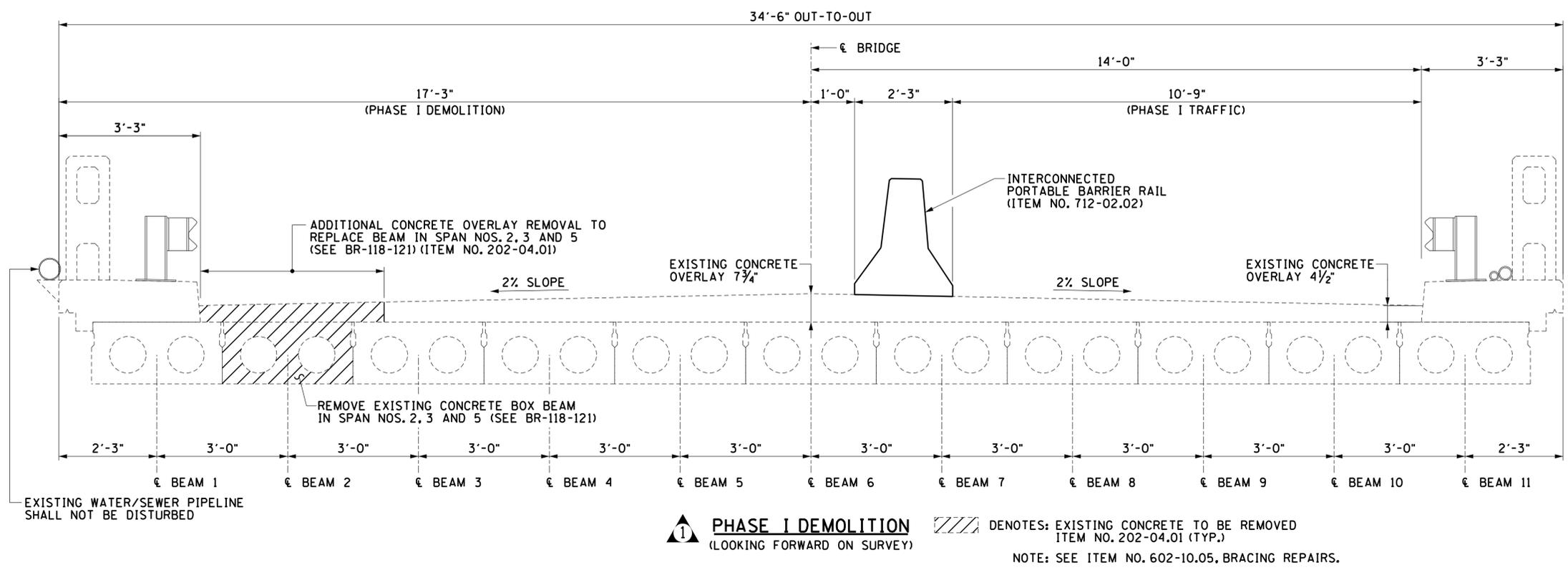
STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

LAYOUT OF BRIDGE TO BE REPAIRED  
 BRIDGE NO. 81-SR233-6.67  
 STATE ROUTE 233 OVER WELLS CREEK  
 STEWART COUNTY  
 2015

BR-118-115



CONST. NO.			
PROJECT NO.	YEAR	SHEET NO.	
81010-4215-04	2015		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
1	07-31-15	DWT	REVISED PHASE I DEMOLITION AND PHASE I CONSTRUCTION DETAILS & NOTES



**1 PHASE I DEMOLITION**  
(LOOKING FORWARD ON SURVEY)

/// DENOTES: EXISTING CONCRETE TO BE REMOVED  
ITEM NO. 202-04.01 (TYP.)

NOTE: SEE ITEM NO. 602-10.05, BRACING REPAIRS.

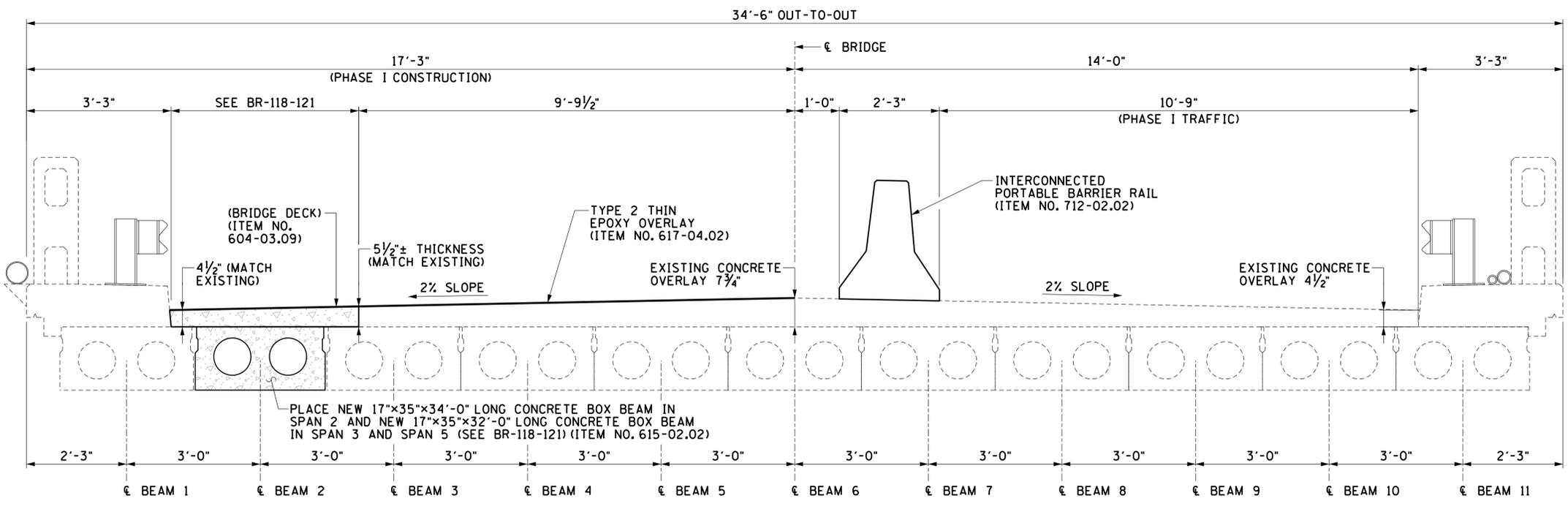
- 1 PHASE I CONSTRUCTION**
1. REMOVE AND REPLACE CRACKED CONCRETE BOX BEAMS.
  2. CONSTRUCT NEW CONCRETE OVERLAY AT LOCATION OF BEAM REPAIR.
  3. PERFORM PARTIAL DEPTH REPAIRS IN OVERLAY.
  4. CONSTRUCT TYPE 2 THIN EPOXY OVERLAY.
  5. CONSTRUCT ASPHALT PAVEMENT AND TRANSITION.
  6. PERFORM W...

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PHASING  
(PHASE I)

BRIDGE NO. 81-SR233-6.67  
STATE ROUTE 233 OVER WELLS CREEK  
STEWART COUNTY  
2015



**1 PHASE I CONSTRUCTION**  
(LOOKING FORWARD ON SURVEY)

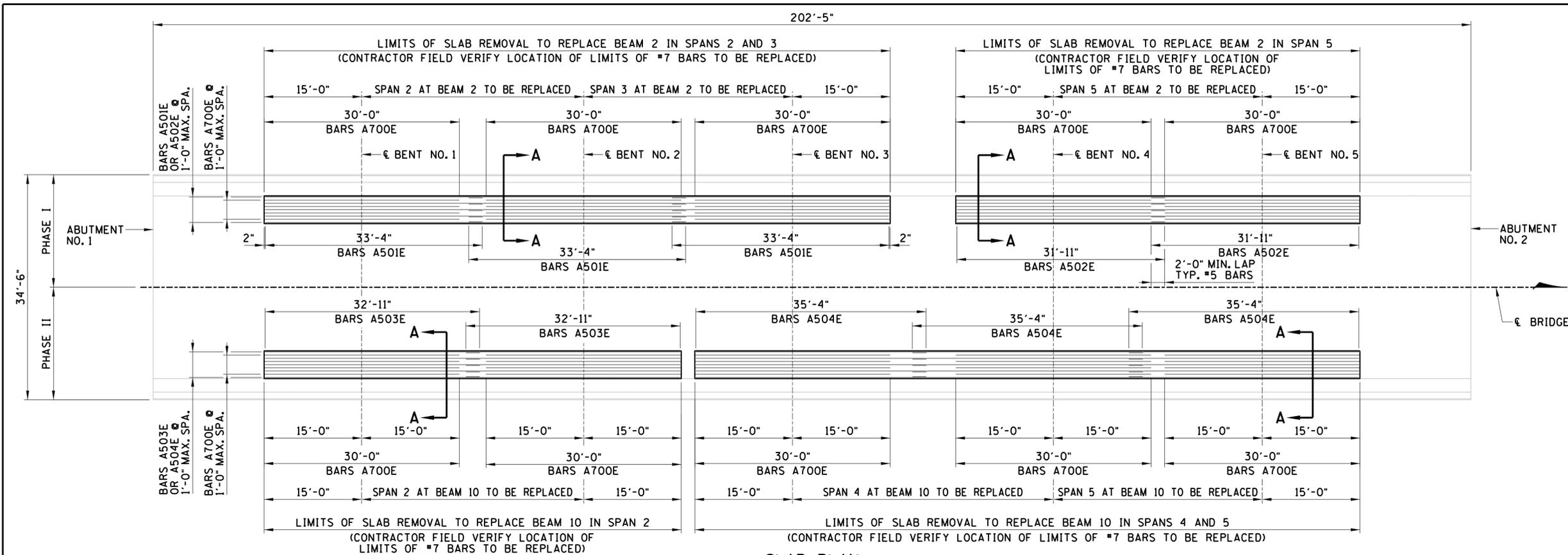
DESIGNED BY: DAVID THOMPSON  
DRAWN BY: ANGELA MOORE  
SUPERVISED BY: DARRELL JAMES  
CHECKED BY: JAMIE GILLESPIE

DATE: \_\_\_\_\_  
DATE: \_\_\_\_\_  
DATE: \_\_\_\_\_  
DATE: \_\_\_\_\_

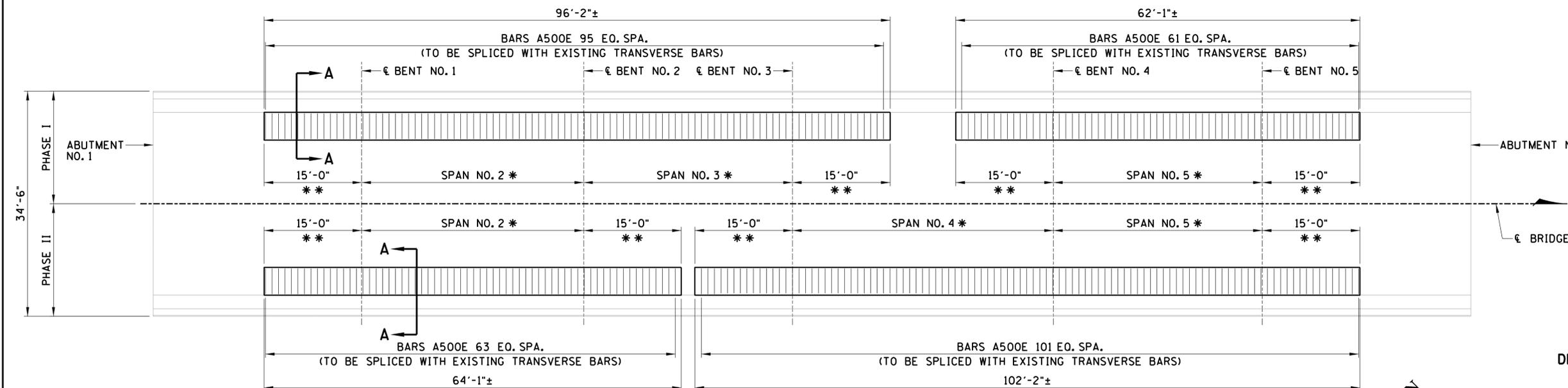








**SLAB PLAN**  
(SHOWING LONGITUDINAL REINFORCEMENT)  
(SEE BR-118-121 FOR SECTION "A-A")



**SLAB PLAN**  
(SHOWING TRANSVERSE REINFORCEMENT)  
(SEE BR-118-121 FOR SECTION "A-A")

- \* DENOTES: FOR REPLACEMENT OF BEAM 2 IN SPANS 2, 3 AND 5 AND BEAM 10 IN SPANS 2, 4 AND 5, EXISTING TRANSVERSE REINFORCEMENT SHALL BE BENT UPWARD TO FACILITATE REPLACEMENT OF BEAM AND SHALL BE BENT BACK TO ORIGINAL POSITION TO SPLICE WITH PROPOSED TRANSVERSE REINFORCING STEEL.
- \*\* DENOTES: EXISTING TRANSVERSE REINFORCEMENT SHALL REMAIN AND BE CLEANED PRIOR TO POURING NEW CONCRETE. NEW #5 LONGITUDINAL REINFORCING SHALL BE SPLICED WITH EXISTING #5 REINFORCING STEEL THAT WILL REMAIN. EXISTING #7 LONGITUDINAL REINFORCEMENT OVER BENTS SHALL BE REPLACED WITH NEW #7 REINFORCING STEEL FOR REPLACEMENT OF BEAM 2 IN SPANS 2, 3 AND 5 AND BEAM 10 IN SPANS 2, 4 AND 5.

CONST. NO.			
PROJECT NO.	YEAR	SHEET NO.	
81010-4215-04	2015		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
1	07-31-15	DWT	REVISED SLAB PLAN

**UNOFFICIAL SET**  
 NOT FOR BIDDING

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

SUPERSTRUCTURE DETAILS

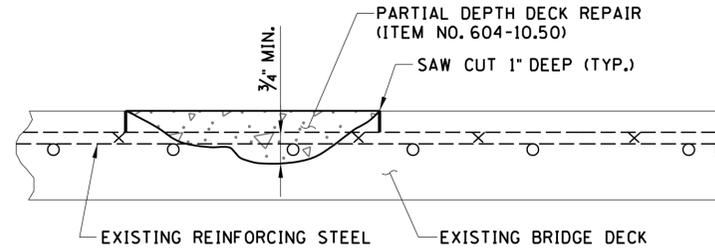
BRIDGE NO. 81-SR233-6.67  
STATE ROUTE 233 OVER WELLS CREEK  
STEWART COUNTY  
2015

DESIGNED BY DAVID THOMPSON  
DRAWN BY ANGELA MOORE  
SUPERVISED BY DARRELL JAMES  
CHECKED BY JAMIE GILLESPIE



**NOTES:**

- REMOVE CONCRETE IN ALL DELAMINATED AREAS TO A DEPTH OF 3/4" BELOW THE TOP BAR OF THE TOP MAT OF REINFORCING STEEL. ALL REINFORCING STEEL IN AREAS OF DECK REPAIR SHALL BE COMPLETELY CLEANED. AREAS OF CONCRETE REMOVAL SHALL BE DESIGNATED BY PERSONNEL FROM THE BRIDGE REPAIR OFFICE. INSPECTIONS, TO DETERMINE AREAS OF DECK REPAIR, WERE SCHEDULED WITH THE BRIDGE REPAIR OFFICE AND ARE SHOWN BELOW FOR REFERENCE. POWER DRIVEN HAND TOOLS USED FOR THE REMOVAL OF UNSOUND CONCRETE IN MAKING PARTIAL DEPTH REPAIRS ARE SUBJECT TO THE FOLLOWING RESTRICTIONS: 1) (PARTIAL DEPTH REPAIRS) PNEUMATIC HAMMERS HEAVIER THAN NOMINAL 60 POUND CLASS SHALL NOT BE USED. 2) (ALSO ALL DECK REPAIR OVER BEAMS WILL BE RESTRICTED TO 60 POUND PNEUMATIC HAMMERS. 3) CHIPPING HAMMERS OF THE 15 POUND CLASS SHALL BE USED TO REMOVE CONCRETE FROM BENEATH ANY REINFORCING STEEL.
- ITEM NO. 604-10.50 MAY BE INCREASED, DECREASED, OR ELIMINATED AS DIRECTED BY THE ENGINEER.
- DETAILS OF ANY TEMPORARY SUPPORT SYSTEM AND DESIGN CALCULATIONS SHALL BE SUBMITTED TO THE ENGINEER FOR REVIEW AND SHALL MEET THE FULL SATISFACTION OF THE ENGINEER BEFORE ANY PARTIAL DECK REPAIR IS BEGUN. COST TO BE INCLUDED IN ITEM NO. 602-10.05 BRACING REPAIRS, LUMP SUM.
- THE CONTRACTOR WILL PROVIDE ALL NECESSARY PROTECTIVE MEASURES TO SAFEGUARD ALL EXISTING UTILITIES FROM DAMAGE DURING CONSTRUCTION OF THIS PROJECT. IN THE EVENT THAT SPECIAL EQUIPMENT IS REQUIRED TO WORK OVER AND AROUND THE UTILITIES, THE CONTRACTOR WILL BE REQUIRED TO FURNISH SUCH EQUIPMENT. THE COST OF PROTECTING UTILITIES FROM DAMAGE AND FURNISHING SPECIAL EQUIPMENT WILL BE INCLUDED IN THE PRICE OF ITEMS BID ON.
- COST OF FRAMING, CONCRETE, ABRASIVE BLASTING, LABOR AND CONCRETE REMOVAL BY PNEUMATIC HAMMERS AND ANY MISCELLANEOUS MATERIALS TO COMPLETE THE PARTIAL DEPTH REPAIRS SHALL BE INCLUDED IN ITEM NUMBER 604-10.50.
- IF ANY EXISTING REINFORCING STEEL IS DAMAGED AFTER THE REMOVAL PROCESS, IT SHALL BE REPLACED AND INCLUDED IN ITEM NO. 604-10.50.

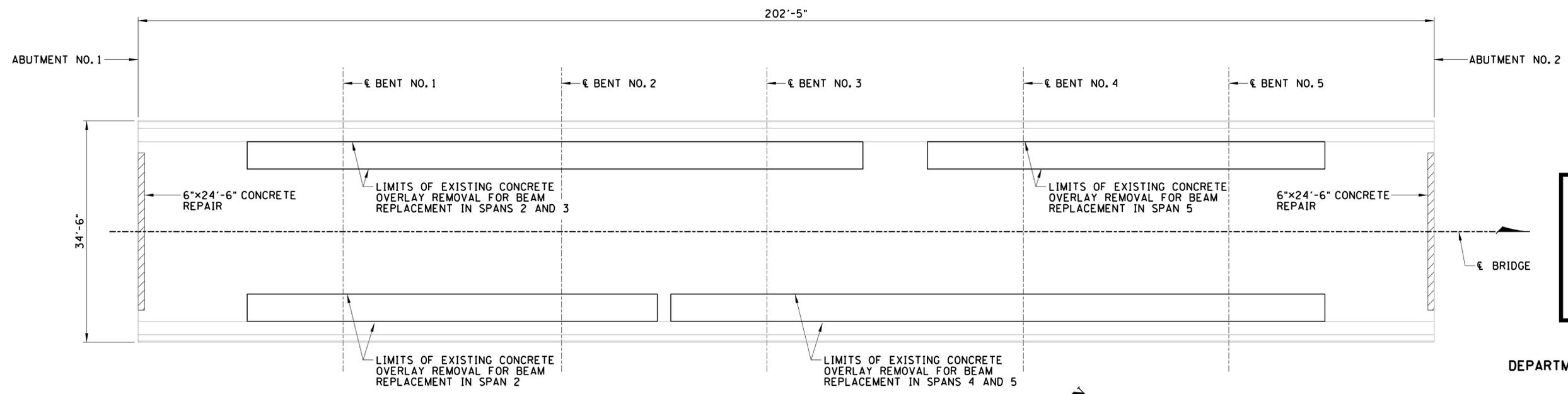


**DETAIL SHOWING PARTIAL DEPTH DECK REPAIR**  
(NOT TO SCALE)

- NOTE: REMOVE ALL DEBRIS AROUND ABUTMENTS AND BENTS. COST IS TO BE INCLUDED IN OTHER ITEMS.
- NOTE: ALL AREAS THAT CONTAIN CALCIUM DEPOSITS SHALL BE REMOVED, ANY CONCRETE THAT HAS DETERIORATED BEHIND THE DEPOSITS SHALL BE REPAIRED.

ESTIMATED QUANTITIES	
ITEM NO. 604-10.50	BRIDGE DECK REPAIRS (PARTIAL DEPTH OF SLAB) S.Y.
3	

CONST. NO.			
PROJECT NO.	YEAR	SHEET NO.	
81010-4215-04	2015		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
1	07-31-15	DWT	REVISED BRIDGE DECK PLAN



**BRIDGE DECK PLAN** ①

**UNOFFICIAL SET**  
NOT FOR BIDDING

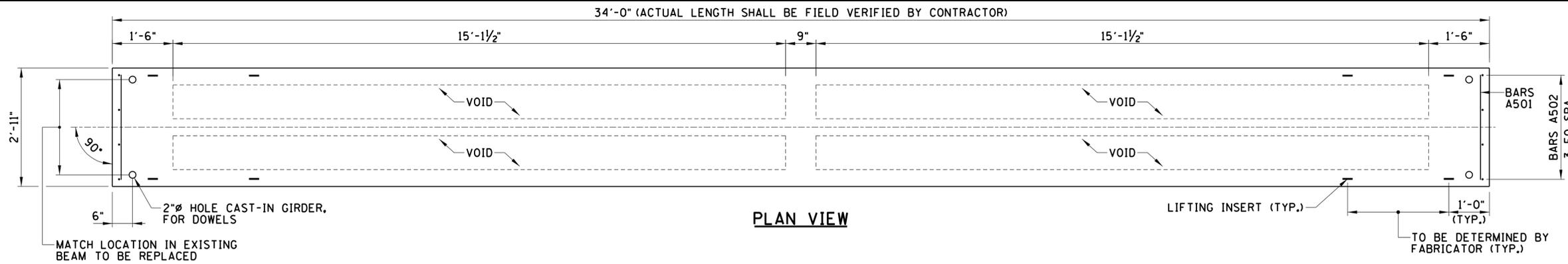
STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

DECK REPAIR DETAILS

BRIDGE NO. 81-SR233-6.67  
STATE ROUTE 233 OVER WELLS CREEK  
STEWART COUNTY  
2015

DESIGNED BY DAVID THOMPSON DATE \_\_\_\_\_  
DRAWN BY ANGELA MOORE DATE \_\_\_\_\_  
SUPERVISED BY DARRELL JAMES DATE \_\_\_\_\_  
CHECKED BY JAMIE GILLESPIE DATE \_\_\_\_\_

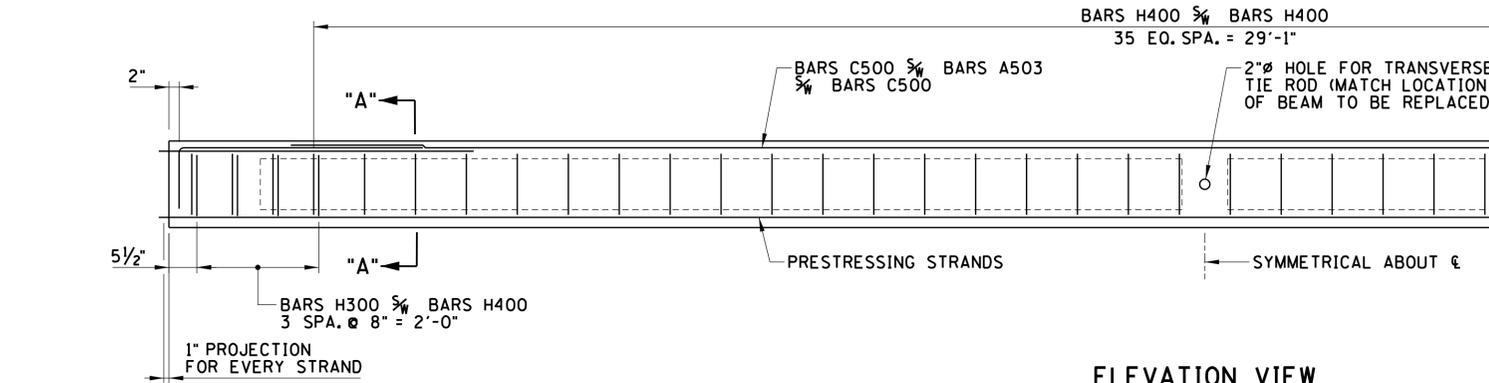
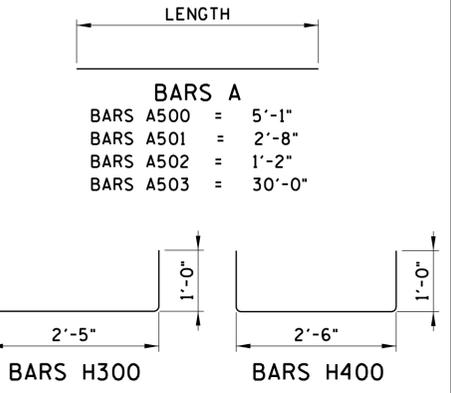




**PLAN VIEW**

CONST. NO.	PROJECT NO.		YEAR	SHEET NO.
	81010-4215-04		2015	

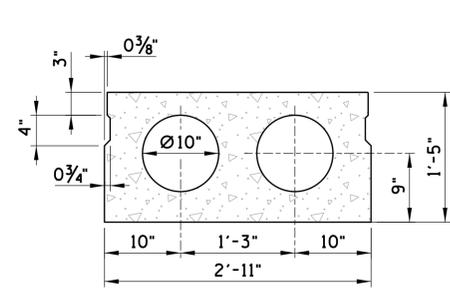
REVISIONS				
NO.	DATE	BY	BRIEF DESCRIPTION	



**ELEVATION VIEW**

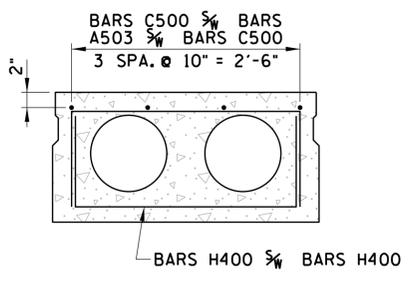
**NOTES:**

1. THE TOP OF ALL BEAMS ARE TO BE ROUGH FLOATED. AT APPROXIMATELY THE TIME OF INITIAL SET, THE TOP OF THE BEAMS SHALL ALSO BE SCRUBBED TRANSVERSELY WITH A COURSE WIRE BRUSH TO REMOVE ALL LAITANCE AND PRODUCE A ROUGH SURFACE.
2. MILD STEEL REINFORCING SHALL BE ASTM A615 GRADE 60.
3. ALL PRESTRESSING STRANDS SHALL BE 3/8" IN DIAMETER, ASTM GRADE 270K.
4. AN INITIAL FORCE OF 14,000 LBS SHALL BE APPLIED TO EACH STRAND IN ALL BEAMS.
5. AFTER THE BEAM IS REMOVED FROM THE PRESTRESSING BED, BARS PROJECTING FROM THE ENDS OF THE BEAM SHALL BE COLD BENT (DO NOT HEAT). THE MINIMUM DIAMETER OF THE BEND SHALL BE IN ACCORDANCE WITH STANDARD CRSI HOOK DETAILS.
6. THE PRESTRESSING STRANDS SHALL BE LEFT PROJECTING AS SHOWN IN THE BENDING DETAILS FROM THE ENDS OF THE BEAMS. THERE SHALL NOT BE ANY PROTECTIVE COATING PLACED ON THE ENDS OF THE BEAM OR ON THE PROJECTING STRANDS.
7. THE CONCRETE FOR THIS CONSTRUCTION SHALL BE OF SUCH PROPERTIES AS TO ATTAIN A COMPRESSIVE STRENGTH NOT LESS THAN 4,000 P.S.I. AT THE AGE OF 28 DAYS AND THE STRESS TRANSFER SHALL NOT BE MADE TO THE BRIDGE MEMBER UNTIL THE TEST SPECIMENS INDICATE THAT THE CONCRETE HAS REACHED A COMPRESSIVE STRENGTH OF AT LEAST 4,500 P.S.I.
8. 1" DIA. WEEP HOLES SHALL BE PROVIDED AT THE LOW POINT OF EACH CELL. VENT HOLES SHALL BE PROVIDED IN THE TOP OF EACH CELL DURING FABRICATION TO RELIEVE GAS PRESSURES THAT OCCUR DURING CURING. THE VENT HOLES SHALL BE PLUGGED AFTER CURING IS COMPLETE.
9. COST OF ELASTOMERIC BEARING PADS, INSERTS, REINFORCING STEEL, AND RUBBER BONDING CEMENT TO BE INCLUDED IN COST OF PRESTRESSED BEAM. NEW BEARING PAD SHALL MATCH DIMENSIONS OF EXISTING PAD (1/4" THICK).
10. THE SEQUENCE FOR TRANSFER OF STRESS OR THE CUTTING STRANDS SHALL BE IN ACCORDANCE WITH ARTICLE 615.14 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND SHALL BE SHOWN ON THE APPROVED SHOP DRAWINGS. AT NO TIME SHALL MORE THAN 1/6TH OF THE TOTAL PRESTRESSING FORCE BE ECCENTRIC ABOUT THE CENTERLINE OF THE BEAM.
11. BEAM FABRICATION: THE CONTRACTOR SHALL CHECK THE LOCATION OF ALL SUBSTRUCTURES AND VERIFY SPAN LENGTHS BEFORE FABRICATING BEAM.
12. SEE F-10-84 STANDARD PRESTRESSED CONCRETE BRIDGE PRETENSIONED CLEAR SPANS 14'-0" TO 80'-0", 28'-0" ROADWAY WITH SAFETY CURBS, 1957, AND F-10-85A DETAILS FOR REQUIRED PRESTRESSING PATTERNS, 1958 FOR ADDITIONAL INFORMATION.
13. SEE BR-118-115 FOR BEAM PLACEMENT.



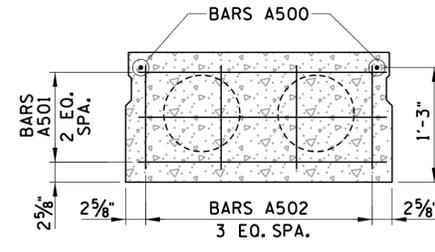
**SECTION "A-A"**

(SECTION SHOWING BEAM PROPERTIES)



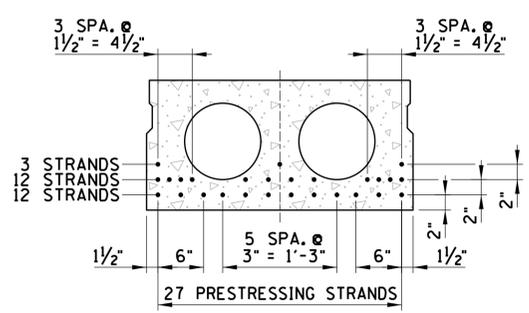
**SECTION "A-A"**

(SECTION SHOWING REINFORCING STEEL)



**END ELEVATION**

(SECTION END PROJECTING)



**SECTION "A-A"**

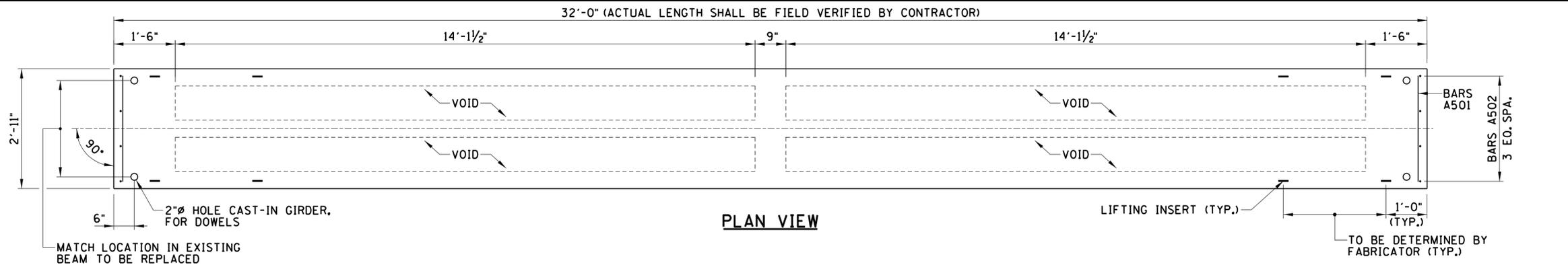
(SECTION SHOWING PRESTRESSING STRANDS)

DESIGNED BY DAVID THOMPSON DATE \_\_\_\_\_  
 DRAWN BY ANGELA MOORE DATE \_\_\_\_\_  
 SUPERVISED BY DARRELL JAMES DATE \_\_\_\_\_  
 CHECKED BY JAMIE GILLESPIE DATE \_\_\_\_\_

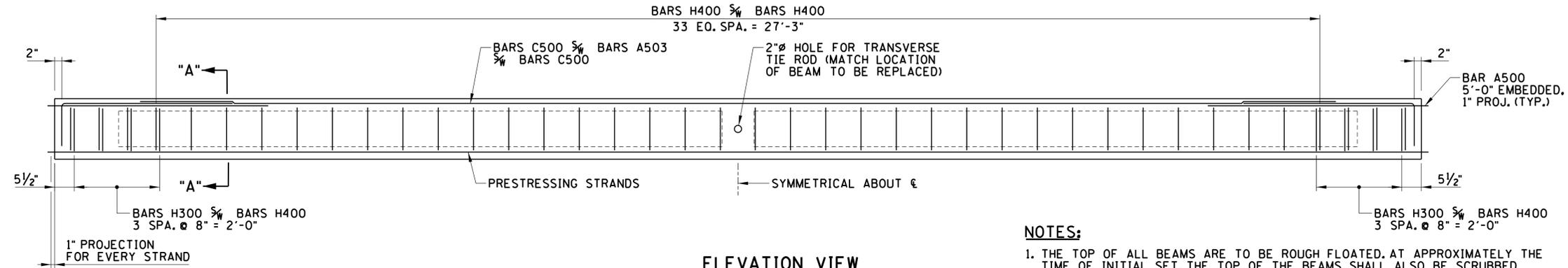
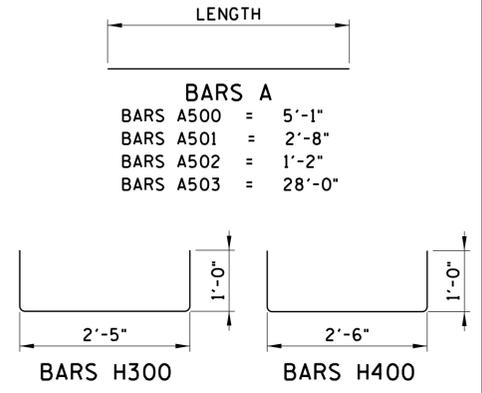


UNOFFICIAL SET  
NOT FOR BIDDING

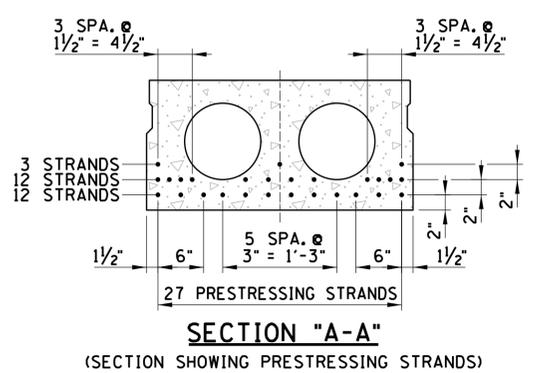
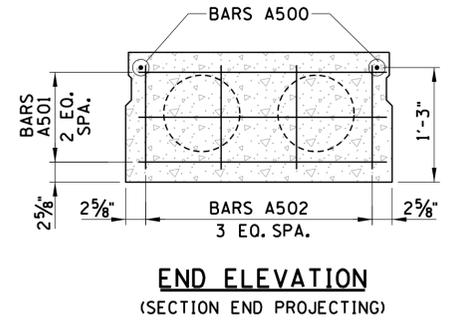
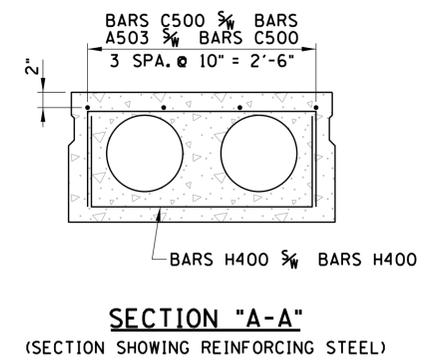
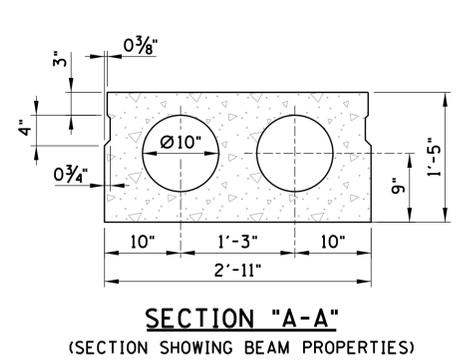
PRESTRESSED CONCRETE  
BOX BEAM DETAILS  
BRIDGE NO. 81-SR233-6.67  
STATE ROUTE 233 OVER WELLS CREEK  
STEWART COUNTY  
2015



CONST. NO.			
PROJECT NO.	YEAR	SHEET NO.	
81010-4215-04	2015		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
1	07-31-15	DWT	ADDED SHEET BR-118-124A



- NOTES:**
1. THE TOP OF ALL BEAMS ARE TO BE ROUGH FLOATED. AT APPROXIMATELY THE TIME OF INITIAL SET, THE TOP OF THE BEAMS SHALL ALSO BE SCRUBBED TRANSVERSELY WITH A COURSE WIRE BRUSH TO REMOVE ALL LAITANCE AND PRODUCE A ROUGH SURFACE.
  2. MILD STEEL REINFORCING SHALL BE ASTM A615 GRADE 60.
  3. ALL PRESTRESSING STRANDS SHALL BE 3/8" IN DIAMETER, ASTM GRADE 270K.
  4. AN INITIAL FORCE OF 14,000 LBS SHALL BE APPLIED TO EACH STRAND IN ALL BEAMS.
  5. AFTER THE BEAM IS REMOVED FROM THE PRESTRESSING BED, BARS PROJECTING FROM THE ENDS OF THE BEAM SHALL BE COLD BENT (DO NOT HEAT). THE MINIMUM DIAMETER OF THE BEND SHALL BE IN ACCORDANCE WITH STANDARD CRSI HOOK DETAILS.
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  11. BEAM FABRICATION: THE CONTRACTOR SHALL CHECK THE LOCATION OF ALL SUBSTRUCTURES AND VERIFY SPAN LENGTHS BEFORE FABRICATING BEAM.
  12. SEE F-10-84 STANDARD PRESTRESSED CONCRETE BRIDGE PRETENSIONED CLEAR SPANS 14'-0" TO 80'-0", 28'-0" ROADWAY WITH SAFETY CURBS, 1957, AND F-10-85A DETAILS FOR REQUIRED PRESTRESSING PATTERNS, 1958 FOR ADDITIONAL INFORMATION.
  13. SEE BR-118-115 FOR BEAM PLACEMENT.



**UNOFFICIAL SET**

NOT FOR BIDDING

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

PRESTRESSED CONCRETE  
BOX BEAM DETAILS

BRIDGE NO. 81-SR233-6.67  
STATE ROUTE 233 OVER WELLS CREEK  
STEWART COUNTY  
2015

DESIGNED BY DAVID THOMPSON DATE \_\_\_\_\_  
DRAWN BY ANGELA MOORE DATE \_\_\_\_\_  
SUPERVISED BY DARRELL JAMES DATE \_\_\_\_\_  
CHECKED BY JAMIE GILLESPIE DATE \_\_\_\_\_

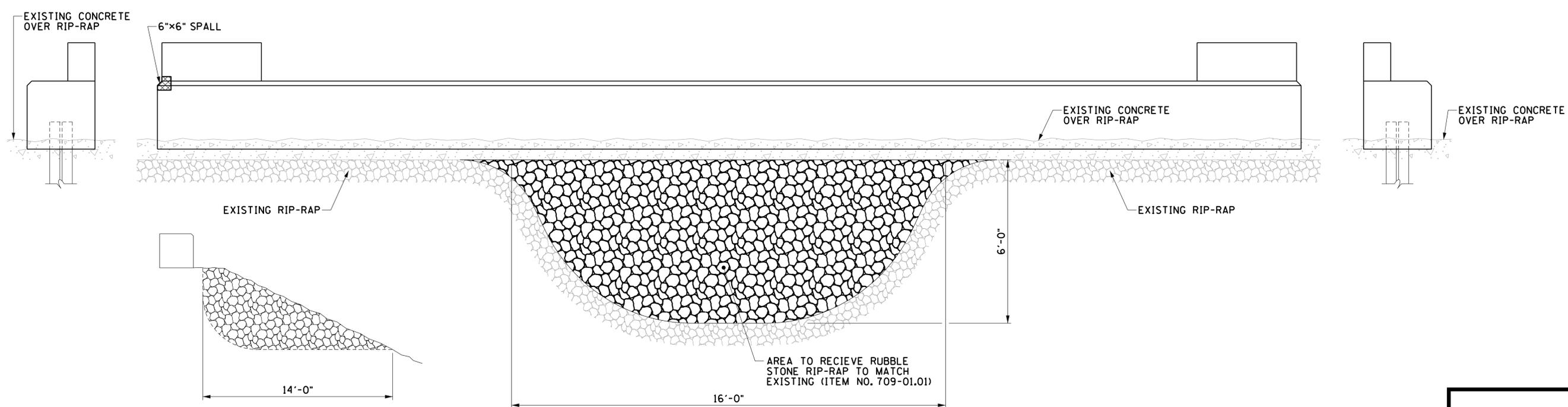
**JA**  
JAMES ASSOCIATES, INC.



CONST. NO.			
PROJECT NO.	YEAR	SHEET NO.	
81010-4215-04	2015		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

42'-2" (OUT TO OUT) (TYP.)  
34'-6"

**ABUTMENT NO. 1**

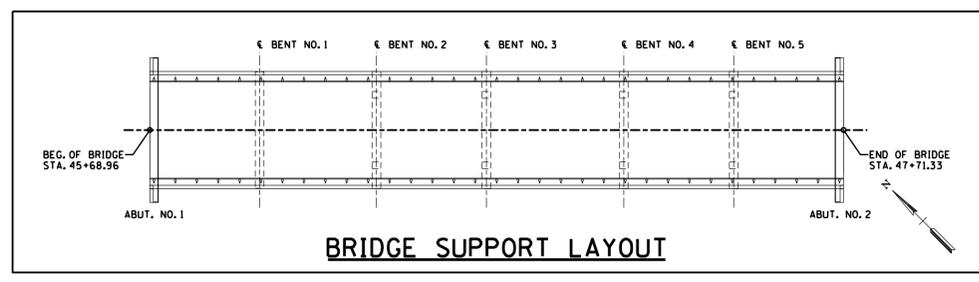


**ABUTMENT NO. 2**

LEGEND	
SPALLED SURFACES	
APPROXIMATE EXISTING GROUND LINE	

NOTE: INFORMATION AND DIMENSIONS FOR CONCRETE REPAIRS ARE PROVIDED FOR ESTIMATING ONLY. TDOT ENGINEER SHALL DELINEATE ACTUAL AREAS TO BE REPAIRED.

- NOTES:**
- (1) REMOVE ALL DEBRIS AROUND ABUTMENTS. COST IS TO BE INCLUDED IN OTHER ITEMS.
  - (2) ALL AREAS THAT CONTAIN CALCIUM DEPOSITS SHALL BE REMOVED. ANY CONCRETE THAT HAS DETERIORATED BEHIND THE DEPOSITS SHALL BE REPAIRED.
  - (3) SEE BR-118-129 "CONCRETE REPAIR DETAILS" FOR ESTIMATED REPAIR QUANTITIES. ALL REPAIR AREAS SHALL BE DESIGNATED BY THE BRIDGE REPAIR OFFICE AS STATED IN NOTE NO. 1 ON BR-118-129 AND SHALL BE INCLUDED IN ITEM NO. 604-10.05 OR ITEM NO. 604-10.54.
- DESIGNED BY DAVID THOMPSON DATE \_\_\_\_\_  
DRAWN BY ANGELA MOORE DATE \_\_\_\_\_  
SUPERVISED BY DARRELL JAMES DATE \_\_\_\_\_  
CHECKED BY JAMIE GILLESPIE DATE \_\_\_\_\_

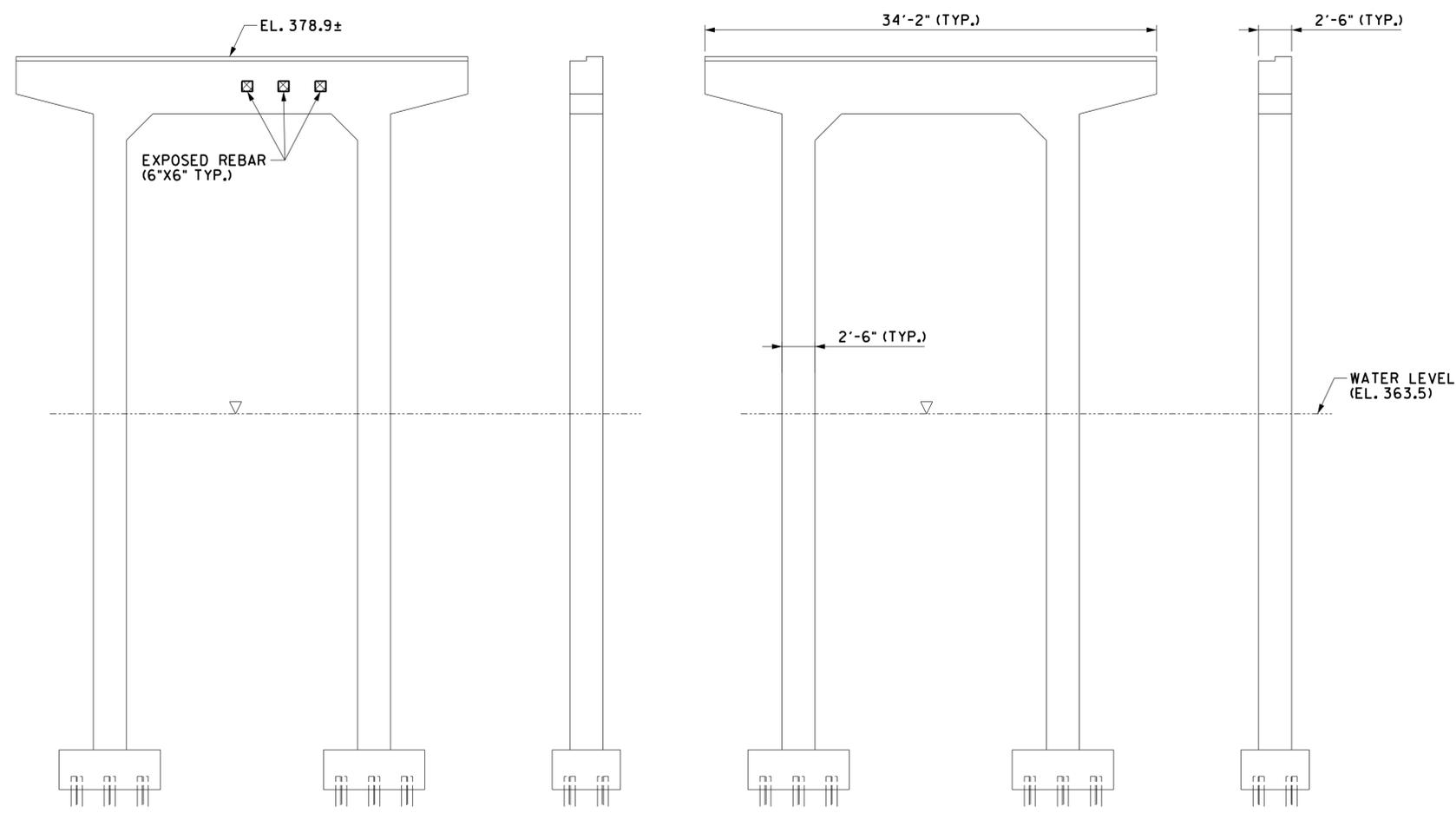


**UNOFFICIAL SET**  
NOT FOR BIDDING

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
CONCRETE REPAIRS  
(ABUTMENTS)  
BRIDGE NO. 81-SR233-6.67  
STATE ROUTE 233 OVER WELLS CREEK  
STEWART COUNTY  
2015



CONST. NO.			
PROJECT NO.	YEAR	SHEET NO.	
81010-4215-04	2015		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



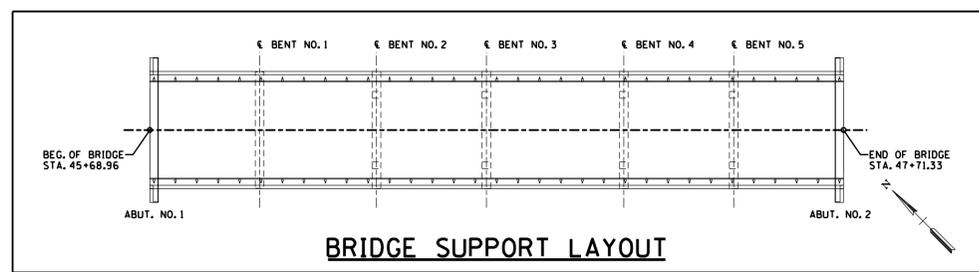
LOOKING NORTHWEST      LOOKING NORTHEAST      LOOKING SOUTHEAST      LOOKING SOUTHWEST

**BENT NO. 3**

LEGEND	
SPALLED SURFACES	
WATER LEVEL	
APPROXIMATE EXISTING GROUND LINE	

NOTE: INFORMATION AND DIMENSIONS FOR CONCRETE REPAIRS ARE PROVIDED FOR ESTIMATING ONLY. TDOT ENGINEER SHALL DELINEATE ACTUAL AREAS TO BE REPAIRED.

- NOTES:**
- (1) REMOVE ALL DEBRIS AROUND BENTS. COST IS TO BE INCLUDED IN OTHER ITEMS.
  - (2) ALL AREAS THAT CONTAIN CALCIUM DEPOSITS SHALL BE REMOVED. ANY CONCRETE THAT HAS DETERIORATED BEHIND THE DEPOSITS SHALL BE REPAIRED.
  - (3) SEE BR-118-129 "CONCRETE REPAIR DETAILS" FOR ESTIMATED REPAIR QUANTITIES. ALL REPAIR AREAS SHALL BE DESIGNATED BY THE BRIDGE REPAIR OFFICE AS STATED IN NOTE NO. 1 ON BR-118-129 AND SHALL BE INCLUDED IN ITEM NO. 604-10.05 OR ITEM NO. 604-10.54.



DESIGNED BY DAVID THOMPSON      DATE \_\_\_\_\_  
 DRAWN BY ANGELA MOORE      DATE \_\_\_\_\_  
 SUPERVISED BY DARRELL JAMES      DATE \_\_\_\_\_  
 CHECKED BY JAMIE GILLESPIE      DATE \_\_\_\_\_



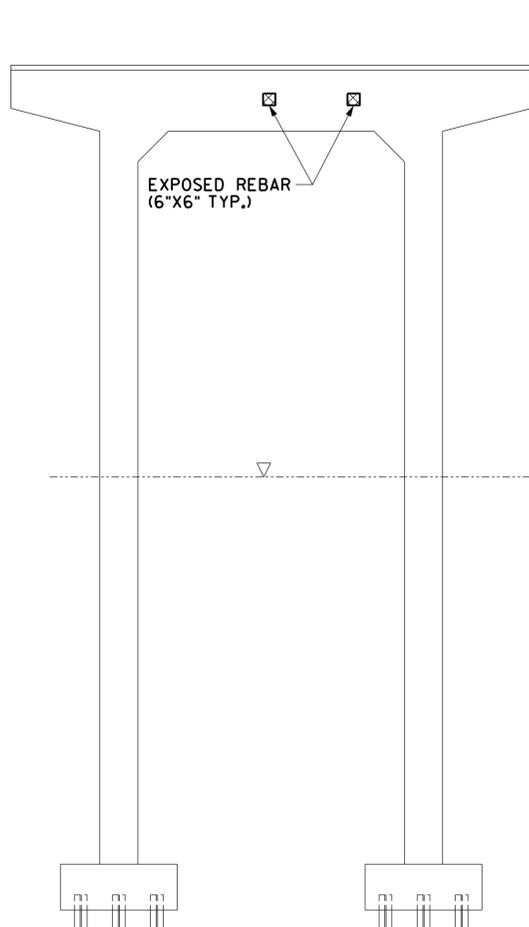
**UNOFFICIAL SET**

NOT FOR BIDDING

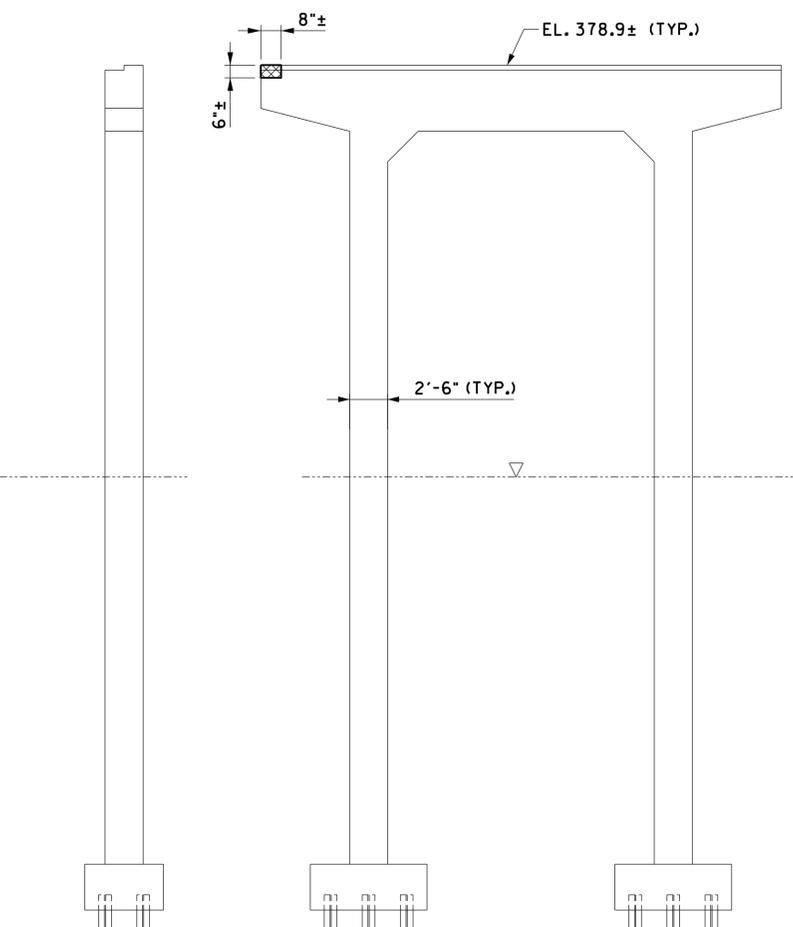
STATE OF TENNESSEE  
 DEPARTMENT OF TRANSPORTATION

CONCRETE REPAIRS  
 (BENT NO. 3)

BRIDGE NO. 81-SR233-6.67  
 STATE ROUTE 233 OVER WELLS CREEK  
 STEWART COUNTY  
 2015

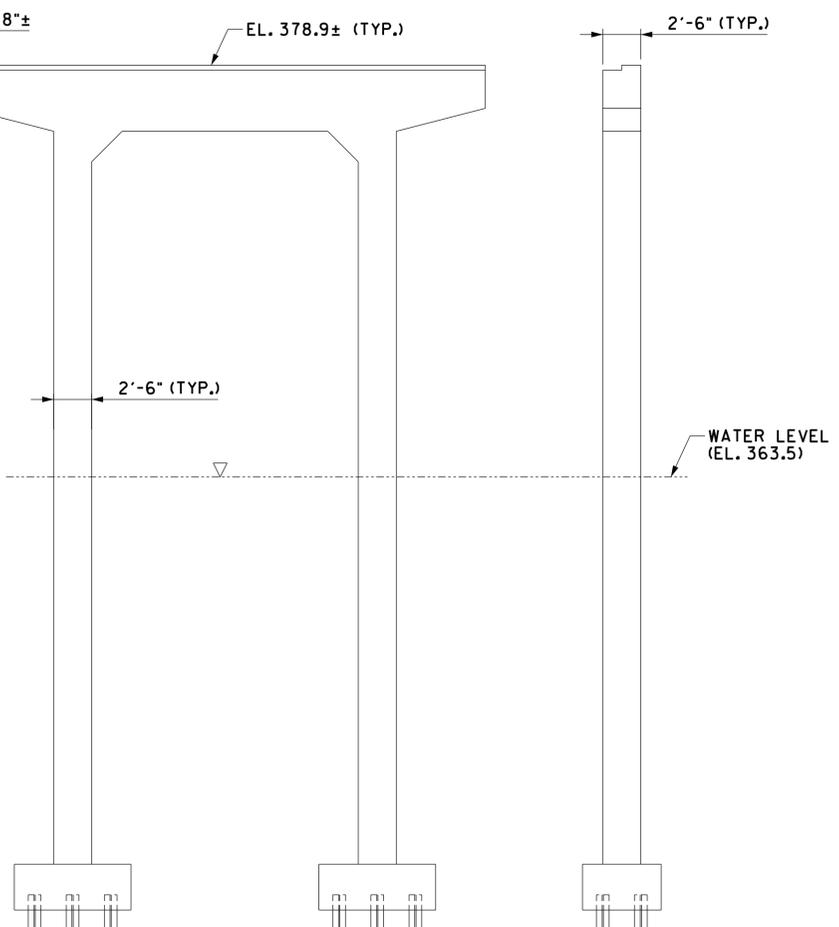


LOOKING NORTHWEST

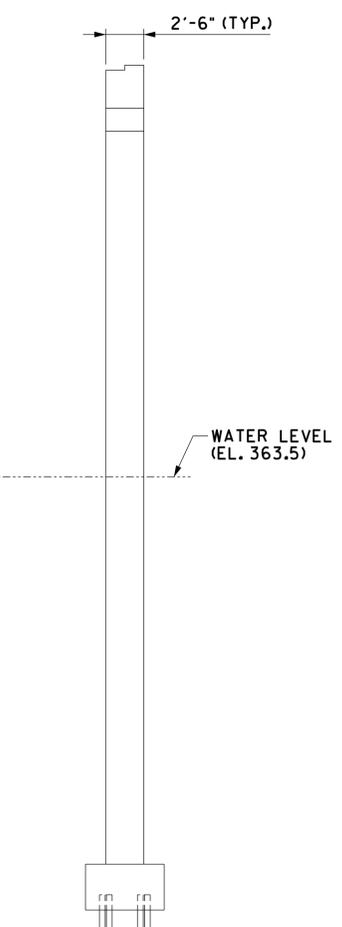


LOOKING NORTHEAST

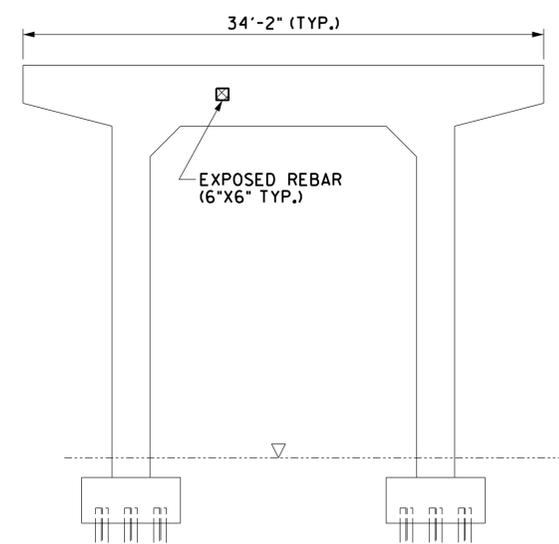
**BENT NO. 4**



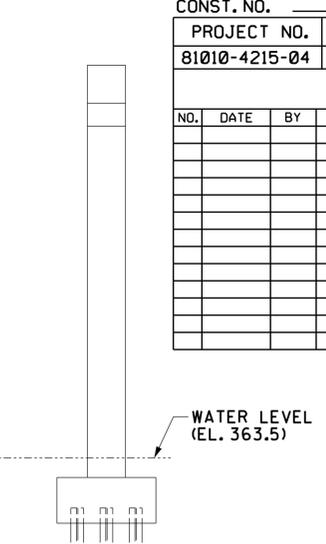
LOOKING SOUTHEAST



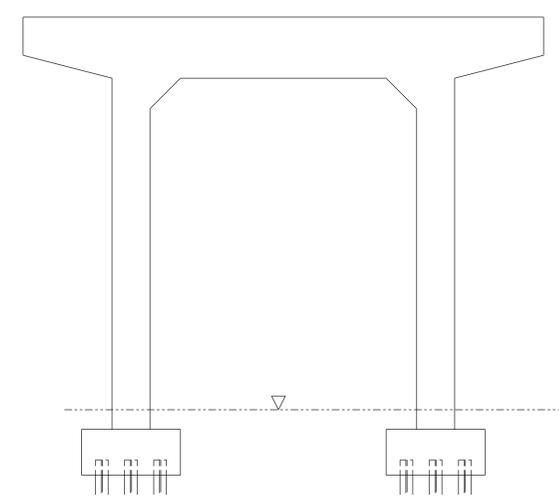
LOOKING SOUTHWEST



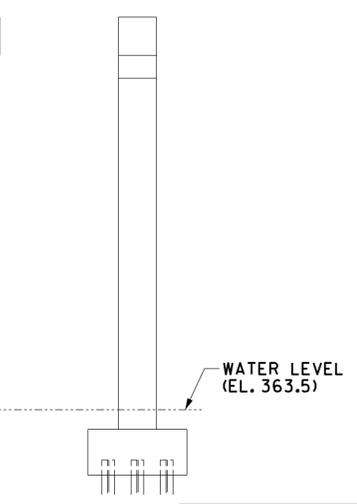
LOOKING NORTHWEST



LOOKING NORTHEAST



LOOKING SOUTHEAST



LOOKING SOUTHWEST

**BENT NO. 5**

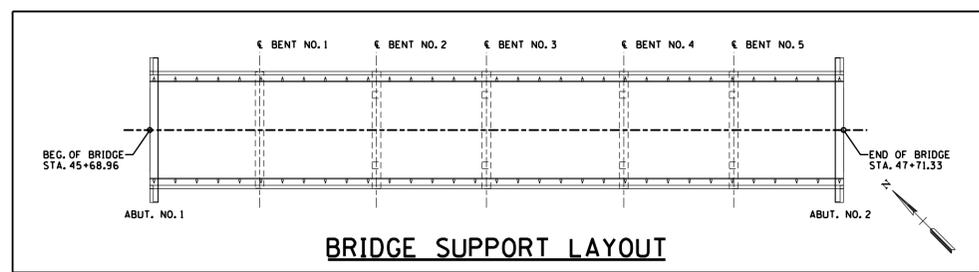
CONST. NO.			
PROJECT NO.	YEAR	SHEET NO.	
81010-4215-04	2015		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

LEGEND	
SPALLED SURFACES	
WATER LEVEL	

NOTE: INFORMATION AND DIMENSIONS FOR CONCRETE REPAIRS ARE PROVIDED FOR ESTIMATING ONLY. TDOT ENGINEER SHALL DELINEATE ACTUAL AREAS TO BE REPAIRED.

- NOTES:**
- (1) REMOVE ALL DEBRIS AROUND BENTS. COST IS TO BE INCLUDED IN OTHER ITEMS.
  - (2) ALL AREAS THAT CONTAIN CALCIUM DEPOSITS SHALL BE REMOVED. ANY CONCRETE THAT HAS DETERIORATED BEHIND THE DEPOSITS SHALL BE REPAIRED.
  - (3) SEE BR-118-129 "CONCRETE REPAIR DETAILS" FOR ESTIMATED REPAIR QUANTITIES. ALL REPAIR AREAS SHALL BE DESIGNATED BY THE BRIDGE REPAIR OFFICE AS STATED IN NOTE NO. 1 ON BR-118-129 AND SHALL BE INCLUDED IN ITEM NO. 604-10.05 OR ITEM NO. 604-10.54.

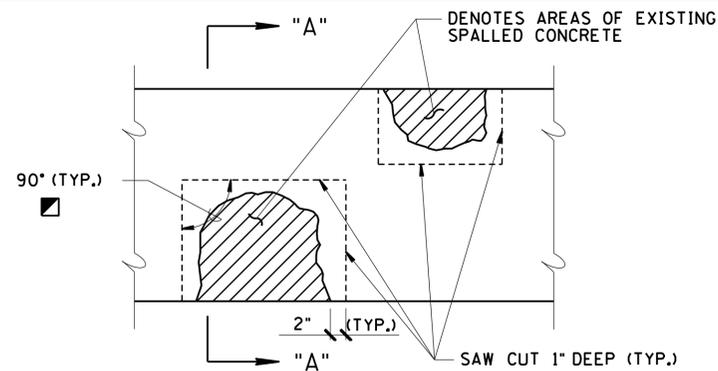
DESIGNED BY DAVID THOMPSON DATE \_\_\_\_\_  
 DRAWN BY ANGELA MOORE DATE \_\_\_\_\_  
 SUPERVISED BY DARRELL JAMES DATE \_\_\_\_\_  
 CHECKED BY JAMIE GILLESPIE DATE \_\_\_\_\_



**BRIDGE SUPPORT LAYOUT**

**UNOFFICIAL SET**  
 NOT FOR BIDDING

STATE OF TENNESSEE  
 DEPARTMENT OF TRANSPORTATION  
 CONCRETE REPAIRS  
 (BENT NOS. 4 AND 5)  
 BRIDGE NO. 81-SR233-6.67  
 STATE ROUTE 233 OVER WELLS CREEK  
 STEWART COUNTY  
 2015

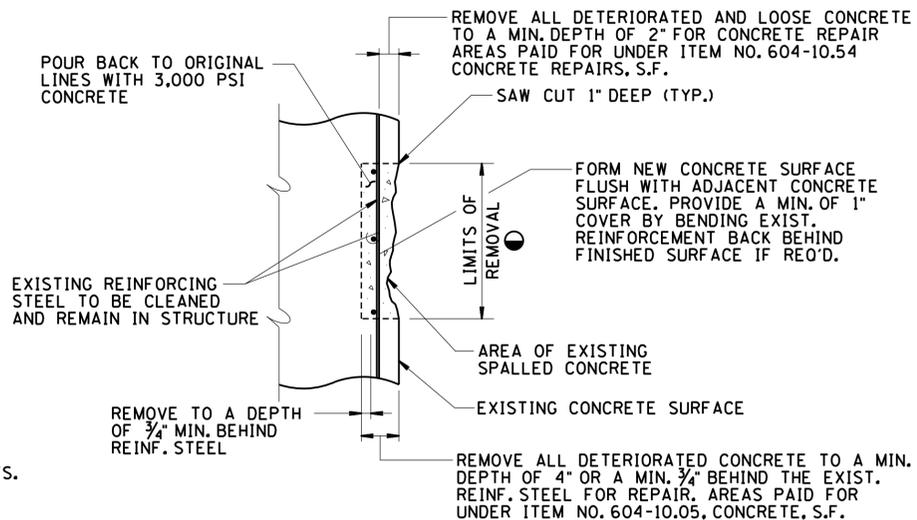


**DETAIL SHOWING AREAS OF EXISTING SPALLED CONCRETE SURFACES TO BE REMOVED AND REPAIRED**

- DENOTES: LIMITS AND LOCATION OF REPAIRS DESIGNATED ON THE CONCRETE REPAIR SHEETS.
- DENOTES: SAW CUT EXISTING CONCRETE SURFACES SO AS TO OBTAIN SQUARED CORNERS.

NOTE: THE COST OF REMOVING PORTIONS OF THE EXISTING CONCRETE WITHIN THE LIMITS SHOWN, SAW CUTTING, COMPLETELY CLEANING EXISTING REINFORCING STEEL, CONCRETE, FORMING, LABOR AND ALL MISCELLANEOUS MATERIAL, INCLUDING REINFORCING STEEL, NECESSARY TO COMPLETE THE REPAIRS AS SHOWN SHALL BE INCLUDED IN PRICE BID FOR ITEM NO. 604-10.54, CONCRETE REPAIRS, S.F.

1. THE ENGINEER SHALL DESIGNATE ALL CONCRETE REPAIR AREAS IN THE FIELD. QUANTITIES GIVEN ARE APPROXIMATE. ITEM NO. 604-10.05 AND ITEM NO. 604-10.54 MAY BE INCREASED, DECREASED, OR ELIMINATED BY THE ENGINEER.
2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE STABILITY OF THE STRUCTURE DURING CONSTRUCTION. DESIGN CALCULATION AND DETAILS OF TEMPORARY SUPPORT SYSTEM OR FALSEWORK REQUIRED SHALL BE SUBMITTED TO THE ENGINEER FOR REVIEW AND SHALL MEET WITH THE FULL SATISFACTION OF THE ENGINEER BEFORE ANY DEMOLITION IS BEGUN. COST OF STABILIZING THE STRUCTURE SHALL BE INCLUDED IN ITEM NO. 602-10.05, BRACING REPAIRS, L.S..
3. THE ENGINEER SHALL HAVE THE OPTION OF DESIGNATING A SPALL AREA TO BE REPAIRED UNDER ITEM NO. 604-10.05 OR ITEM NO. 604-10.54. PATCHING MATERIAL FOR ITEM NO. 604-10.05 SHALL BE 3,000 PSI CONCRETE. PATCHING MATERIAL FOR ITEM NO. 604-10.54 SHALL BE A POLYMER MODIFIED CEMENTITIOUS STRUCTURAL PATCHING MATERIAL. SEE QUALIFIED PRODUCTS LIST 13, SECTION B.6.
4. EXTREME CARE SHALL BE TAKEN WHEN REMOVING THE EXISTING SPALLED CONCRETE SO AS NOT TO DAMAGE THE EXISTING REINFORCING STEEL. ALL EXPOSED REINFORCING STEEL SHALL RECEIVE A COMPLETE CLEANING TO REMOVE ALL RUST. ALL EXISTING REINFORCEMENT SHALL REMAIN IN PLACE. ALL WORK MUST MEET WITH THE FULL APPROVAL OF THE ENGINEER.
5. POWER DRIVEN HAND TOOLS USED FOR REMOVAL OF UNSOUND CONCRETE ARE SUBJECT TO THE FOLLOWING RESTRICTIONS:
  - A. PNEUMATIC HAMMERS HEAVIER THAN 35 LB. CLASS SHALL NOT BE USED.
  - B. CHIPPING HAMMERS OF THE 15 LB. CLASS SHALL BE USED TO REMOVE CONCRETE FROM BEHIND REINFORCING STEEL AND BEAM END REPAIRS.

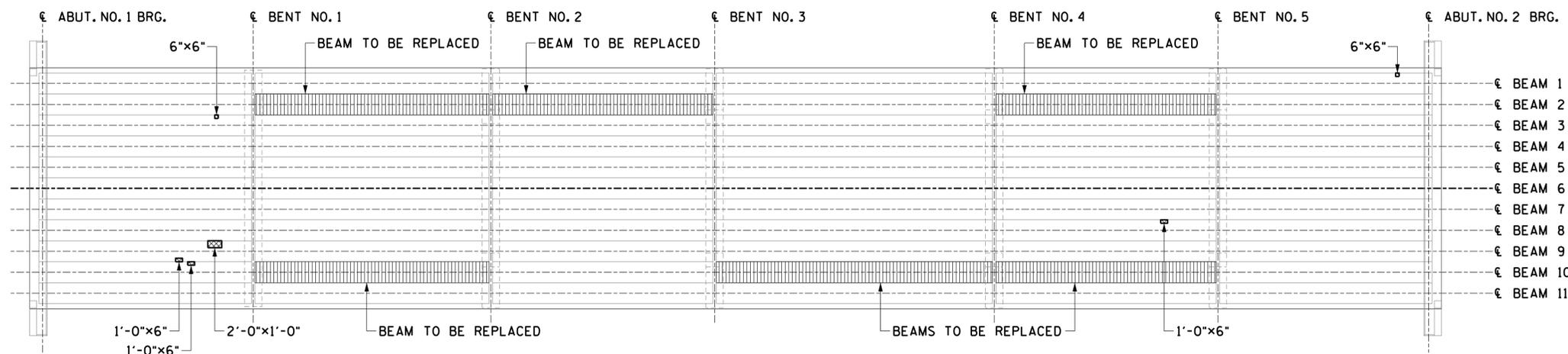


**SECTION "A-A"**

BRIDGE NO. 81-SR233-6.67	
ESTIMATED QUANTITIES	
ABUTMENT NO. 1	
ITEM NO. 604-10.05 CONCRETE S.F.	ITEM NO. 604-10.54 CONCRETE REPAIRS S.F.
0	0
BENT NO. 1	
ITEM NO. 604-10.05 CONCRETE S.F.	ITEM NO. 604-10.54 CONCRETE REPAIRS S.F.
0	0
BENT NO. 2	
ITEM NO. 604-10.05 CONCRETE S.F.	ITEM NO. 604-10.54 CONCRETE REPAIRS S.F.
5	5

BRIDGE NO. 81-SR233-6.67	
ESTIMATED QUANTITIES	
BENT NO. 3	
ITEM NO. 604-10.05 CONCRETE S.F.	ITEM NO. 604-10.54 CONCRETE REPAIRS S.F.
0	1
BENT NO. 4	
ITEM NO. 604-10.05 CONCRETE S.F.	ITEM NO. 604-10.54 CONCRETE REPAIRS S.F.
0	1
BENT NO. 5	
ITEM NO. 604-10.05 CONCRETE S.F.	ITEM NO. 604-10.54 CONCRETE REPAIRS S.F.
0	1
ABUTMENT NO. 2	
ITEM NO. 604-10.05 CONCRETE S.F.	ITEM NO. 604-10.54 CONCRETE REPAIRS S.F.
0	1
CONCRETE BEAM REPAIRS	
ITEM NO. 604-10.05 CONCRETE S.F.	ITEM NO. 604-10.54 CONCRETE REPAIRS S.F.
1	2
CONCRETE RAIL AND CURB REPAIRS	
ITEM NO. 604-10.05 CONCRETE S.F.	ITEM NO. 604-10.54 CONCRETE REPAIRS S.F.
5	10
TOTAL FOR ITEM NO. 604-10.05 CONCRETE S.F.	TOTAL FOR ITEM NO. 604-10.54 CONCRETE REPAIRS S.F.
11	21

CONST. NO.			
PROJECT NO.	YEAR	SHEET NO.	
81010-4215-04	2015		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
1	07-31-15	DWT	REVISED FRAMING PLAN, ADDED ESTIMATED QUANTITIES FOR CONCRETE RAIL AND CURB REPAIRS, REVISED TOTAL QUANTITY FOR ITEM NOS. 604-10.05 AND 604-10.54



**FRAMING PLAN - CONCRETE BEAM REPAIRS**  
(VERTICAL OVERHEAD CONCRETE REPAIR - VIEW FROM BELOW BEAMS)

DESIGNED BY DAVID THOMPSON  
 DRAWN BY ANGELA MOORE  
 SUPERVISED BY DARRELL JAMES  
 CHECKED BY JAMIE GILLESPIE



**UNOFFICIAL SET**  
NOT FOR BIDDING

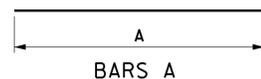
STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

CONCRETE REPAIR DETAILS

BRIDGE NO. 81-SR233-6.67  
STATE ROUTE 233 OVER WELLS CREEK  
STEWART COUNTY  
2015

PHASE I 								
SUPERSTRUCTURE - EPOXY								
BAR	LOCATION	SIZE	NO. REQ'D.	BENDING DIMENSIONS				LENGTH
				A	B	C	D	
A500E	SLAB	5	158	3'-11"				3'-11"
A501E	SLAB	5	15	33'-4"				33'-4"
A502E	SLAB	5	10	31'-11"				31'-11"
A700E	SLAB	7	20	30'-0"				30'-0"

PHASE II 								
SUPERSTRUCTURE - EPOXY								
BAR	LOCATION	SIZE	NO. REQ'D.	BENDING DIMENSIONS				LENGTH
				A	B	C	D	
A500E	SLAB	5	166	3'-11"				3'-11"
A503E	SLAB	5	10	32'-11"				32'-11"
A504E	SLAB	5	15	35'-4"				35'-4"
A700E	SLAB	7	20	30'-0"				30'-0"



CONST. NO.			
PROJECT NO.	YEAR	SHEET NO.	
81010-4215-04	2015		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
1	07-31-15	DWT	ELIMINATED BARS B570E, CD500E AND L500E, REVISED THE QTY FOR BARS A500E, A501E, A502E AND A700E, ADDED BARS A503E AND A504E

UNOFFICIAL SET  
NOT FOR BIDDING

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

BILL OF STEEL

BRIDGE NO. 81-SR233-6.67  
STATE ROUTE 233 OVER WELLS CREEK  
STEWART COUNTY  
2015

DESIGNED BY DAVID THOMPSON DATE \_\_\_\_\_  
DRAWN BY ANGELA MOORE DATE \_\_\_\_\_  
SUPERVISED BY DARRELL JAMES DATE \_\_\_\_\_  
CHECKED BY JAMIE GILLESPIE DATE \_\_\_\_\_

