

Index Of Sheets

SEE SHEET NO. 1A FOR INDEX

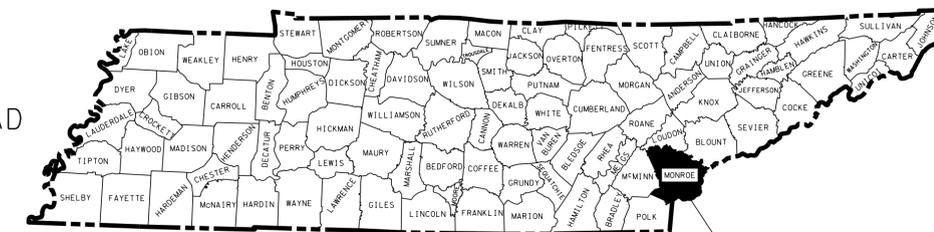
STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
BUREAU OF ENGINEERING  
**MONROE COUNTY**

TENN.	YEAR	SHEET NO.
	2015	1
FED. AID PROJ. NO.	PHSIP-68(41)	
STATE PROJ. NO.	62069-3228-94	
S.R. 68	MONROE CO.	

S.R. 68: INTERSECTIONS AT HAPPY HOLLOW ROAD AND ELEAZAR ROAD

**PAVING AND STRIPING**

STATE HIGHWAY NO. 68 F.A.H.S. NO. N/A



MONROE CO.  
S.R. 68

**PROJECT LOCATION**  
L.M. 16.91 AT HAPPY HOLLOW RD.

**PROJECT LOCATION**  
L.M. 20.01 AT ELEAZAR RD.



SCALE: 1" = 1 MILE



**SPECIAL NOTES**

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED JANUARY 1, 2015 AND ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS AND IN THE PROPOSAL CONTRACT.

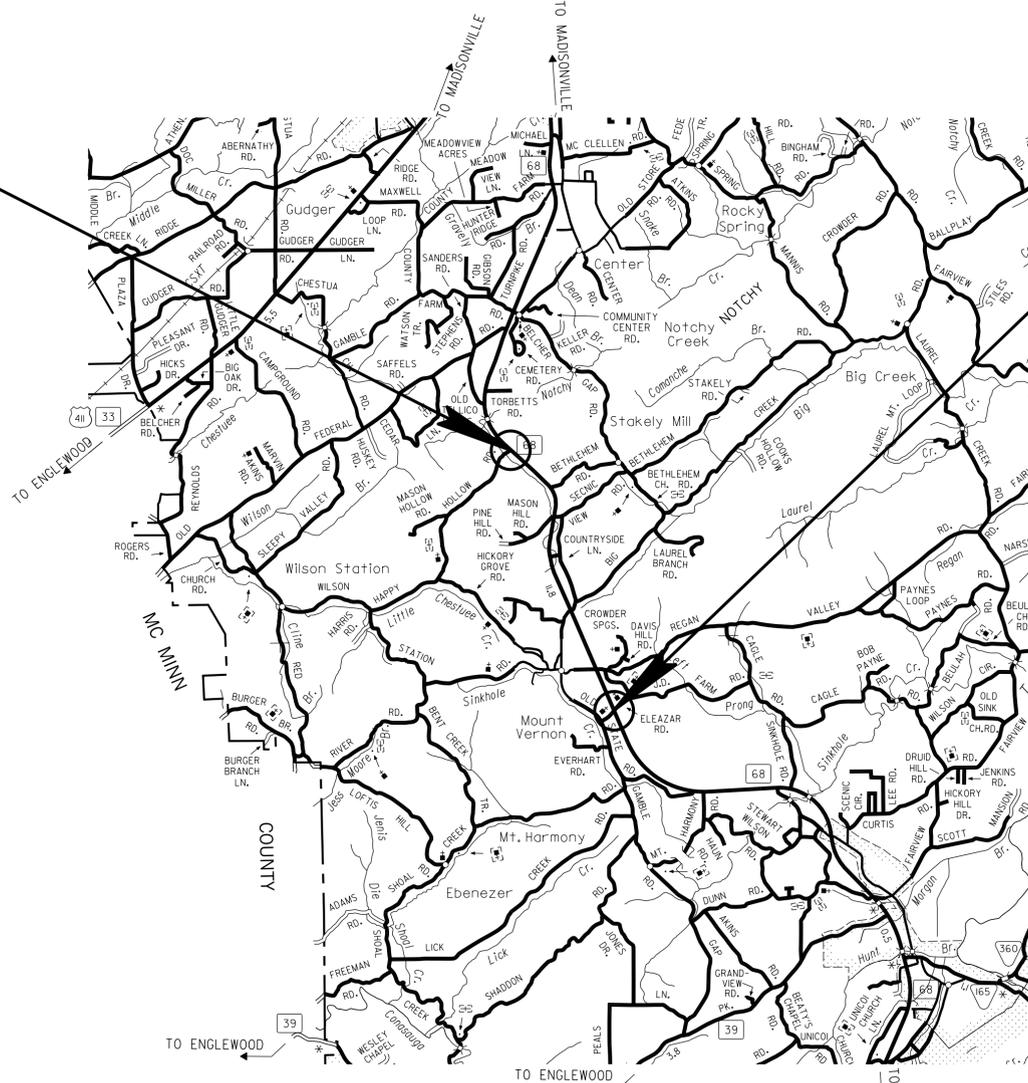
TDOT TRANS. PROJ. SP. SV. 2 MAYSOON HADDAD

DESIGNER ED. HAMMETT

CHECKED BY: PAUL T. BEEBE

P.E. NO. 62069-1228-94 (DESIGN)

PIN NO. 120058.00



**HAPPY HOLLOW ROAD (L.M. 16.91)**  
PROJECT LENGTH **0.18 MILES**

**ELEAZAR ROAD (L.M. 20.01)**  
PROJECT LENGTH **0.22 MILES**

**NO EXCLUSIONS**  
**NO EQUATIONS**

**NO ROW ACQUISITION AND**  
**NO UTILITY ADJUSTMENTS**

**PROJECT OF LIMITED SCOPE**

**UNOFFICIAL SET**  
**NOT FOR BIDDING**

SEALED BY

S.R. 68 AT HAPPY HOLLOW RD. (L.M. 16.91)

TRAFFIC DATA	
ADT (2015)	10040
ADT (2035)	11030
DHV (2035)	1209
D	60 - 40
T (ADT)	4 %
T (DHV)	3 %
V (POSTED SPEED)	55 MPH

S.R. 68 AT ELEAZAR RD. (L.M. 20.01)

TRAFFIC DATA	
ADT (2015)	6140
ADT (2035)	6770
DHV (2035)	813
D	65 - 35
T (ADT)	6 %
T (DHV)	4 %
V (POSTED SPEED)	55 MPH

APPROVED: Paul D. Degges  
PAUL D. DEGGES, CHIEF ENGINEER

DATE: \_\_\_\_\_

APPROVED: [Signature]  
JOHN SCHROER, COMMISSIONER

U.S. DEPARTMENT OF TRANSPORTATION  
FEDERAL HIGHWAY ADMINISTRATION

APPROVED: \_\_\_\_\_  
DIVISION ADMINISTRATOR

DATE: \_\_\_\_\_

3-JAN-2015 15:18 \\J01WF01\dot.state.tn.us\GIS\hard\SURVDES\DESIGN\PROJECTS\MRSR68\001.sht

TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	2015	PHS1P-68(41)	1 A

S.R. 68 MONROE CO.  
62069-3228-94 (CONST.)

## INDEX

SHEET NAME	SHEET NO.
TITLE SHEET .....	1
INDEX AND STANDARD DRAWINGS .....	1A
ESTIMATED ROADWAY QUANTITIES .....	2
TYPICAL SECTIONS AND PAVEMENT SCHEDULE .....	2A
GENERAL NOTES.....	2B
SPECIAL NOTES AND SCOPE OF WORK.....	2C
PROPOSED LAYOUT .....	3,4
UTILITIES INDEX .....	U1-1

**NO PROJECT COMMITMENTS ON THIS PROJECT**

## STANDARD ROADWAY DRAWINGS

DWG. NO	REV.	DESCRIPTION
<b>ROADWAY DESIGN STANDARDS</b>		
RD-A-1	12-18-99	STANDARD ABBREVIATIONS
RD-L-1	10-26-94	STANDARD LEGEND
RD01-SD-1		INTERSECTION SIGHT DISTANCE DESIGN AND GENERAL NOTES
RD01-SD-2		INTERSECTION SIGHT DISTANCE LANDSCAPE AND OBSTRUCTION
RD01-SD-3		INTERSECTION SIGHT DISTANCE 2-LANE ROADWAYS
<b>ROADWAY AND PAVEMENT APPURTENANCES</b>		
RP-R-1	05-27-01	STANDARD RAMPS TO SIDE ROADS
<b>SAFETY APPURTENANCES AND FENCE</b>		
S-CZ-1		CLEAR ZONE CRITERIA
S-PL-1		SAFETY PLAN AT ROADSIDE HAZARDS
S-PL-2		SAFETY PLAN AT SIDE ROADS OR PRIVATE DRIVES
<b>TRAFFIC CONTROL APPURTENANCES</b>		
T-M-1	07-24-14	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS AND MARKING ABBREVIATIONS
T-M-2	07-24-14	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS
T-M-3	07-24-14	MARKING STANDARDS FOR TRAFFIC ISLANDS, MEDIANS & PAVED SHOULDERS ON CONVENTIONAL ROADS
T-M-4	07-24-14	STANDARD INTERSECTION PAVEMENT MARKINGS
T-M-16	11-01-11	ASPHALT SHOULDER RUMBLE STRIPE INSTALLATION DETAILS FOR NON-ACCESS CONTROLLED ROUTES
T-WZ-10	04-02-12	ADVANCE ROAD WORK SIGNING ON HIGHWAYS AND FREEWAYS
T-WZ-31	09-01-05	TRAFFIC CONTROL 2-LANE, 2-WAY DIVERSION (GREATER THAN 40 MPH)
<b>EROSION PREVENTION AND SEDIMENT CONTROL</b>		
EC-STR-3B	08-01-12	SILT FENCE
EC-STR-3E	04-01-08	SILT FENCE FABRIC JOINING DETAILS

**UNOFFICIAL  
SET  
NOT FOR  
BIDDING**

SEALED BY

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

INDEX  
AND  
STANDARD  
DRAWINGS

TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	2015	PHS1P-68(41)	2

S.R. 68  
62069-3228-94  
MONROE CO.

### ESTIMATED ROADWAY QUANTITIES

ITEM NO.	DESCRIPTION	UNIT	QUANTITY	QUANTITY	TOTAL QUANTITY
			L.M. 16.91	L.M. 20.01	
⑥ 209-08.03	TEMPORARY SILT FENCE (WITHOUT BACKING)	L.F.	50	50	100
⑤ 303-01	MINERAL AGGREGATE, TYPE A BASE, GRADING D	TON	7	24	31
307-01.08	ASPHALT CONCRETE MIX (PG64-22) (BPMB-HM) GRADING B-M2	TON	515	635	1150
403-01	BITUMINOUS MATERIAL FOR TACK COAT (TC)	TON	3	4	7
① 411-01.10	ACS MIX(PG64-22) GRADING D	TON	302	372	674
411-12.03	SCORING FOR RUMBLE STRIPE (NON-CONTINUOUS) (8IN WIDTH)	L.M.	0.27	0.33	0.6
712-01	TRAFFIC CONTROL	LS	0.5	0.5	1
② 712-06	SIGNS (CONSTRUCTION)	S.F.	216	240	456
712-04.02	FLEXIBLE DRUMS (CHANNELIZATION)	EACH	30	40	70
④ 716-05.01	PAINTED PAVEMENT MARKING (4" LINE)	L.M.	1	1	2
716-01.21	SNWPLWBLE PVMT MRKRS (BI-DIR)(1 COLOR)	EACH	82	90	172
716-01.22	SNWPLWBLE PVMT MRKRS (MONO-DIR)(1 COLOR)	EACH	5	10	15
716-02.04	PLASTIC PAVEMENT MARKING(CHANNELIZATION STRIPING)	S.Y.	55	65	120
716-02.05	PLASTIC PAVEMENT MARKING (STOP LINE)	L.F.	23	70	93
③ 716-02.06	PLASTIC PAVEMENT MARKING (TURN LANE ARROW)	EACH	1	2	3
716-12.02	ENHANCED FLATLINE THERMO PVMT MRKNG (6IN LINE)	L.M.	0.7	0.8	1.5
716-13.02	SPRAY THERMO PVMT MRKNG (60 MIL) (6IN LINE)	L.M.	0.4	0.5	0.9
717-01	MOBILIZATION	LS	0.5	0.5	1

① INCLUDES 51 TONS FOR SIDE ROADS.

② QUANTITY INCLUDES THE FOLLOWING SIGNS:

S.R. 68, MONROE CO., L.M. 16.91

CONSTRUCTION SIGNS ITEM NO. 712-06			
NO.	TYPE	SIZE	QUANTITY
3	G20-2	48"X24"	24. S.F.
2	W20-7A	48"X48"	32. S.F.
5	W20-1	48"X48"	80. S.F.
2	W1-4AR	48"X48"	32. S.F.
2	W1-4AL	48"X48"	32. S.F.
4	W13-1	24"X24"	16. S.F.
<b>TOTAL</b>			<b>216. S.F.</b>

S.R. 68, MONROE CO., L.M. 20.01

CONSTRUCTION SIGNS ITEM NO. 712-06			
NO.	TYPE	SIZE	QUANTITY
4	G20-2	48"X24"	32. S.F.
2	W20-7A	48"X48"	32. S.F.
6	W20-1	48"X48"	96. S.F.
2	W1-4AR	48"X48"	32. S.F.
2	W1-4AL	48"X48"	32. S.F.
4	W13-1	24"X24"	16. S.F.
<b>TOTAL</b>			<b>240. S.F.</b>

NOTE: CONSTRUCTION SIGNS ARE CONSIDERED TO BE MINIMUM REQUIRED, OTHER SIGNS MAY BE REQUIRED AS DIRECTED BY THE TDOT SUPERVISOR.

- ③ THE CONTRACTOR MAY ELECT TO SUBSTITUTE PREFORMED PLASTIC FOR THERMOPLASTIC. PREFORMED PLASTIC SHALL BE PAID FOR AT THE SAME UNIT PRICE AS BID FOR THERMOPLASTIC.
- ④ TO BE USED FOR TEMPORARY PAVEMENT MARKING ON INTERMEDIATE LAYERS.
- ⑤ INCLUDES ALL MATERIAL NECESSARY FOR CONSTRUCTION OF BASE LAYER AND ANY NECESSARY EPSC MEASURES.
- ⑥ SEE SUBSECTION 209.07 OF THE STANDARD SPECIFICATIONS FOR MAINTENANCE REPLACEMENT.

**UNOFFICIAL  
SET  
NOT FOR  
BIDDING**

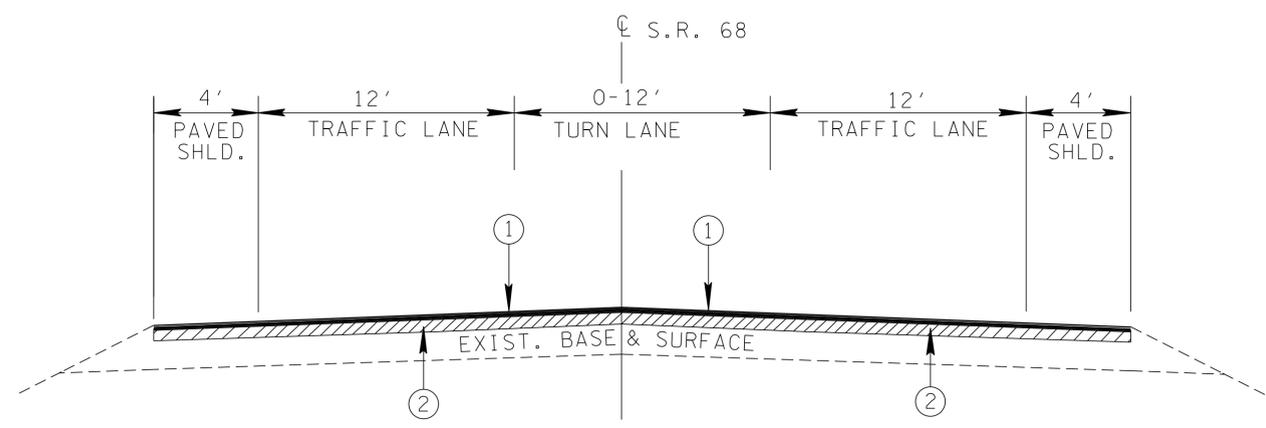
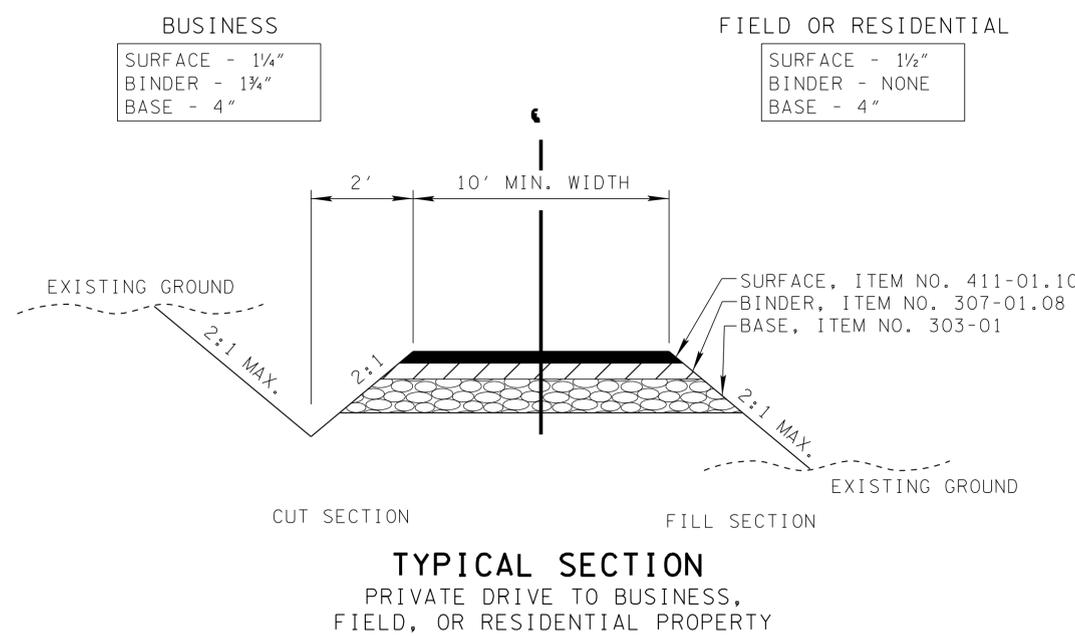
SEALED BY

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

**ESTIMATED  
ROADWAY  
QUANTITIES**

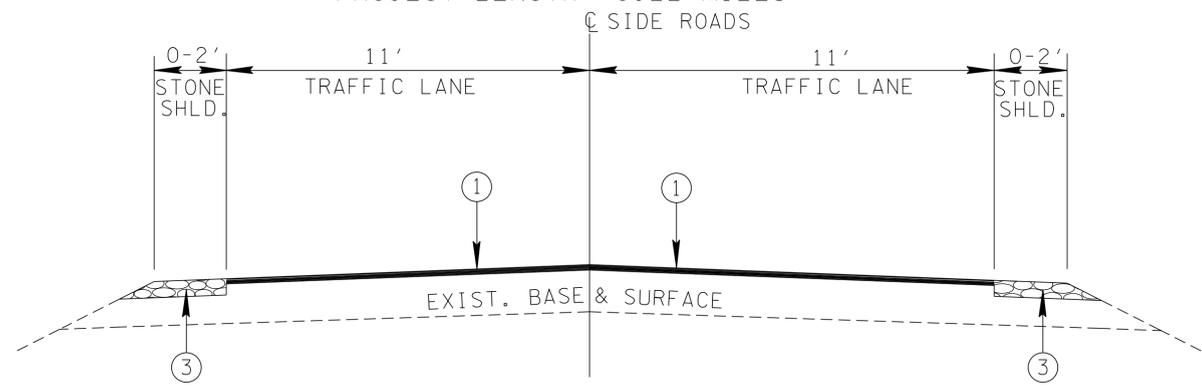
TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2014	PHSIP-68(41)	2
CONST.	2015	PHSIP-68(41)	2A

S.R. 68  
62069-0228-94 (UTILITIES) MONROE CO.  
62069-3228-94 (CONST.)



**TYPICAL CROSS-SECTION OF IMPROVEMENT**

L.M. 16.92  
PROJECT LENGTH= 0.18 MILES  
L.M. 20.01  
PROJECT LENGTH= 0.22 MILES



**TYPICAL CROSS-SECTION OF IMPROVEMENT**

HAPPY HOLLOW ROAD  
ROADWAY LENGTH= 54.72 FEET  
ELEAZAR ROAD  
ROADWAY LENGTH= NB 85.40 FEET AND SB 86.50 FEET

PROPOSED PAVEMENT SCHEDULE	
①	ASPHALTIC CONCRETE SURFACE (HOT MIX) (ROADWAY) @ 1.25" THICK (APPROX. 132.5 LBS / S.Y. ) 411-01.10 ACS MIX (PG 64-22) GRADING "D" 403-01 BITUMINOUS MATERIAL FOR TACK COAT (TC) (RATE 0.07 GAL / S.Y. )
②	BITUMINOUS PLANT MIX BASE (HOT MIX) (ROADWAY) @ 2" THICK (APPROX. 226 LBS / S.Y. ) 307-01.08 ASPHALT CONCRETE MIX (PG 64-22) (BPMB-HM) GRADING "B-M2" 403-01 BITUMINOUS MATERIAL FOR TACK COAT (TC) (RATE 0.07 GAL / S.Y. )
③	MINERAL AGGREGATE BASE - 3.25" DEPTH (SHOULDERS) 303-01 MINERAL AGGREGATE, TYPE "A" BASE GRADING "D"

**UNOFFICIAL  
SET  
NOT FOR  
BIDDING**

SEALED BY

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

**TYPICAL  
SECTIONS  
AND  
PAVEMENT  
SCHEDULE**

TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	2015	PHS1P-68(41)	2B

S. R. 68  
62069-3228-94 (CONST.) MONROE CO.

# GENERAL NOTES

## GRADING

- ANY AREA THAT IS DISTURBED OUTSIDE LIMITS OF CONSTRUCTION DURING THE LIFE OF THIS PROJECT SHALL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE.
- THE CONTRACTOR SHALL NOT DISPOSE OF ANY MATERIAL EITHER ON OR OFF STATE-OWNED R.O.W. IN A REGULATORY FLOOD WAY AS DEFINED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY WITHOUT APPROVAL BY SAME. ALL MATERIAL SHALL BE DISPOSED OF IN UPLAND (NON-WETLAND) AREAS AND ABOVE ORDINARY HIGH WATER OF ANY ADJACENT WATERCOURSE. THIS DOES NOT ELIMINATE THE NEED TO OBTAIN ANY OTHER LICENSES OR PERMITS THAT MAY BE REQUIRED BY ANY OTHER FEDERAL, STATE OR LOCAL AGENCY.

## MISCELLANEOUS

- ALL DETOUR, ACCESS, SERVICE AND FRONTAGE ROADS SHALL BE CONSTRUCTED WITH A MINIMUM OF ONE (1) COURSE OF BASE MATERIAL BEFORE TRAFFIC IS INTERRUPTED ON EXISTING ROADS.
- THE CONTRACTOR SHALL BE REQUIRED TO REMOVE AND RESET MAILBOXES WHERE AND AS DIRECTED BY THE ENGINEER.
- NOTHING IN THE GENERAL NOTES OR SPECIAL PROVISIONS SHALL RELIEVE THE CONTRACTOR FROM HIS RESPONSIBILITIES TOWARD THE SAFETY AND CONVENIENCE OF THE GENERAL PUBLIC AND THE RESIDENTS ALONG THE PROPOSED CONSTRUCTION AREA

## RIGHT - OF - WAY

- EXISTING PAVED DRIVEWAY PER TRACT REMAINDER WILL BE REPLACED IN KIND TO A TOUCHDOWN POINT.
- WHERE THE EXISTING DRIVEWAY IS UNPAVED AND THE PROPOSED DRIVEWAY EXCEEDS 7 PERCENT IN GRADE, EACH DRIVEWAY WILL BE PAVED TO A TOUCHDOWN POINT OR UNTIL THE GRADE IS LESS THAN 7 PERCENT.
- WHERE THE EXISTING DRIVEWAY IS UNPAVED AND THE PROPOSED DRIVEWAY IS LESS THAN 7 PERCENT IN GRADE, EACH DRIVEWAY WILL BE PAVED A SHOULDER WIDTH FROM THE EDGE OF PAVEMENT AND THE REMAINDER OF THAT DRIVEWAY REPLACED IN KIND TO A TOUCHDOWN POINT.
- ANY NECESSARY PAVING OF DRIVEWAYS WILL BE DONE DURING PAVING OPERATIONS ON THE MAIN ROADWAY.
- ON PROJECTS WITHOUT CURB AND GUTTER THAT ARE ON STATE ROUTES, IT WILL BE THE RESPONSIBILITY OF THE OWNER TO SECURE A PERMIT AND TO CONSTRUCT ADDITIONAL DRIVEWAYS AND FIELD ENTRANCES OTHER THAN THOSE PROVIDED IN THE PLANS.

## TEMPORARY PAVEMENT MARKING ON INTERMEDIATE LAYERS

- TEMPORARY PAVEMENT LINE MARKINGS ON INTERMEDIATE LAYERS OF PAVEMENT SHALL BE REFLECTIVE TAPE OR REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAYS WORK. SHORT, UNMARKED SECTIONS SHALL NOT BE ALLOWED. THESE MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-05.01, PAINTED PAVEMENT MARKING (4" LINE), L.M.

## FINAL PAVEMENT MARKING IF 6" ENHANCED FLATLINE THERMOPLASTIC IS USED

- PERMANENT PAVEMENT LINE MARKINGS SHALL BE 6" ENHANCED FLATLINE THERMOPLASTIC INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-12.02, ENHANCED FLATLINE THERMO PVMT MRKNG (6IN LINE), L.M. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.

## FINAL PAVEMENT MARKING IF 6" SPRAY THERMOPLASTIC (60 mil) IS USED

- PERMANENT PAVEMENT LINE MARKINGS SHALL BE 6" SPRAY THERMOPLASTIC (60 mil) INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-13.02, SPRAY THERMO PVMT MRKNG (60 mil) (6IN LINE), L.M. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.

## PAVING

- THE CONTRACTOR SHALL BE REQUIRED TO PAVE IN THE DIRECTION OF TRAFFIC.
- THE CONTRACTOR SHALL BE REQUIRED TO COLD PLANE AND PAVE IN THE DIRECTION OF TRAFFIC.

## RESURFACING

- WHERE DIRECTED BY THE TDOT ENGINEER, THE CONTRACTOR SHALL BE REQUIRED TO SHAPE PUBLIC SIDE ROADS, BUSINESS ENTRANCES, AND PRIVATE DRIVES, AS WELL AS CLEANING OF EXISTING DRAINS BEFORE PLACING MATERIALS. ALL COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- ALL PUBLIC SIDE ROADS SHALL BE PAVED ONE PAVER WIDTH THROUGH THE INTERSECTION AS A MINIMUM. A SATISFACTORY TRANSITION FROM THE NEW PAVEMENT TO THE EXISTING GRADE OF THE INTERSECTING PUBLIC ROAD OR BUSINESS ENTRANCE SHALL BE PROVIDED. SHOULD THE PAVEMENT OF THE INTERSECTING PUBLIC ROAD BE DISTRESSED, THE RESURFACING WIDTH MAY BE INCREASED TO THE NORMAL RIGHT OF WAY LINE.
- PRIVATE DRIVEWAYS, FIELD ENTRANCES, AND BUSINESS ENTRANCES WILL BE RESURFACED A PAVER WIDTH (LANE WIDTH) AS A MINIMUM. A PAVEMENT TAPER TO TRANSITION THE NEW PAVEMENT SHALL BE REQUIRED, IT SHALL BE BASED ON AN ADDITIONAL ONE FOOT OF WIDTH PER ONE INCH DEPTH OF PAVEMENT. IF THE SHOULDER IS NARROW ENOUGH THAT THE SUM OF THE SHOULDER AND THE TRANSITION ARE LESS THAN A PAVER WIDTH, THE TRANSITION SHALL OCCUR WITHIN THE PAVER WIDTH. IF THE SUM OF THE SHOULDER AND THE TRANSITION IS GREATER THAN A PAVER WIDTH (LANE WIDTH), THE TRANSITION SHALL OCCUR OUTSIDE OF THE PAVER WIDTH.
- IN ALL CASES, THE LENGTH OF THE PAVEMENT TRANSITION, THE THICKNESS AND WIDTH OF THE RESURFACING AND ANY ADDITIONAL PAVEMENT MATERIALS SHALL BE AS DIRECTED BY THE TDOT ENGINEER.

## CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

- ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE WEEK BEFORE NEEDED, IF THE SIGN FACE IS FULLY COVERED.
- IF THE CONTRACTOR MOVES OFF THE PROJECT, HE SHALL COVER OR REMOVE ALL UNNEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OF REMOVAL, COVERING, AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO 712-06, SIGNS (CONSTRUCTION) PER SQUARE FOOT.
- A LONG TERM BUT SPORADIC USE WARNING SIGN, SUCH AS A FLAGGER SIGN, MAY REMAIN IN PLACE WHEN NOT REQUIRED PROVIDED THE SIGN FACE IS FULLY COVERED.
- TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.
- USE OF BARRICADES, PORTABLE BARRIER RAILS, VERTICAL PANELS, AND DRUMS SHALL BE LIMITED TO THE IMMEDIATE AREAS OF CONSTRUCTION WHERE A HAZARD IS PRESENT. THESE DEVICES SHALL NOT BE STORED ALONG THE ROADWAY WITHIN THIRTY (30) FEET OF THE EDGE OF THE TRAVELED WAY BEFORE OR AFTER USE UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL INCREASE TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR

GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. THESE DEVICES SHALL BE REMOVED FROM THE CONSTRUCTION WORK ZONE WHEN THE ENGINEER DETERMINES THEY ARE NO LONGER NEEDED. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.

- THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY, WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHEN THE LANE IS OPEN TO TRAFFIC UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO PARK WITHIN THIRTY (30) FEET OF A OPEN TRAFFIC LANE AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE.. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.
- ALL DETOUR AND CONSTRUCTION SIGNING SHALL BE IN STRICT ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- ALL DETOURS SHALL BE PAVED, STRIPED, SIGNED AND THE VERTICAL PANELS ARE TO BE IN PLACE BEFORE IT IS OPENED TO TRAFFIC.

## LITTER, DEBRIS, WASTE, PETROLEUM

- THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT LITTER, CONSTRUCTION DEBRIS, AND CONSTRUCTION WASTES FROM ENTERING WATERS OF THE STATE/U.S. THESE MATERIALS WILL BE PICKED UP AND REMOVED FROM STORMWATER EXPOSURE PRIOR TO ANTICIPATED STORM EVENTS. AFTER USE, MATERIALS USED FOR EPSC WILL BE REMOVED FROM THE SITE.
- THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS TO ENSURE THAT PETROLEUM PRODUCTS OR OTHER CHEMICAL POLLUTANTS ARE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. ALL EQUIPMENT REFUELING, SERVICING, AND STAGING AREAS SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL LAWS, RULES, REGULATIONS, AND ORDINANCES, INCLUDING THOSE OF THE NATIONAL FIRE PROTECTION ASSOCIATION (NFPA). APPROPRIATE CONTAINMENT MEASURES FOR THESE AREAS SHALL BE USED. ALL SPILLS MUST BE REPORTED TO THE APPROPRIATE AGENCY, AND MEASURES SHALL BE TAKEN IMMEDIATELY TO PREVENT THE POLLUTION OF WATERS OF THE STATE/U.S., INCLUDING GROUNDWATER, SHOULD A SPILL OCCUR.

**UNOFFICIAL  
SET  
NOT FOR  
BIDDING**

SEALED BY

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

**GENERAL  
NOTES**

3-JAN-2015 15:19 \\J00WF01.tdot.state.tn.us\GISharded\SURVIDES\DESIGN\PROJECTS\MRSR68\002B.shx

TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	2015	PHS1P-68(41)	2C

S.R. 68  
62069-3228-94 (CONST.) MONROE CO.

# SPECIAL NOTES

## CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

- (1) THE CONTRACTOR SHALL COMPLY WITH SECTION 712 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION REGARDING TEMPORARY TRAFFIC CONTROL AND THE CURRENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- (2) ANY WORK REQUIRING LANE CLOSURES INCLUDING PAVEMENT MARKING OPERATIONS SHALL BE AT NON-PEAK HOURS (9:00 A.M. TO 3:00 P.M.) UNLESS OTHERWISE DIRECTED BY THE TDOT OPERATIONS DISTRICT ENGINEER.

## PAVEMENT EDGE DROP-OFF TRAFFIC CONTROL NOTES

- A. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES OR TRAFFIC LANE AND SHOULDER WHERE THE TRAFFIC LANE IS BEING USED BY TRAFFIC, CAUSED BY BASE, PAVING OR RESURFACING:
  - 1. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 0.75 INCH AND NOT EXCEEDING 2 INCHES:
    - a. WARNING SIGNS, UNEVEN LANES (W8-11) AND/OR SHOULDER DROP-OFF WITH PLAQUE (W8-17 AND W8-17P), SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.
    - b. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY ADDED PAVEMENT SHALL BE ELIMINATED WITHIN THREE WORKDAYS.
    - c. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY COLD PLANING SHALL BE ELIMINATED WITHIN THREE WORKDAYS.
    - d. WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE TRAFFIC LANE BEING UTILIZED BY TRAFFIC AND SHOULDER THE DIFFERENCE IN ELEVATION SHALL BE ELIMINATED WITHIN SEVEN WORKDAYS AFTER THE CONDITION IS CREATED.
  - 2. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 2 INCHES AND NOT EXCEEDING 6 INCHES. TRAFFIC IS NOT TO BE ALLOWED TO TRAVERSE THIS DIFFERENCE IN ELEVATION.
    - a. SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:
      - (1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.
      - (2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.
    - b. IF THE DIFFERENCE IN ELEVATION IS ELIMINATED OR DECREASED TO 2 INCHES OR LESS BY THE END OF EACH WORKDAY, CONES MAY BE USED DURING DAYLIGHT HOURS IN LIEU OF DRUMS, BARRICADES OR OTHER APPROVED PROTECTIVE DEVICES MENTIONED IN PARAGRAPH a, PROVIDED WARNING SIGNS ARE ERECTED. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

- c. WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE THROUGH TRAFFIC LANE AND THE SHOULDER AND THE ELEVATION DIFFERENCE IS LESS THAN 3.5 INCHES, THE CONTRACTOR MAY USE WARNING SIGNS AND/OR PROTECTIVE DEVICES AS APPLICABLE AND APPROVED BY THE ENGINEER. SEE PARAGRAPH a REGARDING USE OF DRUMS, BARRICADES OR OTHER APPROVED PROTECTIVE DEVICES. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) WILL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

IN THESE SITUATIONS, THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 2 MILES IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

## EROSION PREVENTION AND SEDIMENT CONTROL

### DISTURBED AREA

- (1) ALL DISTURBED AREAS SHALL BE PROPERLY STABILIZED AS SOON AS PRACTICABLE.

### SEDIMENT CONTROL

THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT THE OFF-SITE MIGRATION OR DEPOSIT OF SEDIMENT ON ROADWAYS USED BY THE GENERAL PUBLIC. OFFSITE VEHICLE TRACKING OF SEDIMENTS AND THE GENERATION OF DUST SHALL BE MINIMIZED.

### STREAM/WETLAND

- (1) WETLANDS SHALL NOT BE USED AS EQUIPMENT STORAGE, STAGING OR TRANSPORTATION AREAS.

### NPDES

- (1) DUE TO THE TYPE OF WORK IT IS OUR UNDERSTANDING THAT THE PROJECT WILL NOT DISTURB MORE THAN ONE ACRE. IF THE DISTURBED AREA IS MORE THAN ONE ACRE, PLEASE CONTACT THE TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION BECAUSE A NPDES PERMIT WILL BE REQUIRED.
- (2) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS, BEFORE CLEANING OUT OR REMOVAL/REPLACEMENT OF DITCHES, SIDE DRAINS, AND CROSS DRAINS, TO ENSURE THAT THE DRAINAGE FEATURES / WATERCOURSES ARE NOT STREAMS OR WETLANDS. IF THE CONTRACTOR / TDOT INSPECTOR IS UNSURE WHETHER THE DRAINAGE FEATURES ARE STREAMS OR WETLANDS, THE INSPECTOR SHALL CONTACT THE TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION TO OBTAIN THE APPROPRIATE PERMITS.

### ECOLOGY

- (1) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS BEFORE CONSTRUCTING EARTH PADS FOR THE TYPE 38 TERMINALS TO ENSURE THAT ENVIRONMENTAL FEATURES (STREAMS, WETLANDS, OR SPRINGS) ARE NOT IMPACTED DURING CONSTRUCTION. IF THE CONTRACTOR/ TDOT INSPECTOR IS UNSURE WHETHER THE ENVIRONMENTAL FEATURES ARE STREAMS, SPRINGS OR WETLANDS, THE INSPECTOR SHALL CONTACT THE TDOT ENVIRONMENTAL DIVISION ECOLOGY OR PERMITS SECTION SO AN ACCURATE DETERMINATION CAN BE ASSESSED.
- (2) DURING CONSTRUCTION OF THE EARTH PADS FOR THE TYPE 38 GUARDRAIL TERMINALS, EXISTING VEGETATION WILL BE REQUIRED TO BE REMOVED FOR THE PROPOSED PAD AND SLOPES. IF DURING THE VEGETATION REMOVAL, ANY TREES GREATER THAN 5 INCHES IN DIAMETER ARE REQUIRED TO BE REMOVED, CONTACT THE T.D.O.T. ENVIRONMENTAL DIVISION ECOLOGY SECTION IMMEDIATELY. DUE TO THE RECENT MODIFICATION OF THE USFWS STANCE REGARDING THE INDIANA BAT NO TREES >5" DBH SHOULD BE CUT WITHOUT CLEARANCE FROM THE TDOT ENVIRONMENTAL DIVISION ECOLOGY SECTION.

# SCOPE OF WORK

- (1) THIS PROJECT INCLUDES THE RESURFACING AND RESTRIPING OF S.R. 68 AS INDICATED ON THE PLANS WHICH WILL INCLUDE INSTALLATION OF LEFT TURN LANES.
- (2) THIS PROJECT INCLUDES THE INSTALLATION OF PAVEMENT MARKINGS AND THE USE OF TEMPORARY TRAFFIC CONTROL DEVICES AS DIRECTED BY THE TDOT SUPERVISOR.
- (3) THIS PROJECT INCLUDES TRAFFIC CONTROL AND EPSC DEVICES AS DIRECTED BY THE TDOT OPERATIONS DISTRICT SUPERVISOR.

**UNOFFICIAL  
SET  
NOT FOR  
BIDDING**

SEALED BY

[Blank Signature Line]

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

**SPECIAL NOTES  
AND  
SCOPE OF WORK**

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2014	PHSIP-68(41)	3
CONST.	2015	PHSIP-68(41)	3

S.R. 68 MONROE CO.  
62069-0228-94 (UTILITIES)  
62069-3228-94 (CONST.)



**UNOFFICIAL  
SET  
NOT FOR  
BIDDING**

SEALED BY

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

**PROPOSED  
LAYOUT**  
(L.M. 16.92)  
SCALE: 1" = 50'

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2014	PHSIP-68(41)	4
CONST.	2015	PHSIP-68(41)	4

S.R. 68 MONROE CO.  
 62069-0228-94 (UTILITIES)  
 62069-3228-94 (CONST.)



**UNOFFICIAL  
 SET  
 NOT FOR  
 BIDDING**

SEALED BY

STATE OF TENNESSEE  
 DEPARTMENT OF TRANSPORTATION

**PROPOSED  
 LAYOUT**  
 (L.M. 20.01)  
 SCALE: 1" = 50'