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SEE SHEET IA

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
BUREAU OF ENGINEERING

**MOORE COUNTY**

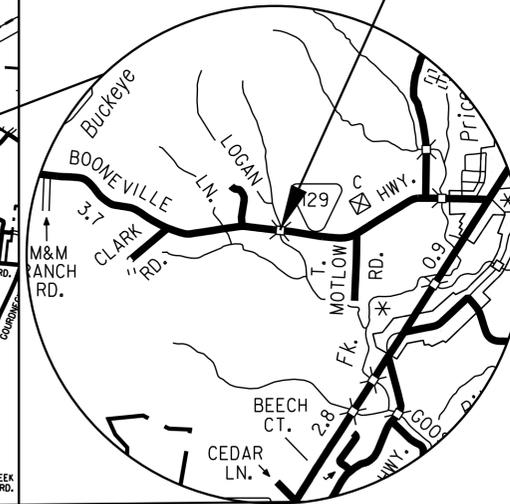
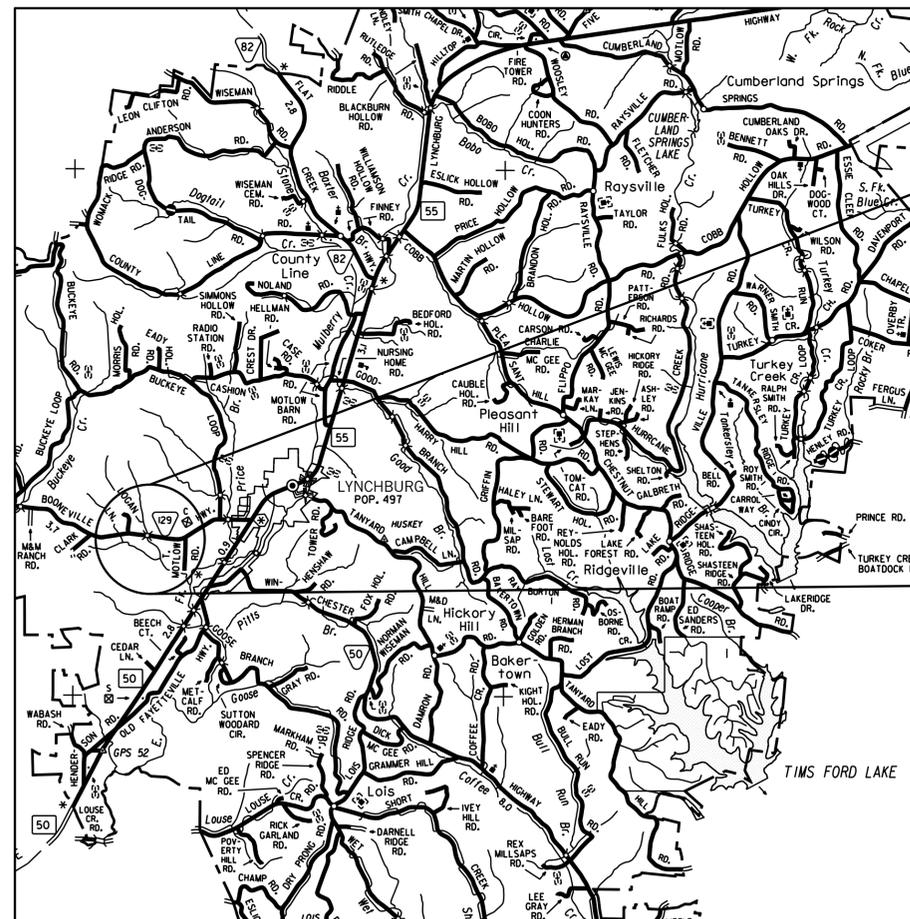
STATE ROUTE 129 OVER UNNAMED BRANCH  
BRIDGE NO. 64-SR129-2.51

TENN.	YEAR	SHEET NO.
	2015	1
FED. AID PROJ. NO.		
STATE PROJ. NO.	64009-4205-04	

PROJECT NO. 64009-4205-04



**BRIDGE REPAIR**



BRIDGE NO. 64-SR129-2.51  
OVER  
UNNAMED BRANCH

**UNOFFICIAL SET**  
NOT FOR BIDDING

**SPECIAL NOTES**

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED JANUARY 1, 2015 AND ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS AND IN THE PROPOSAL CONTRACT.

TDOT C.E. MANAGER 1 OR  
TDOT DESIGN MANAGER 1 BRIAN EGLI

TDOT PROJECT MANAGER TERRY MACKIE

DESIGNED BY JAMES + ASSOCIATES, INC.

DESIGNER DAVID THOMPSON, P.E. CHECKED BY JAMIE GILLESPIE, P.E.

P.E. NO. 64009-4205-04

PIN NO. 121392.00

SCALE: 1" = 5,280'

TRAFFIC DATA	
ADT (2015)	350
ADT (2035)	450
DHV (2035)	59
D	65-35
T (ADT)	5 %
T (DHV)	3 %
V	45 MPH

ROADWAY LENGTH 0.055 MILES  
BRIDGE LENGTH 0.006 MILES  
PROJECT LENGTH 0.061 MILES

APPROVED: Paul D. Degges  
PAUL D. DEGGES, CHIEF ENGINEER

DATE: \_\_\_\_\_

APPROVED: John Schroer  
JOHN SCHROER, COMMISSIONER

U.S. DEPARTMENT OF TRANSPORTATION  
FEDERAL HIGHWAY ADMINISTRATION

APPROVED: \_\_\_\_\_  
DIVISION ADMINISTRATOR DATE

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## LIST OF REFERENCE DRAWINGS

## DWG. NO.

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## ROADWAY DESIGN STANDARDS

DWG. NO.	REVISION DATE	DESCRIPTION
RD-A-1	12-18-99	STANDARD ABBREVIATIONS
RD-L-1	10-26-94	STANDARD LEGEND
RD-L-2	09-05-01	STANDARD LEGEND FOR UTILITY INSTALLATIONS
RD-L-3	04-15-04	STANDARD LEGEND FOR SIGNALIZATION AND LIGHTING
RD-L-4	04-15-04	STANDARD LEGEND FOR SIGNALIZATION AND LIGHTING
RD-L-5	05-01-08	STANDARD LEGEND FOR EROSION PREVENTION AND SEDIMENT CONTROL
RD-L-6	03-30-10	STANDARD LEGEND FOR EROSION PREVENTION AND SEDIMENT CONTROL
RD-L-7	05-24-12	STANDARD LEGEND FOR EROSION PREVENTION AND SEDIMENT CONTROL

## SAFETY APPURTENANCES AND FENCE

S-PL-3		SAFETY PLAN: MINIMUM INSTALLATION AT BRIDGE ENDS
S-PL-6	12-01-14	SAFETY PLAN: SAFETY HARDWARE PLACEMENT
S-GR31-1	12-01-14	W-BEAM GUARDRAIL
S-GRC-1		GUARDRAIL CONNECTION TO BRIDGE ENDS OR BARRIER WALL
S-GRT-2	11-03-14	TYPE 38 GUARDRAIL TERMINAL
S-GRT-2R		EARTH PAD FOR TYPE 38 (RETROFIT)
S-GRA-3		GUARDRAIL ANCHOR FOR TYPE 21, 13, AND IN-LINE TERMINALS

## TRAFFIC CONTROL APPURTENANCES

T-M-1	07-24-14	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS AND MARKING ABBREVIATIONS
T-M-2	07-24-14	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS
T-M-15A	01-30-15	ASPHALT SHOULDER RUMBLE STRIP INSTALLATION
		DETAILS FOR NON-ACCESS CONTROLLED ROUTES
T-M-16	12-01-14	ASPHALT SHOULDER RUMBLE STRIPE INSTALLATION
		DETAILS FOR NON-ACCESS CONTROLLED ROUTES
T-PBR-1	06-30-09	INTERCONNECTED PORTABLE BARRIER RAIL
T-PBR-2	11-01-11	DETAIL FOR VERTICAL PANELS AND FLEXIBLE DELINEATORS
T-S-11	06-06-11	DELINEATOR AND MILEPOST DETAILS
T-SG-1	11-01-11	WOOD POLE, DETAILS FOR SPAN MOUNTED SIGNALS
T-SG-2	07-29-04	LOOP LEAD-INS, CONDUIT AND PULL BOXES
T-SG-3	11-11-04	NOTES AND DETAILS OF INDUCTIVE LOOPS
T-SG-3A		ALTERNATE DETECTION DETAILS
T-SG-4		SPAN WIRE AND MESSENGER CABLE DETAILS
T-SG-5	12-04-13	CONTROLLER CABINET DETAILS
T-SG-7A	11-01-11	TYPICAL SIGNAL HEAD PLACEMENT
T-SG-8	12-04-13	STRAIN POLE DETAILS FOR SPAN MOUNTED SIGNALS
T-SG-9A	05-01-14	MISCELLANEOUS SIGNAL DETAILS
T-SG-12	11-01-11	TYPICAL WIRING FOR SIGNAL HEADS AND DETECTION LOOPS
T-WZ-10	04-02-12	ADVANCE ROAD WORK SIGNING ON HIGHWAYS AND FREEWAYS
T-WZ-32	10-29-13	TRAFFIC CONTROL PLAN SIGNAL LAYOUT FOR TRAFFIC SIGNAL AT TWO LANE BRIDGE RECONSTRUCTION SITE
T-WZ-34	09-01-05	TRAFFIC CONTROL PLAN GENERAL NOTES FOR TRAFFIC SIGNAL AT TWO LANE BRIDGE RECONSTRUCTION SITE
T-WZ-35	04-02-12	TRAFFIC CONTROL PLAN PAY ITEM AND SIGN DETAILS FOR TRAFFIC SIGNAL AT TWO LANE BRIDGE RECONSTRUCTION SITE

## EROSION PREVENTION AND SEDIMENT CONTROL

EC-STR-2	08-01-12	SEDIMENT FILTER BAGS
EC-STR-3C	08-01-12	SILT FENCE WITH WIRE BACKING
EC-STR-3E	04-01-08	SILT FENCE FABRIC JOINING DETAILS
EC-STR-6A	08-01-12	ENHANCED ROCK CHECK DAM
EC-STR-25	08-01-12	TEMPORARY CULVERT CROSSING, CONSTRUCTION EXIT, CONSTRUCTION FORD
EC-STR-30		INSTREAM DIVERSION (WITHOUT TRAFFIC)
EC-STR-30A		INSTREAM DIVERSION (WITH TRAFFIC)
EC-STR-37	06-10-14	SEDIMENT TUBE

## BRIDGE APPURTENANCES (NEW STRUCTURES)

STD-1-1SS	05-01-14	BRIDGE RAILING SINGLE SLOPE CONCRETE PARAPET - 2006
STD-1-2SS		STEEL SLIDER PLATE ASSEMBLIES FOR SINGLE SLOPE CONCRETE AND BRIDGE DECK DRAIN DETAILS - 2007
STD-1-5	03-26-14	REINFORCED CONCRETE PAVEMENT AT BRIDGE ENDS - 1995
STD-2-1	11-01-10	BRIDGE MOUNTED INTERCONNECTED PORTABLE BARRIER RAIL - 2005
STD-4-1	04-08-05	STANDARD PRECAST PRESTRESSED BRIDGE DECK PANELS GENERAL DETAILS - 1992
STD-4-2	04-08-05	STANDARD PRECAST PRESTRESSED BRIDGE DECK PANELS DESIGN CRITERIA - 1992
STD-4-3	03-02-02	STANDARD PRECAST PRESTRESSED BRIDGE DECK PANELS GENERAL DETAILS - 1992
STD-4-4	06-10-96	STANDARD PRECAST PRESTRESSED BRIDGE DECK PANELS CONSTRUCTION DETAILS - 1992
STD-6-1	11-01-10	STANDARD SEISMIC DETAILS - 1990
STD-9-1	10-07-08	REINFORCING BAR SUPPORT DETAILS FOR CONCRETE SLABS
STD-10-1	04-08-05	MISCELLANEOUS ABUTMENT AND DRAINAGE DETAILS - 1971
STD-14-3	10-15-08	STANDARD DETAILS FOR PRESTRESSED BOX BEAMS

TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	2015	64009-4205-04	1A

UNOFFICIAL SET

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STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

INDEX AND STANDARD DRAWINGS

BRIDGE NO. 64-SR129-2.51  
STATE ROUTE 129  
OVER UNNAMED BRANCH  
MOORE COUNTY  
2015

TENNESSEE D.O.T.  
DESIGN DIVISION

FILE NO.

TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	2015	64009-4205-04	IB

**PROJECT COMMITMENTS**

COMMITMENT TO	SOURCE DIVISION	DESCRIPTION	STA./LOCATION
-----	ENVIRONMENTAL DIVISION, ENVIRONMENT	AN ASBESTOS-CONTAINING MATERIAL (ACM) SURVEY WAS CONDUCTED ON BRIDGE# 64S42860003, SR-129 OVER BRANCH, LM 2.51 (64-SR129-2.51). NO ACM WAS DETECTED. NO SPECIAL ACCOMMODATIONS FOR DEMOLITION AND WASTE DISPOSAL ARE ANTICIPATED FOR THIS BRIDGE AND THE MATERIAL CAN BE DEPOSITED IN A C&D LANDFILL. PLEASE NOTE THAT EVEN THOUGH THE SAMPLES WERE FOUND TO CONTAIN NO ASBESTOS, PRIOR TO THE DEMOLITION OF ANY BRIDGE COMPONENT, THE CONTRACTOR IS REQUIRED TO SUBMIT THE NATIONAL EMISSION STANDARDS FOR HAZARDOUS AIR POLLUTANTS STANDARD 10-DAY NOTICE OF DEMOLITION TO THE TENNESSEE DIVISION OF AIR POLLUTION CONTROL (STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION (JANUARY 1, 2015) SECTIONS 107.08D AND 202.03).	LM 2.51

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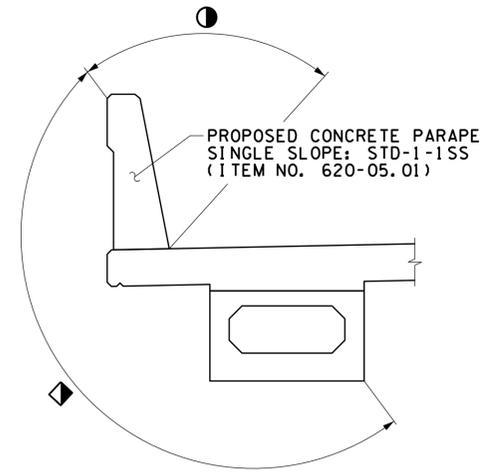
STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

BRIDGE NO. 64-SR129-2.51  
STATE ROUTE 129  
OVER UNNAMED BRANCH  
MOORE COUNTY  
2015

**PROJECT  
COMMITMENTS**

**ESTIMATED BRIDGE QUANTITIES**

ITEM NO.	DESCRIPTION	UNIT	QUANTITY
① 201-07.01	REMOVAL AND DISPOSAL OF BRUSH AND TREES	LS	1
② 202-04.01	REMOVAL OF STRUCTURE (BR.NO.64-SR129-02.51)	LS	1
③ 204-11	BRIDGE EXCAVATION (UNCLASSIFIED)	C.Y.	50
⑤ 303-01.02	GRANULAR BACKFILL (BRIDGES)	TON	55
⑥ 602-10.05	BRACING REPAIRS	LS	1
⑦ 604-02.03	EPOXY COATED REINFORCING STEEL	LB	12,725
⑧ 604-03.01	CLASS A CONCRETE (BRIDGES)	C.Y.	30
⑨ 604-03.04	PAVEMENT AT BRIDGE ENDS	S.Y.	100
⑩ 604-03.09	CLASS D CONCRETE (BRIDGE DECK)	C.Y.	30
⑪ 604-04.01	APPLIED TEXTURE FINISH (NEW STRUCTURES)	S.Y.	130
⑫ 604-04.02	APPLIED TEXTURE FINISH (EXISTING STRUCTURES)	S.Y.	44
⑬ 604-05.31	BRIDGE DECK GROOVING (MECHANICAL)	S.Y.	200
⑭ ④ 604-10.05	CONCRETE	S.F.	88
⑮ ④ 604-10.54	CONCRETE REPAIRS	S.F.	88
⑯ 615-02.02	PRESTRESSED CONCRETE BOX BEAM (17"x36")	L.F.	104
⑰ 617-02	BRIDGE DECK CRACK SEALING	L.F.	195
⑱ 617-05	SEALANT (HMWM)	GAL.	1
⑲ 620-05.01	CONCRETE PARAPET SINGLE SLOPE (STD-1-ISS)	L.F.	60
⑳ 712-02.47	BRIDGE MOUNTED INTERCONNECTED PORTABLE BARRIER RAIL	L.F.	90



**TYPICAL ① PARAPETS**

**APPLIED TEXTURE FINISH SKETCH**

N.T.S.

- ① DENOTES: APPLIED TEXTURE FINISH  
ITEM NO. 604-04.01  
WHITE NO. 37886
- ◆ DENOTES: APPLIED TEXTURE FINISH  
ITEM NO. 604-04.01  
MOUNTAIN GRAY NO. 36440

NOTE: COST OF TEXTURE COATING SHALL BE INCLUDED IN ITEM NOS. 604-04.01 AND 604-04.02 AND INCLUDES CONCRETE BRIDGE PARAPETS, WINGWALLS, NEW EXTERIOR BOX BEAMS, ALL EXPOSED SURFACES OF ABUTMENTS AND PIERS.

NOTE: BEFORE APPLYING ANY TEXTURE FINISH, ALL SURFACES SHALL BE COMPLETELY CLEANED OF ALL DEBRIS AND FOREIGN MATERIALS.

NOTE: THE CONTRACTOR SHALL USE CONTAINMENT SCREENS OR OTHER MEASURES AS NECESSARY TO PREVENT ANY TEXTURE COATING FROM ENTERING THE ENVIRONMENT. CONTAINMENT MEASURES SHALL BE APPROVED BY THE ENGINEER AND COST SHALL BE INCLUDED IN ITEMS BID ON.

NOTE: CONCRETE FINISHING SHALL BE IN ACCORDANCE WITH SECTION 604.22 OF THE TENNESSEE STANDARD SPECIFICATION. A CLASS I FINISH FOLLOWED BY AN APPLIED TEXTURE FINISH SHALL BE USED IN LIEU OF A CLASS II FINISH. THE COLOR OF THE FINISH SHALL BE SIMILAR TO MOUNTAIN GRAY, FEDERAL SPECIFICATION NO. 36440, FEDERAL COLOR STANDARD NO. 595A, EXCEPT THAT THE INSIDE FACE AND TOP OF THE PARAPET SHALL BE WHITE, FEDERAL SPECIFICATION NO. 37886. A COLOR SAMPLE SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL. NO TEXTURE FINISH SHALL BE APPLIED PRIOR TO COMPLETION OF PAVING AND HAUL OPERATIONS AT THE BRIDGE SITE. THE APPLIED TEXTURE FINISH TO EXISTING CONCRETE SHALL BE MEASURED AND PAID FOR UNDER ITEM NO. 604-04.02.

CONST. NO.			
PROJECT NO.	YEAR	SHEET NO.	
64009-4205-04	2015	2	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

**FOOTNOTES:**

- ① INCLUDES ALL LABOR, EQUIPMENT AND MATERIALS NECESSARY TO REMOVE VEGETATION FROM BRIDGE COMPONENTS IN ACCORDANCE WITH NOTE (29) ON BR-119-154.
- ② INCLUDES THE COST OF PHASED REMOVAL OF THE EXISTING BRIDGE DECK, GIRDERS AND PORTIONS OF ABUTMENTS.
- ③ INCLUDES EXCAVATION FOR THE MODIFICATIONS TO THE ABUTMENTS.
- ④ ITEM MAY BE INCREASED, DECREASED, OR ELIMINATED AS DIRECTED BY THE ENGINEER.
- ⑤ GRANULAR BACKFILL SHALL BE CLASS "A", GRADING "D" MATERIAL. INCLUDES THE QUANTITY USED FOR THE MODIFICATIONS OF THE ABUTMENTS. SEE STANDARD DRAWING STD-10-1.
- ⑥ INCLUDES ALL NECESSARY SHEET PILING (INSTALLATION AND REMOVAL) AND TEMPORARY SUPPORT OF THE STRUCTURE FOR THE DURATION OF THE PROJECT. ALSO INCLUDES FORMWORK TO PREVENT DEBRIS FROM FALLING INTO THE STREAM. BRACING PLANS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PRIOR TO BEGINNING DEMOLITION.
- ⑦ INCLUDES QUANTITY OF STEEL REQUIRED FOR BRIDGE DECK, BACKWALLS, AND ABUTMENTS. REBAR QUANTITIES NOT INCLUDED IN THIS ITEM FOR PAVEMENT AT BRIDGE ENDS AND PARAPETS. ALSO INCLUDES THE COST OF 110 MECHANICAL COUPLERS (2 NO. 4, 68 NO. 5 AND 40 NO. 6).
- ⑧ INCLUDES THE QUANTITY FOR THE ABUTMENTS, CLASS "A" CONCRETE REQUIRED FOR THE CONSTRUCTION OF THE ABUTMENTS. SEE CONCRETE STRENGTH AND CURE TIME NOTES ON SHEET 2A, BRIDGE GENERAL AND SPECIAL NOTES.
- ⑨ PRIOR TO CONSTRUCTION OF THE PAVEMENT AT BRIDGE ENDS, THE CONTRACTOR SHALL SUBMIT A PROPOSED BILL OF STEEL TO THE ENGINEER FOR APPROVAL. 15'-0" PAVEMENT AT BRIDGE ENDS REQUIRED AT BOTH BRIDGE ENDS. COST OF CONCRETE, REINFORCING STEEL, JOINT MATERIALS, LABOR AND ALL OTHER MATERIALS REQUIRED TO CONSTRUCT THE PAVEMENT AT BRIDGE ENDS AND ASPHALT APPROACH BEAM SHALL BE INCLUDED IN THIS ITEM. SEE STD. DWG. STD-1-5 FOR QUANTITIES TO BE INCLUDED IN THIS ITEM. STD-1-5 SHALL BE MODIFIED AS NEEDED TO CHANGE THE 24'-0" LENGTH TO 15'-0". COST SHALL ALSO INCLUDE REMOVAL OF EXISTING ROADWAY AND ASSOCIATED EXCAVATION.
- ⑩ INCLUDES ALL LABOR AND MATERIALS (EXCEPT REINFORCING STEEL) NECESSARY TO POUR THE NEW CONCRETE DECK, ABUTMENTS AND BACKWALLS.
- ⑪ INCLUDES THE COST OF ALL LABOR AND MATERIALS TO APPLY TEXTURE FINISH TO THE NEW PARAPETS, OVERHANGS, NEW EXTERIOR BOX BEAMS, AND ALL NEW SURFACES ON ABUTMENTS.
- ⑫ INCLUDES THE COST OF ALL LABOR AND MATERIALS TO APPLY TEXTURE FINISH TO THE EXPOSED SURFACES OF ABUTMENTS AND WINGWALLS NOT COVERED BY ITEM NO. 604-04.01.

- ⑬ INCLUDES ALL LABOR, EQUIPMENT AND MATERIALS REQUIRED TO MECHANICALLY GROOVE THE BRIDGE DECK AND PAVEMENT AT BRIDGE ENDS AND TO REMOVE GROOVING RESIDUE.
- ⑭ INCLUDES THE COST OF ALL LABOR AND MATERIALS REQUIRED TO REPAIR SPALLED AREAS WITH HIGH EARLY STRENGTH CONCRETE.
- ⑮ INCLUDES THE COST OF ALL LABOR AND MATERIALS REQUIRED TO REPAIR SPALLED AREAS WITH QUICK-SET PATCHING MATERIALS.
- ⑯ COST OF ELASTOMERIC BEARING PADS, REINFORCING STEEL, AND RUBBER BONDING CEMENT TO BE INCLUDED IN ITEM NO. 615-02.02, PRESTRESSED CONCRETE BOX BEAM (17"x36").
- ⑰ INCLUDES ALL COSTS FOR INSTALLING THE BRIDGE DECK CRACK SEALER (HMWM) INCLUDING CRACK PREPARATION, CLEANING, LABOR AND ALL MISCELLANEOUS MATERIALS REQUIRED TO SEAL THE LONGITUDINAL PHASE CONSTRUCTION JOINTS AT TOP OF THE BRIDGE DECK AND PAVEMENT AT BRIDGE ENDS. ALSO INCLUDES LONGITUDINAL JOINT AT THE TOE OF THE PARAPET AND THE TRANSVERSE CONSTRUCTION JOINT BETWEEN THE BRIDGE DECK AND THE PAVEMENT AT BRIDGE ENDS ACCORDING TO STANDARD SPECIFICATIONS AND MANUFACTURER SPECIFICATIONS. THIS ITEM DOES NOT INCLUDE THE COST OF FURNISHING THE BRIDGE SEALANT ITSELF.
- ⑱ INCLUDES ALL COSTS FOR FURNISHING THE SEALANT (HMWM) FOR SEALING CRACKS. THE SEALANT (HMWM) SHALL BE IN ACCORDANCE WITH STANDARD SPECIFICATIONS.
- ⑲ INCLUDES THE COST FOR ALL LABOR AND MATERIALS (EXCEPT TEXTURE FINISH) TO CONSTRUCT THE NEW SINGLE SLOPE PARAPET WITH PARAPET DRAINS AND DECORATIVE SURFACE FINISH. FOR DETAILS, SEE STD. DWG. STD-1-ISS, BR-119-152 AND BR-119-167. CARE SHALL BE TAKEN TO ASSURE THE PROFILE OF THE PARAPET IS CORRECT FOR THE ENTIRE LENGTH OF THE BRIDGE.
- ⑳ INCLUDES 60 L.F. FOR PHASE I AND 30 L.F. FOR PHASE II. SEE STANDARD DRAWING STD-2-1, BRIDGE MOUNTED INTERCONNECTED PORTABLE BARRIER RAIL - 2005.

**UNOFFICIAL SET**

NOT FOR BIDDING

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
ESTIMATED BRIDGE QUANTITIES  
AND MISC. DETAILS

BRIDGE NO. 64-SR129-2.51  
STATE ROUTE 129  
OVER UNNAMED BRANCH  
MOORE COUNTY  
2015

DESIGNED BY D. THOMPSON/D. KEATON DATE \_\_\_\_\_  
DRAWN BY ANGELA MOORE DATE \_\_\_\_\_  
SUPERVISED BY DARRELL JAMES DATE \_\_\_\_\_  
CHECKED BY JAMIE GILLESPIE DATE \_\_\_\_\_





## ESTIMATED ROADWAY QUANTITIES

ITEM NO.	DESCRIPTION	UNIT	QUANTITY
① 105-01	CONSTRUCTION STAKES, LINES AND GRADES	LS	1
② 202-03.01	REMOVAL OF ASPHALT PAVEMENT	S.Y.	89
⑫ 203-01	ROAD AND DRAINAGE EXCAVATION (UNCLASSIFIED)	C.Y.	36
⑤③ 209-05	SEDIMENT REMOVAL	C.Y.	5
⑭⑤③ 209-08.02	TEMPORARY SILT FENCE (WITH BACKING)	L.F.	472
209-08.08	ENHANCED ROCK CHECK DAM	EACH	2
⑤③ 209-09.04	SEDIMENT FILTER BAG (15'X10')	EACH	2
⑤④ 209-65.04	TEMPORARY INSTREAM DIVERSION	L.F.	126
303-01	MINERAL AGGREGATE, TYPE A BASE, GRADING D	TON	30
⑮⑤ 303-10.01	MINERAL AGGREGATE (SIZE 57)	TON	12
307-01.08	ASPHALT CONCRETE MIX (PG64-22) (BPMB-HM) GRADING B-M2	TON	13
402-01	BITUMINOUS MATERIAL FOR PRIME COAT (PC)	TON	1
403-01	BITUMINOUS MATERIAL FOR TACK COAT (TC)	TON	1
411-01.10	ACS MIX (PG64-22) GRADING D	TON	49
411-12.03	SCORING FOR RUMBLE STRIPE (NON-CONTINUOUS) (8IN WIDTH)	L.M.	0.10
415-01.02	COLD PLANING BITUMINOUS PAVEMENT	S.Y.	702
705-01.01	GUARDRAIL AT BRIDGE ENDS	L.F.	108
705-04.07	TAN ENERGY ABSORBING TERM (NCHRP 350, TL3)	EACH	4
705-04.09	EARTH PAD FOR TYPE 38 GR END TREATMENT	EACH	4
⑥ 705-08.51	PORTABLE IMPACT ATTENUATOR (NCHRP 350, TL-3)	EACH	2
709-05.05	MACHINED RIP-RAP (CLASS A-3)	TON	346
⑦ 712-01	TRAFFIC CONTROL	LS	1
712-02.02	INTERCONNECTED PORTABLE BARRIER RAIL	L.F.	204
⑧ 712-04.01	FLEXIBLE DRUMS (CHANNELIZING)	EACH	15
712-04.50	PORTABLE BARRIER RAIL DELINEATOR	EACH	7
712-05.03	WARNING LIGHTS (TYPE C)	EACH	15
⑨ 712-06	SIGNS (CONSTRUCTION)	S.F.	785
712-09.01	REMOVABLE PAVEMENT MARKING LINE	L.F.	4,884
712-09.04	REMOVABLE PAVEMENT MARKING (STOP LINE)	L.F.	26
712-09.05	REMOVABLE PAVEMENT MARKING (ARROW)	EACH	2
⑩ 713-16.01	CHANGEABLE MESSAGE SIGN UNIT	EACH	2
713-16.20	SIGNS (OM3-L)	EACH	2
713-16.21	SIGNS (OM3-R)	EACH	2
716-01.21	SNWPLWBLE PVMT MRKRS (BI-DIR) (1 COLOR)	EACH	5
716-12.02	ENHANCED FLATLINE THERMO PVMT MRKNG (6IN LINE)	L.M.	0.12
716-13.02	SPRAY THERMO PVMT MRKNG (60 MIL) (6" LINE)	L.M.	0.12
717-01	MOBILIZATION	LS	1
⑪ 730-40	TEMPORARY TRAFFIC SIGNAL SYSTEM	EACH	1
⑯ 740-10.03	GEOTEXTILE (TYPE III) (EROSION CONTROL)	S.Y.	148
⑤③ 740-11.01	TEMPORARY SEDIMENT TUBE (8 INCH) (STRAW WATTLES)	L.F.	80
⑫ 801-01	SEEDING (WITH MULCH)	UNIT	6
⑬ 801-03	WATER (SEEDING AND SODDING)	M.G.	1

### FOOTNOTES

- ① ALL DIMENSIONAL DETAILS SHOWN ON PLANS, INCLUDING ELEVATIONS, SHALL BE CHECKED BY THE CONTRACTOR TO ASSURE ACCURACY OF THE LAYOUT PRIOR TO CONSTRUCTION. ALL BRIDGE SUBSTRUCTURES SHALL BE CHECKED AS TO LOCATION, DIMENSIONAL LAYOUTS AND ELEVATIONS, BY MEANS OF TWO INDEPENDENT LAYOUT METHODS. ANY ERRORS AND APPARENT DISCREPANCIES FOUND IN PREVIOUS SURVEYS, OR IN EITHER THE SPECIFICATIONS OR SPECIAL PROVISIONS, SHALL BE CALLED TO THE ENGINEER'S ATTENTION BY THE CONTRACTOR IMMEDIATELY AND PRIOR TO PROCEEDING WITH WORK.
- ② UNIT PRICE INCLUDES REMOVAL OF ALL MATERIALS NECESSARY TO INSTALL 9'-0" PAVEMENT REINFORCEMENT SECTION AT BRIDGE ENDS.
- ③ SEDIMENT SHALL BE REMOVED FROM BEHIND THE SILT FENCE WITH WIRE BACKING, DEWATERING STRUCTURE, CATCH BASIN PROTECTION OR SEDIMENT TUBE WHEN IT HAS ACCUMULATED TO ONE-HALF THE ORIGINAL HEIGHT OF THE STRUCTURE AND PAID FOR UNDER ITEM NO. 209-05, SEDIMENT REMOVAL, C.Y.
- ④ INCLUDES ALL MATERIALS AND LABOR NECESSARY FOR CONSTRUCTION, MAINTENANCE, AND REMOVAL OF INSTREAM DIVERSION.
- ⑤ SEE SUBSECTION 209.07 OF THE STANDARD SPECIFICATIONS FOR MAINTENANCE REPLACEMENT. ALL QUANTITIES ARE TO BE USED AS DIRECTED BY THE ENGINEER.
- ⑥ THIS ITEM SHALL BE A PORTABLE ENERGY ABSORBING TERMINAL MEETING THE REQUIREMENTS OF NCHRP 350 FOR TEST LEVEL 3. EXAMPLES WOULD BE A QUAD-GUARD, A REACT 350 OR A TRACC. THE PAY ITEM WILL INCLUDE FURNISHING AND INSTALLING ALL COMPONENTS AS LISTED ON THE MANUFACTURERS BILL OF MATERIALS.
- ⑦ INCLUDES COST FOR REMOVAL OF EXISTING OR CONFLICTING MARKINGS.
- ⑧ INCLUDES RELOCATION AND INSTALLATION FOR EACH PHASE OF THE CONSTRUCTION SEQUENCE: PHASE I = 15 AND PHASE II = 15 ON STATE ROUTE 129.
- ⑨ BASED ON SECTION 712.10 OF STANDARD SPECIFICATIONS, 462 S.F. FOR WIDE LOAD DETOUR.
- ⑩ COORDINATE WITH T.D.O.T. CONSTRUCTION DIVISION FOR LOCATION AND MESSAGE FOR CHANGEABLE MESSAGE SIGN.
- ⑪ THE COST ASSOCIATED WITH COORDINATING THE TRAFFIC SIGNAL PHASING AND TIMING SHALL BE INCLUDED IN THE UNIT PRICE.
- ⑫ ITEM MAY BE INCREASED, DECREASED OR ELIMINATED AS DIRECTED BY THE ENGINEER.
- ⑬ INCLUDES 1 THOUSAND GALLONS FOR EROSION PREVENTION AND SEDIMENT CONTROL.
- ⑭ INCLUDES 174 LINEAR FEET FOR (TWO) 2 SEDIMENT FILTER BAGS.
- ⑮ FOR (TWO) 2 SEDIMENT FILTER BAGS.
- ⑯ FOR (TWO) 2 SEDIMENT FILTER BAGS.
- ⑰ INCLUDES 13 C.Y. OF EXCAVATION FOR DITCHES.

TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	2015	64009-4205-04	2B

UNOFFICIAL  
SET

NOT FOR  
BIDDING

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

BRIDGE NO. 64-SR129-2.51  
STATE ROUTE 129  
OVER UNNAMED BRANCH  
MOORE COUNTY  
2015

ESTIMATED  
ROADWAY  
QUANTITIES

# ROADWAY GENERAL NOTES

## GRADING

- (1) ANY AREA THAT IS DISTURBED OUTSIDE LIMITS OF CONSTRUCTION DURING THE LIFE OF THIS PROJECT SHALL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE.
- (2) THE CONTRACTOR SHALL NOT DISPOSE OF ANY MATERIAL EITHER ON OR OFF STATE-OWNED R.O.W. IN A REGULATORY FLOODWAY AS DEFINED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY WITHOUT APPROVAL BY SAME. ALL MATERIAL SHALL BE DISPOSED OF IN UPLAND (NON-WETLAND) AREAS AND ABOVE ORDINARY HIGH WATER OF ANY ADJACENT WATERCOURSE. THIS DOES NOT ELIMINATE THE NEED TO OBTAIN ANY OTHER LICENSES OR PERMITS THAT MAY BE REQUIRED BY ANY OTHER FEDERAL, STATE OR LOCAL AGENCY.

## SEEDING AND SODDING

- (3) ITEM NO. 801-01, SEEDING (WITH MULCH), SHALL BE USED WHERE EROSION CONTROL BLANKET OR SOD ARE NOT APPLIED.

## GUARDRAIL

- (4) THE PROPOSED GUARDRAIL, INCLUDING ANY ANCHOR SYSTEM, SHALL BE INSTALLED QUICKLY TO MINIMIZE TRAFFIC EXPOSURE TO ANY HAZARD. NO PAYMENT WILL BE MADE FOR A SECTION OF PROPOSED GUARDRAIL, INCLUDING ANCHORS, UNTIL IT IS COMPLETE IN PLACE.

## DRAINAGE

- (5) THE CONTRACTOR SHALL SHAPE DITCHES TO THE SPECIFIED DESIGN. THIS WORK WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COST WILL BE INCLUDED IN THE COST OF OTHER ITEMS.

## UTILITIES

- (6) THE LOCATIONS OF UTILITIES SHOWN WITHIN THESE PLANS ARE APPROXIMATE ONLY. EXACT LOCATIONS SHALL BE DETERMINED IN THE FIELD BY CONTACTING THE UTILITY COMPANIES INVOLVED. NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC., AT 1-800-351-1111 AS REQUIRED BY TCA 65-61-106 WILL BE REQUIRED.
- (7) UNLESS OTHERWISE NOTED, ALL UTILITY ADJUSTMENTS WILL BE PERFORMED BY THE UTILITY OR ITS REPRESENTATIVE. THE CONTRACTOR AND UTILITY OWNERS WILL BE REQUIRED TO COOPERATE WITH EACH OTHER IN ORDER TO EXPEDITE THE WORK REQUIRED BY THIS CONTRACT. ON CONTRACTS WHERE CONSTRUCTION STAKES, LINES, AND GRADES ARE CONTRACT ITEMS, THE CONTRACTOR WILL BE REQUIRED TO PROVIDE RIGHT-OF-WAY OR SLOPE STAKES, DITCH OR STREAM BED GRADES, OR OTHER ESSENTIAL SURVEY STAKING TO PREVENT CONFLICTS WITH THE HIGHWAY CONSTRUCTION. FREQUENTLY, THIS WILL BE REQUIRED AS THE FIRST ITEM OF WORK AND AT ANY LOCATION ON THE PROJECT DIRECTED BY THE ENGINEER.
- (8) THE CONTRACTOR WILL PROVIDE ALL NECESSARY PROTECTIVE MEASURES TO SAFEGUARD EXISTING UTILITIES FROM DAMAGE DURING CONSTRUCTION OF THIS PROJECT. IN THE EVENT THAT SPECIAL EQUIPMENT IS REQUIRED TO WORK OVER AND AROUND THE UTILITIES, THE CONTRACTOR WILL BE REQUIRED TO FURNISH SUCH EQUIPMENT. THE COST OF PROTECTING UTILITIES FROM DAMAGE AND FURNISHING SPECIAL EQUIPMENT WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (9) THE CONTRACTOR SHALL NOTIFY EACH INDIVIDUAL UTILITY OWNER OF HIS PLAN OF OPERATION IN THE AREA OF THE UTILITIES. PRIOR TO COMMENCING WORK, THE CONTRACTOR SHALL CONTACT THE UTILITY OWNERS AND REQUEST THEM TO PROPERLY LOCATE THEIR RESPECTIVE UTILITY ON THE GROUND. THIS NOTIFICATION SHALL BE GIVEN AT LEAST THREE (3) BUSINESS DAYS PRIOR TO COMMENCEMENT OF OPERATIONS AROUND THE UTILITY IN ACCORDANCE WITH TCA 65-31-106.

## MISCELLANEOUS

- (10) ALL DETOUR, ACCESS, SERVICE AND FRONTAGE ROADS SHALL BE CONSTRUCTED WITH A MINIMUM OF ONE (1) COURSE OF BASE MATERIAL BEFORE TRAFFIC IS INTERRUPTED ON EXISTING ROADS.
- (11) THE CONTRACTOR SHALL BE REQUIRED TO REMOVE AND RESET MAILBOXES WHERE AND AS DIRECTED BY THE ENGINEER.
- (12) NOTHING IN THE GENERAL NOTES OR SPECIAL PROVISIONS SHALL RELIEVE THE CONTRACTOR FROM HIS RESPONSIBILITIES TOWARD THE SAFETY AND CONVENIENCE OF THE GENERAL PUBLIC AND THE RESIDENTS ALONG THE PROPOSED CONSTRUCTION AREA.

## PAVEMENT MARKINGS

- (13) PERMANENT PAVEMENT LINE MARKINGS SHALL BE 6" ENHANCED FLATLINE THERMOPLASTIC INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-12.02, ENHANCED FLATLINE THERMO PAVT MRKNG (6IN LINE), L.M. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.
- (14) PERMANENT PAVEMENT LINE MARKINGS SHALL BE 6" SPRAY THERMOPLASTIC (60 MIL) INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-13.02, SPRAY THERMO PAVT MRKNG (60 MIL) (6IN LINE), L.M. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.

## DETOURS, LANE SHIFTS AND MEDIAN CROSS-OVERS

- (15) BEFORE OPENING THE LANE SHIFT TO TRAFFIC, THE TRANSITIONAL MARKINGS ON THE EXISTING ROADWAY MUST BE IN PLACE. THESE MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM 712-09.01 REMOVABLE PAVEMENT MARKING LINE, LIN. FT. ALL EXISTING MARKINGS IN THE AREA OF THESE TRANSITIONAL MARKINGS SHALL BE OBLITERATED AND ALL EXISTING RAISED PAVEMENT MARKERS SHALL BE REMOVED TO ELIMINATE CONFLICTING MARKINGS. REMOVAL OF THE EXISTING CONFLICTING MARKINGS AND RAISED PAVEMENT MARKERS WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COST WILL BE INCLUDED IN ITEM NO. 712-01 TRAFFIC CONTROL, LUMP SUM.

## PAVING

- (16) THE CONTRACTOR SHALL BE REQUIRED TO COLD PLANE AND PAVE IN THE DIRECTION OF TRAFFIC.

## RESURFACING

- (17) WHERE DIRECTED BY THE TDOT ENGINEER, THE CONTRACTOR SHALL BE REQUIRED TO SHAPE PUBLIC SIDE ROADS, BUSINESS ENTRANCES, AND PRIVATE DRIVES, AS WELL AS CLEANING OF EXISTING DRAINS BEFORE PLACING MATERIALS. ALL COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (18) ALL PUBLIC SIDE ROADS SHALL BE PAVED ONE PAVER WIDTH THROUGH THE INTERSECTION AS A MINIMUM. A SATISFACTORY TRANSITION FROM THE NEW PAVEMENT TO THE EXISTING GRADE OF THE INTERSECTING PUBLIC ROAD OR BUSINESS ENTRANCE SHALL BE PROVIDED. SHOULD THE PAVEMENT OF THE INTERSECTING PUBLIC ROAD BE DISTRESSED, THE RESURFACING WIDTH MAY BE INCREASED TO THE NORMAL RIGHT OF WAY LINE.
- (19) PRIVATE DRIVEWAYS, FIELD ENTRANCES, AND BUSINESS ENTRANCES WILL BE RESURFACED A PAVER WIDTH (LANE WIDTH) AS A MINIMUM. A PAVEMENT TAPER TO TRANSITION THE NEW PAVEMENT SHALL BE REQUIRED. IT SHALL BE BASED ON AN ADDITIONAL ONE FOOT OF WIDTH PER ONE INCH DEPTH OF PAVEMENT. IF THE SHOULDER IS NARROW ENOUGH THAT THE SUM OF THE SHOULDER AND THE TRANSITION ARE LESS THAN A PAVER WIDTH, THE TRANSITION SHALL OCCUR WITHIN THE PAVER WIDTH. IF THE SUM OF THE SHOULDER AND THE TRANSITION IS GREATER THAN A PAVER WIDTH (LANE WIDTH), THE TRANSITION SHALL OCCUR OUTSIDE OF THE PAVER WIDTH.
- (20) IN ALL CASES, THE LENGTH OF THE PAVEMENT TRANSITION, THE THICKNESS AND WIDTH OF THE RESURFACING AND ANY ADDITIONAL PAVEMENT MATERIALS SHALL BE AS DIRECTED BY THE TDOT ENGINEER.

## CONSTRUCTION WORK ZONE AND TRAFFIC CONTROL NOTES

- (21) ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE WEEK BEFORE NEEDED, IF THE SIGN FACE IS FULLY COVERED.
- (22) IF THE CONTRACTOR MOVES OFF THE PROJECT, HE SHALL COVER OR REMOVE ALL UNNEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OR REMOVAL, COVERING, AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION) PER SQUARE FOOT.
- (23) A LONG TERM BUT SPORADIC USE WARNING SIGN, SUCH AS A FLAGGER SIGN, MAY REMAIN IN PLACE WHEN NOT REQUIRED PROVIDED THE SIGN FACE IS FULLY COVERED.
- (24) TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.

- (25) USE OF BARRICADES, PORTABLE BARRIER RAILS, VERTICAL PANELS, AND DRUMS SHALL BE LIMITED TO THE IMMEDIATE AREAS OF CONSTRUCTION WHERE A HAZARD IS PRESENT. THESE DEVICES SHALL NOT BE STORED ALONG THE ROADWAY WITHIN THIRTY (30) FEET OF THE EDGE OF THE TRAVELED WAY BEFORE OR AFTER USE UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL INCREASE TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. THESE DEVICES SHALL BE REMOVED FROM THE CONSTRUCTION WORK ZONE WHEN THE ENGINEER DETERMINES THEY ARE NO LONGER NEEDED. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.
- (26) THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY, WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHEN THE LANE IS OPEN TO TRAFFIC UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO PARK WITHIN THIRTY (30) FEET OF A OPEN TRAFFIC LANE AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCTIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.
- (27) ALL DETOUR AND CONSTRUCTION SIGNING SHALL BE IN STRICT ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

## EROSION PREVENTION AND SEDIMENT CONTROL DISTURBED AREA

- (28) AREAS TO BE UNDISTURBED SHALL BE CLEARLY MARKED IN THE FIELD BEFORE CONSTRUCTION ACTIVITIES BEGIN. THE TOTAL AREA TO BE DISTURBED NOT INCLUDING EXCLUSIVE BORROW/WASTE AREAS AND STAGING AREAS IS 0.124 ACRES. IF THE TOTAL DISTURBED AREA FOR THE PROJECT SITE (INCLUDING STAGING AND EXCLUSIVE WASTE/BORROW AREAS) INCREASES TO MORE THAN 1 ACRE, THEN THE NATURAL RESOURCES OFFICE OF THE ENVIRONMENTAL DIVISION MUST BE NOTIFIED SO THAT A SWPPP CAN BE PREPARED.
- (29) PRE-CONSTRUCTION VEGETATIVE GROUND COVER SHALL NOT BE DESTROYED, REMOVED OR DISTURBED (I.E. CLEARING AND GRUBBING INITIATED) MORE THAN 15 CALENDAR DAYS (10 CALENDAR DAYS FOR SITES WITH AN ACTIVE ARAP) PRIOR TO GRADING OR EARTH MOVING ACTIVITIES UNLESS THE AREA IS MULCHED, SEEDED WITH MULCH, OR OTHER TEMPORARY COVER IS INSTALLED.
- (30) CLEARING, GRUBBING, AND OTHER DISTURBANCE TO RIPARIAN VEGETATION SHALL BE LIMITED TO THE MINIMUM NECESSARY FOR SLOPE CONSTRUCTION AND EQUIPMENT OPERATIONS. EXISTING VEGETATION SHOULD BE PRESERVED TO THE MAXIMUM EXTENT POSSIBLE. UNNECESSARY VEGETATION REMOVAL IS PROHIBITED.
- (31) ALL DISTURBED AREAS SHALL BE PROPERLY STABILIZED AS SOON AS PRACTICABLE. PRIORITY SHALL BE GIVEN TO FINISHING OPERATIONS AND PERMANENT EPSC MEASURE OVER TEMPORARY EPSC MEASURES ON ALL PROJECTS.
- (32) CONSTRUCTION SHALL BE SEQUENCED AND STAGED TO MINIMIZE THE EXPOSURE TIME GRADED OR DENUDED SOIL AREAS, PRESERVE TOPSOIL, AND MINIMIZE SOIL COMPACTION.
- (33) IF OFFSITE BORROW AND WASTE AREAS BECOME NECESSARY DURING THE LIFE OF THE PROJECT, THIS SUPPORT ACTIVITY SHALL BE ADDRESSED PER THE TDOT WASTE AND BORROW MANUAL.
- (34) EPSC MEASURES SHALL BE INSTALLED AND FUNCTIONAL PRIOR TO ANY EARTH MOVING OPERATIONS, AND SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD.

## SEDIMENT CONTROL

TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	2015	64009-4205-04	2C

UNOFFICIAL SET

NOT FOR BIDDING

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

BRIDGE NO. 64-SR129-2.51  
STATE ROUTE 129  
OVER UNNAMED BRANCH  
MOORE COUNTY  
2015

ROADWAY  
GENERAL  
NOTES

# ROADWAY GENERAL NOTES (CONT'D)

## EROSION PREVENTION AND SEDIMENT CONTROL SEDIMENT CONTROL (CONT'D)

- (35) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT THE OFF-SITE MIGRATION OR DEPOSIT OF SEDIMENT ON ROADWAYS USED BY THE GENERAL PUBLIC. IF SEDIMENT ESCAPES THE CONSTRUCTION SITE, OFF-SITE ACCUMULATIONS OF SEDIMENT THAT HAVE NOT REACHED A STREAM MUST BE REMOVED AT A FREQUENCY SUFFICIENT TO MINIMIZE OFF-SITE IMPACTS (E.G., FUGITIVE SEDIMENT THAT HAS ESCAPED THE CONSTRUCTION SITE AND HAS COLLECTED IN A STREET MUST BE REMOVED SO THAT IT IS NOT SUBSEQUENTLY WASHED INTO STORM SEWERS AND STREAMS BY THE NEXT RAIN AND/OR SO THAT IT DOES NOT POSE A SAFETY HAZARD TO USERS OF PUBLIC STREETS). ARRANGEMENTS CONCERNING REMOVAL OF SEDIMENT ON ADJOINING PROPERTY MUST BE SETTLED WITH THE ADJOINING PROPERTY OWNER BEFORE REMOVAL OF SEDIMENT.
  - (36) WATER PUMPED FROM WORK AREAS AND EXCAVATION MUST BE HELD IN SETTLING BASINS OR TREATED BY FILTRATION OR CHEMICAL TREATMENT PRIOR TO ITS DISCHARGE INTO SURFACE WATERS. ALL PHYSICAL AND/OR CHEMICAL TREATMENT WILL BE APPLIED IN ACCORDANCE WITH MANUFACTURER'S GUIDELINES AND FULLY DESCRIBED IN THE EPSC PLANS. WATER MUST BE HELD IN SETTLING BASINS UNTIL AT LEAST AS CLEAR AS THE RECEIVING WATERS. SETTLING BASINS SHALL NOT BE LOCATED CLOSER THAN 20 FEET FROM THE TOP BANK OF A STREAM. SETTLING BASINS AND SEDIMENT TRAPS SHALL BE PROPERLY DESIGNED ACCORDING TO THE SIZE OF THE DRAINAGE AREAS OR VOLUME OF WATER TO BE TREATED. TREATED WATER MUST BE DISCHARGED THROUGH A PIPE OR WELL-VEGETATED OR LINED CHANNEL, SO THAT THE DISCHARGE DOES NOT CAUSE EROSION OR SEDIMENT TRANSPORT. DISCHARGES FROM BASINS AND IMPOUNDMENTS SHALL UTILIZE OUTLET STRUCTURES THAT ONLY WITHDRAW WATER FROM NEAR THE SURFACE OF THE BASIN OR IMPOUNDMENT. DISCHARGES MUST NOT CAUSE AN OBJECTIONABLE COLOR CONTRAST WITH THE RECEIVING STREAM.
  - (37) CHECK DAMS SHALL BE USED WHERE RUNOFF IS CONCENTRATED. CLEAN ROCK, BRUSH, GABION, OR SANDBAG CHECK DAMS SHALL BE PROPERLY CONSTRUCTED TO REDUCE VELOCITY AND CONTROL EROSION.
  - (38) DELAYING PLANTING OF PERMANENT COVER VEGETATION UNTIL WINTER MONTHS OR DRY MONTHS SHOULD BE AVOIDED, IF POSSIBLE.
  - (39) OFFSITE VEHICLE TRACKING OF SEDIMENTS AND THE GENERATION OF DUST SHALL BE MINIMIZED. A STABILIZED CONSTRUCTION ACCESS (A POINT OF ENTRANCE/EXIT TO THE CONSTRUCTION PROJECT) SHALL BE PROVIDED, AS NEEDED, TO REDUCE THE TRACKING OF MUD AND DIRT ONTO PUBLIC ROADS BY CONSTRUCTION VEHICLES.
  - (40) TEMPORARY EPSC MEASURES MAY BE REMOVED AT THE BEGINNING OF THE WORKDAY, BUT MUST BE REPLACED AT THE END OF THE WORKDAY.
  - (41) ALL PHYSICAL AND/OR CHEMICAL TREATMENT WILL BE APPLIED IN ACCORDANCE WITH MANUFACTURER'S GUIDELINES AND DESCRIBED ON THE EPSC PLANS FOR ALL PROJECTS REQUIRING ADDITIONAL PHYSICAL OR CHEMICAL TREATMENT OF STORMWATER RUNOFF.
- ### STREAM/WETLAND
- (42) SOIL MATERIALS MUST BE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. EPSC MEASURES TO PROTECT WATER QUALITY MUST BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD. APPROPRIATE EPSC MEASURES MUST BE INSTALLED ALONG THE BASE OF ALL FILLS AND CUTS, ON THE DOWNHILL SIDE OF STOCKPILED SOIL, AND ALONG STREAM BANKS IN CLEARED AREAS TO PREVENT SEDIMENT MIGRATION INTO STREAMS IN ACCORDANCE WITH TDOT STANDARDS. THEY MUST BE INSTALLED ON THE CONTOUR, ENTRENCHED AND STAKED, AND EXTEND THE WIDTH OF THE AREA TO BE CLEARED.
  - (43) INSTREAM EPSC DEVICES REQUIRE THE ENVIRONMENTAL DIVISION'S PERMITS SECTION REVIEW AND MUST BE PROCESSED BY THE PERMITS SECTION TO OBTAIN TDEC, USACE, AND TVA PERMITS.
  - (44) THE OPERATION OF EQUIPMENT IN WATERS OF THE STATE/U.S., INCLUDING WETLANDS, SHALL BE ONLY AS SHOWN ON THE PROJECT PLANS AND/OR AS SO SPECIFIED IN THE ARAP/401, SECTION 404 PERMITS(S) AND/OR TVA26(A), IF APPLICABLE. ANY ADDITIONAL PERMITS REQUIRED BY THE CONTRACTOR'S METHOD OF OPERATION SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO OBTAIN, AFTER RECEIVING THE APPROVAL OF TDOT ENVIRONMENTAL DIVISION.
  - (45) STREAM BEDS SHALL NOT BE USED AS TRANSPORTATION ROUTES FOR CONSTRUCTION EQUIPMENT. TEMPORARY CROSSING MUST BE LIMITED TO ONE POINT PER STREAM AND EPSC MEASURES MUST BE USED WHERE THE STREAM BANKS ARE DISTURBED. WHERE THE STREAMBED IS NOT COMPOSED OF BEDROCK, A PAD OF CLEAN ROCK MUST BE USED AT THE CROSSING POINT AND CULVERTED TO PREVENT THE IMPOUNDMENT OF WATER FLOW. CLEAN ROCK IS ROCK OF VARIOUS TYPE AND SIZE, DEPENDING UPON APPLICATION, WHICH CONTAINS NO FINES, SOILS, OR OTHER WASTES OR CONTAMINANTS. OTHER MATERIALS USED FOR ALL TEMPORARY FILLS MUST BE COMPLETELY REMOVED IN THEIR ENTIRETY AFTER THE WORK IS COMPLETED AND THE AFFECTED AREAS RETURNED TO THEIR PREEXISTING ELEVATION. ALL TEMPORARY CROSSINGS MUST BE CONSTRUCTED IN ACCORDANCE WITH STD. DWG. EC-STR-25 UNLESS SPECIFICALLY ADDRESSED IN THE EPSC PLANS. ALTERNATIVELY, PLACING A TEMPORARY BRIDGE (BAILEY BRIDGE OR EQUIVALENT, TIMBERS, ETC.) FROM TOP OF BANK TO TOP OF BANK OR THE APPROPRIATE USE OF BARGES AT THE CROSSING TO AVOID DISTURBANCE OF THE STREAMBED IS AN ACCEPTABLE OPTION.
  - (46) HEAVY EQUIPMENT WORKING IN WETLANDS MUST BE PLACED ON MATS, OR OTHER MEASURES MUST BE TAKEN TO MINIMIZE SOIL DISTURBANCE UNLESS SPECIFICALLY ADDRESSED IN THE EPSC PLANS. ANY MATS AND OTHER MEASURES USED FOR HEAVY EQUIPMENT MUST BE REMOVED IN THEIR ENTIRETY AFTER THE WORK IS COMPLETED.
  - (47) WETLANDS SHALL NOT BE USED AS EQUIPMENT STORAGE, STAGING, OR TRANSPORTATION AREAS, UNLESS PROVIDED FOR IN THE PLANS.

## SPECIES

- (48) NO ACTIVITY MAY SUBSTANTIALLY DISRUPT THE MOVEMENT OF THOSE SPECIES OF AQUATIC LIFE INDIGENOUS TO THE WATER BODY, INCLUDING THOSE SPECIES THAT NORMALLY MIGRATE THROUGH THE AREA. THE SWPPP SHALL BE MODIFIED TO INCLUDE EPSC MEASURES TO PREVENT NEGATIVE IMPACTS TO LEGALLY PROTECTED STATE OR FEDERAL FAUNA OR FLORA OR AS INDICATED IN THE ECOLOGICAL STUDIES OR ON THE PERMIT(S).

## INSPECTION, MAINTENANCE, REPAIR

- (49) EPSC CONTROLS WILL BE INSTALLED AND MAINTAINED IN ACCORDANCE WITH TDOT STANDARD DRAWINGS AND GOOD ENGINEERING PRACTICES.
- (50) INSPECTION, REPAIR, AND MAINTENANCE OF EPSC MEASURES/STRUCTURES IS TO BE PERFORMED ON A REGULAR BASIS. SEDIMENT SHALL BE REMOVED FROM SEDIMENT CONTROL STRUCTURES WHEN THE DESIGN CAPACITY HAS BEEN REDUCED BY FIFTY PERCENT (50%). DURING SEDIMENT REMOVAL, THE CONTRACTOR SHALL TAKE CARE TO ENSURE THAT STRUCTURAL COMPONENTS OF EPSC MEASURES ARE NOT DAMAGED AND THUS MADE INEFFECTIVE. IF DAMAGE DOES OCCUR, THE CONTRACTOR SHALL REPAIR THE STRUCTURES AT THE CONTRACTOR'S OWN EXPENSE.
- (51) SEDIMENT REMOVED FROM SEDIMENT CONTROL STRUCTURES SHALL BE PLACED AND BE TREATED IN A MANNER SO THAT THE SEDIMENT IS CONTAINED WITHIN THE PROJECT LIMITS AND DOES NOT MIGRATE INTO WATERS OF THE STATE/U.S. COST FOR THIS TREATMENT IS TO BE INCLUDED IN PRICE BID FOR ITEM NO. 209-05 SEDIMENT REMOVAL, C.Y.
- (52) DISCHARGE POINTS SHALL BE INSPECTED TO ASCERTAIN WHETHER EPSC MEASURES ARE EFFECTIVE IN PREVENTING SIGNIFICANT IMPACTS TO SURROUNDING WATERS. WHERE DISCHARGE LOCATIONS ARE INACCESSIBLE, NEARBY DOWNSTREAM LOCATIONS SHALL BE INSPECTED. LOCATIONS WHERE VEHICLES ENTER AND EXIT THE SITE SHALL BE INSPECTED FOR EVIDENCE OF OFF-SITE ROADWAY SEDIMENT TRACKING.
- (53) THE TDOT PROJECT SUPERVISOR (OR THEIR DESIGNEE) AND THE CONTRACTOR'S SITE SUPERINTENDENT ARE RESPONSIBLE FOR INSPECTIONS, MAINTENANCE AND REPAIR ACTIVITIES ARE THE RESPONSIBILITY OF THE CONTRACTOR.
- (54) EPSC MEASURES FOUND TO BE INEFFECTIVE SHALL BE REPAIRED, REPLACED, OR MODIFIED BEFORE THE NEXT RAIN EVENT, IF POSSIBLE.

## MATERIALS AND STAGING

- (55) WASTE AND BORROW AREAS SHALL BE LOCATED IN NON-WETLAND AREAS AND ABOVE THE 100-YEAR, FEDERAL EMERGENCY MANAGEMENT AGENCY FLOODPLAIN. BORROW AND WASTE DISPOSAL AREAS SHALL NOT AFFECT ANY WATERS OF THE STATE/U.S. UNLESS THESE AREAS ARE SPECIFICALLY COVERED BY AN ARAP, 404, OR NPDES PERMIT, OBTAINED SOLELY BY THE CONTRACTOR.
- (56) THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR AND OBTAIN ANY NECESSARY ENVIRONMENTAL PERMITS OR APPROVALS, INCLUDING BUT NOT LIMITED TO ARCHAEOLOGY, ECOLOGY, HISTORICAL, HAZARDOUS MATERIALS, AIR AND NOISE, TDEC ARAP/401, USACE SECTION 404, TVA SECTION 26A, AND TDEC NPDES PERMITS, FROM FEDERAL, STATE AND/OR LOCAL AGENCIES REGARDING ANY STAGING AREAS TO BE USED. ANY SUCH PERMITS SHALL BE SUPPLIED TO THE TDOT PROJECT RESPONSIBLE PARTY PRIOR TO THE USE OF THE PERMITTED AREAS.

## PERMITS, PLANS, RECORDS

- (57) THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR AND OBTAIN ANY NECESSARY ENVIRONMENTAL PERMITS OR APPROVALS, INCLUDING BUT NOT LIMITED TO TDEC ARAP/401, USACE SECTION 404, TVA SECTION 26A, AND TDEC NPDES PERMITS, FROM FEDERAL, STATE AND/OR LOCAL AGENCIES REGARDING THE OPERATION OF ANY PROJECT-DEDICATED ASPHALT AND/OR CONCRETE PLANTS.
- (58) ANY DISAGREEMENT BETWEEN THE PROJECT PLANS, THE PROJECT AS CONSTRUCTED, AND THE PERMIT(S) ISSUED FOR THE PROJECT, SHALL BE BROUGHT TO THE ATTENTION OF THE TDOT PROJECT ENGINEER, THE ENVIRONMENTAL DIVISION, ROADWAY DESIGN DIVISION, AND HEADQUARTERS CONSTRUCTION OFFICE SHALL BE CONTACTED IN THESE INSTANCES AND DECIDE WHICH HAS PRECEDENCE AND WHETHER PERMIT OR PLANS REVISIONS ARE NEEDED. IN GENERAL, PERMIT CONDITIONS WILL PREVAIL.
- (59) ALL PROJECT RELATED ENVIRONMENTAL PERMITS SHALL BE MAINTAINED AT OR NEAR THE PROJECT SITE.
- (60) IF A CHANGE IN PROJECT SCOPE OCCURS DURING CONSTRUCTION, INCLUDING VALUE ENGINEERING, THE ENVIRONMENTAL DIVISION SHALL BE CONTACTED TO DETERMINE WHETHER PERMIT REVISIONS ARE NEEDED. THE ROADWAY DESIGN DIVISION SHALL BE CONTACTED TO DETERMINE IF ANY PLAN REVISIONS ARE NEEDED.
- (61) THE EPSC PLAN SHALL BE UPDATED WHENEVER EPSC INSPECTIONS INDICATE, OR WHERE STATE OR FEDERAL OFFICIALS DETERMINE EPSC MEASURES ARE PROVING INEFFECTIVE IN ELIMINATING OR SIGNIFICANTLY MINIMIZING POLLUTANT SOURCES OR ARE OTHERWISE NOT ACHIEVING THE GENERAL OBJECTIVES OF CONTROLLING POLLUTANTS IN STORM WATER DISCHARGES ASSOCIATED WITH THE CONSTRUCTION ACTIVITY.
- (62) THE TDOT PROJECT RESPONSIBLE PARTY (INCLUDING TDOT STAFF, CONSULTANTS AND CONTRACTOR STAFF) RESPONSIBLE FOR THE IMPLEMENTATION AND MAINTENANCE OF EPSC PLANS SHALL SUCCESSFULLY COMPLETE THE TDEC "LEVEL 1 - FUNDAMENTALS OF EROSION PREVENTION AND SEDIMENT CONTROL FOR CONSTRUCTION SITES" COURSE AND ANY REFRESHER COURSES AS REQUIRED TO MAINTAIN CERTIFICATION. A COPY OF CERTIFICATION RECORDS FOR THE COURSES SHALL BE KEPT ON SITE AND AVAILABLE UPON REQUEST.

- (63) THE EPSC PLAN IS TO SERVE AS AN INITIAL GUIDE FOR SITE PERSONNEL AS THE CONSTRUCTION PROCESS DEVELOPS. IT MUST BE AMENDED, MODIFIED, AND UPDATED WHENEVER A CHANGE IN THE DESIGN OR CONSTRUCTION OF THE PROJECT OCCURS. THE PHASES DEPICTED IN THE EPSC PLANS MAY NOT COINCIDE WITH THE ACTUAL PHASES OF CONSTRUCTION ESTABLISHED BY THE CONTRACTOR DURING CONSTRUCTION. THUS MODIFICATIONS WILL BE REQUIRED TO ENSURE THE EPSC PLAN IS MAINTAINED TO DEPICT CURRENT SITE CONDITIONS. IT SHOULD BE MAINTAINED SUCH THAT IT WILL ALWAYS REFLECT THE MEASURES THAT ARE INSTALLED DURING THE VARIOUS STAGES OF CONSTRUCTION. IT IS IMPRACTICAL TO DETERMINE ALL THE INTERMEDIATE PHASES OF CONSTRUCTION THAT WILL OCCUR. THUS THESE DOCUMENTS WILL HAVE TO BE UPDATED THROUGHOUT THE LIFE OF THE CONSTRUCTION PROJECT.

## LITTER, DEBRIS, WASTE, PETROLEUM

- (64) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT LITTER, CONSTRUCTION DEBRIS, AND CONSTRUCTION WASTES FROM ENTERING WATERS OF THE STATE/U.S. THESE MATERIALS WILL BE PICKED UP AND REMOVED FROM STORMWATER EXPOSURE PRIOR TO ANTICIPATED STORM EVENTS. AFTER USE, MATERIALS USED FOR EPSC WILL BE REMOVED FROM THE SITE.
  - (65) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS TO ENSURE THAT PETROLEUM PRODUCTS OR OTHER CHEMICAL POLLUTANTS ARE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. ALL EQUIPMENT REFUELING, SERVICING, AND STAGING AREAS SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL LAWS, RULES, REGULATIONS, AND ORDINANCES, INCLUDING THOSE OF THE NATIONAL FIRE PROTECTION ASSOCIATION (NFPA). APPROPRIATE CONTAINMENT MEASURES FOR THESE AREAS SHALL BE USED. ALL SPILLS MUST BE REPORTED TO THE APPROPRIATE AGENCY, AND MEASURES SHALL BE TAKEN IMMEDIATELY TO PREVENT THE POLLUTION OF WATERS OF THE STATE/U.S., INCLUDING GROUNDWATER, SHOULD A SPILL OCCUR.
  - (66) IF PORTABLE SANITARY FACILITIES ARE PROVIDED ON CONSTRUCTION SITES, SANITARY WASTE WILL BE COLLECTED FROM THE PORTABLE UNITS IN A TIMELY MANNER BY A LICENSED WASTE MANAGEMENT CONTRACTOR OR AS REQUIRED BY ANY LOCAL REGULATIONS. THE CONTRACTOR WILL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF SANITARY WASTE.
  - (67) ALL HAZARDOUS WASTE MATERIALS WILL BE DISPOSED OF IN A MANNER WHICH IS COMPLIANT WITH LOCAL OR STATE REGULATIONS. SITE PERSONNEL WILL BE INSTRUCTED IN THESE PRACTICES, AND THE INDIVIDUAL DESIGNATED AS THE CONTRACTOR'S ON-SITE REPRESENTATIVE WILL BE RESPONSIBLE FOR SEEING THAT THESE PRACTICES ARE FOLLOWED. THE CONTRACTOR WILL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF HAZARDOUS MATERIAL.
  - (68) WASTE MATERIAL (EARTH, ROCK, ASPHALT, CONCRETE, ETC.) NOT REQUIRED FOR THE CONSTRUCTION OF THE PROJECT WILL BE DISPOSED OF BY THE CONTRACTOR. THE CONTRACTOR WILL OBTAIN ANY AND ALL NECESSARY PERMITS INCLUDING, BUT NOT LIMITED TO NPDES, AQUATIC RESOURCES ALTERATION PERMIT(S) CORPS OF ENGINEERS SECTION 404 PERMITS, AND TVA SECTION 26A PERMITS TO DISPOSE OF WASTE MATERIALS.
- ### SPILL PREVENTION, MANAGEMENT AND NOTIFICATION
- (69) ONLY NEEDED PRODUCTS WILL BE STORED ON-SITE BY THE CONTRACTOR. THE CONTRACTOR WILL STORE ALL MATERIALS UNDER COVER AND IN APPROPRIATE CONTAINERS. PRODUCTS MUST BE STORED IN ORIGINAL CONTAINERS AND LABELED. MATERIAL MIXING WILL BE CONDUCTED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS. THE CONTRACTOR'S SITE SUPERINTENDENT WILL INSPECT MATERIALS STORAGE AREAS REGULARLY TO ENSURE PROPER USE AND DISPOSAL.
  - (70) WHEN POSSIBLE, ALL PRODUCTS WILL BE USED COMPLETELY BEFORE PROPERLY DISPOSING OF THE CONTAINER OFF SITE. THE MANUFACTURER'S DIRECTIONS FOR DISPOSAL OF MATERIALS AND CONTAINERS WILL BE FOLLOWED.
  - (71) WHEEL WASH WATER WILL BE COLLECTED AND ALLOWED TO SETTLE OUT SUSPENDED SOLIDS PRIOR TO DISCHARGE. WHEEL WASH WATER WILL NOT BE DISCHARGED DIRECTLY INTO ANY STORMWATER SYSTEM OR STORMWATER TREATMENT SYSTEM.
  - (72) ALL ON-SITE VEHICLES WILL BE MONITORED FOR LEAKS AND RECEIVE REGULAR PREVENTIVE MAINTENANCE TO REDUCE THE CHANCE OF LEAKAGE.

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DEPARTMENT OF TRANSPORTATION

**ROADWAY  
GENERAL  
NOTES**

BRIDGE NO. 64-SR129-2.51  
STATE ROUTE 129  
OVER UNNAMED BRANCH  
MOORE COUNTY  
2015

## ROADWAY GENERAL AND SPECIAL NOTES (CONT'D)

### EROSION PREVENTION AND SEDIMENT CONTROL SPILL PREVENTION, MANAGEMENT AND NOTIFICATION (CONT'D)

- (73) FERTILIZERS WILL BE APPLIED ONLY IN THE AMOUNTS SPECIFIED BY TDOT. ONCE APPLIED, FERTILIZERS WILL BE WORKED INTO THE SOIL TO LIMIT THE EXPOSURE TO STORMWATER.
- (74) ALL PAINT CONTAINERS WILL BE TIGHTLY SEALED AND STORED WHEN NOT REQUIRED FOR USE. EXCESS PAINT WILL BE DISPOSED OF ACCORDING TO THE MANUFACTURER'S INSTRUCTIONS AND APPLICABLE STATE AND LOCAL REGULATIONS.
- (75) CONTRACTORS WILL PROVIDE DESIGNATED TRUCK WASHOUT AREAS ON THE SITE. THESE AREAS MUST BE SELF CONTAINED AND NOT CONNECTED TO ANY STORMWATER OUTLET OF THE SITE. UPON COMPLETION OF CONSTRUCTION WASHOUT AREAS WILL BE PROPERLY STABILIZED. WASH DOWN OR WASTE DISCHARGE OF CONCRETE TRUCKS WILL NOT BE PERMITTED ON-SITE UNLESS PROPER SETTLEMENT AREAS HAVE BEEN PROVIDED IN ACCORDANCE WITH BOTH STATE AND FEDERAL REGULATIONS.
- (76) APPROPRIATE CLEANUP MATERIALS AND EQUIPMENT WILL BE MAINTAINED BY THE CONTRACTOR IN THE MATERIALS STORAGE AREA ON-SITE AND UNDER COVER. SPILL RESPONSE EQUIPMENT WILL BE INSPECTED AND MAINTAINED BY THE CONTRACTOR AS NECESSARY TO REPLACE ANY MATERIALS USED IN SPILL RESPONSE ACTIVITIES.
- (77) ALL SPILLS WILL BE CLEANED IMMEDIATELY AFTER DISCOVERY AND THE MATERIALS DISPOSED OF PROPERLY. THE SPILL AREA WILL BE KEPT WELL VENTILATED AND PERSONNEL WILL WEAR APPROPRIATE PROTECTIVE CLOTHING TO PREVENT INJURY FROM CONTACT WITH A HAZARDOUS SUBSTANCE.
- (78) THE CONTRACTOR'S SITE SUPERINTENDENT WILL BE THE SPILL PREVENTION AND CLEANUP COORDINATOR. THE CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT THE SITE SUPERINTENDENT HAS HAD APPROPRIATE TRAINING FOR HAZARDOUS MATERIALS HANDLING, SPILL MANAGEMENT, AND CLEANUP.
- (79) IF OIL SHEEN IS OBSERVED ON SURFACE WATER (E.G. SETTLING PONDS, DETENTION PONDS, SWALES), ACTION WILL BE TAKEN IMMEDIATELY TO REMOVE THE MATERIAL CAUSING THE SHEEN. THE CONTRACTOR WILL USE APPROPRIATE MATERIALS TO CONTAIN AND ABSORB THE SPILL. THE SOURCE OF THE OIL SHEEN WILL ALSO BE IDENTIFIED AND REMOVED OR REPAIRED AS NECESSARY TO PREVENT FURTHER RELEASES.
- (80) WHERE A RELEASE CONTAINING A HAZARDOUS SUBSTANCE IN AN AMOUNT EQUAL TO OR IN EXCESS OF A REPORTABLE QUANTITY ESTABLISHED UNDER EITHER 40 CFR 117 OR 40 CFR 302 OCCURS DURING A 24 HOUR PERIOD, SEE THE LATEST TENNESSEE GENERAL PERMIT NO. TNRI00000 STORMWATER DISCHARGES FROM CONSTRUCTION ACTIVITIES SECTION 5.1 FOR REPORTING REQUIREMENTS.

## SPECIAL NOTES

### DEMOLITION, REPAIR, OR REHABILITATION OF BRIDGES

- (81) THE CONTRACTOR SHALL VERIFY THAT AN ASBESTOS SURVEY HAS BEEN COMPLETED PRIOR TO ANY DEMOLITION, REPAIR OR REHABILITATIONS ACTIVITIES (NOT INCLUDING ASPHALT MILLING OR OVERLAY).
- (82) ASBESTOS-CONTAINING MATERIALS (ACM) ABATEMENT IS THE RESPONSIBILITY OF THE CONTRACTOR AND SHALL BE COMPLETED PRIOR TO ANY DEMOLITION, REPAIR OR REHABILITATION OF BRIDGE(S). ABATEMENT SHOULD BE ACCOMPLISHED PER SP202ACM SPECIAL PROVISION REGARDING REMOVAL OF ASBESTOS-CONTAINING MATERIALS. STATE OF TENNESSEE ASBESTOS ACCREDITATION REQUIREMENTS (TCA 1200-01-20) MANDATE THAT ACM ABATEMENT WORK BE PERFORMED BY AN ACCREDITED FIRM (CONTRACTOR) USING ACCREDITED ABATEMENT WORKERS AND SUPERVISORS.
- (83) THE CONTRACTOR SHALL BE RESPONSIBLE FOR SUBMITTING A NOTICE TO THE TDEC, DIVISION OF AIR POLLUTION CONTROL TEN (10) DAYS IN ADVANCE OF ANY ACM ABATEMENT, DEMOLITION, OR MAJOR REPAIR INVOLVING THE REMOVAL/REPLACEMENT OF A STRUCTURAL COMPONENT.

### ENVIRONMENTAL

- (84) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION COMPREHENSIVE INSPECTION OFFICE SHALL BE INVITED TO ALL PRE-CONSTRUCTION MEETINGS.

### ECOLOGY

- (85) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION OR A DESIGNEE WILL ADVISE THE CONTRACTOR DURING THE PRE-CONSTRUCTION MEETING CONCERNING WHEN ENVIRONMENTAL DIVISION PERSONNEL OR DESIGNATED CONSULTANT WILL NEED TO BE ON-SITE FOR WORK BEING DONE WHICH COULD AFFECT THE STREAM OR SPECIES.
- (86) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION OR A DESIGNEE WILL ATTEND THE PRE-CONSTRUCTION MEETING FOR ALL PROJECTS WHICH HAVE THREATENED OR ENDANGERED SPECIES OR CRITICAL HABITAT PROXIMAL TO SCHEDULED BRIDGE WORK. THIS WILL PROVIDE THE OPPORTUNITY TO ENSURE THAT PERSONNEL INCLUDING THE CONTRACTOR'S PERSONNEL AND SUBCONTRACTORS ARE MADE AWARE OF THE NECESSARY PRECAUTIONS WHICH MUST BE FOLLOWED.
- (87) ALL BRIDGE PROJECTS WITH THREATENED OR ENDANGERED SPECIES OR CRITICAL HABITAT IDENTIFIED MUST HAVE MEASURES IN PLACE TO CONTAIN CONCRETE DUST, CEMENT DUST AND ALL OTHER MATERIALS. THESE MATERIALS ARE NOT ALLOWED TO ENTER THE STREAM.

### PROJECT COMMITMENTS

- (88) SEE PROJECT COMMITMENTS, SHEET 1B, FOR DETAILS RELATING TO SPECIAL ENVIRONMENTAL COMMITMENTS REQUIRED BY THIS PROJECT.

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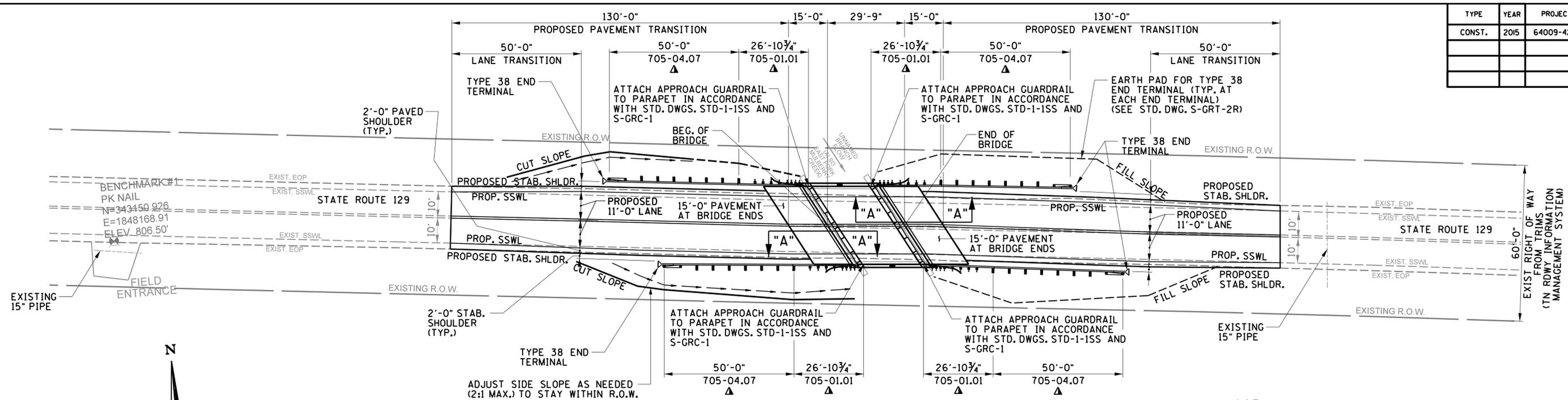
STATE OF TENNESSEE

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

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MOORE COUNTY  
2015

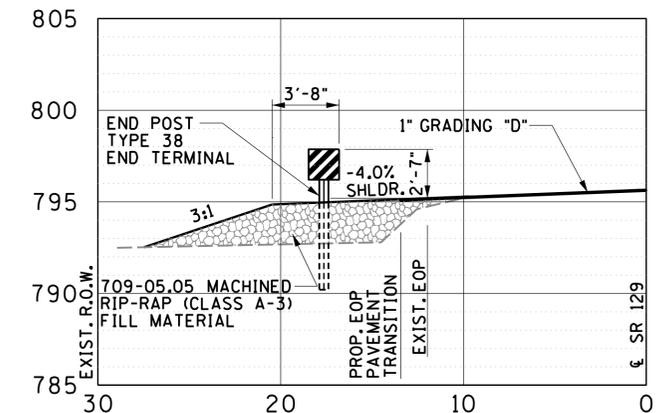
ROADWAY  
GENERAL AND  
SPECIAL NOTES

TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	2015	64009-4205-04	2F

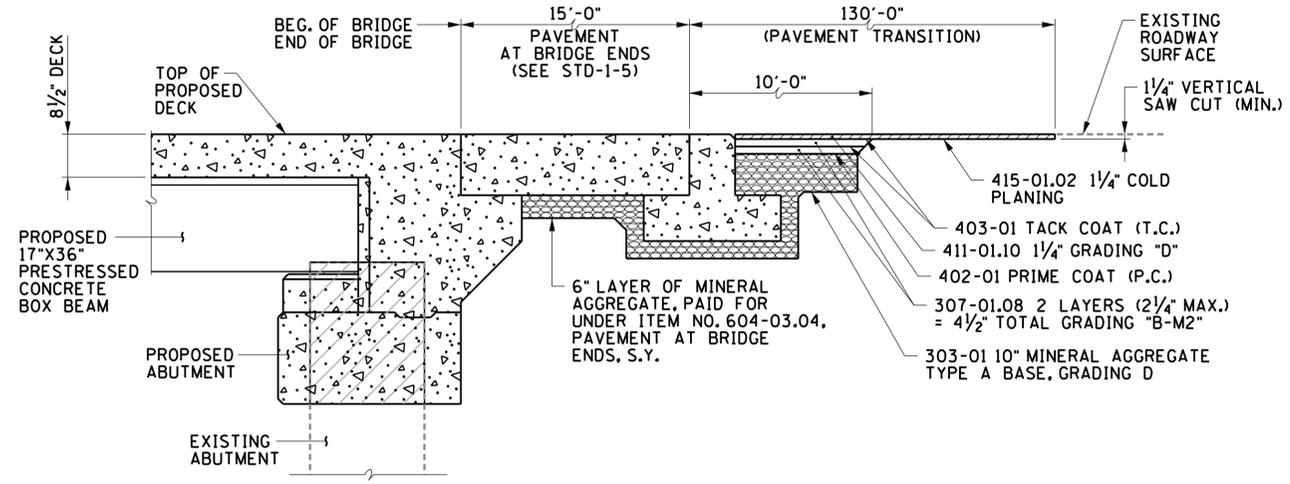


**PLAN**

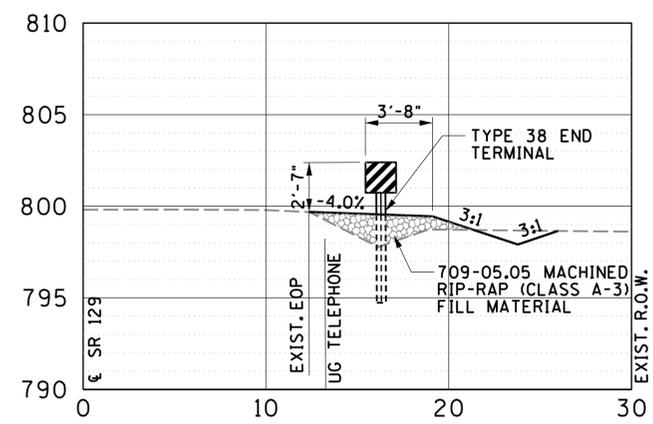
- DENOTES: PROPOSED BRIDGE PARAPET DRAINS (2 TOTAL) SHALL BE IN ACCORDANCE WITH STD. DWG. STD-1-1SS (ITEM NO. 620-05.01)
- ▲ DENOTES: GUARDRAIL SHALL BE IN ACCORDANCE WITH STD. DWGS. S-PL-3, S-GRC-1, AND S-GRT-2
- NOTE: UNNAMED BRANCH ALSO KNOWN AS EAST FORK MULBERRY CREEK.
- NOTE: SEE STD. DWGS. S-PL-3, S-PL-6, S-GR31-1, S-GRC-1, S-GRT-2, S-GRT-2R, AND S-GRA-3 FOR INSTALLATION DETAILS.
- NOTE: SEE SHEET NO. 2L (UTILITIES) FOR BENCHMARK #3 LOCATION.
- NOTE: USE CAUTION TO LOCATE AND AVOID UNDERGROUND TELEPHONE LINES WHEN INSTALLING NEW GUARDRAIL. SEE SHEET NO. 2L (UTILITIES) FOR TELEPHONE LOCATIONS.



**GUARDRAIL AND EARTH PAD TYPICAL FILL SECTION DETAIL**  
N.T.S.



**SECTION "A-A"**  
(P.A.B.E AND PAVEMENT TRANSITION)  
(AT ABUTMENT NO. 1 AND NO. 2)  
N.T.S.



**GUARDRAIL AND EARTH PAD TYPICAL CUT SECTION DETAIL**  
N.T.S.

**UNOFFICIAL SET**  
NOT FOR BIDDING

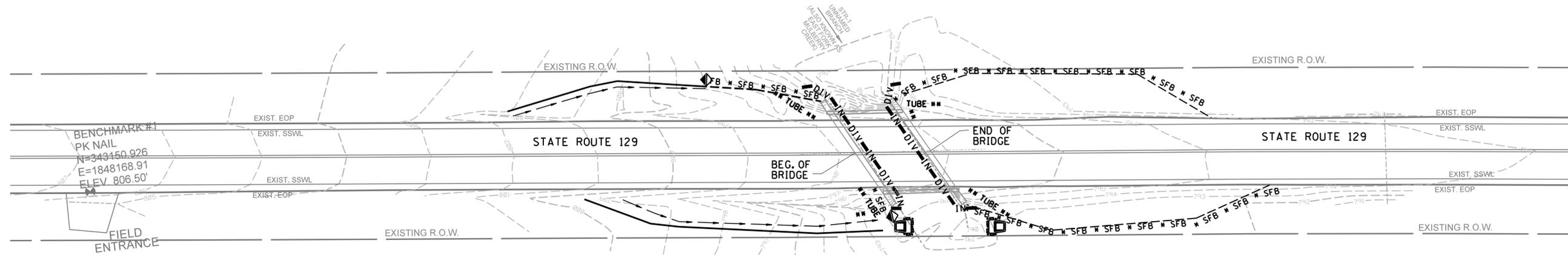
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ROADWAY  
DETAILS

EPSC LEGEND		
	SEDIMENT FILTER BAG	EC-STR-2
	SILT FENCE WITH WIRE BACKING (SFB)	EC-STR-3C
	ENHANCED ROCK CHECK DAM (V-DITCH)	EC-STR-6A
	INSTREAM DIVERSION	EC-STR-30 EC-STR-30A
	SEDIMENT TUBE	EC-STR-37

TYPE	YEAR	PROJECT NO.	SHEET NO.
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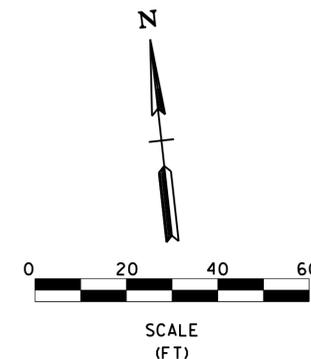
**PLAN**

AREA OF DISTURBANCE = 5,402 S.F. (0.124 ACRES)

- NOTE: SILT FENCE SHALL BE INSTALLED ON EXISTING GROUND CONTOURS WITH A 3' MINIMUM UPHILL TURN ON EACH END.
- NOTE: SEEDING (WITH MULCH) SHALL BE PLACED WITH A 4' MINIMUM WIDTH AFTER REMOVAL OF THE SILT FENCE.
- NOTE: ALL WORK AND EQUIPMENT TO REMAIN INSIDE EXISTING RIGHT OF WAY
- NOTE: SEE SHEET NO. 2B (ESTIMATED ROADWAY QUANTITIES) FOR FOOTNOTES TO EACH ITEM.
- NOTE: SEE SHEET NO. 2L (UTILITIES) FOR BENCHMARK#3 LOCATION.
- NOTE: STR-1 UNNAMED BRANCH ALSO KNOWN AS EAST FORK MULBERRY CREEK.

- NOTE: THE CONTRACTOR SHALL TAKE APPROPRIATE MEASURES BELOW THE BRIDGE DURING THE REMOVAL OF CONCRETE FROM SUBSTRUCTURES AND DECK TO PREVENT ANY DEBRIS FROM FALLING INTO THE WATER. COST FOR CONTAINMENT WILL NOT BE PAID DIRECTLY BUT SHALL BE INCLUDED IN ITEMS BID ON.
- NOTE: THE CONTRACTOR SHALL REMOVE ANY DEBRIS THAT FALLS ONTO THE AREAS BELOW THE BRIDGE, AND ANY PAINT CHIPS SHALL BE HANDLED AND DISPOSED OF PER THE SPECIFICATIONS SET FORTH IN THE GENERAL NOTES.
- NOTE: RIGHT-OF-WAY LOCATIONS DETERMINED FROM TRIMS (TN RDWY INFORMATION MANAGEMENT SYSTEM) AND TN PROPERTY VIEWER IMAGERY.

EROSION CONTROL QUANTITIES			
ITEM NO.	DESCRIPTION	UNIT	QUANTITY
209-05	SEDIMENT REMOVAL	C.Y.	5
209-08.02	TEMPORARY SILT FENCE (WITH BACKING)	L.F.	472
209-08.08	ENHANCED ROCK CHECK DAM (V-DITCH)	EACH	2
209-09.04	SEDIMENT FILTER BAG (15'X10')	EACH	2
209-65.04	TEMPORARY INSTREAM DIVERSION	L.F.	126
303-10.01	MINERAL AGGREGATE (SIZE 57)	TON	12
740-10.03	GEOTEXTILE (TYPE III) (EROSION CONTROL)	S.Y.	148
740-11.01	TEMPORARY SEDIMENT TUBE (8 INCH) (STRAW WATTLES)	L.F.	80
801-01	SEEDING (WITH MULCH)	UNIT	6
801-03	WATER (SEEDING AND SODDING)	M.G.	1



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**UNOFFICIAL SET**  
  
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EROSION CONTROL PLAN  
AND  
SPECIAL NOTES



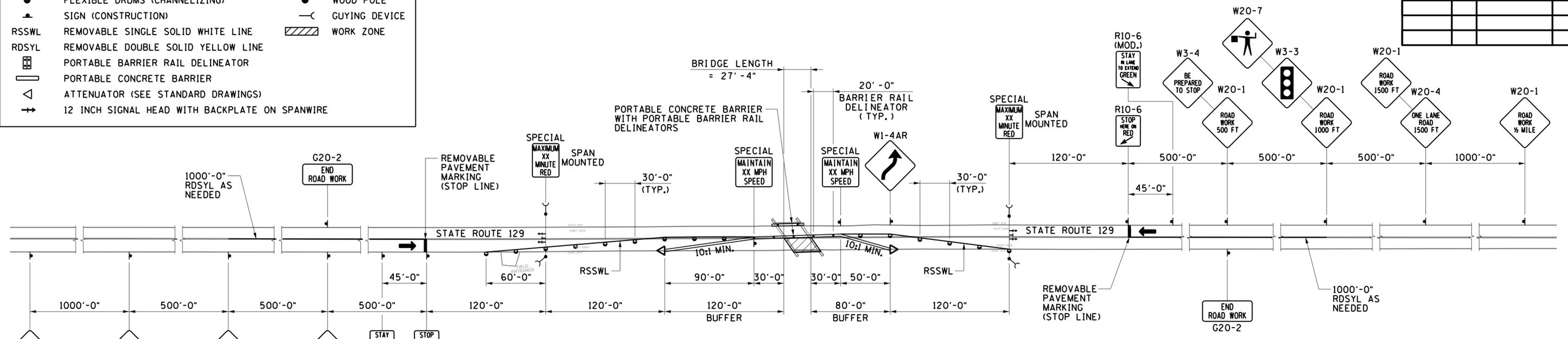
TENNESSEE D.O.T. DESIGN DIVISION

TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	2015	64009-4205-04	2J

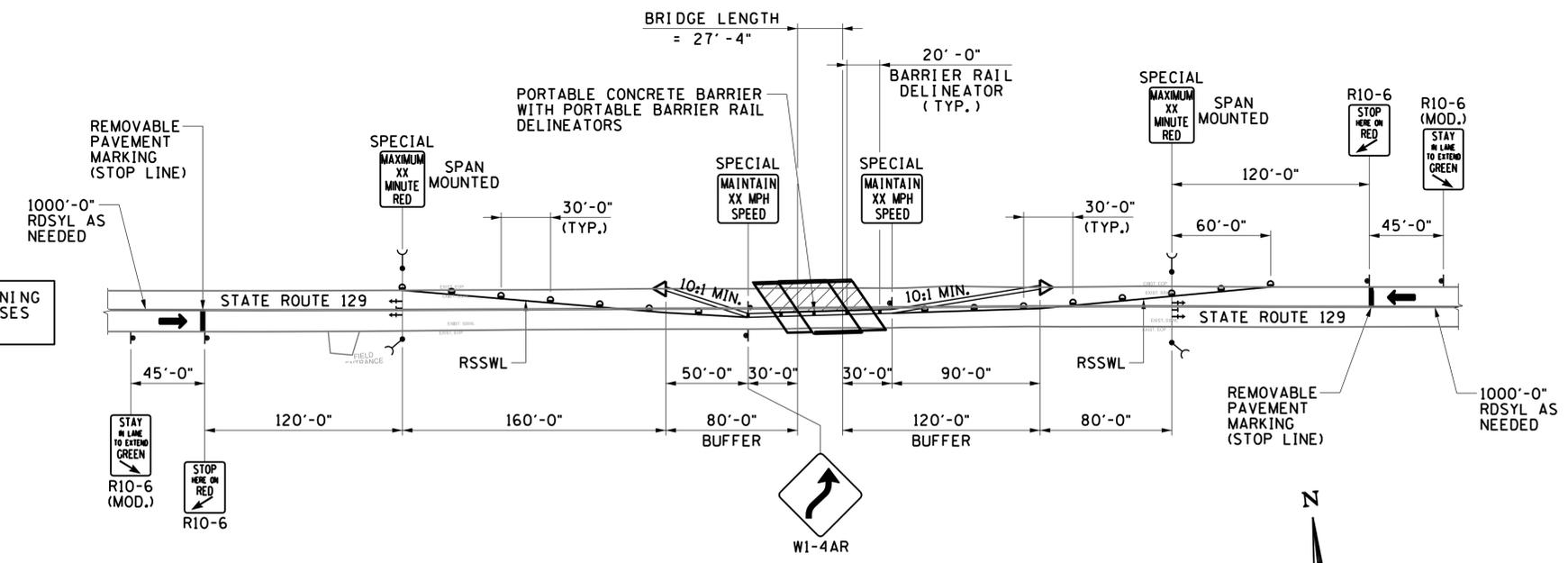
### TRAFFIC CONTROL LEGEND

- FLEXIBLE DRUMS (CHANNELIZING)
- SIGN (CONSTRUCTION)
- RSSWL REMOVABLE SINGLE SOLID WHITE LINE
- RDSYL REMOVABLE DOUBLE SOLID YELLOW LINE
- PORTABLE BARRIER RAIL DELINEATOR
- PORTABLE CONCRETE BARRIER
- ATTENUATOR (SEE STANDARD DRAWINGS)
- 12 INCH SIGNAL HEAD WITH BACKPLATE ON SPANWIRE
- WOOD POLE
- GUYING DEVICE
- WORK ZONE

### PHASE I TRAFFIC CONTROL PLAN



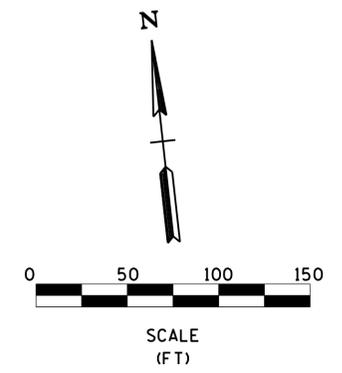
### PHASE II TRAFFIC CONTROL PLAN



ADVANCE WARNING SIGNING TYPICAL FOR ALL PHASES OF CONSTRUCTION

### SPECIAL NOTES

- CONTRACTOR SHALL REMOVE OR COVER ANY EXISTING STRIPING THAT CONFLICTS WITH TEMPORARY TRAFFIC CONTROL STRIPING. CONTRACTOR TO REPLACE STRIPING TO PRECONSTRUCTION DISPOSITION PRIOR TO OPENING ROAD TO NORMAL TRAFFIC.
- COORDINATE WITH T.D.O.T. CONSTRUCTION DIVISION FOR LOCATION AND MESSAGE FOR CHANGEABLE MESSAGE SIGN.



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STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLAN  
STATE ROUTE 129



TENNESSEE D.O.T. DESIGN DIVISION

FILE NO.

**REQUIRED LOCATION REQUEST INFORMATION**

NAME OF CALLER  
TELEPHONE NUMBER  
BEST TIME TO CALL  
COUNTY  
TOWN  
STREET ADDRESS  
START DATE START TIME  
TYPE OF WORK  
BLASTING ?  
WORK BEING DONE BY  
WORK BEING DONE FOR

CALL THREE WORKING DAYS BEFORE YOU DIG  
1-800-351-1111  
IN NASHVILLE : 366-1987



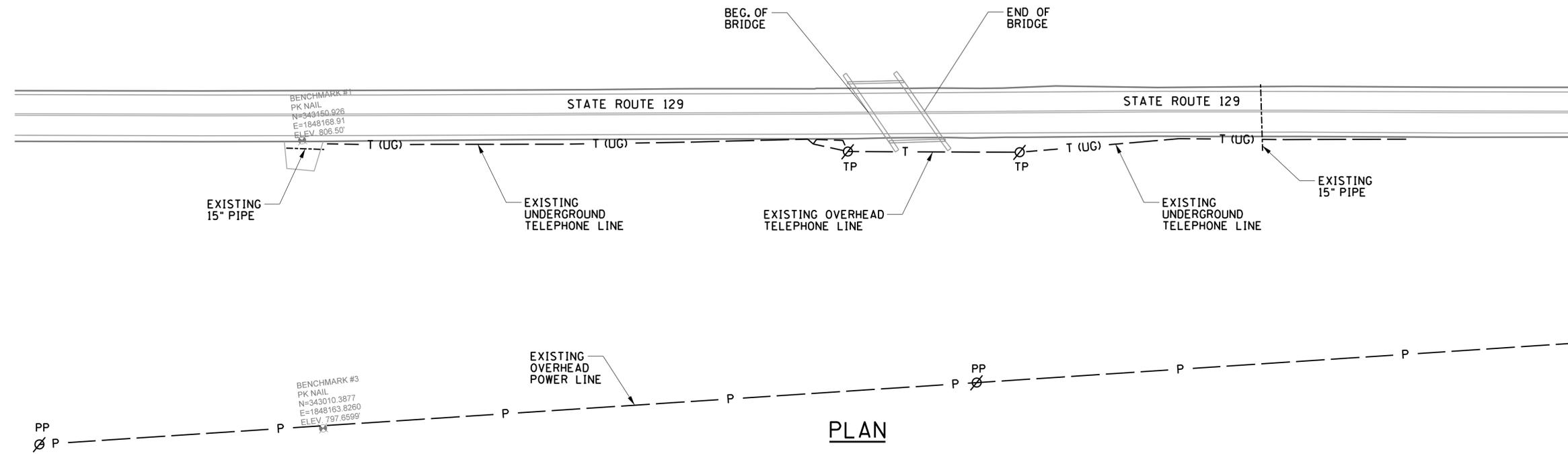
SYSTEM INCORPORATED

**UTILITY CONTACTS**

<p>AT&amp;T 116 SOUTH CANON AVENUE MURFREESBORO, TN 37129 KENNETH LEE KORNEGAY KK4096@ATT.COM O: 615-848-2082 C: 615-631-7221</p> <p>CHARTER COMMUNICATIONS MID-SOUTH 215 INDUSTRIAL BLVD TULAHOMA, TN 37388 RICHARD RIDDLE (PRIMARY) RICHARD.RIDDLE@CHARTER.COM O: 931-461-4315 C: 931-224-8787 F: 931-455-5392</p>	<p>DUCK RIVER ELECTRIC MEM. CORP 1411 MADISON STREET SHELBYVILLE, TN 37160 JEFF GRAVES JGRAVES@DREMC.COM O: 931-680-5850</p> <p>METRO WATER &amp; SEWER OF LYNCHBURG 705 FAYETTEVILLE HIGHWAY LYNCHBURG, TN 37352 RONNIE CUNNINGHAM RONCUNNINGHAM1966@HOTMAIL.COM O: 931-759-4297 C: 931-580-0789</p>	<p>METRO WATER &amp; SEWER OF LYNCHBURG 705 FAYETTEVILLE HIGHWAY LYNCHBURG, TN 37352 PAUL SIGMAN PES@MSUC.NET O: 901-373-7278</p> <p>SPECTRA ENERGY (SENIOR ENGINEER-PIPELINE INTEGRITY) 555 MARRIOT DRIVE, SUITE 600 NASHVILLE, TN 37214 ERIC MCNEELY (PRIMARY TN PROJECTS) EMCNEELY@SPECTRAENERGY.COM O: 615-872-5182 C: 615-582-9967</p>	<p>TULLAHOMA UTILITY BOARD 901 S. JACKSON P.O. BOX 788 TULLAHOMA, TN 37388 R. BRIAN SKELTON (PRIMARY) DALE WILLIS (SECONDARY) DWILLIS@TUB.NET O: 931-455-4515 F: 931-393-4148</p>
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PORTIONS OF THIS DRAWING DEPICTS EXISTING CONDITIONS. LIMITS OF PROPOSED BRIDGE CONSTRUCTION MAY VARY FROM EXISTING CONDITIONS.

TYPE	YEAR	PROJECT NO.	SHEET NO.
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**PLAN**

**LEGEND**

**MAPPING SYMBOLS AND CODES**

- [Symbol] CATCH BASIN (CB)
- [Symbol] CATCH BASIN DOUBLE (DCB)
- [Symbol] CATCH BASIN TRIPLE (TCB)
- [Symbol] ELECTRIC JUNCTION BOX (EJB)
- [Symbol] ELECTRIC METER (EM)
- [Symbol] ELECTRIC TRANSFORMER (ETF)
- [Symbol] FIBER OPTIC MARKER (FOM)
- [Symbol] FIRE HYDRANT (FH)
- [Symbol] FLOOD LIGHT (FLT)
- [Symbol] FLAG POLE (FP)
- [Symbol] GAS METER (GM)
- [Symbol] GUY POLE (GP)
- [Symbol] GAS VALVE (GV)
- [Symbol] GUY WIRE (GW)
- [Symbol] IRON PIN NEW (IR(N))
- [Symbol] IRON PIN OLD (IR(O))
- [Symbol] IRON PIPE OLD (IP(O))
- [Symbol] IRON SPIKE OLD (IS(O))
- [Symbol] LIGHT STANDARD METAL/WOOD (LS/LP)
- [Symbol] MAIL BOX (MB)

- [Symbol] MAN HOLE (MH)
- [Symbol] MONUMENT CONCRETE OLD (MON(O))
- [Symbol] MONUMENT CONCRETE NEW (MON(N))
- [Symbol] P.K. NAIL OLD (PK(O))
- [Symbol] P.K. NAIL NEW (PK(N))
- [Symbol] LIGHT POLE WITH POWER (LP)
- [Symbol] LIGHT POLE WITH TELEPHONE (T)
- [Symbol] POWER MAN HOLE (PMH)
- [Symbol] POWER POLE (PP)
- [Symbol] POWER AND TELEPHONE POLE (P/T)
- [Symbol] RIGHT-OF-WAY MONUMENT (RWM)
- [Symbol] SPRINKLER HEAD/VALVE (SPH,SPV)
- [Symbol] SANITARY MAN HOLE (SSMH)
- [Symbol] SIGN (SN)
- [Symbol] STEAM MAN HOLE (STMH)
- [Symbol] STORM WATER MAN HOLE (SWMH)
- [Symbol] TELEPHONE POLE (T)
- [Symbol] TREE
- [Symbol] WATER METER (WM)
- [Symbol] WATER VALVE (WV)

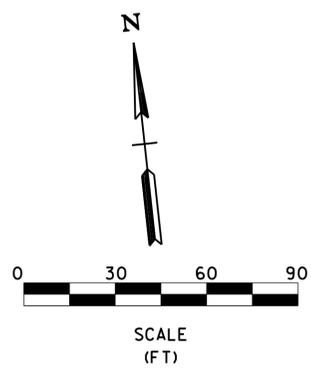
**LINE STYLES**

CABLE LINE	— C —
CENTER LINE	— C —
CONCRETE AREA	[Symbol]
EDGE OF WOODS LINE	[Symbol]
FENCE LINE	— X — X — X —
GAS LINE	— X* — G —
OVERHEAD POWER LINE	— P —
OVERHEAD TELEPHONE LINE	— T —
OVERHEAD POWER AND TELEPHONE LINE	— P — T —
PROPERTY LINE	— P —
SANITARY SEWER	— X* — SA —
STORM WATER	— X* — ST —
FIBER OPTIC CABLE	— F —
UNDERGROUND FIBER OPTIC CABLE	— F (UG) —
UNDERGROUND POWER LINE	— P (UG) —
UNDERGROUND POWER AND TELEPHONE LINE	— UG — P — T —
UNDERGROUND TELEPHONE LINE	— T (UG) —
WATER LINE	— X* — W —

NOTE: UTILITY INFORMATION SHOWN WAS DERIVED FROM FIELD MEASUREMENT AND OBSERVATION. THE CONTRACTOR SHALL FIELD VERIFY ALL EXISTING UTILITY LOCATIONS AND SERVICES PRIOR TO ROADWAY AND BRIDGE CONSTRUCTION.

NOTE: CONTRACTOR SHALL USE EXTREME CAUTION WHEN OPERATING IN ANY AREA WHERE UTILITIES ARE PRESENT AND AVOID WORKING IN THESE AREAS AS MUCH AS PRACTICALLY POSSIBLE. DAMAGES TO ANY UTILITIES ARE TO BE REPAIRED BY THE CONTRACTOR AT HIS OWN EXPENSE.

NOTE: THE CONTRACTOR SHALL VERIFY THE LOCATION OF EXISTING POLES, GUY WIRES AND ROADWAY SIGNS PRIOR TO COMMENCEMENT OF CONSTRUCTION. THE CONTRACTOR SHALL COORDINATE THE MOVEMENT OF SUCH SIGNS, POLES AND GUY WIRES WITH THEIR RESPECTIVE OWNERS. THE CONTRACTOR SHALL RE-SET EXISTING SIGNAGE IN ITS ORIGINAL LOCATION. THE COST OF RE-SETTING SIGNAGE WILL NOT BE PAID FOR DIRECTLY BUT WILL BE INCLUDED IN OTHER ITEMS OF CONSTRUCTION.



BRIDGE NO. 64-SR129-2.51  
STATE ROUTE 129  
OVER UNNAMED BRANCH  
MOORE COUNTY  
2015

**UNOFFICIAL SET**  
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DEPARTMENT OF TRANSPORTATION

UTILITIES

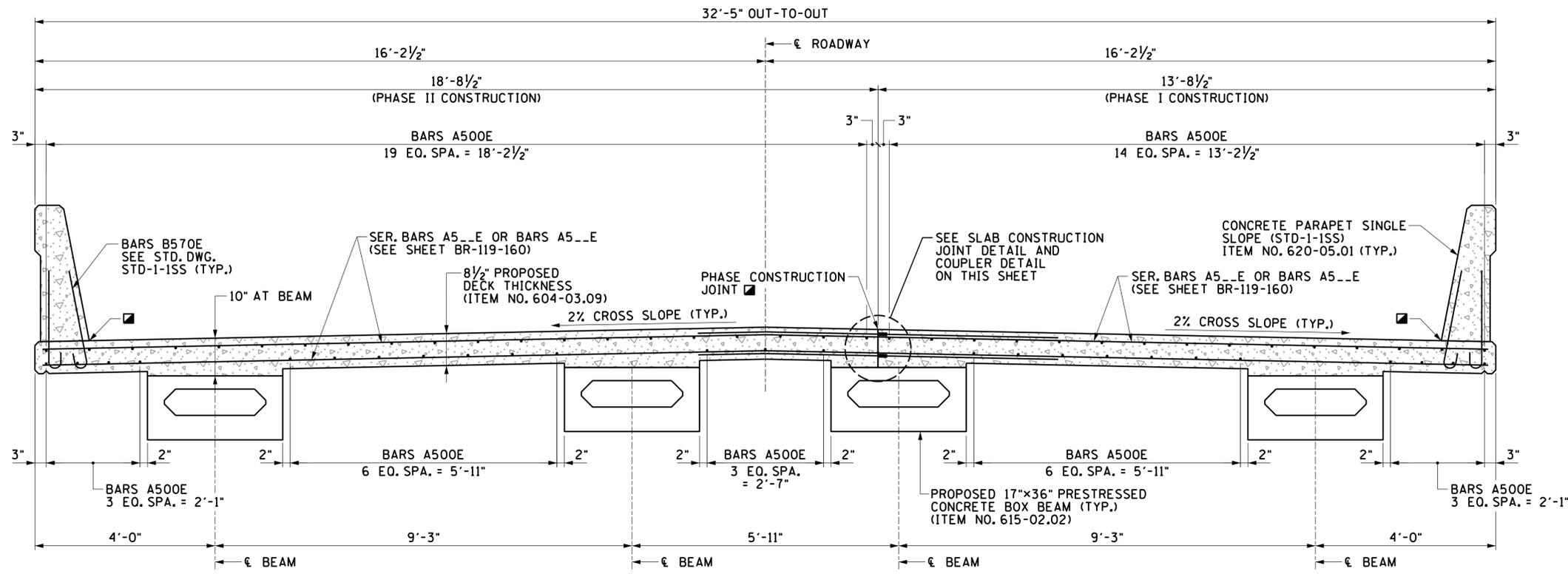








CONST. NO.			
PROJECT NO.	YEAR	SHEET NO.	
64009-4205-04	2015		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



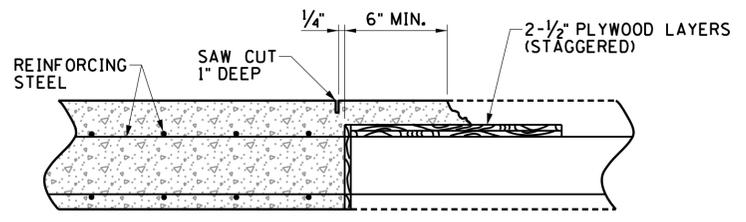
**TYPICAL CROSS-SECTION**

(LOOKING FORWARD ON SURVEY)  
(SEE BR-119-159 AND BR-119-160 FOR ADDITIONAL INFORMATION)

■ DENOTES: SEAL CONSTRUCTION JOINTS WITH CRACK SEALER. TO BE INCLUDED IN ITEM NO. 617-02 AND ITEM NO. 617-05.

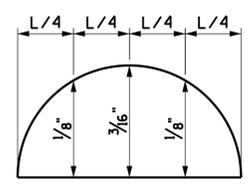
**NOTES:**

1. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE STABILITY OF THE STRUCTURE DURING SLAB REPLACEMENT. DESIGN CALCULATION AND DETAILS OF TEMPORARY SUPPORT SYSTEM OR FALSEWORK REQUIRED SHALL BE SUBMITTED TO THE ENGINEER FOR REVIEW AND SHALL MEET WITH THE FULL SATISFACTION OF THE ENGINEER BEFORE ANY DEMOLITION IS BEGUN. COST OF STABILIZING THE STRUCTURE SHALL BE INCLUDED IN ITEM NO. 602-10.05, BRACING REPAIRS, L.S.
2. FOR STANDARD REINFORCING DETAILS OF PARAPET, SEE STD-1-1SS.
3. WHEN POURING SLAB, PROVISIONS SHALL BE MADE FOR SETTING REINFORCING STEEL FOR THE PARAPET. THE PARAPET SHALL NOT BE POURED UNTIL THE SLAB IS POURED AND CURED.
4. THE COST OF CONCRETE REQUIRED FOR THE NEW BRIDGE DECK SLAB SHALL BE INCLUDED IN THE PRICE BID FOR ITEM NO. 604-03.09, CLASS "D" CONCRETE (BRIDGE DECK), C.Y.
5. ALL REINFORCING STEEL FOR THE NEW CONCRETE SLAB SHALL BE EPOXY COATED. REINFORCING STEEL TO BE INCLUDED IN THE PRICE BID FOR ITEM NO. 604-02.03, EPOXY COATED REINFORCING STEEL, LBS.
6. THE COST OF REMOVING AND DISPOSING OF THE EXISTING CONCRETE AND REINFORCING STEEL, WITHIN THE LIMITS SHOWN, SHALL BE INCLUDED IN ITEM NO. 202-04.01, REMOVAL OF STRUCTURES, L.S.



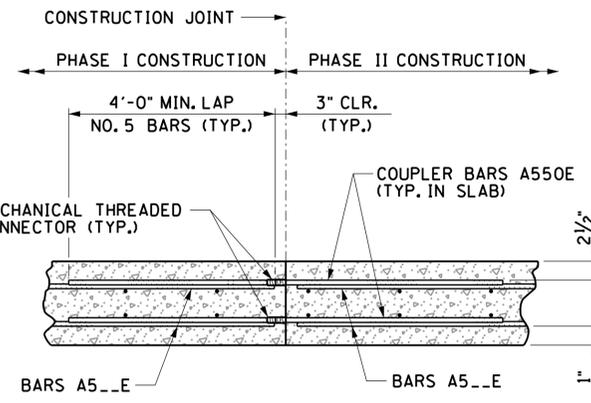
**SLAB CONSTRUCTION JOINT DETAIL**

(NOT TO SCALE)



**DEAD LOAD CORRECTION CURVE**

NOTE: THIS CURVE IS FOR SLAB DEAD LOAD AND ALL DEAD LOADS THAT ARE APPLIED AFTER THE SLAB IS IN PLACE.



**COUPLER DETAIL**

(NOT TO SCALE)

NOTE: THE COST OF MECHANICAL THREADED CONNECTORS AND THE COST OF THREADING THE COUPLER BARS SHALL BE INCLUDED IN THE PRICE BID FOR ITEM NO. 604-02.03, EPOXY COATED REINFORCING STEEL, L.B.  
NOTE: DETAIL IS TYPICAL FOR CONSTRUCTION JOINT BETWEEN PHASES I AND II.  
NOTE: SEAL CONSTRUCTION JOINT WITH CRACK SEALER (ITEM NOS. 617-02 AND 617-05). SEALER SHALL BE APPLIED AFTER CONCRETE HAS BEEN IN PLACE AT LEAST 10 DAYS.

ESTIMATED QUANTITIES PHASE I CONSTRUCTION	
CLASS "D" CONCRETE (BRIDGE DECK) (C.Y.)	EPOXY COATED REINFORCING STEEL (BRIDGES) (LBS.)
12.7	2,760

ESTIMATED QUANTITIES PHASE II CONSTRUCTION	
CLASS "D" CONCRETE (BRIDGE DECK) (C.Y.)	EPOXY COATED REINFORCING STEEL (BRIDGES) (LBS.)
17.3	3,520

UNOFFICIAL SET  
NOT FOR BIDDING

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

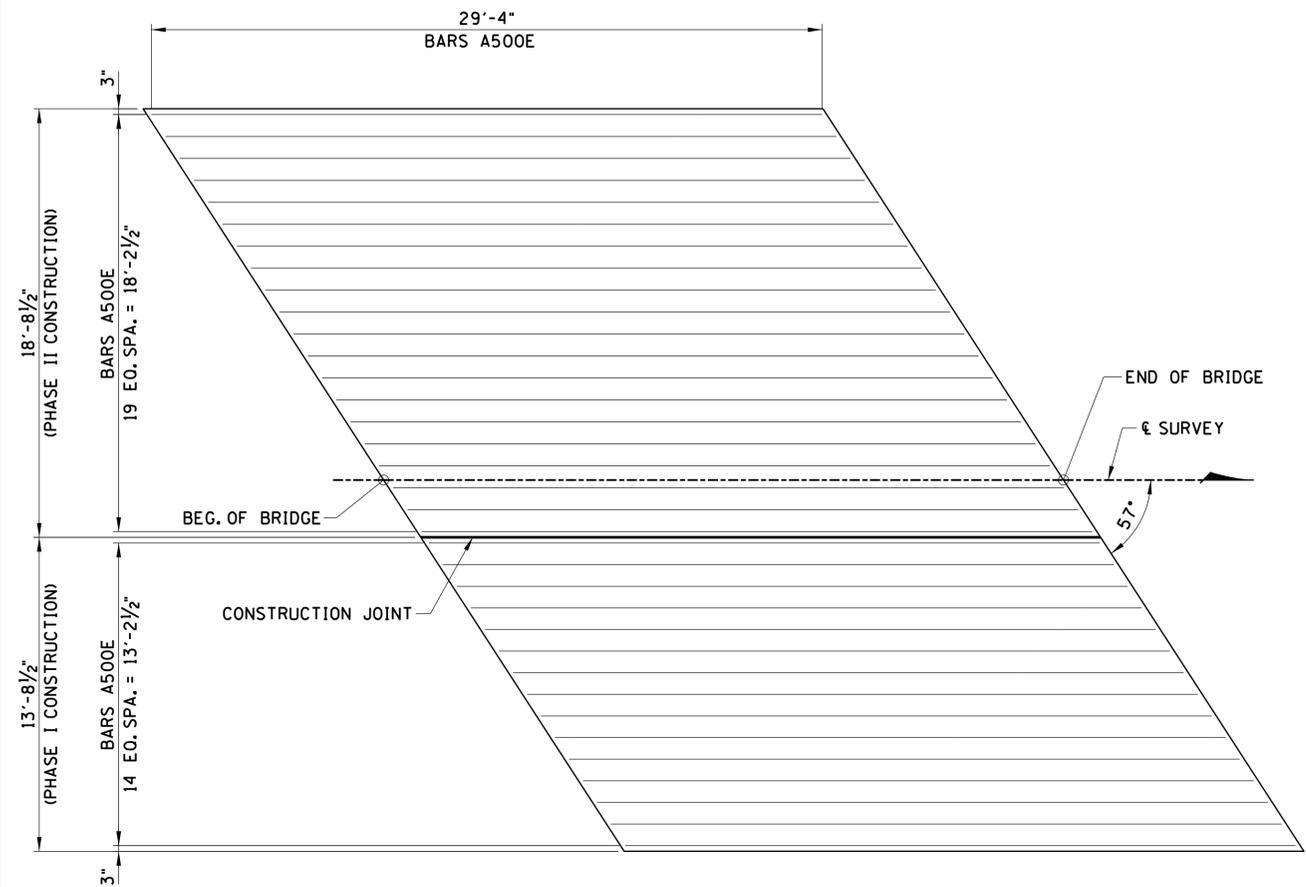
SUPERSTRUCTURE

BRIDGE NO. 64-SR129-2.51  
STATE ROUTE 129  
OVER UNNAMED BRANCH  
MOORE COUNTY  
2015

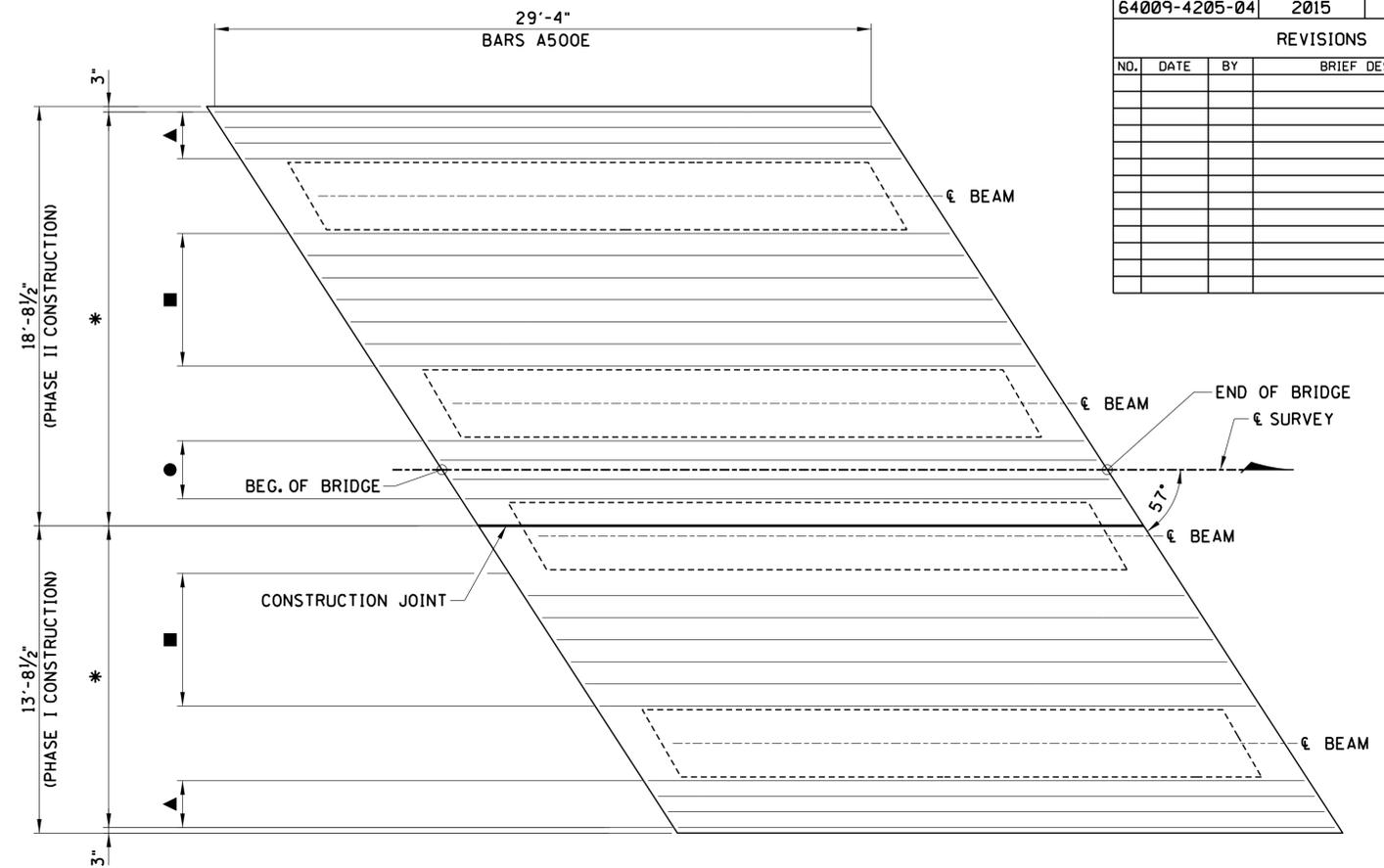
DESIGNED BY: D. THOMPSON/D. KEATON  
DRAWN BY: ANGELA MOORE  
SUPERVISED BY: DARRELL JAMES  
CHECKED BY: JAMIE GILLESPIE



CONST. NO.			
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64009-4205-04	2015		
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**SLAB PLAN**  
 (SHOWING TOP LONGITUDINAL REINFORCEMENT)  
 NOTE: SEE SHEET BR-119-163 FOR BARS R601E  
 PLACEMENT DETAILS.



**SLAB PLAN**  
 (SHOWING BOTTOM LONGITUDINAL REINFORCEMENT)  
 \* DENOTES: SEE SHEET BR-119-158 FOR BAR SPACING.  
 ▲ DENOTES: BARS BARS A500E (3 EQ. SPA. = 2'-1")  
 ■ DENOTES: BARS BARS A500E (6 EQ. SPA. = 5'-11")  
 ● DENOTES: BARS BARS A500E (3 EQ. SPA. = 2'-7")

**SUPERSTRUCTURE GENERAL NOTES:**

1. WHEN POURING SLAB, PROVISIONS SHALL BE MADE FOR SETTING REINFORCING STEEL FOR THE PARAPET. NO PORTION OF THE PARAPET SHALL BE POURED UNTIL THE ENTIRE DECK SLAB (PER PHASE) IS IN PLACE AND CURED.
2. APPROVAL OF MATERIALS: NO FABRICATION SHALL BE STARTED UNTIL THE MATERIALS INVOLVED HAVE BEEN APPROVED BY THE TENNESSEE DEPARTMENT OF TRANSPORTATION, DIVISION OF MATERIALS AND TESTS.

DESIGNED BY D. THOMPSON/D. KEATON DATE \_\_\_\_\_  
 DRAWN BY ANGELA MOORE DATE \_\_\_\_\_  
 SUPERVISED BY DARRELL JAMES DATE \_\_\_\_\_  
 CHECKED BY JAMIE GILLESPIE DATE \_\_\_\_\_



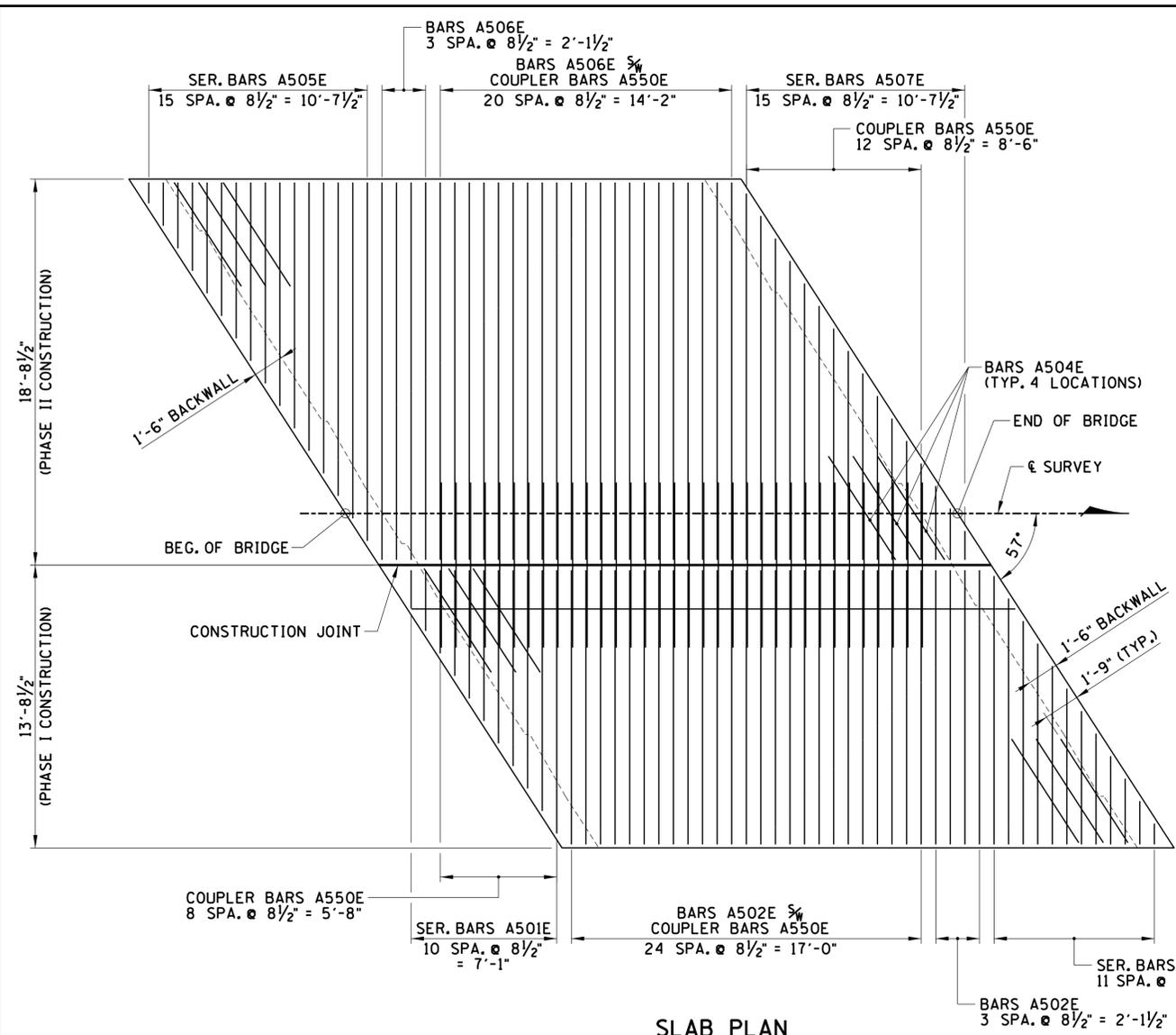
**UNOFFICIAL SET**  
 NOT FOR BIDDING

STATE OF TENNESSEE  
 DEPARTMENT OF TRANSPORTATION

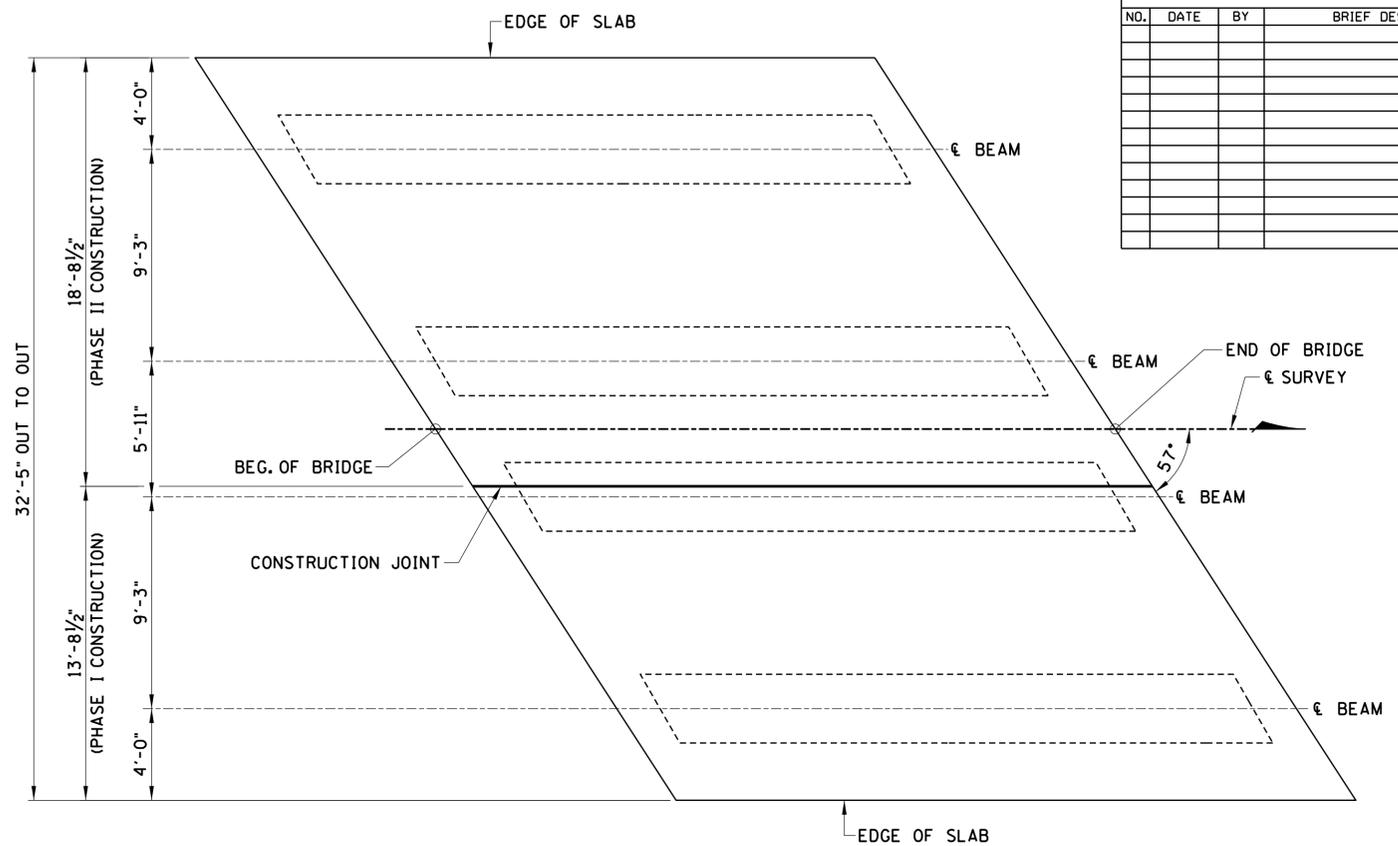
SUPERSTRUCTURE DETAILS

BRIDGE NO. 64-SR129-2.51  
 STATE ROUTE 129  
 OVER UNNAMED BRANCH  
 MOORE COUNTY  
 2015

CONST. NO.			
PROJECT NO.	YEAR	SHEET NO.	
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**SLAB PLAN**  
(SHOWING TOP AND BOTTOM TRANSVERSE REINFORCEMENT)  
NOTE: SEE SHEET BR-119-163 FOR BARS R601E PLACEMENT DETAILS.



NOTE: SEE PRESTRESSED CONCRETE BOX BEAM DETAILS ON BR-119-164.

**FRAMING PLAN**

**SUPERSTRUCTURE GENERAL NOTES:**

1. WHEN POURING SLAB, PROVISIONS SHALL BE MADE FOR SETTING REINFORCING STEEL FOR THE PARAPET. NO PORTION OF THE PARAPET SHALL BE POURED UNTIL THE ENTIRE DECK SLAB (PER PHASE) IS IN PLACE AND CURED.
2. APPROVAL OF MATERIALS: NO FABRICATION SHALL BE STARTED UNTIL THE MATERIALS INVOLVED HAVE BEEN APPROVED BY THE TENNESSEE DEPARTMENT OF TRANSPORTATION, DIVISION OF MATERIALS AND TESTS.

DESIGNED BY D. THOMPSON/D. KEATON DATE \_\_\_\_\_  
 DRAWN BY ANGELA MOORE DATE \_\_\_\_\_  
 SUPERVISED BY DARRELL JAMES DATE \_\_\_\_\_  
 CHECKED BY JAMIE GILLESPIE DATE \_\_\_\_\_



**UNOFFICIAL SET**  
NOT FOR BIDDING

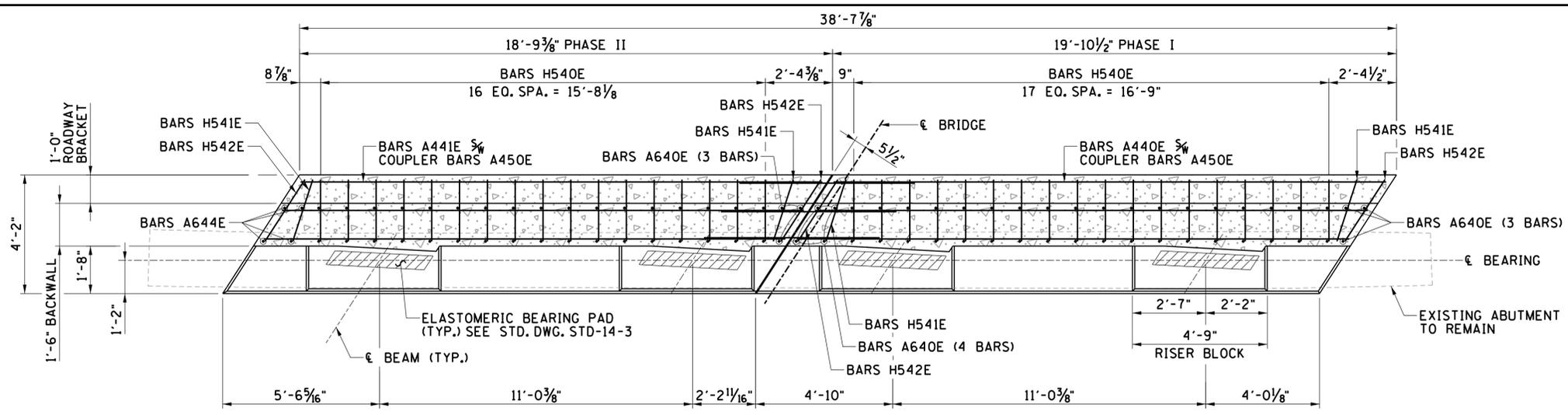
STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
SUPERSTRUCTURE DETAILS  
AND FRAMING PLAN  
BRIDGE NO. 64-SR129-2.51  
STATE ROUTE 129  
OVER UNNAMED BRANCH  
MOORE COUNTY  
2015

CONST. NO.		
PROJECT NO.	YEAR	SHEET NO.
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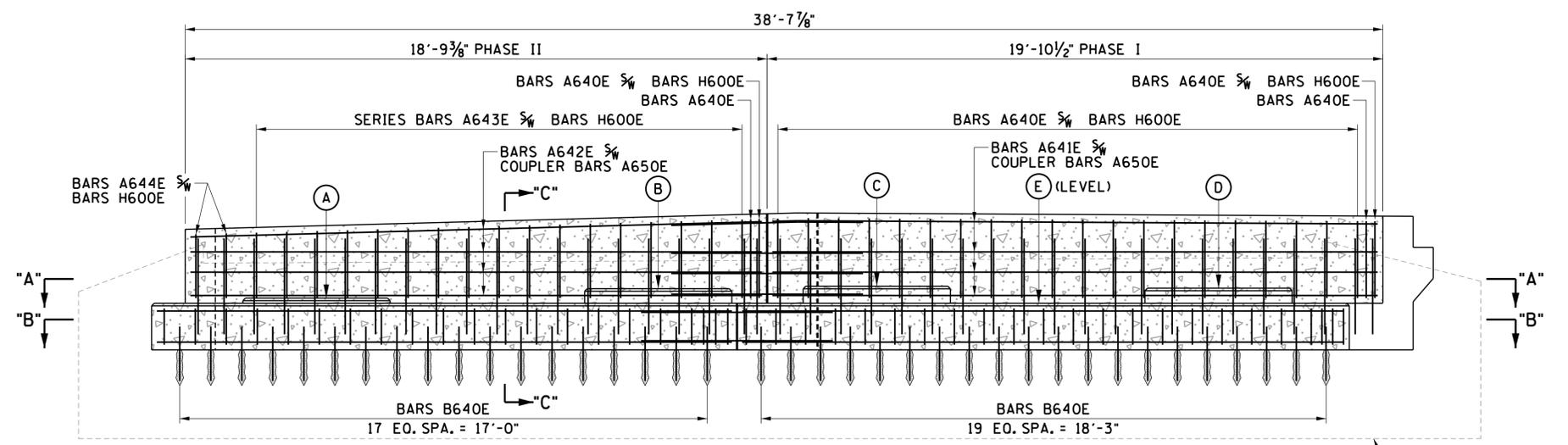
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

ELEVATIONS	
LOCATION	ELEVATION
A	795.12
B	795.42
C	795.50
D	795.44
E	794.95

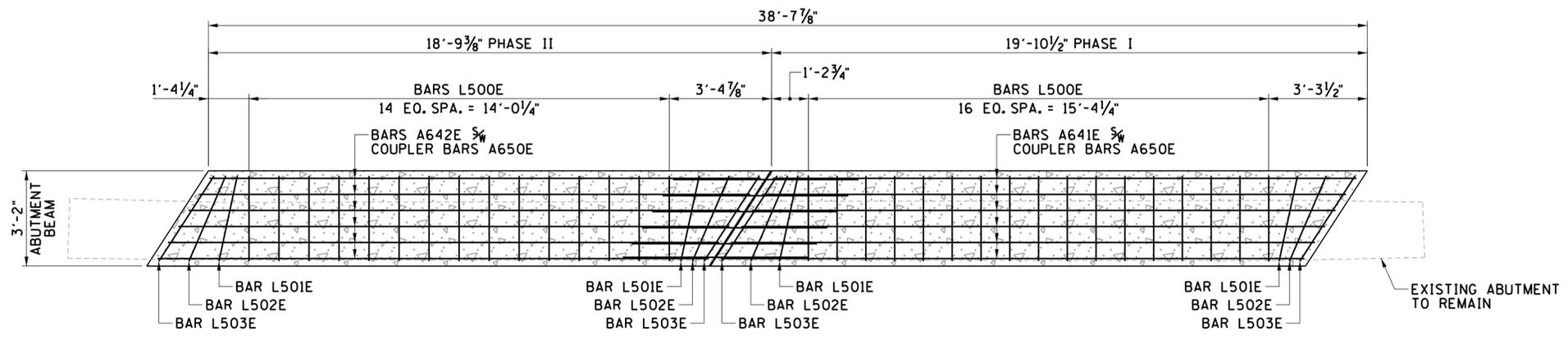
NOTE: ELEVATIONS A THRU D REPRESENT CONCRETE BEAM SEAT ELEVATION BEFORE 1/2" BEARING PAD INSTALLATION.



SECTION "A-A"



ELEVATION  
(LOOKING BACK ON SURVEY)  
(SEE BR-119-163 FOR SECTION "C-C")



SECTION "B-B"

DESIGNED BY D. THOMPSON/D. KEATON  
 DRAWN BY ANGELA MOORE  
 SUPERVISED BY DARRELL JAMES  
 CHECKED BY JAMIE GILLESPIE



UNOFFICIAL  
SET  
  
NOT FOR  
BIDDING

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

ABUTMENT NO. 1

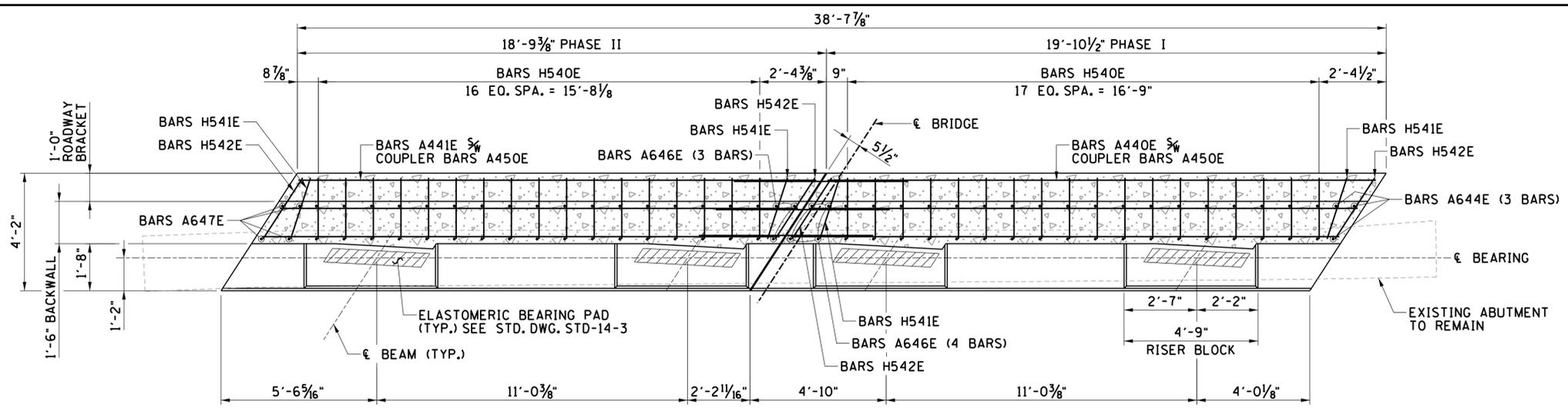
BRIDGE NO. 64-SR129-2.51  
STATE ROUTE 129  
OVER UNNAMED BRANCH  
MOORE COUNTY  
2015

CONST. NO.		
PROJECT NO.	YEAR	SHEET NO.
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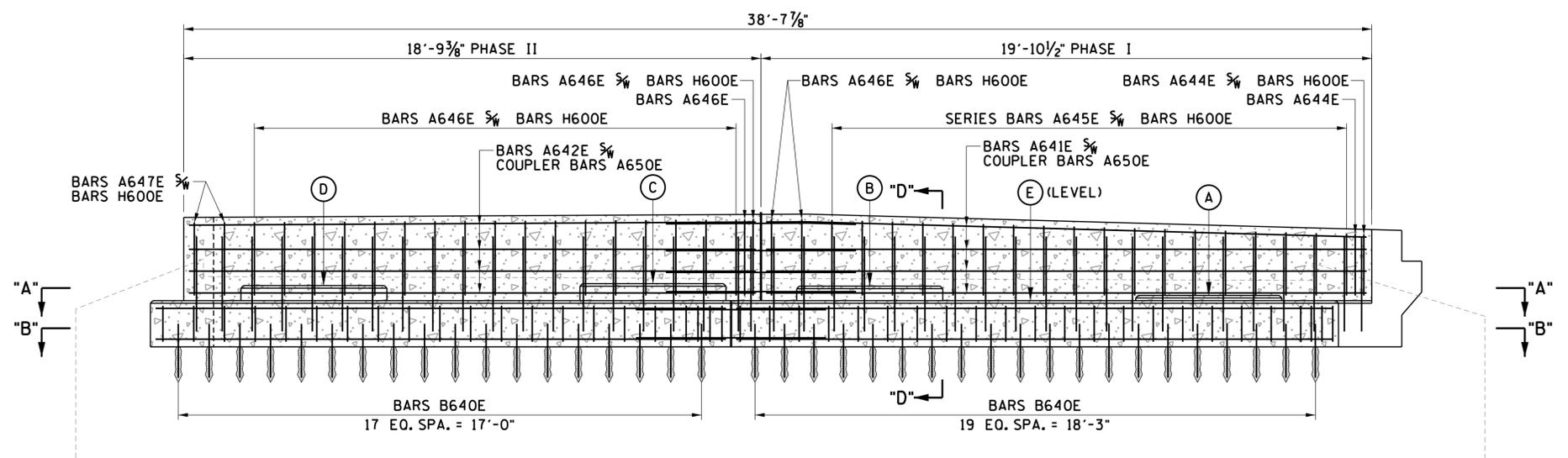
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

ELEVATIONS	
LOCATION	ELEVATION
A	794.61
B	794.92
C	795.00
D	794.93
E	794.44

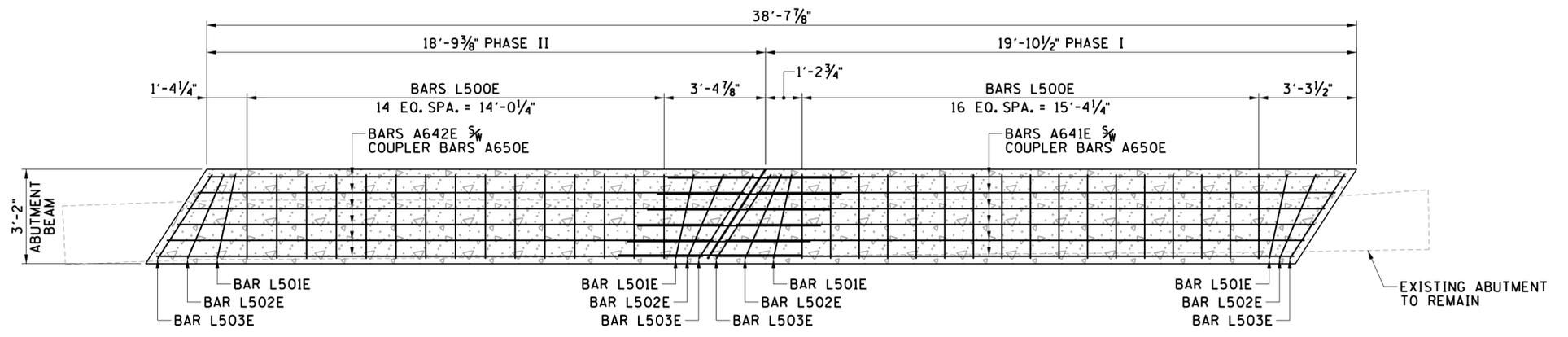
NOTE: ELEVATIONS A THRU D REPRESENT CONCRETE BEAM SEAT ELEVATION BEFORE 1/2" BEARING PAD INSTALLATION.



SECTION "A-A"



ELEVATION  
(LOOKING FORWARD ON SURVEY)  
(SEE BR-119-163 FOR SECTION "D-D")



SECTION "B-B"

DESIGNED BY D. THOMPSON/D. KEATON  
 DRAWN BY ANGELA MOORE  
 SUPERVISED BY DARRELL JAMES  
 CHECKED BY JAMIE GILLESPIE

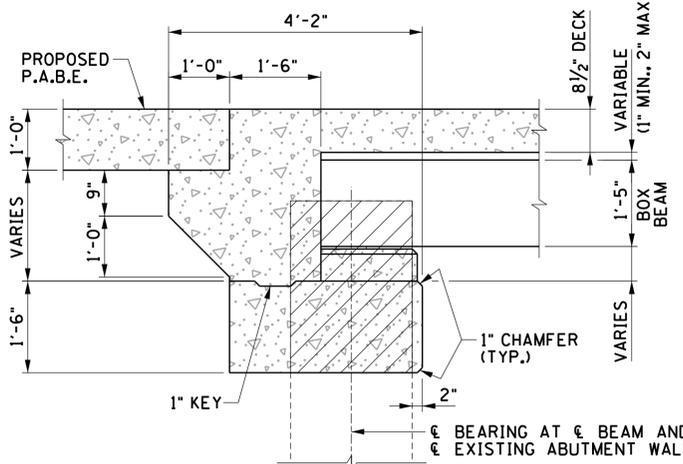


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 NOT FOR BIDDING

STATE OF TENNESSEE  
 DEPARTMENT OF TRANSPORTATION

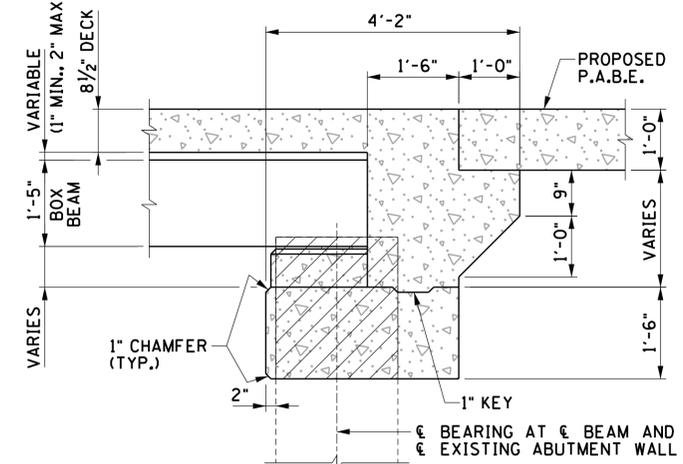
ABUTMENT NO. 2

BRIDGE NO. 64-SR129-2.51  
 STATE ROUTE 129  
 OVER UNNAMED BRANCH  
 MOORE COUNTY  
 2015



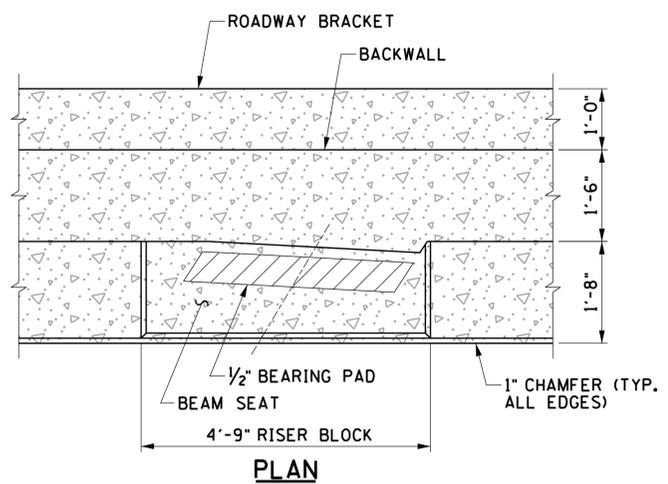
**SECTION "C-C"**  
(ABUTMENT NO. 1)  
(NOT TO SCALE)

▨ DENOTES: AREA OF EXISTING ABUTMENT TO BE REMOVED (SEE BR-119-157 FOR LIMITS OF DEMOLITION).

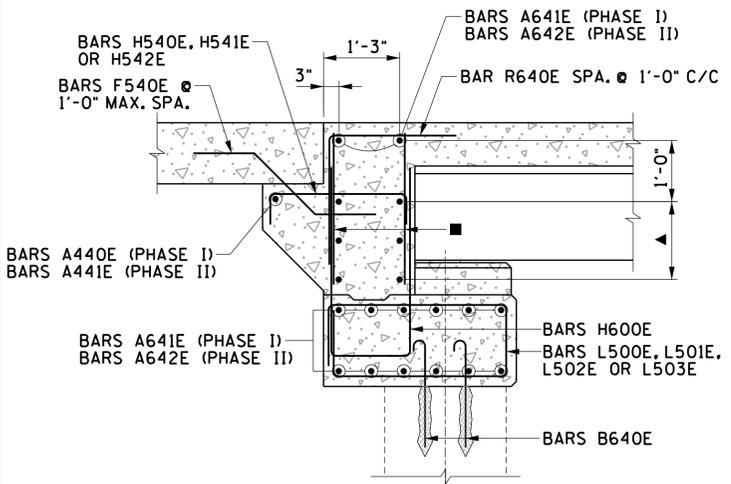


**SECTION "D-D"**  
(ABUTMENT NO. 2)  
(NOT TO SCALE)

▨ DENOTES: AREA OF EXISTING ABUTMENT TO BE REMOVED (SEE BR-119-157 FOR LIMITS OF DEMOLITION).

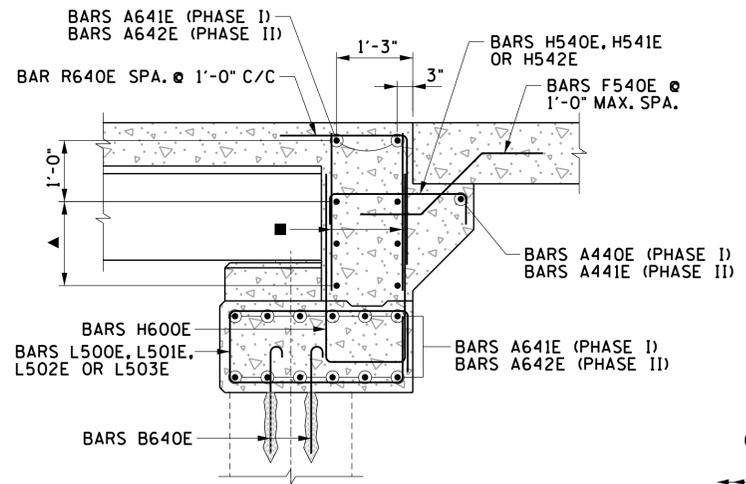


**PLAN**  
**ELEVATION**  
**RISER BLOCK DETAIL**  
(NOT TO SCALE)



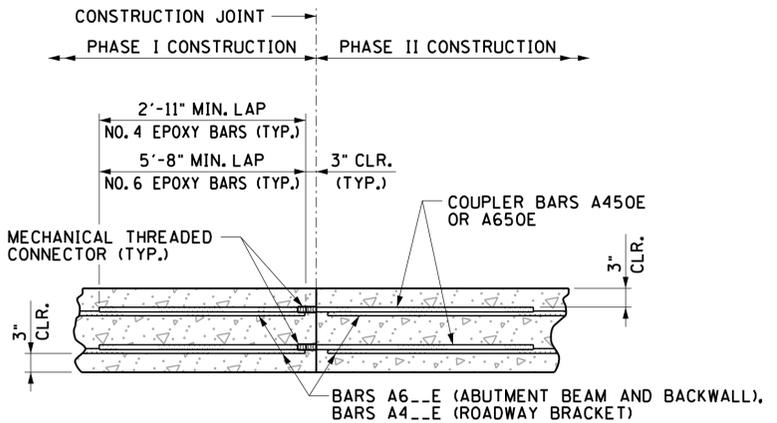
**SECTION "C-C"**  
(ABUTMENT NO. 1 SHOWING REINFORCING)  
(NOT TO SCALE)

▲ DENOTES: BARS A641E (PHASE I)  
BARS A642E (PHASE II)  
■ DENOTES: BARS A640E (PHASE I)  
BARS A640E, SERIES BARS A643E  
OR BARS A644E (PHASE II)



**SECTION "D-D"**  
(ABUTMENT NO. 2 SHOWING REINFORCING)  
(NOT TO SCALE)

▲ DENOTES: BARS A641E (PHASE I)  
BARS A642E (PHASE II)  
■ DENOTES: BARS A644E, SERIES BARS A645E  
OR BARS A646E (PHASE I)  
BARS A646E OR BARS A647E (PHASE II)



**COUPLER DETAIL**  
(NOT TO SCALE)

NOTE: THE COST OF MECHANICAL THREADED CONNECTORS AND THE COST OF THREADING THE COUPLER BARS SHALL BE INCLUDED IN THE PRICE BID FOR ITEM NO. 604-02.03, EPOXY COATED REINFORCING STEEL, L.B.  
NOTE: DETAIL IS TYPICAL FOR CONSTRUCTION JOINT BETWEEN PHASES I AND II.

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**ESTIMATED QUANTITIES PHASE I CONSTRUCTION**

	CLASS "A" CONCRETE (BRIDGES) (C.Y.)	EPOXY COATED REINFORCING STEEL (BRIDGES) (LBS.)
ABUT NO. 1	7.7	1,730
ABUT NO. 2	7.7	1,660

**ESTIMATED QUANTITIES PHASE II CONSTRUCTION**

	CLASS "A" CONCRETE (BRIDGES) (C.Y.)	EPOXY COATED REINFORCING STEEL (BRIDGES) (LBS.)
ABUT NO. 1	7.3	1,570
ABUT NO. 2	7.3	1,485

**UNOFFICIAL SET**  
NOT FOR BIDDING

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

ABUTMENT NOS. 1 AND 2 DETAILS

BRIDGE NO. 64-SR129-2.51  
STATE ROUTE 129  
OVER UNNAMED BRANCH  
MOORE COUNTY  
2015

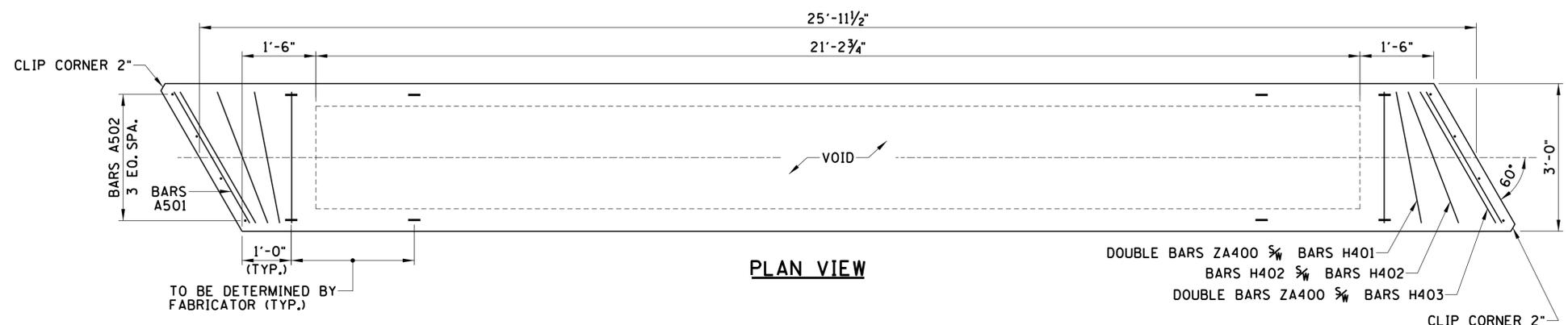
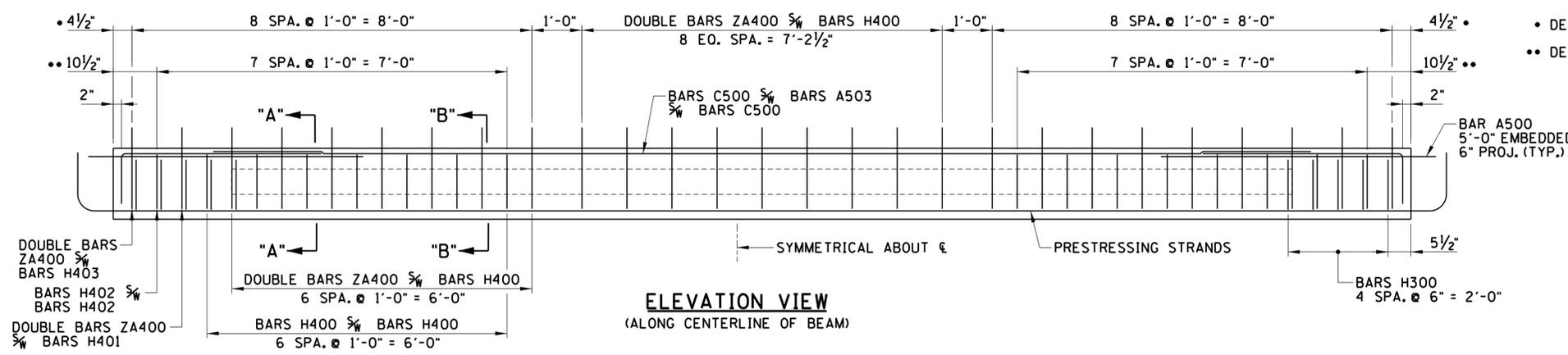
- NOTES:**
- BEARING PAD SURFACE TO CONFORM TO BOTTOM OF BEAM GRADE.
  - SEE STD. DWG. NO. STD-10-1 FOR LIMITS OF CLASS "A" GRADING "D".
  - ELASTOMERIC PADS SHALL BE IN PLACE A MINIMUM OF ONE DAY BEFORE BEING DISTURBED BY SETTING BEAMS. PLACE RUBBER BONDING CEMENT IN SUCH A WAY THAT VISIBLE CONCRETE SURFACES WILL NOT BE STAINED.
  - THE COST OF FORMING, LABOR, CONCRETE, AND ALL MISCELLANEOUS MATERIAL (EXCEPT REINFORCING STEEL) NECESSARY TO MODIFY ABUTMENT BEAM SHALL BE INCLUDED IN ITEM NO. 604-03.01, CLASS "A" CONCRETE (BRIDGES), C.Y.

- THE COST OF DRILLING AND EPOXY GROUTING SHALL BE INCLUDED IN THE PRICE BID FOR ITEM NO. 604-02.03, EPOXY COATED REINFORCING STEEL, LBS.
- THE COST OF REINFORCING STEEL FOR ABUTMENT SHALL BE INCLUDED IN ITEM NO. 604-02.03, EPOXY COATED REINFORCING STEEL, LBS.
- THE SLAB SHALL BE POURED SUBSEQUENT TO OR CONCURRENTLY WITH PLACEMENT OF ANY PART OF THE ABUTMENT BACKWALL. AT LEAST THE TOP 12 INCHES OF THE BACKWALL SHALL BE POURED CONCURRENTLY WITH THE END OF THE SLAB.
- ALL BARS IN ABUTMENTS AND BACKWALLS SHALL BE MECHANICALLY SPICED AT THE CONSTRUCTION JOINT ACCORDING TO THE DETAIL ON THIS SHEET.
- SEE DEMOLITION DETAILS FOR AREAS TO BE REMOVED AND ADDITIONAL NOTES REGARDING EXISTING REINFORCING STEEL.
- CONTRACTOR SHALL SPlice NEW REINFORCING STEEL WITH EXISTING REINFORCING STEEL WHERE APPLICABLE.

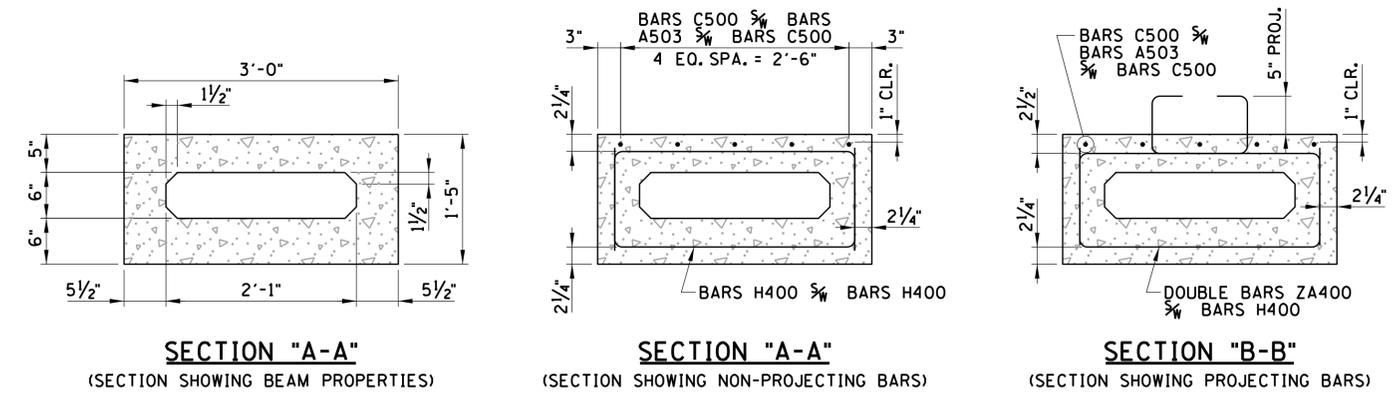
DESIGNED BY: D. THOMPSON/D. KEATON  
DRAWN BY: ANGELA MOORE  
SUPERVISED BY: DARRELL JAMES  
CHECKED BY: JAMIE GILLESPIE



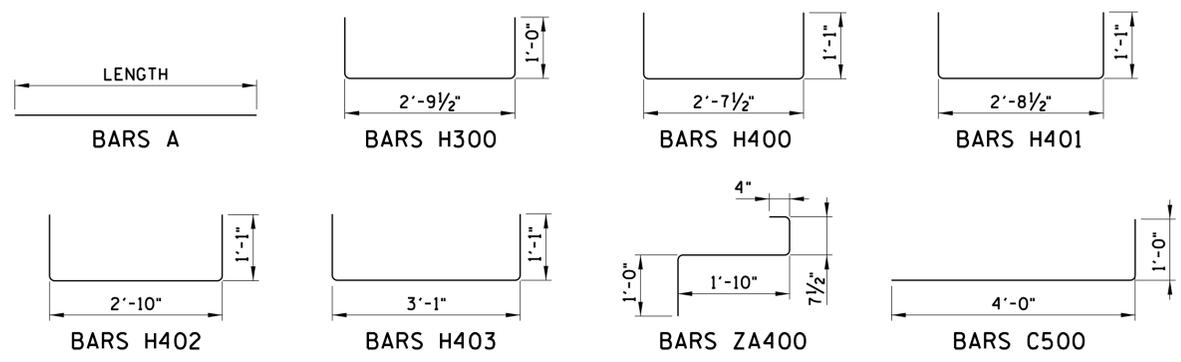
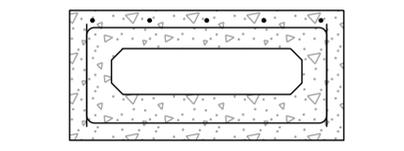
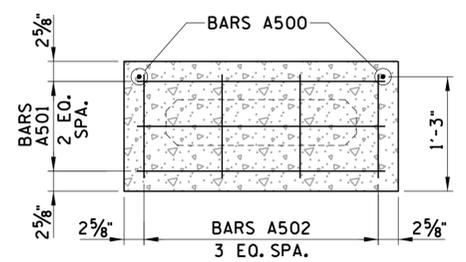
CONST. NO.			
PROJECT NO.	YEAR	SHEET NO.	
64009-4205-04	2015		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



BILL OF STEEL PER BEAM			
BAR	SIZE	NO. REQ'D.	LENGTH
A500	5	4	5'-6"
A501	5	6	3'-1"
A502	5	8	1'-1"
A503	5	5	21'-11 1/2"
C500	5	10	5'-0"
H300	3	10	4'-9 1/2"
H400	4	51	4'-9 1/2"
H401	4	2	4'-10 1/2"
H402	4	4	5'-0"
H403	4	2	5'-3"
ZA400	4	54	3'-9 1/2"



**BEAM NOTES:**  
SEE STD-14-3 FOR BOX BEAM STANDARD DETAILS, NOTES AND REINFORCING. THE CONCRETE FOR THIS CONSTRUCTION SHALL BE OF SUCH PROPERTIES AS TO ATTAIN A COMPRESSIVE STRENGTH OF NOT LESS THAN 5,500 PSI AT THE AGE OF 28 DAYS AND STRESS TRANSFER SHALL NOT BE MADE TO THE BRIDGE MEMBER UNTIL THE TEST SPECIMENS INDICATE THAT THE CONCRETE HAS REACHED A COMPRESSIVE STRENGTH OF AT LEAST 4,000 PSI. SEE GENERAL NOTES (SHEET 2A) FOR CONCRETE FINISHING NOTE.



**UNOFFICIAL SET**  
NOT FOR BIDDING

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
PRESTRESSED CONCRETE  
BOX BEAM DETAILS  
BRIDGE NO. 64-SR129-2.51  
STATE ROUTE 129  
OVER UNNAMED BRANCH  
MOORE COUNTY  
2015

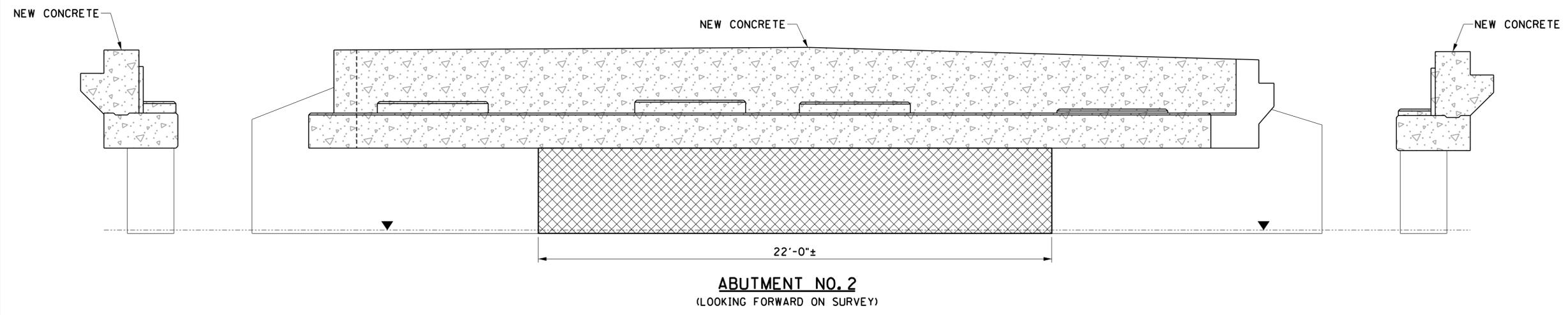
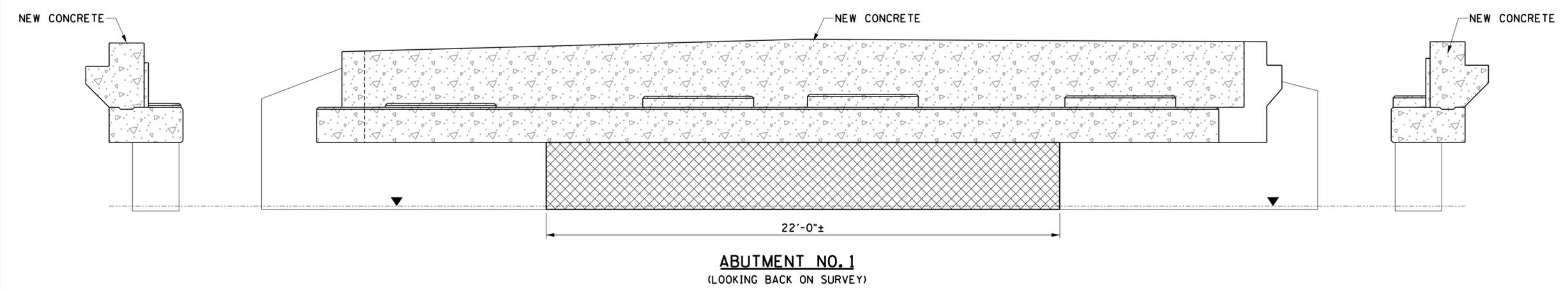
DESIGNED BY: D. THOMPSON/D. KEATON  
DRAWN BY: ANGELA MOORE  
SUPERVISED BY: DARRELL JAMES  
CHECKED BY: JAMIE GILLESPIE

DATE: \_\_\_\_\_  
DATE: \_\_\_\_\_  
DATE: \_\_\_\_\_  
DATE: \_\_\_\_\_

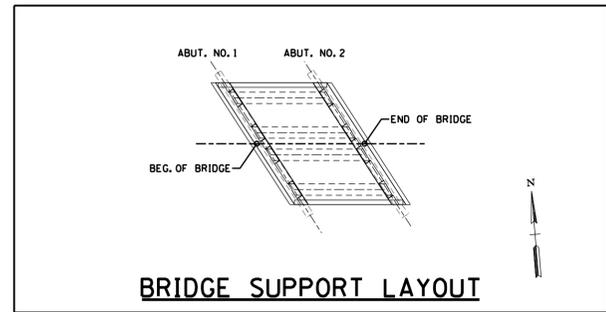


CONST. NO.		
PROJECT NO.	YEAR	SHEET NO.
64009-4205-04	2015	

REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



NOTE: INFORMATION AND DIMENSIONS FOR CONCRETE REPAIRS ARE PROVIDED FOR ESTIMATING ONLY. TDOT ENGINEER SHALL DELINEATE ACTUAL AREAS TO BE REPAIRED.



- NOTES:**
- (1) REMOVE ALL DEBRIS AROUND ABUTMENTS. COST IS TO BE INCLUDED IN OTHER ITEMS.
  - (2) ALL AREAS THAT CONTAIN CALCIUM DEPOSITS SHALL BE REMOVED. ANY CONCRETE THAT HAS DETERIORATED BEHIND THE DEPOSITS SHALL BE REPAIRED.
  - (3) SEE BR-119-166 "CONCRETE REPAIR DETAILS" FOR ESTIMATED REPAIR QUANTITIES FOR ABUTMENTS. ALL REPAIR AREAS SHALL BE DESIGNATED BY THE BRIDGE REPAIR OFFICE AS STATED IN NOTE NO. 1 ON BR-119-166 AND SHALL BE INCLUDED IN ITEM NO. 604-10.05 OR ITEM NO. 604-10.54.

LEGEND	
SPALLED SURFACES	
WATER LEVEL	

DESIGNED BY D. THOMPSON/D. KEATON DATE \_\_\_\_\_  
 DRAWN BY ANGELA MOORE DATE \_\_\_\_\_  
 SUPERVISED BY DARRELL JAMES DATE \_\_\_\_\_  
 CHECKED BY JAMIE GILLESPIE DATE \_\_\_\_\_



**UNOFFICIAL SET**  
NOT FOR BIDDING

STATE OF TENNESSEE  
 DEPARTMENT OF TRANSPORTATION  
 CONCRETE REPAIRS  
 (ABUTMENTS)  
 BRIDGE NO. 64-SR129-2.51  
 STATE ROUTE 129  
 OVER UNNAMED BRANCH  
 MOORE COUNTY  
 2015





