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SEE SHEET NO. 1A

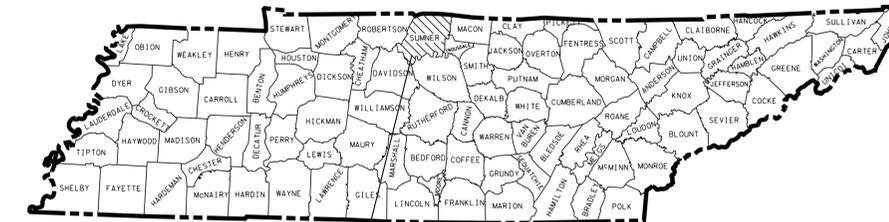
STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
BUREAU OF ENGINEERING

**SUMNER COUNTY**

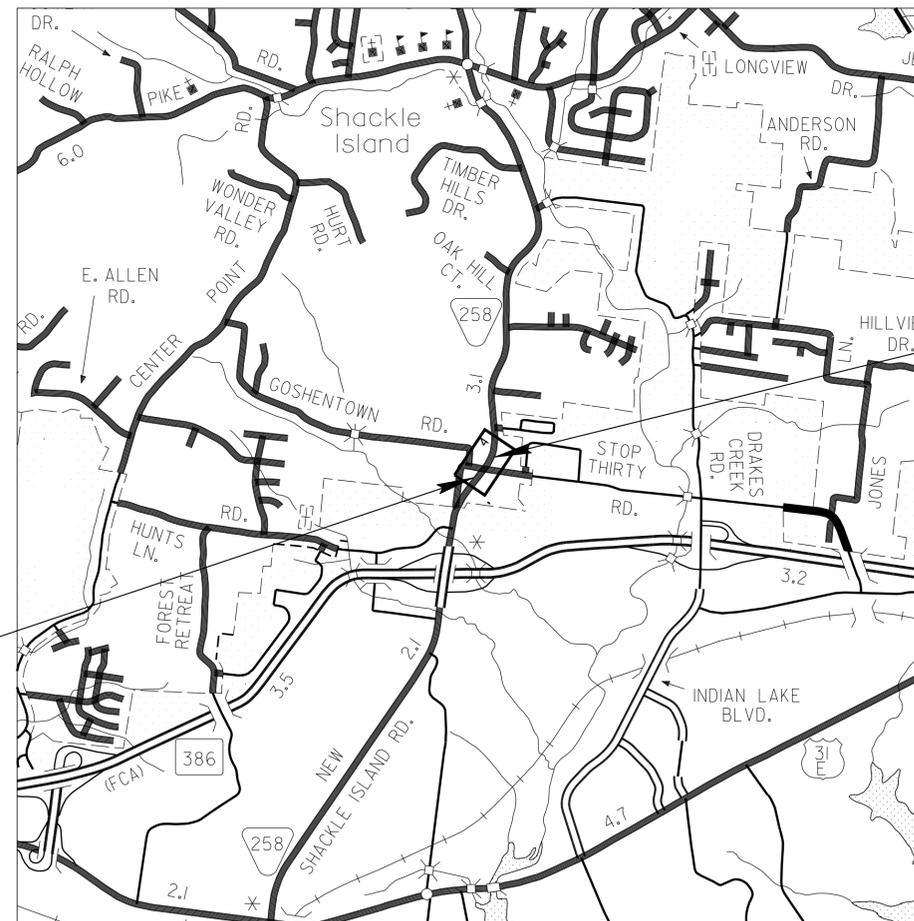
STATE ROUTE 258 (NEW SHACKLE ISLAND ROAD)  
INTERSECTION WITH STOP THIRTY ROAD/OLD  
SHACKLE ISLAND ROAD  
**CONSTRUCTION**

STATE HIGHWAY NO. 258 F.A.H.S. NO. 258

TENN.	YEAR	SHEET NO.
	2013	1
FED. AID PROJ. NO.	HSIP-258(8)	
STATE PROJ. NO.	83018-3219-94	



PROJECT LOCATION



**NO EXCLUSIONS  
NO EQUATIONS**

HSIP-258(8)  
END PROJ. 83018-3219-94 (CONST.)  
STA. 108+64.95

**UNOFFICIAL  
SET  
NOT FOR  
BIDDING**

SEALED BY

APPROVED: Paul D. Degges  
PAUL D. DEGGES, CHIEF ENGINEER

DATE: \_\_\_\_\_

APPROVED: John Schroer  
JOHN SCHROER, COMMISSIONER

HSIP-258(8)  
BEG. PROJ. 83018-3219-94 (CONST.)  
STA. 101+50.00

**SPECIAL NOTES**

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED FEBRUARY 4, 2014 AND ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS AND IN THE PROPOSAL CONTRACT.

TDOT C.E. MANAGER 1 ROLAND LEE JONES, P.E.

DESIGNED BY TRANSYSTEMS

DESIGNER ROBERT BAIRD CHECKED BY STEPHEN K. BRYAN, P.E., PTOE

P.E. NO. 83018-0219-94

PIN NO. 114604.00

SCALE: 1" = 1/2 MILE (2,640')



ROADWAY LENGTH	0.135 MILES
BRIDGE LENGTH	0.000 MILES
BOX BRIDGE LENGTH	0.000 MILES
PROJECT LENGTH	0.135 MILES

TRAFFIC DATA	
ADT (2013)	13,340
ADT (2033)	17,240
DHV (2033)	1,941
D	50 - 50
T (ADT)	4 %
T (DHV)	3 %
V	40 MPH

U.S. DEPARTMENT OF TRANSPORTATION  
FEDERAL HIGHWAY ADMINISTRATION

APPROVED: \_\_\_\_\_  
DIVISION ADMINISTRATOR DATE

TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	2013	HSIP-258(8)	1A

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# STANDARD ROADWAY DRAWINGS

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**NO PROJECT COMMITMENT SHEET INCLUDED IN THIS SET OF PLANS**

DWG. NO	REV.	DESCRIPTION
<b>ROADWAY DESIGN STANDARDS</b>		
RD-A-1	12-18-99	STANDARD ABBREVIATIONS
RD-L-1	10-26-94	STANDARD LEGEND
RD-L-2	09-05-01	STANDARD LEGEND FOR UTILITY INSTALLATIONS
RD-L-3	04-15-04	STANDARD LEGEND FOR SIGNALIZATION AND LIGHTING
RD-L-5	05-01-08	STANDARD LEGEND FOR EROSION PREVENTION AND SEDIMENT CONTROL
RD-L-6	03-30-10	STANDARD LEGEND FOR EROSION PREVENTION AND SEDIMENT CONTROL
RD-L-7	05-24-12	STANDARD LEGEND FOR EROSION PREVENTION AND SEDIMENT CONTROL
RD-L-8		STANDARD LEGEND FOR EROSION PREVENTION AND SEDIMENT CONTROL
RD01-S-11	04-04-03	DESIGN AND CONSTRUCTION DETAILS FOR ROADSIDE SLOPE DEVELOPMENT
RD01-S-11A	10-15-02	ROADSIDE DITCH DETAILS FOR DESIGN AND CONSTRUCTION
RD01-S-12	08-01-09	CLEAR ZONE CRITERIA
RD01-TS-1	10-15-02	DESIGN STANDARD FOR LOCAL ROADS AND STREETS
RD01-TS-3	10-15-02	DESIGN STANDARD FOR 2-LANE ARTERIAL HIGHWAYS
<b>ROADWAY AND PAVEMENT APPURTENANCES</b>		
RP-I-5	12-18-96	EXAMPLES OF STREET AND ALLEY INTERSECTIONS
RP-R-1	05-27-01	STANDARD RAMPS TO SIDE ROADS
<b>TRAFFIC CONTROL APPURTENANCES</b>		
T-FAB-1	05-27-97	FLASHING YELLOW ARROW BOARD
T-M-1	11-01-11	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS AND MARKING ABBREVIATIONS
T-M-2	01-15-13	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS
T-M-3	09-19-91	MARKING STANDARDS FOR TRAFFIC ISLANDS, MEDIANS & PAVED SHOULDERS ON CONVENTIONAL ROADS
T-M-4	11-01-11	STANDARD INTERSECTION PAVEMENT MARKINGS
T-PBR-1	06-30-09	INTERCONNECTED PORTABLE BARRIER RAIL
T-SG-2	07-29-04	LOOP LEAD-INS, CONDUIT AND PULL BOXES
T-SG-3	11-11-04	STANDARD NOTES AND DETAILS OF INDUCTIVE LOOPS
T-SG-5	07-29-04	CONTROLLER CABINET DETAILS
T-SG-7	11-01-11	SIGNAL HEAD ASSEMBLIES AND PEDESTRIAN PUSH BUTTON SIGNS
T-SG-7A	11-01-11	TYPICAL SIGNAL HEAD PLACEMENT
T-SG-9	11-16-07	DETAILS OF CANTILEVER SIGNAL SUPPORT
T-SG-9A		MISCELLANEOUS SIGNAL DETAILS
T-SG-10	05-06-13	MAST ARM POLE AND STRAIN POLES FOUNDATION DETAILS
T-SG-12	11-01-11	TYPICAL WIRING FOR SIGNAL HEADS AND DETECTION LOOPS
<b>EROSION PREVENTION AND SEDIMENT CONTROL</b>		
EC-STR-3B	08-01-12	SILT FENCE

DWG. NO	REV.	DESCRIPTION
EC-STR-3E	04-01-08	SILT FENCE FABRIC JOINING DETAILS
EC-STR-6	08-01-12	ROCK CHECK DAM
EC-STR-6A	08-01-12	ENHANCED ROCK CHECK DAM
EC-STR-11	08-01-12	CULVERT PROTECTION TYPE 1

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DEPARTMENT OF TRANSPORTATION

**INDEX  
AND  
STANDARD  
DRAWINGS**

**ESTIMATED ROADWAY QUANTITIES**

ITEM NO.	DESCRIPTION	UNIT	QUANTITY
105-01	CONSTRUCTION STAKES, LINES AND GRADES	LS	1
201-01	CLEARING AND GRUBBING	LS	1
203-01	ROAD & DRAINAGE EXCAVATION (UNCLASSIFIED)	C.Y.	2347
203-04	PLACING AND SPREADING TOPSOIL	C.Y.	500
(1) 209-05	SEDIMENT REMOVAL	C.Y.	5
(1) 209-08.03	TEMPORARY SILT FENCE (WITHOUT BACKING)	L.F.	1700
(1) 209-08.07	ROCK CHECK DAM	EACH	8
(1) 209-08.08	ENHANCED ROCK CHECK DAM	EACH	1
303-01	MINERAL AGGREGATE, TYPE A BASE, GRADING D	TON	3699
303-10.01	MINERAL AGGREGATE (SIZE 57)	TON	15
307-01.01	ASPHALT CONCRETE MIX (PG64-22) (BPMB-HM) GRADING A	TON	55
307-01.08	ASPHALT CONCRETE MIX (PG64-22) (BPMB-HM) GRADING B-M2	TON	35
307-02.01	ASPHALT CONCRETE MIX (PG70-22) (BPMB-HM) GRADING A	TON	59
307-02.08	ASPHALT CONCRETE MIX (PG70-22) (BPMB-HM) GRADING B-M2	TON	58
402-01	BITUMINOUS MATERIAL FOR PRIME COAT (PC)	TON	1
402-02	AGGREGATE FOR COVER MATERIAL (PC)	TON	3
403-01	BITUMINOUS MATERIAL FOR TACK COAT (TC)	TON	1
411-01.07	ACS MIX (PG64-22) GRADING E SHOULDER	TON	36
411-01.10	ACS MIX (PG64-22) GRADING D	TON	52
411-02.10	ACS MIX (PG70-22) GRADING D	TON	131
415-01.02	COLD PLANING BITUMINOUS PAVEMENT	S.Y.	483
607-39.02	18" PIPE CULVERT (SIDE DRAIN)	L.F.	74
(2) 705-04.50	PORTABLE BARRIER RAIL DELINEATOR	EACH	28
(2) 705-08.51	PORTABLE IMPACT ATTENUATOR NCHRP350 TL-3	EACH	4
709-05.05	MACHINED RIPRAP (CLASS A-3)	TON	125
712-01	TRAFFIC CONTROL	LS	1
712-02.02	INTERCONNECTED PORTABLE BARRIER RAIL	L.F.	1100
712-04.01	FLEXIBLE DRUMS (CHANNELIZING)	EA.	25
712-06	SIGNS (CONSTRUCTION)	S.F.	144
712-08.03	ARROW BOARD (TYPE C)	EA.	2
713-15.07	SUSPENDED FLAT SHEET ALUMINUM SIGNS (0.080" THICK)	SF	94
713-16.01	CHANGEABLE MESSAGE SIGN UNIT	EA.	2
716-02.05	PLASTIC PAVEMENT MARKING (STOP LINE)	L.F.	75
716-02.06	PLASTIC PAVEMENT MARKING (TURN LANE ARROW)	EACH	2
716-02.04	PLASTIC PAVEMENT MARKING (CHANNELIZED STRIPING)	S.Y.	12
716-05.01	PAINTED PAVEMENT MARKING (4" LINE)	L.M.	0.500
716-13.01	SPRAY THERMO PVMT MRKNG (60 MIL) (4IN LINE)	L.M.	0.568
717-01	MOBILIZATION	LS	1
730-02.09	SIGNAL HEAD ASSEMBLY (130 WITH BACKPLATE)	EACH	6
730-02.17	SIGNAL HEAD ASSEMBLY (150 A2H WITH BACKPLATE)	EACH	2
730-03.21	INSTALL PULL BOX (TYPE B)	EA	7
(3) 730-05.01	ELECTRICAL SERVICE CONNECTION	EA	1
730-05.02	SERVICE CABLE (2 CONDUCTOR, # 8 AWG)	LF	60
730-08.03	SIGNAL CABLE - 7 CONDUCTOR	L.F.	720
730-12.01	CONDUIT 1" DIAMETER (PVC)	LF	400
730-12.02	CONDUIT 2" DIAMETER (PVC)	LF	250
730-12.13	CONDUIT 2" DIAMETER (JACK AND BORE)	LF	440
730-13.03	VEHICLE DETECTOR (4-CHANNEL, RACK MOUNT)	EA	2
730-14.01	SHIELDED DETECTOR CABLE	LF	1110
730-14.02	SAW SLOT	LF	800
730-14.03	LOOP WIRE	LF	2200
730-15.32	CABINET (EIGHT PHASE BASE MOUNTED)	EA	1
730-16.02	EIGHT PHASE ACTUATED CONTROLLER	EA	1
730-23.96	CANTILEVER SIGNAL SUPPORT (2@50'&40')	EA	1
730-23.97	CANTILEVER SIGNAL SUPPORT (2@50'&50')	EA	1
740-10.03	GEOTEXTILE (TYPE III) (EROSION CONTROL)	S.Y.	185
740-11.02	TEMPORARY SEDIMENT TUBE 12IN (EPSC)	L.F.	500
801-01	SEEDING (WITH MULCH)	UNIT	34
801-01.07	TEMPORARY SEEDING (WITH MULCH)	UNIT	54
801-03	WATER (SEEDING & SODDING)	M.G.	11
(4) 803-01	SODDING (NEW SOD)	S.Y.	183

**FOOTNOTES:**

- (1) EPSC ITEMS. SEE SUBSECTION 209.07 OF THE STANDARD SPECIFICATIONS FOR MAINTENANCE REPLACEMENT. ALL QUANTITIES ARE TO BE USED AS DIRECTED BY THE ENGINEER.
- (2) THIS ITEM SHALL BE A PORTABLE ENERGY ABSORBING TERMINAL MEETING THE REQUIREMENTS OF NCHRP 350 FOR TEST LEVEL 3. EXAMPLES WOULD BE A QUAD-GUARD, A REACT 350 OR A TRACC. THE PAY ITEM WILL INCLUDE FURNISHING AND INSTALLING ALL COMPONENTS AS SHOWN ON THE MANUFACTURER'S DRAWING.
- (3) CONTRACTOR TO BE RESPONSIBLE FOR COSTS OF OBTAINING ELECTRICAL SERVICE CONNECTION.
- (4) FOR "V" DITCH. INCLUDES QUANTITY FOR 2.0 LF ON EITHER SIDE OF DITCH BOTTOM (STA. 101+50 LT TO 104+50 LT & 102+00 RT TO 103+11.75 RT).

TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	2013	HSIP-258(B)	2

REV. 01-14-14: UPDATED EROSION CONTROL QUANTITIES.

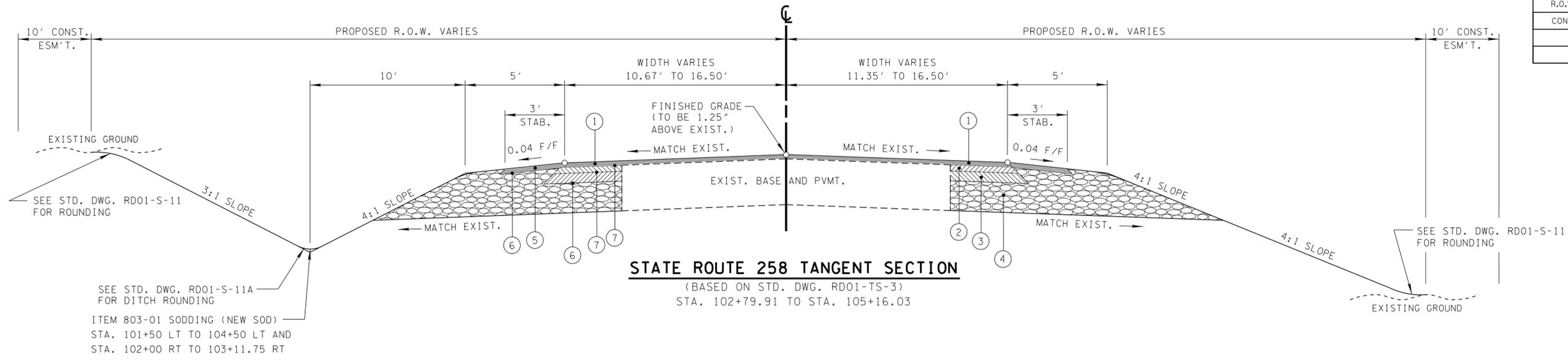
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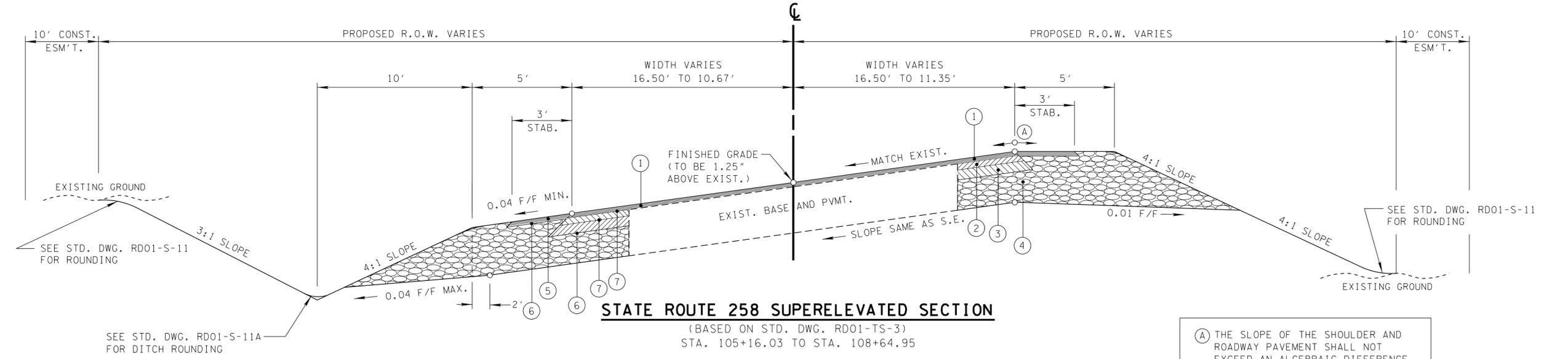
STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

**ESTIMATED  
ROADWAY  
QUANTITIES**

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2013	HSIP-258(B)	2
CONST.	2013	HSIP-258(B)	2A



SEE STD. DWG. RD01-S-11A FOR DITCH ROUNDING  
ITEM 803-01 SODDING (NEW SOD)  
STA. 101+50 LT TO 104+50 LT AND  
STA. 102+00 RT TO 103+11.75 RT



SEE STD. DWG. RD01-S-11A FOR DITCH ROUNDING

(A) THE SLOPE OF THE SHOULDER AND ROADWAY PAVEMENT SHALL NOT EXCEED AN ALGEBRAIC DIFFERENCE OF 0.07 FT./FT.

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**PROPOSED PAVEMENT SCHEDULE**

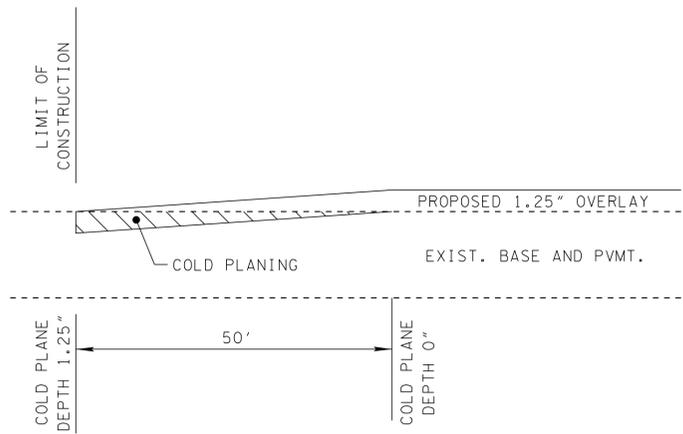
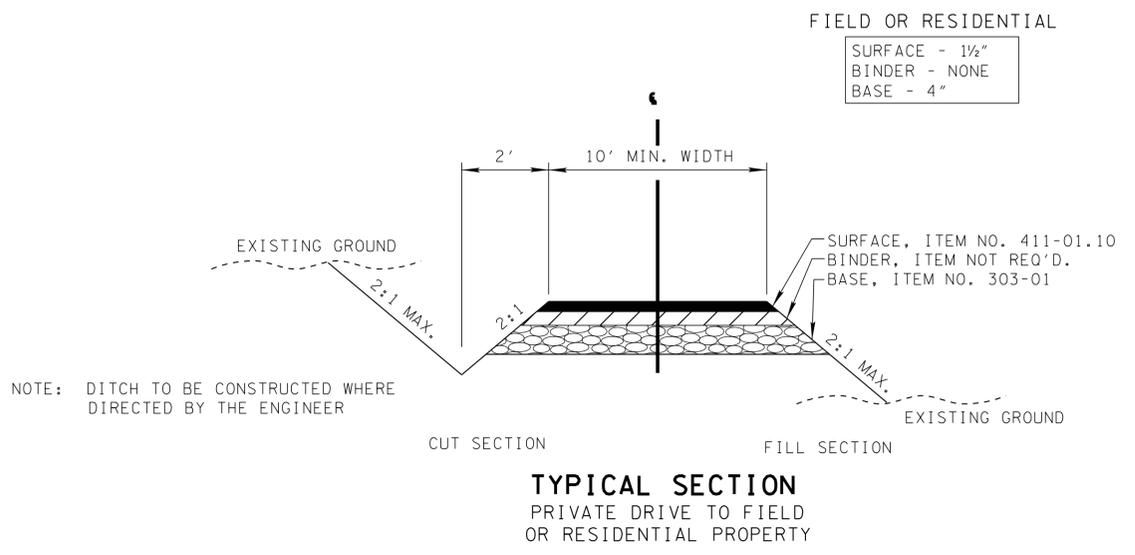
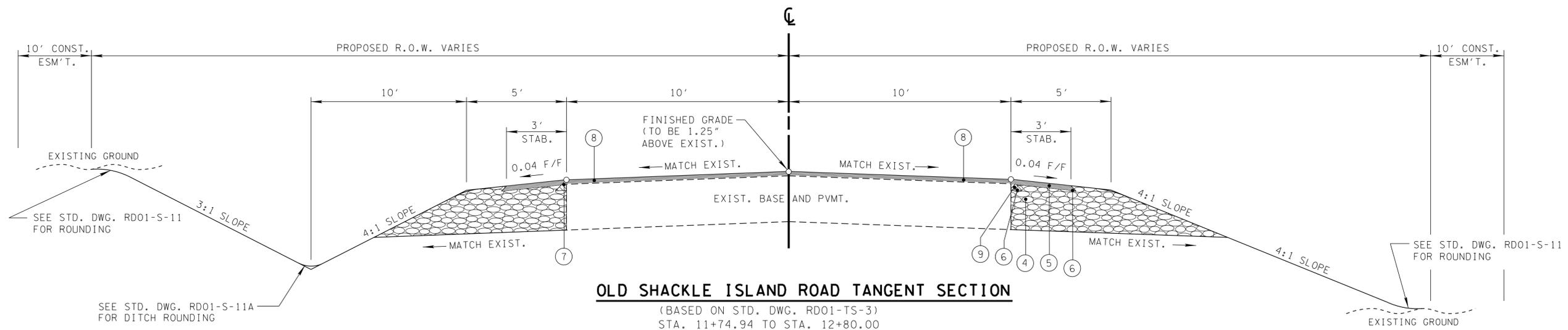
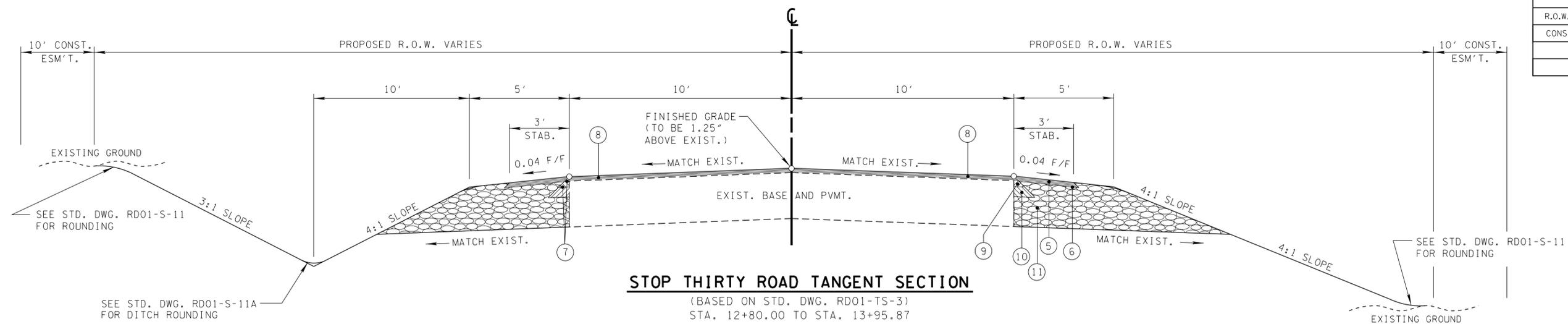
① SURFACE (APPROX. 132.5 LBS./S.Y.) (1.25"± DEPTH) ITEM 411-02.10 ACS MIX (PG70-22) GRADING D	⑤ SURFACE (APPROX. 154.5 LBS./S.Y.) (1.50"± DEPTH) ITEM 411-01.07 ACS MIX (PG64-22) GRADING E SHOULDER	⑨ BINDER (APPROX. 226 LBS./S.Y.) (2"± DEPTH) ITEM 307-01.08 ASPHALT CONCRETE MIX (PG 64-22) (BPMB-HM) GRADING B-M2
② BINDER (APPROX. 226 LBS./S.Y.) (2"± DEPTH) ITEM 307-02.08 ASPHALT CONCRETE MIX (PG 70-22) (BPMB-HM) GRADING B-M2	⑥ PRIME COAT ITEM 402-01 BIT. MATERIAL FOR PRIME COAT (PC) @ 0.30-0.35 GAL/S.Y. ITEM 402-02 AGGREGATE FOR COVER MATERIAL (PC) @ 8-12 LBS./S.Y.	⑩ BLACK BASE (APPROX. 345.00 LBS./S.Y.) (3.00"± DEPTH) ITEM 307-01.01 ASPHALT CONCRETE MIX (PG64-22) (BPMB-HM) GRADING A
③ BLACK BASE (APPROX. 373.75 LBS./S.Y.) (3.25"± DEPTH) ITEM 307-02.01 ASPHALT CONCRETE MIX (PG70-22) (BPMB-HM) GRADING A	⑦ TACK COAT ITEM 403-01 BIT. MATERIAL FOR TACK COAT (TC) @ 0.02 GAL/S.Y.	⑪ MINERAL AGGREGATE BASE (8"± DEPTH) ITEM 303-01 MINERAL AGGREGATE TYPE A BASE, GRADING D
④ MINERAL AGGREGATE BASE (10"± DEPTH) ITEM 303-01 MINERAL AGGREGATE TYPE A BASE, GRADING D	⑧ SURFACE (APPROX. 132.5 LBS./S.Y.) (1.25"± DEPTH) ITEM 411-01.10 ACS MIX (PG64-22) GRADING D	

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**TYPICAL SECTIONS**

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2013	HSIP-258(8)	2A
CONST.	2013	HSIP-258(8)	2B



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**TYPICAL SECTIONS**

# GENERAL NOTES

## GRADING

- ANY AREA THAT IS DISTURBED OUTSIDE LIMITS OF CONSTRUCTION DURING THE LIFE OF THIS PROJECT SHALL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE.
- THE CONTRACTOR SHALL NOT DISPOSE OF ANY MATERIAL EITHER ON OR OFF STATE-OWNED R.O.W. IN A REGULATORY FLOOD WAY AS DEFINED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY WITHOUT APPROVAL BY SAME. ALL MATERIAL SHALL BE DISPOSED OF IN UPLAND (NON-WETLAND) AREAS AND ABOVE ORDINARY HIGH WATER OF ANY ADJACENT WATERCOURSE. THIS DOES NOT ELIMINATE THE NEED TO OBTAIN ANY OTHER LICENSES OR PERMITS THAT MAY BE REQUIRED BY ANY OTHER FEDERAL, STATE OR LOCAL AGENCY.

## SEEDING AND SODDING

- SOD SHALL BE PLACED AT LOCATIONS SHOWN ON THE PLANS TO PREVENT DAMAGE TO ADJACENT FACILITIES AND PROPERTY DUE TO EROSION ON ALL NEWLY GRADED CUT AND FILL SLOPES AS WORK PROGRESSES.
- ITEM NO. 801-01, SEEDING (WITH MULCH), SHALL BE USED WHERE EROSION CONTROL BLANKET OR SOD ARE NOT APPLIED.

## DRAINAGE

- THE CONTRACTOR SHALL SHAPE DITCHES TO THE SPECIFIED DESIGN. THIS WORK WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COST WILL BE INCLUDED IN THE COST OF OTHER ITEMS.
- THE CUTTING OF INLET AND OUTLET DITCHES WHERE SHOWN ON PLANS OR AS DIRECTED BY THE ENGINEER WILL BE MEASURED AND PAID FOR AS ITEM NO. 203-01 ROAD AND DRAINAGE EXCAVATION (UNCLASSIFIED).
- DURING CONSTRUCTION OF DRAINAGE STRUCTURES ALL COST ASSOCIATED WITH MAINTAINING THE FLOW OF WATER AND TRAFFIC, AT THESE STRUCTURES, DURING THE PHASED CONSTRUCTION OF THIS PROJECT ARE TO BE INCLUDED IN THE UNIT PRICE OF THE DRAINAGE STRUCTURES AND TRAFFIC CONTROL ITEMS.

## MISCELLANEOUS

- THE CONTRACTOR SHALL BE REQUIRED TO REMOVE AND RESET MAILBOXES WHERE AND AS DIRECTED BY THE ENGINEER.
- NOTHING IN THE GENERAL NOTES OR SPECIAL PROVISIONS SHALL RELIEVE THE CONTRACTOR FROM HIS RESPONSIBILITIES TOWARD THE SAFETY AND CONVENIENCE OF THE GENERAL PUBLIC AND THE RESIDENTS ALONG THE PROPOSED CONSTRUCTION AREA

## PAVEMENT MARKINGS

### TEMPORARY PAVEMENT MARKING ON INTERMEDIATE LAYERS

- TEMPORARY PAVEMENT LINE MARKINGS ON INTERMEDIATE LAYERS OF PAVEMENT SHALL BE REFLECTIVE TAPE OR REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAYS WORK. SHORT, UNMARKED SECTIONS SHALL NOT BE ALLOWED. THESE MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-05.01, PAINTED PAVEMENT MARKING (4" LINE), L.M.

### FINAL PAVEMENT MARKING IF 4" ENHANCED FLATLINE THERMOPLASTIC IS USED

- PERMANENT PAVEMENT LINE MARKINGS SHALL BE 4" ENHANCED FLATLINE THERMOPLASTIC INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-13.01, SPRAY THERMO PVMT MRKNG (60 MIL) (4IN LINE), L.M. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.

## DETOURS, LANE SHIFTS AND MEDIAN CROSS-OVERS

- THE PAVEMENT MARKING ON THE LANE SHIFT FOR CENTERLINES, EDGELINES AND LANE LINES WILL BE INSTALLED AND MAINTAINED TO THE SAME STANDARDS AS FOR PERMANENT MARKINGS ON THE MAIN ROADWAY. THESE MARKINGS SHALL BE IN PLACE PRIOR TO ALLOWING TRAFFIC ONTO THE PAVEMENT. THESE PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-05.01, LIN. MI.

## PAVEMENT

### PAVING

- THE CONTRACTOR SHALL BE REQUIRED TO COLD PLANE AND PAVE IN THE DIRECTION OF TRAFFIC.

### SIGNALIZATION

- EQUIPMENT AND INSTALLATION OF TRAFFIC SIGNALS SHALL COMPLY WITH TDOT STANDARD SPECIFICATIONS, SECTION 730.
- IF RESURFACING IS INCLUDED IN THE PROJECT, SIGNAL DETECTION LOOPS SHALL BE INSTALLED BEFORE THE FINAL SURFACE IS APPLIED.
- ANY SIGNAL HEADS, WHEN VISIBLE TO DRIVERS BUT NOT OPERATIONAL, SHALL BE COMPLETELY COVERED.
- SIGNAL HEADS SHALL FLASH A MINIMUM OF SEVEN (7) DAYS PRIOR TO ACTIVATION OF THE SIGNAL.
- THE PROJECT ENGINEER SHALL NOTIFY THE LOCAL GOVERNMENTAL AGENCY RESPONSIBLE FOR TRAFFIC CONTROL MAINTENANCE AT LEAST ONE DAY IN ADVANCE OF THE COLD PLANING ACTIVITY AT SIGNALIZED INTERSECTIONS WHERE DETECTOR LOOPS ARE ON THE PAVEMENT. THE MAINTAINING AGENCY WILL THEN BE RESPONSIBLE FOR DISCONNECTING THE LOOP DETECTORS AND MAKING ANY NECESSARY TIMING ADJUSTMENTS IN THE SIGNAL CONTROLLER PRIOR TO THE CONSTRUCTION.
- LOOPS SHALL BE INSTALLED IN THE LEVELING COURSE IF A LEVELING COURSE IS PROVIDED.
- LOOP REPLACEMENT SHALL BE IN ACCORDANCE WITH SECTION 730 OF THE STANDARD SPECIFICATIONS.

### CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

- ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE WEEK BEFORE NEEDED, IF THE SIGN FACE IS FULLY COVERED.
- IF THE CONTRACTOR MOVES OFF THE PROJECT, HE SHALL COVER OR REMOVE ALL UNNEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OF REMOVAL, COVERING, AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO 712-06, SIGNS (CONSTRUCTION) PER SQUARE FOOT.
- A LONG TERM BUT SPORADIC USE WARNING SIGN, SUCH AS A FLAGGER SIGN, MAY REMAIN IN PLACE WHEN NOT REQUIRED PROVIDED THE SIGN FACE IS FULLY COVERED.
- TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.
- USE OF BARRICADES, PORTABLE BARRIER RAILS, VERTICAL PANELS, AND DRUMS SHALL BE LIMITED TO THE IMMEDIATE AREAS OF CONSTRUCTION WHERE A HAZARD IS PRESENT. THESE DEVICES SHALL NOT BE STORED ALONG THE ROADWAY WITHIN THIRTY (30) FEET OF THE EDGE OF THE TRAVELED WAY BEFORE OR AFTER USE UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL INCREASE TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. THESE DEVICES SHALL BE REMOVED FROM THE CONSTRUCTION WORK ZONE WHEN THE ENGINEER DETERMINES THEY ARE NO LONGER NEEDED. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.
- THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY, WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHEN THE LANE IS OPEN TO TRAFFIC UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO PARK WITHIN THIRTY (30) FEET OF A OPEN TRAFFIC LANE AT ANY TIME UNLESS

PROTECTED AS DESCRIBED ABOVE FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE.. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.

- ALL DETOUR AND CONSTRUCTION SIGNING SHALL BE IN STRICT ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

### EROSION PREVENTION AND SEDIMENT CONTROL DISTURBED AREA

- AREAS TO BE UNDISTURBED SHALL BE CLEARLY MARKED IN THE FIELD BEFORE CONSTRUCTION ACTIVITIES BEGIN.
- PRE-CONSTRUCTION VEGETATIVE GROUND COVER SHALL NOT BE DESTROYED, REMOVED OR DISTURBED (I.E. CLEARING AND GRUBBING INITIATED) MORE THAN 15 CALENDAR DAYS PRIOR TO GRADING OR EARTH MOVING ACTIVITIES UNLESS THE AREA IS MULCHED, SEEDED WITH MULCH, OR OTHER TEMPORARY COVER IS INSTALLED.
- CLEARING, GRUBBING, AND OTHER DISTURBANCE TO RIPARIAN VEGETATION SHALL BE LIMITED TO THE MINIMUM NECESSARY FOR SLOPE CONSTRUCTION AND EQUIPMENT OPERATIONS. EXISTING VEGETATION SHOULD BE PRESERVED TO THE MAXIMUM EXTENT POSSIBLE. UNNECESSARY VEGETATION REMOVAL IS PROHIBITED.
- ALL DISTURBED AREAS SHALL BE PROPERLY STABILIZED AS SOON AS PRACTICABLE. PRIORITY SHALL BE GIVEN TO FINISHING OPERATIONS AND PERMANENT EPSC MEASURES OVER TEMPORARY EPSC MEASURES ON ALL PROJECTS.
- CONSTRUCTION SHALL BE SEQUENCED AND STAGED TO MINIMIZE THE EXPOSURE TIME OF GRADED OR DENUDED SOIL AREAS, PRESERVE TOPSOIL, AND MINIMIZE SOIL COMPACTION.

### SEDIMENT CONTROL

- EPSC MEASURES SHALL BE INSTALLED AND FUNCTIONAL PRIOR TO ANY EARTH MOVING OPERATIONS, AND SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD.
- THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT THE OFF-SITE MIGRATION OR DEPOSIT OF SEDIMENT ON ROADWAYS USED BY THE GENERAL PUBLIC. IF SEDIMENT ESCAPES THE CONSTRUCTION SITE, OFF-SITE ACCUMULATIONS OF SEDIMENT THAT HAVE NOT REACHED A STREAM MUST BE REMOVED AT A FREQUENCY SUFFICIENT TO MINIMIZE OFF-SITE IMPACTS (E.G., FUGITIVE SEDIMENT THAT HAS ESCAPED THE CONSTRUCTION SITE AND HAS COLLECTED IN A STREET MUST BE REMOVED SO THAT IT IS NOT SUBSEQUENTLY WASHED INTO STORM SEWERS AND STREAMS BY THE NEXT RAIN AND/OR SO THAT IT DOES NOT POSE A SAFETY HAZARD TO USERS OF PUBLIC STREETS). ARRANGEMENTS CONCERNING REMOVAL OF SEDIMENT ON ADJOINING PROPERTY MUST BE SETTLED WITH THE ADJOINING PROPERTY OWNER BEFORE REMOVAL OF SEDIMENT.
- CHECK DAMS SHALL BE USED WHERE RUNOFF IS CONCENTRATED. CLEAN ROCK, BRUSH, GABION, OR SANDBAG CHECK DAMS SHALL BE PROPERLY CONSTRUCTED TO REDUCE VELOCITY AND CONTROL EROSION.
- IF PERMANENT OR TEMPORARY VEGETATION IS TO BE USED AS AN EPSC MEASURE, THEN THE TIMING OF PLANTING OF VEGETATION SHALL BE SHOWN IN THE SWPPP. DELAYING PLANTING OF COVER VEGETATION UNTIL WINTER MONTHS OR DRY MONTHS SHOULD BE AVOIDED, IF POSSIBLE.
- OFFSITE VEHICLE TRACKING OF SEDIMENTS AND THE GENERATION OF DUST SHALL BE MINIMIZED. A STABILIZED CONSTRUCTION ACCESS (A POINT OF ENTRANCE/EXIT TO THE CONSTRUCTION PROJECT) SHALL BE PROVIDED, AS NEEDED, TO REDUCE THE TRACKING OF MUD AND DIRT ONTO PUBLIC ROADS BY CONSTRUCTION VEHICLES.
- TEMPORARY EPSC MEASURES MAY BE REMOVED AT THE BEGINNING OF THE WORKDAY, BUT MUST BE REPLACED AT THE END OF THE WORKDAY.

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**GENERAL  
NOTES**

# GENERAL NOTES (CONTINUED)

## STREAM/WETLAND

- (1) SOIL MATERIALS MUST BE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. EPSC MEASURES TO PROTECT WATER QUALITY MUST BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD. APPROPRIATE EPSC MEASURES MUST BE INSTALLED ALONG THE BASE OF ALL FILLS AND CUTS, ON THE DOWNHILL SIDE OF STOCKPILED SOIL, AND ALONG STREAM BANKS IN CLEARED AREAS TO PREVENT SEDIMENT MIGRATION INTO STREAMS IN ACCORDANCE WITH TDOT STANDARDS. THEY MUST BE INSTALLED ON THE CONTOUR, ENTRENCHED AND STAKED, AND EXTEND THE WIDTH OF THE AREA TO BE CLEARED.

## INSPECTION, MAINTENANCE, REPAIR

- (2) EPSC CONTROLS WILL BE MAINTAINED IN ACCORDANCE WITH TDOT STANDARD DRAWINGS AND GOOD ENGINEERING PRACTICES.
- (3) INSPECTION, REPAIR, AND MAINTENANCE OF EPSC MEASURES/STRUCTURES IS TO BE PERFORMED ON A REGULAR BASIS. SEDIMENT SHALL BE REMOVED FROM SEDIMENT CONTROL STRUCTURES WHEN THE DESIGN CAPACITY HAS BEEN REDUCED BY FIFTY PERCENT (50%). DURING SEDIMENT REMOVAL, THE CONTRACTOR SHALL TAKE CARE TO ENSURE THAT STRUCTURAL COMPONENTS OF EPSC MEASURES ARE NOT DAMAGED AND THUS MADE INEFFECTIVE. IF DAMAGE DOES OCCUR, THE CONTRACTOR SHALL REPAIR THE STRUCTURES AT THE CONTRACTOR'S OWN EXPENSE.
- (4) SEDIMENT REMOVED FROM SEDIMENT CONTROL STRUCTURES SHALL BE PLACED AND BE TREATED IN A MANNER SO THAT THE SEDIMENT IS CONTAINED WITHIN THE PROJECT LIMITS AND DOES NOT MIGRATE INTO WATERS OF THE STATE/U.S. COST FOR THIS TREATMENT IS TO BE INCLUDED IN PRICE BID FOR ITEM NO. 209-05 SEDIMENT REMOVAL, C.Y.
- (5) THE CONTRACTOR SHALL INSTALL A RAIN GAUGE EVERY LINEAR MILE AT ALL SITES WHERE CLEARING, GRUBBING, EXCAVATION, GRADING CUTTING OR FILLING IS BEING ACTIVELY PERFORMED, OR EXPOSED SOIL HAS NOT YET BEEN PERMANENTLY STABILIZED. IF THE PROJECT LENGTH IS LESS THAN ONE LINEAR MILE, ONE RAIN GAUGE SHALL BE INSTALLED AT THE CENTER OF THE PROJECT OR AS INDICATED BY THE TDOT EPSC INSPECTOR. THE CONTRACTOR SHALL ENSURE THAT EACH GAUGE IS MAINTAINED IN GOOD WORKING CONDITION. TDOT AND/OR THE CONTRACTOR SHALL RECORD DAILY PRECIPITATION AND FORECASTED PERCENTAGE OF PRECIPITATION IN DETAILED RECORDS OF RAINFALL EVENTS INCLUDING DATES, AMOUNTS OF RAINFALL PER GAUGE, THE ESTIMATED DURATION (OR STARTING AND ENDING TIMES), AND FORECASTED PERCENTAGE OF PRECIPITATION FOR THE PROJECT. THIS INFORMATION SHALL BE PROVIDED TO THE ENGINEER ON A MONTHLY BASIS. THE COST FOR THE RAIN GAUGES IS TO BE INCLUDED IN THE UNIT BID PRICES FOR OTHER ITEMS. RAIN GAUGES SHALL BE AS SPECIFIED IN THE APPROVED TDOT RAINFALL MONITORING PLAN.
- (6) INSPECTION OF EPSC MEASURES SHALL BE DONE AT LEAST TWICE PER CALENDAR WEEK AT LEAST 72 HOURS APART. A CALENDAR WEEK IS DEFINED AS SUNDAY THROUGH SATURDAY. QUALITY ASSURANCE/QUALITY CONTROL SITE ASSESSMENT OF EPSC SHALL BE PERFORMED PER THE TDOT ENVIRONMENTAL DIVISION'S COMPREHENSIVE INSPECTION OFFICE GUIDELINES.
- (7) OUTFALL POINTS SHALL BE INSPECTED TO ASCERTAIN WHETHER EPSC MEASURES ARE EFFECTIVE IN PREVENTING SIGNIFICANT IMPACTS TO SURROUNDING WATERS. WHERE DISCHARGE LOCATIONS ARE INACCESSIBLE, NEARBY DOWNSTREAM LOCATIONS SHALL BE INSPECTED. LOCATIONS WHERE VEHICLES ENTER AND EXIT THE SITE SHALL BE INSPECTED FOR EVIDENCE OF OFF-SITE ROADWAY SEDIMENT TRACKING.
- (8) UPON CONCLUSION OF THE INSPECTIONS, EPSC MEASURES FOUND TO BE INEFFECTIVE SHALL BE REPAIRED, REPLACED, OR MODIFIED BEFORE THE NEXT RAIN EVENT, IF POSSIBLE, BUT IN NO CASE MORE THAN 24 HOURS AFTER THE INSPECTION OR WHEN THE CONDITION IS IDENTIFIED. IF THE REPAIR, REPLACEMENT OR MODIFICATION IS NOT PRACTICAL WITHIN THE TIMEFRAME, WRITTEN DOCUMENTATION MUST BE PROVIDED IN THE FIELD BOOK AND AN ESTIMATED REPAIR, REPLACEMENT OR MODIFICATION SCHEDULE SHALL BE DOCUMENTED WITHIN 24 HOURS AFTER IDENTIFICATION.
- (9) THE TDOT PROJECT SUPERVISOR (OR THEIR DESIGNEE) AND THE CONTRACTOR'S SITE SUPERINTENDENT ARE RESPONSIBLE FOR INSPECTIONS. MAINTENANCE AND REPAIR ACTIVITIES ARE THE RESPONSIBILITY OF THE CONTRACTOR. THE TDOT PROJECT SUPERVISOR OR THEIR DESIGNEE WILL COMPLETE THE INSPECTION REPORTS AND DISTRIBUTE COPIES PER THE CONTRACT.

## MATERIALS

- (10) WASTE AND BORROW AREAS SHALL BE LOCATED IN NON-WETLAND AREAS AND ABOVE THE 100-YEAR, FEDERAL EMERGENCY MANAGEMENT AGENCY FLOODPLAIN. BORROW AND WASTE DISPOSAL AREAS SHALL NOT AFFECT ANY WATERS OF THE STATE/U.S. UNLESS THESE AREAS ARE SPECIFICALLY COVERED BY AN ARAP, 404, OR NPDES PERMIT, OBTAINED SOLELY BY THE CONTRACTOR.

## SWPPP, PERMITS, PLANS, RECORDS

- (11) THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR AND OBTAIN ANY NECESSARY ENVIRONMENTAL PERMITS OR APPROVALS, INCLUDING BUT NOT LIMITED TO TDEC ARAP/401, USACE SECTION 404, TVA SECTION 26A, AND TDEC NPDES PERMITS, FROM FEDERAL, STATE AND/OR LOCAL AGENCIES REGARDING THE OPERATION OF ANY PROJECT-DEDICATED ASPHALT AND/OR CONCRETE PLANTS.
- (12) ANY DISAGREEMENT BETWEEN THE PROJECT PLANS, THE PROJECT AS CONSTRUCTED, AND THE PERMIT(S) ISSUED FOR THE PROJECT, SHALL BE BROUGHT TO THE ATTENTION OF THE TDOT PROJECT ENGINEER. THE ENVIRONMENTAL DIVISION, DESIGN DIVISION, AND HEADQUARTERS CONSTRUCTION OFFICE SHALL BE CONTACTED IN THESE INSTANCES AND DECIDE WHICH HAS PRECEDENCE AND WHETHER PERMIT OR PLANS REVISIONS ARE NEEDED. IN GENERAL, PERMIT CONDITIONS WILL PREVAIL.
- (13) THE FOLLOWING INFORMATION SHALL BE MAINTAINED ON OR NEAR THE SITE: DATES THAT MAJOR GRADING ACTIVITIES OCCUR, DATES WHERE CONSTRUCTION ACTIVITIES TEMPORARILY OR PERMANENTLY CEASE ON A PORTION OF THE SITE, DATES WHEN STABILIZATION MEASURES ARE INITIATED, EPSC INSPECTION RECORDS, QUALITY ASSURANCE SITE ASSESSMENT RECORDS, PRECIPITATION RECORDS, SWPPP, PROJECT ENVIRONMENTAL PERMITS, AND A COPY OF THE PROJECT EPSC INSPECTOR'S TDEC LEVEL 1 CERTIFICATION.
- (14) ALL WATER QUALITY AND STORM WATER PERMITS, INCLUDING A COPY OF THE NOC WITH NPDES PERMIT TRACKING NUMBER AND THE LOCATION OF THE SWPPP, SHALL BE POSTED NEAR THE MAIN ENTRANCE OF THE CONSTRUCTION SITE ACCESSIBLE TO THE PUBLIC. THE NAME, COMPANY NAME, EMAIL ADDRESS, TELEPHONE NUMBER AND ADDRESS OF THE PROJECT SITE OWNER, OPERATOR, OR A LOCAL CONTACT PERSON WITH A BRIEF DESCRIPTION OF THE PROJECT SHALL ALSO BE POSTED. IF POSTING THIS INFORMATION NEAR A MAIN ENTRANCE IS INFEASIBLE, THE INFORMATION SHALL BE PLACED IN A PUBLICLY ACCESSIBLE LOCATION NEAR WHERE THE CONSTRUCTION IS ACTIVELY UNDERWAY AND MOVED AS NECESSARY. THIS LOCATION SHALL BE POSTED AT THE CONSTRUCTION SITE. ALL POSTINGS SHALL BE MAINTAINED IN LEGIBLE CONDITION.
- (15) IF A CHANGE IN PROJECT SCOPE OCCURS DURING CONSTRUCTION, INCLUDING VALUE ENGINEERING, THE ENVIRONMENTAL DIVISION SHALL BE CONTACTED TO DETERMINE WHETHER PERMIT REVISIONS OR MODIFICATIONS OF THE SWPPP ARE NEEDED. THE DESIGN DIVISION SHALL BE CONTACTED TO DETERMINE IF ANY PLAN REVISIONS ARE NEEDED.
- (16) THE SWPPP SHALL BE UPDATED BY CONSTRUCTION WHENEVER EPSC INSPECTIONS INDICATE, OR WHERE STATE OR FEDERAL OFFICIALS DETERMINE EPSC MEASURES ARE PROVING INEFFECTIVE IN ELIMINATING OR SIGNIFICANTLY MINIMIZING POLLUTANT SOURCES OR ARE OTHERWISE NOT ACHIEVING THE GENERAL OBJECTIVES OF CONTROLLING POLLUTANTS IN STORM WATER DISCHARGES ASSOCIATED WITH THE CONSTRUCTION ACTIVITY. THE ENVIRONMENTAL DIVISION SHALL BE CONTACTED WHEN MAJOR DESIGN REVISIONS ARE REQUESTED BY CONSTRUCTION. THE ENVIRONMENTAL DIVISION MAY BE CONTACTED FOR GUIDANCE ON SPECIFIC SWPPP NEEDS. A COPY OF ANY CORRESPONDENCE REGARDING THE EFFECTIVENESS OF THE SWPPP OR EPSC CONTROLS SHALL BE RETAINED IN THE SWPPP.
- (17) PROJECT INSPECTORS AND SUPERVISORS (INCLUDING TDOT STAFF, CONSULTANTS AND CONTRACTOR STAFF) RESPONSIBLE FOR THE IMPLEMENTATION AND MAINTENANCE OF EPSC PLANS SHALL SUCCESSFULLY COMPLETE THE TDEC "LEVEL 1 - FUNDAMENTALS OF EROSION PREVENTION AND SEDIMENT CONTROL FOR CONSTRUCTION SITES" COURSE AND ANY REFRESHER COURSES AS REQUIRED TO MAINTAIN CERTIFICATION. A COPY OF CERTIFICATION RECORDS FOR THE COURSES SHALL BE KEPT ON SITE AND AVAILABLE UPON REQUEST.

- (18) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT LITTER, CONSTRUCTION DEBRIS, AND CONSTRUCTION WASTES FROM ENTERING WATERS OF THE STATE/U.S. THESE MATERIALS WILL BE PICKED UP AND REMOVED FROM STORMWATER EXPOSURE PRIOR TO ANTICIPATED STORM EVENTS. AFTER USE, MATERIALS USED FOR EPSC WILL BE REMOVED FROM THE SITE.

## LITTER, DEBRIS, WASTE, PETROLEUM

- (18) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT LITTER, CONSTRUCTION DEBRIS, AND CONSTRUCTION WASTES FROM ENTERING WATERS OF THE STATE/U.S. THESE MATERIALS WILL BE PICKED UP AND REMOVED FROM STORMWATER EXPOSURE PRIOR TO ANTICIPATED STORM EVENTS. AFTER USE, MATERIALS USED FOR EPSC WILL BE REMOVED FROM THE SITE.

- (19) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS TO ENSURE THAT PETROLEUM PRODUCTS OR OTHER CHEMICAL POLLUTANTS ARE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. ALL EQUIPMENT REFUELING, SERVICING, AND STAGING AREAS SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL LAWS, RULES, REGULATIONS, AND ORDINANCES, INCLUDING THOSE OF THE NATIONAL FIRE PROTECTION ASSOCIATION (NFPA). APPROPRIATE CONTAINMENT MEASURES FOR THESE AREAS SHALL BE USED. ALL SPILLS MUST BE REPORTED TO THE APPROPRIATE AGENCY, AND MEASURES SHALL BE TAKEN IMMEDIATELY TO PREVENT THE POLLUTION OF WATERS OF THE STATE/U.S., INCLUDING GROUNDWATER, SHOULD A SPILL OCCUR.

# SPECIAL NOTES

## SIGNALIZATION

- (1) THE DESIGN OF TRAFFIC SIGNAL SUPPORT POLES, MAST ARMS, STRAIN POLES, ETC. SHALL BE IN CONFORMANCE WITH THE AASHTO STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES AND TRAFFIC SIGNALS, CURRENT EDITION. OVERHEAD CANTILEVERED TRAFFIC SIGNAL STRUCTURES SHALL BE DESIGNED FOR FATIGUE CATEGORY 1.
- (2) ALL CIRCULAR AND ARROW INDICATIONS WITHIN ALL VEHICULAR SIGNAL HEADS PROPOSED FOR THIS PROJECT SHALL CONSIST OF AN LED (LIGHT EMITTING DIODE) SIGNAL MODULE UNLESS OTHERWISE NOTED IN THE PLANS.
- (3) ALL PEDESTRIAN TRAFFIC CONTROL INDICATIONS, WHERE CALLED FOR, SHALL CONSIST OF LED MODULES DISPLAYING "WALKING PERSON" AND "HAND" SYMBOLS WITHIN THE SAME FACU UNLESS OTHERWISE NOTED IN THE PLANS.
- (4) CIRCULAR INDICATIONS SHALL MEET "ITE VTC SH-LED CIRCULAR SIGNAL SUPPLEMENT" FOR EXPANDED/EXTENDED VIEW.
- (5) ARROW INDICATIONS SHALL MEET "ITE VTC SH-3 LED ARROW SPECIFICATION" FOR EXPANDED/EXTENDED VIEW.
- (6) PEDESTRIAN INDICATIONS SHALL MEET "ITE PTC SI PART 2".
- (7) INCANDESCENT OR SCREW-IN MODULES ARE NOT ACCEPTABLE.
- (8) COMPATIBILITY WITH CONFLICT MONITORS AND LOAD SWITCHES SHALL BE TESTED AND CONFIRMED.
- (9) MANUFACTURER SHALL PROVIDE A MINIMUM FIVE YEAR WARRANTY FOR OPERATION OF THE UNIT.
- (10) ALL SIGNAL HEADS SHALL INCLUDE SWIVEL BALANCE ADJUSTERS TO MAINTAIN THE PROPER VISIBILITY. COSTS OF ADJUSTERS TO BE INCLUDED IN COSTS OF SIGNAL HEADS.
- (11) THE ATTACHMENT OF THE TETHER WIRE TO THE POLE SHALL BE LOCATED BELOW THE LOWEST ELEVATION OF THE SIGNAL HEADS.

## EROSION PREVENTION AND SEDIMENT CONTROL

### NPDES

- (1) REFER TO THE EROSION PREVENTION AND SEDIMENT CONTROL PLAN, SHEET 7, FOR NOTES REGARDING SEASONAL WORK LIMITATION OR LIMITATION ON THE TOTAL AREA OF EXPOSED SOIL.

TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	2013	HSIP-258(8)	2D

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**GENERAL NOTES  
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SPECIAL NOTES**

TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	2013	HSIP-258(8)	2E

ESTIMATED GRADING QUANTITIES							
STATION TO STATION	ROAD & DRAINAGE EXC. (UNCL.)		BORROW EXCAVATION		CHANNEL EXC. C.Y.	EXCESS EXC. WASTE C.Y.	EMB. C.Y.
	COMMON - C.Y.	S. ROCK - C.Y.	UNCL. - C.Y.	S. ROCK - C.Y.			
S.R. 258	1741					1630	92
OLD SHACKLE ISLAND	325					0	0
STOP THIRTY	281					0	0
<b>TOTALS</b>	<b>2347</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1630</b>	<b>92</b>

SHRINKAGE = 20%

PAVEMENT QUANTITIES											
LOCATION	PAY ITEMS										
	303-01 (TON)	307-01.01 (TON)	307-01.08 (TON)	307-02.01 (TON)	307-02.08 (TON)	402-01 (TON)	402-02 (TON)	403-01 (TON)	411-01.07 (TON)	411-01.10 (TON)	411-02.10 (TON)
S.R. 258	3334.21			58.31	57.29	0.86	2.63	0.82	35.59		130.80
OLD SHACKLE ISLAND	171.60		34.47							20.14	
STOP THIRTY	142.72	54.55								20.92	
DR. RT. 103+32.27	22.11										
DR. LT. 106+63.41	28.28									10.07	
<b>TOTALS</b>	<b>3698.92</b>	<b>54.55</b>	<b>34.47</b>	<b>58.31</b>	<b>57.29</b>	<b>0.86</b>	<b>2.63</b>	<b>0.82</b>	<b>35.59</b>	<b>51.13</b>	<b>130.80</b>

SIDE DRAIN TABULATION														
STATION	LOCATION		DESCRIPTION	SURFACE WIDTH (L.F.)	RCP CLASS III OR CMP 16 GA. OR HDPE OR PVC OR SRTRP (L.F.) FILL HEIGHT < OR = 10 FT.					END TREATMENT			REMARKS	
					18"	24"	30"	36"	48"	TYPE	DRAWING NO.	CLASS A CONC.		REINF. STEEL
	611-07.01 (C.Y.)	611-07.02 (LB.)										611-07.03 (LB.)		
103+32.27		X	PVT. DR.	30	44									NO EW. REQ'D.
106+63.41	X		PVT. DR.	16	30									NO EW. REQ'D.
<b>TOTALS</b>					<b>74</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	

PERMANENT SIGN QUANTITIES										
ITEM NO.	DESCRIPTION	UNIT	QUANTITY	SIZE	ITEM NO. 713-15.07 (S.F.)	M.U.T.C.D. NO.	MATERIAL	THICKNESS	S.N.S. LETTER SIZE	REMARKS
713-15.07	OLD SHACKLE ISLAND RD	S.F.	1	120" X 18"	15	D3-1	ALUMINUM	0.080"	12" UC/9"LC	SHEET 11, SIGN S1
713-15.07	STOP THIRTY RD	S.F.	1	96" X 18"	12	D3-1	ALUMINUM	0.080"	12" UC/9"LC	SHEET 11, SIGN S2
713-15.07	NEW SHACKLE ISLAND RD	S.F.	2	120" X 18"	30	D3-1	ALUMINUM	0.080"	12" UC/9"LC	SHEET 11, SIGN S3
713-15.07	NO TURN ON RED	S.F.	2	24" x 30"	10	R10-11	ALUMINUM	0.080"	-	SHEET 11, SIGN S4
713-15.07	STOP THIRTY RD	S.F.	1	96" X 18"	12	D3-1	ALUMINUM	0.080"	12" UC/9"LC	SHEET 11, SIGN S5
713-15.07	OLD SHACKLE ISLAND RD	S.F.	1	120" X 18"	15	D3-1	ALUMINUM	0.080"	12" UC/9"LC	SHEET 11, SIGN S6
<b>TOTAL</b>					<b>94</b>					

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**TABULATED QUANTITIES**

**RIGHT-OF-WAY NOTES**

- (1) IT IS INTENDED THAT ALL BUILDINGS AND/OR PORTIONS OF BUILDINGS THAT ARE WITHIN THE PROPOSED RIGHT-OF-WAY AND/OR EASEMENT LINES FOR THE PROJECT BE REMOVED THERE FROM IN THE PROCESS OF RIGHT-OF-WAY ACQUISITION. IF ANY SUCH BUILDINGS OR IMPROVEMENTS ARE NOT REMOVED IN THE COURSE OF RIGHT-OF-WAY ACQUISITION, THE CIVIL ENGINEERING MANAGER 2, DESIGN DIVISION IS TO BE NOTIFIED IN SUFFICIENT TIME TO PERMIT HAVING SUCH REMOVALS DESIGNATED AS A PART OF THE CONSTRUCTION CONTRACT.
- (2) ALL RAMPS MUST CONFORM TO THE DEPARTMENT'S "POLICY ON FINANCING CONSTRUCTION OF PUBLIC ROAD INTERSECTIONS AND DRIVEWAYS ON HIGHWAY RESURFACING, RECONSTRUCTION AND CONSTRUCTION PROJECTS ON NEW LOCATIONS", THE MANUAL ON RULES AND REGULATIONS FOR CONSTRUCTING DRIVEWAYS ON STATE HIGHWAY RIGHT-OF-WAY, STANDARD DRAWING RP-R-1, AND OTHER ACCEPTED DESIGN AND SAFETY STANDARDS.
- (3) EXISTING PAVED DRIVEWAY PER TRACT REMAINDER WILL BE REPLACED IN KIND TO A TOUCHDOWN POINT.
- (4) WHERE THE EXISTING DRIVEWAY IS UNPAVED AND THE PROPOSED DRIVEWAY EXCEEDS 7 PERCENT IN GRADE, EACH DRIVEWAY WILL BE PAVED TO A TOUCHDOWN POINT OR UNTIL THE GRADE IS LESS THAN 7 PERCENT.
- (5) WHERE THE EXISTING DRIVEWAY IS UNPAVED AND THE PROPOSED DRIVEWAY IS LESS THAN 7 PERCENT IN GRADE, EACH DRIVEWAY WILL BE PAVED A SHOULDER WIDTH FROM THE EDGE OF PAVEMENT AND THE REMAINDER OF THAT DRIVEWAY REPLACED IN KIND TO A TOUCHDOWN POINT.
- (6) ANY NECESSARY PAVING OF DRIVEWAYS WILL BE DONE DURING PAVING OPERATIONS ON THE MAIN ROADWAY.
- (7) NEW DRIVEWAYS PROVIDED IN THE PLANS WILL BE PAVED BASED ON THE 7 PERCENT CRITERIA. THOSE 7 PERCENT OR STEEPER IN GRADE WILL BE PAVED AND THOSE FLATTER THAN 7 PERCENT WILL BE COVERED WITH BASE STONE.
- (8) ON NON-STATE ROUTES, ADDITIONAL DRIVEWAYS AND FIELD ENTRANCES OTHER THAN THOSE PROVIDED IN THE PLANS SHALL REQUIRE A PERMIT ONLY IF THE LOCAL AGENCY SPECIFIES THE NEED FOR THAT PERMIT.
- (9) ON PROJECTS WITHOUT CURB AND GUTTER THAT ARE ON STATE ROUTES, IT WILL BE THE RESPONSIBILITY OF THE OWNER TO SECURE A PERMIT AND TO CONSTRUCT ADDITIONAL DRIVEWAYS AND FIELD ENTRANCES OTHER THAN THOSE PROVIDED IN THE PLANS.

**UTILITIES**

- (1) THE LOCATIONS OF UTILITIES SHOWN WITHIN THESE PLANS ARE APPROXIMATE ONLY. EXACT LOCATIONS SHALL BE DETERMINED IN THE FIELD BY CONTACTING THE UTILITY COMPANIES INVOLVED. NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC., AT 1-800-351-1111 AS REQUIRED BY TCA 65-31-106 WILL BE REQUIRED.
- (2) UNLESS OTHERWISE NOTED, ALL UTILITY ADJUSTMENTS WILL BE PERFORMED BY THE UTILITY OR ITS REPRESENTATIVE. THE CONTRACTOR AND UTILITY OWNERS WILL BE REQUIRED TO COOPERATE WITH EACH OTHER IN ORDER TO EXPEDITE THE WORK REQUIRED BY THIS CONTRACT. ON CONTRACTS WHERE CONSTRUCTION STAKES, LINES, AND GRADES ARE CONTRACT ITEMS, THE CONTRACTOR WILL BE REQUIRED TO PROVIDE RIGHT-OF-WAY OR SLOPE STAKES, DITCH OR STREAM BED GRADES, OR OTHER ESSENTIAL SURVEY STAKING TO PREVENT CONFLICTS WITH THE HIGHWAY CONSTRUCTION. FREQUENTLY, THIS WILL BE REQUIRED AS THE FIRST ITEM OF WORK AND AT ANY LOCATION ON THE PROJECT DIRECTED BY THE ENGINEER.
- (3) THE CONTRACTOR WILL PROVIDE ALL NECESSARY PROTECTIVE MEASURES TO SAFEGUARD EXISTING UTILITIES FROM DAMAGE DURING CONSTRUCTION OF THIS PROJECT. IN THE EVENT THAT SPECIAL EQUIPMENT IS REQUIRED TO WORK OVER AND AROUND THE UTILITIES, THE CONTRACTOR WILL BE REQUIRED TO FURNISH SUCH EQUIPMENT. THE COST OF PROTECTING UTILITIES FROM DAMAGE AND FURNISHING SPECIAL EQUIPMENT WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (4) PRIOR TO SUBMITTING HIS BID, THE CONTRACTOR WILL BE SOLELY RESPONSIBLE FOR CONTACTING OWNERS OF ALL AFFECTED UTILITIES IN ORDER TO DETERMINE THE EXTENT TO WHICH UTILITY RELOCATIONS AND/OR ADJUSTMENTS WILL HAVE UPON THE SCHEDULE OF WORK FOR THE PROJECT. WHILE SOME WORK MAY BE REQUIRED 'AROUND' UTILITY FACILITIES THAT WILL REMAIN IN PLACE, OTHER UTILITY FACILITIES MAY NEED TO BE ADJUSTED CONCURRENTLY WITH THE CONTRACTOR'S OPERATIONS. ADVANCE CLEAR CUTTING MAY BE REQUIRED BY THE ENGINEER AT ANY LOCATION WHERE CLEARING IS CALLED FOR IN THE SPECIFICATIONS AND CLEAR CUTTING IS NECESSARY FOR A UTILITY RELOCATION. ANY ADDITIONAL COST WILL BE INCLUDED IN THE UNIT PRICE BID FOR THE CLEARING ITEM SPECIFIED IN THE PLANS.
- (5) THE CONTRACTOR SHALL NOTIFY EACH INDIVIDUAL UTILITY OWNER OF HIS PLAN OF OPERATION IN THE AREA OF THE UTILITIES. PRIOR TO COMMENCING WORK, THE CONTRACTOR SHALL CONTACT THE UTILITY OWNERS AND REQUEST THEM TO PROPERLY LOCATE THEIR RESPECTIVE UTILITY ON THE GROUND. THIS NOTIFICATION SHALL BE GIVEN AT LEAST THREE (3) BUSINESS DAYS PRIOR TO COMMENCEMENT OF OPERATIONS AROUND THE UTILITY IN ACCORDANCE WITH TCA 65-31-106.

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2013	HSIP-258(B)	3
CONST.	2013	HSIP-258(B)	3

REV. 01-14-14; UPDATED PROPERTY RESOLUTION TO REFLECT CURRENT OWNERSHIP AND GEOMETRY FOR TRACT 2 AND 2A.

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STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

**RIGHT-OF-WAY  
NOTES AND  
ACQUISITION  
TABLE**

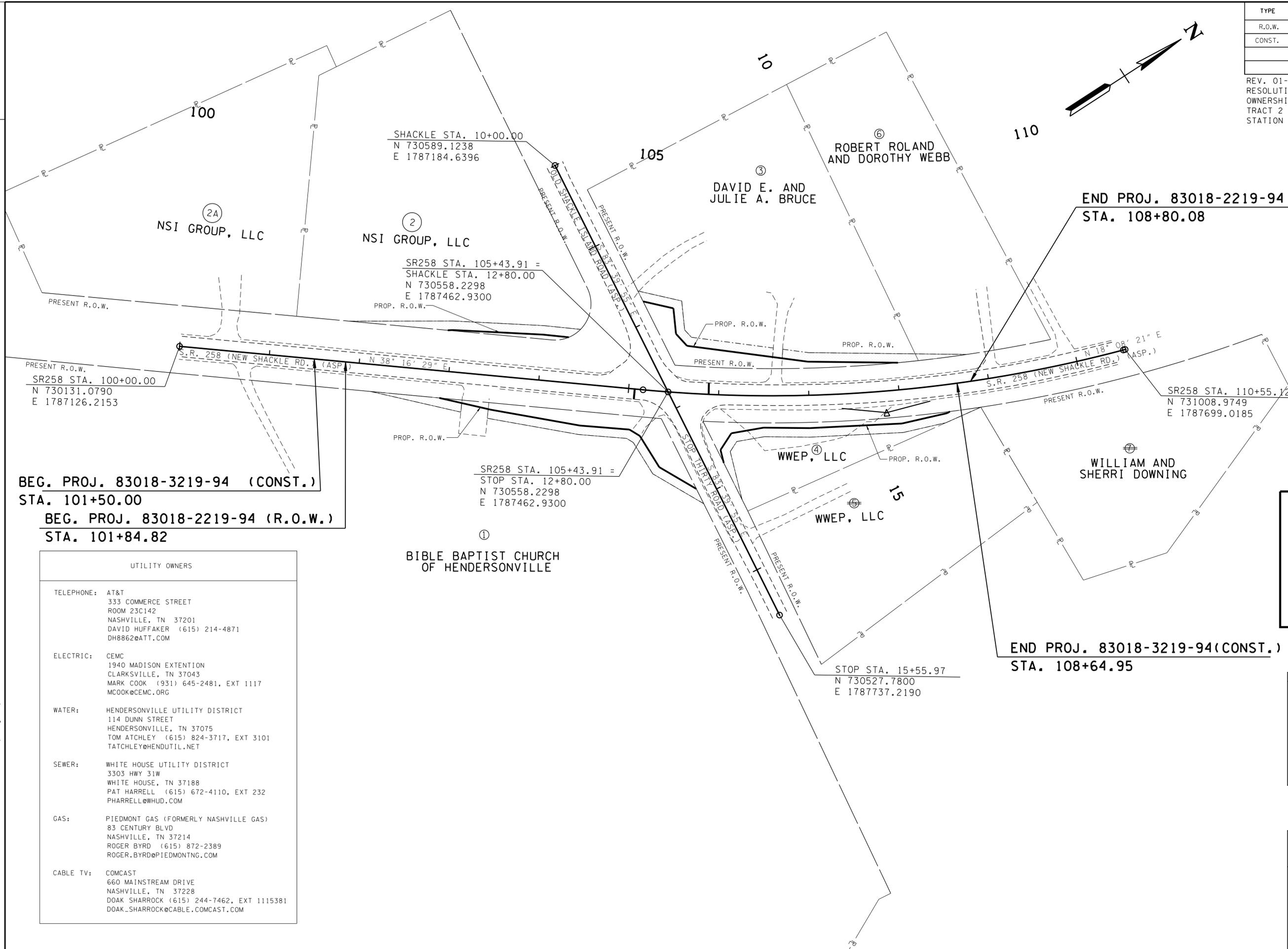
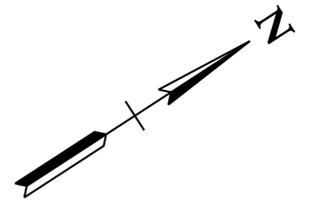
**R.O.W. ACQUISITION TABLE**

TRACT NO.	PROPERTY OWNERS	COUNTY RECORDS				TOTAL AREA ACRES			AREA TO BE ACQUIRED ACRES			AREA REMAINING ACRES		EASEMENT (SQUARE FEET)		
		TAX MAP NO.	PARCEL NO.	DEED DOCUMENT REFERENCE		LEFT	RIGHT	TOTAL	LEFT	RIGHT	TOTAL	LEFT	RIGHT	PERM. DRAINAGE	SLOPE	CONST. ①
				BK.	PAGE											
1	BIBLE BAPTIST CHURCH OF HENDERSONVILLE	145	56	1895	861		35.230	35.230		0.102	0.102		35.128			3299
2A	THE NSI GROUP, LLC	145	57	3846	670	1.520		1.520				1.520				
2	THE NSI GROUP, LLC	145	57	3846	670	1.840		1.840	401 S.F.		401 S.F.	1.831				1979
3	DAVID E. BRUCE AND JULIE A. BRUCE	145	15	3262	159	1.600		1.600	4010 S.F.		4010 S.F.	1.508				3265
4	WWEP, LLC	145	16	2770	375		0.480	0.480		2099 S.F.	2099 S.F.		0.432			3113
-5	WWEP, LLC	145	17	2770	373		1.200	1.200				1.200				
6	ROBERT ROLAND WEBB AND WIFE DOROTHY WEBB	145	15.01	336	809	1.100		1.100				1.100				221
-7	WILLIAM DOWNING AND WIFE SHERRI DOWNING	145	19	2825	594		1.300	1.300				1.300				

① FOR WORKING AREA AND EROSION CONTROL OUTSIDE RIGHT-OF-WAY.

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2013	HSIP-258(8)	3A
CONST.	2013	HSIP-258(8)	3A

REV. 01-14-14; UPDATED PROPERTY RESOLUTION TO REFLECT CURRENT OWNERSHIP AND GEOMETRY FOR TRACT 2 AND 2A. UPDATED STATION FOR BEG. PROJ.



**BEG. PROJ. 83018-3219-94 (CONST.)  
STA. 101+50.00**  
**BEG. PROJ. 83018-2219-94 (R.O.W.)  
STA. 101+84.82**

UTILITY OWNERS	
TELEPHONE:	AT&T 333 COMMERCE STREET ROOM 23C142 NASHVILLE, TN 37201 DAVID HUFFAKER (615) 214-4871 DH8862@ATT.COM
ELECTRIC:	CEMC 1940 MADISON EXTENTION CLARKSVILLE, TN 37043 MARK COOK (931) 645-2481, EXT 1117 MCOOK@CEMC.ORG
WATER:	HENDERSONVILLE UTILITY DISTRICT 114 DUNN STREET HENDERSONVILLE, TN 37075 TOM ATCHLEY (615) 824-3717, EXT 3101 TATCHLEY@HENDUTIL.NET
SEWER:	WHITE HOUSE UTILITY DISTRICT 3303 HWY 31W WHITE HOUSE, TN 37188 PAT HARRELL (615) 672-4110, EXT 232 PHARRELL@WHUD.COM
GAS:	PIEDMONT GAS (FORMERLY NASHVILLE GAS) 83 CENTURY BLVD NASHVILLE, TN 37214 ROGER BYRD (615) 872-2389 ROGER.BYRD@PIEDMONTNG.COM
CABLE TV:	COMCAST 660 MAINSTREAM DRIVE NASHVILLE, TN 37228 DOAK SHARROCK (615) 244-7462, EXT 1115381 DOAK_SHARROCK@CABLE.COMCAST.COM

**UNOFFICIAL  
SET  
NOT FOR  
BIDDING**

**END PROJ. 83018-3219-94 (CONST.)  
STA. 108+64.95**

SEALED BY

COORDINATES ARE NAD/83(1995), ARE DATUM ADJUSTED BY THE FACTOR OF 1.00003 AND TIED TO THE TGRN. ALL ELEVATIONS ARE REFERENCED TO THE NAVD 1988.

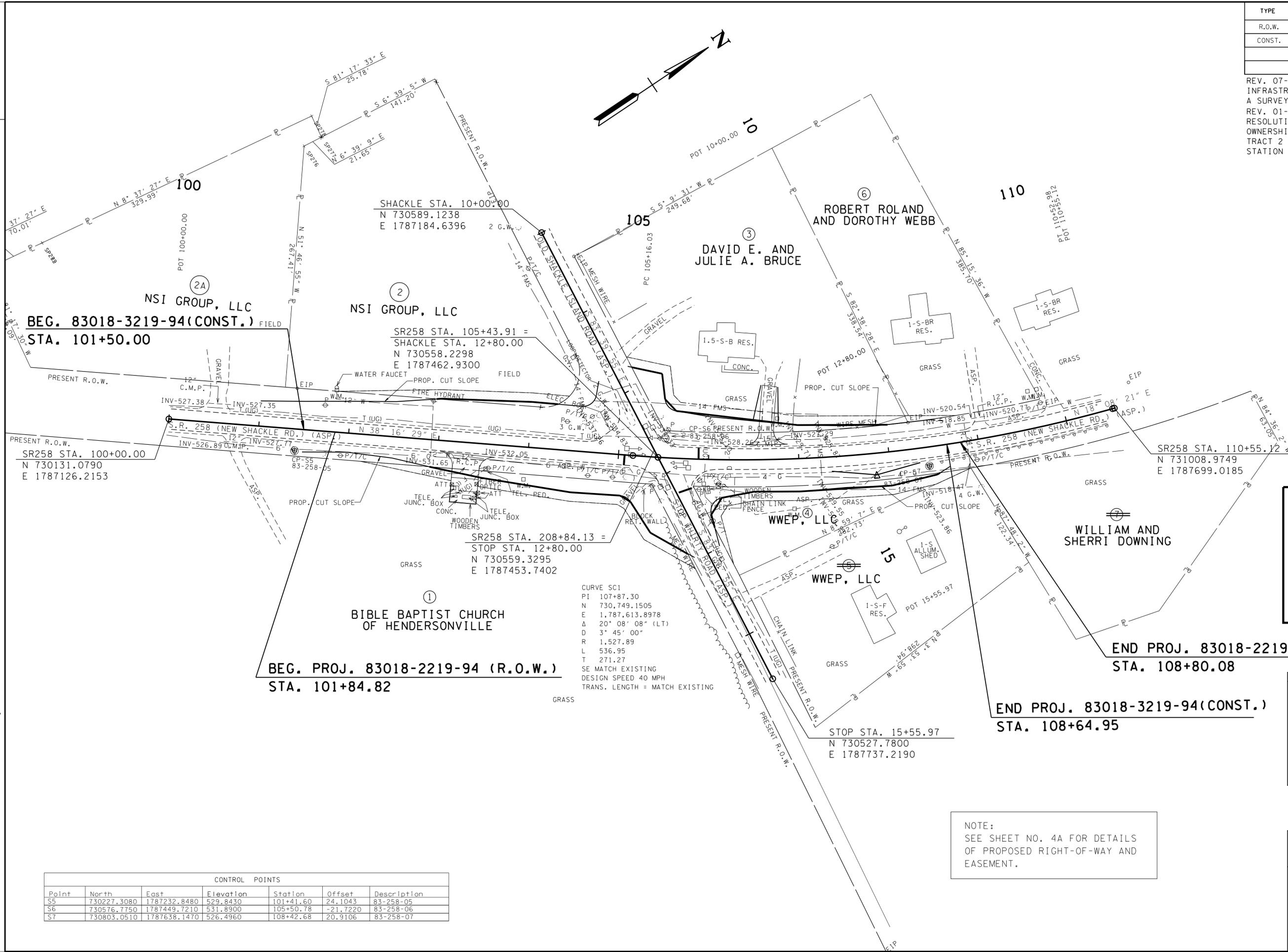
STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

**PROPERTY  
MAP**

SCALE: 1" = 50'

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2013	HSIP-258(8)	4
CONST.	2013	HSIP-258(8)	4

REV. 07-23-13: ADDED NEW UTILITY INFRASTRUCTURE AS DETERMINED BY A SURVEY UPDATE.  
REV. 01-14-14: UPDATED PROPERTY RESOLUTION TO REFLECT CURRENT OWNERSHIP AND GEOMETRY FOR TRACT 2 AND 2A. UPDATED STATION FOR BEG. PROJ.



**UNOFFICIAL SET**  
**NOT FOR BIDDING**

END PROJ. 83018-2219-94 (R.O.W.)  
STA. 108+80.08

END PROJ. 83018-3219-94 (CONST.)  
STA. 108+64.95

NOTE:  
SEE SHEET NO. 4A FOR DETAILS OF PROPOSED RIGHT-OF-WAY AND EASEMENT.

SEALED BY

COORDINATES ARE NAD/83(1995), ARE DATUM ADJUSTED BY THE FACTOR OF 1.00003 AND TIED TO THE TGRN. ALL ELEVATIONS ARE REFERENCED TO THE NAVD 1988.

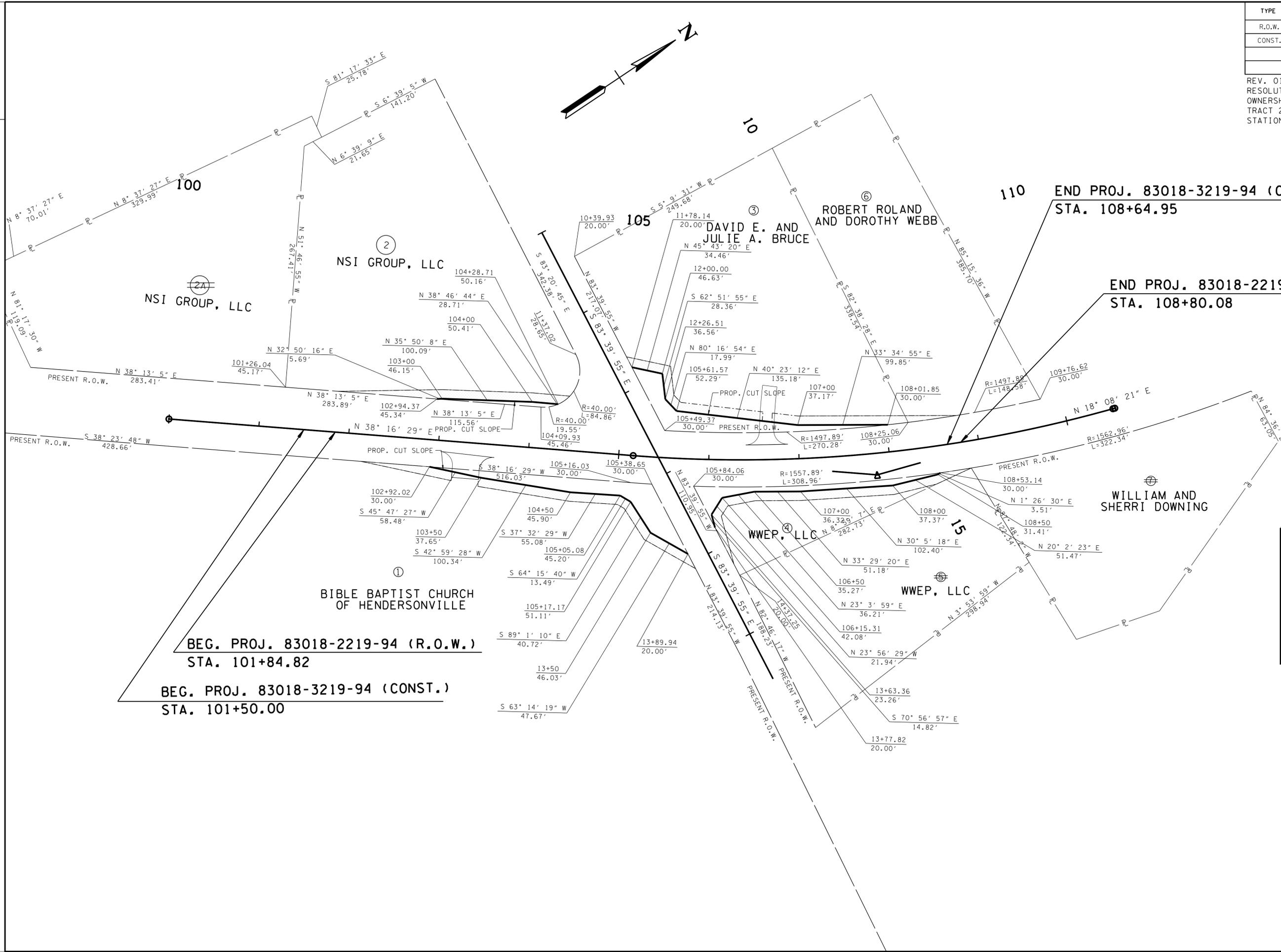
STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

**PRESENT LAYOUT**  
SCALE: 1" = 50'

Point	North	East	Elevation	Station	Offset	Description
S5	730227.3080	1787232.8480	529.8430	101+41.60	24.1043	83-258-05
S6	730576.7750	1787449.7210	531.8900	105+50.78	-21.7220	83-258-06
S7	730803.0510	1787638.1470	526.4960	108+42.68	20.9106	83-258-07

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2013	HSIP-258(B)	4A
CONST.	2013	HSIP-258(B)	4A

REV. 01-14-14: UPDATED PROPERTY RESOLUTION TO REFLECT CURRENT OWNERSHIP AND GEOMETRY FOR TRACT 2 AND 2A. UPDATED STATION FOR BEG. PROJ.



**UNOFFICIAL SET**  
**NOT FOR BIDDING**

SEALED BY

COORDINATES ARE NAD/83(1995), ARE DATUM ADJUSTED BY THE FACTOR OF 1.00003 AND TIED TO THE TGRN. ALL ELEVATIONS ARE REFERENCED TO THE NAVD 1988.

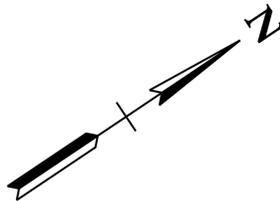
STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

**R.O.W. DETAILS**

SCALE: 1" = 50'

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2013	HSIP-258(8)	4B
CONST.	2013	HSIP-258(8)	4B

REV. 01-14-14: UPDATED STATION FOR BEG. PROJ.



BEG. PROJ. 83018-2219-94 (R.O.W.)  
STA. 101+84.82

END PROJ. 83018-3219-94 (CONST.)  
STA. 108+64.95

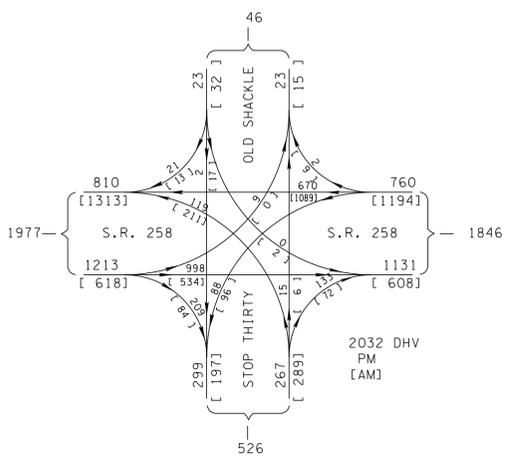
SR258 STA. 100+00.00  
N 730131.0790  
E 1787126.2153

SR258 STA. 110+55.12  
N 731008.9749  
E 1787699.0185

BEG. PROJ. 83018-3219-94 (CONST.)  
STA. 101+50.00

END PROJ. 83018-2219-94 (R.O.W.)  
STA. 108+80.08

SR258 STA. 105+43.91 =  
STOP THIRTY ROAD STA. 12+80.00 =  
OLD SHACKLE ISLAND ROAD STA. 12+80.00  
N 730558.2298  
E 1787462.9300



CURVE SC1  
PI 107+87.30  
N 730.749.1505  
E 1,787,613.8978  
Δ 20° 08' 08" (LT)  
D 3' 45" 00"  
R 1,527.89  
L 536.95  
T 271.27  
SE MATCH EXISTING  
DESIGN SPEED 40 MPH  
TRANS. LENGTH = MATCH EXISTING

2032 DHV  
PM  
[AM]

**UNOFFICIAL SET**  
**NOT FOR BIDDING**

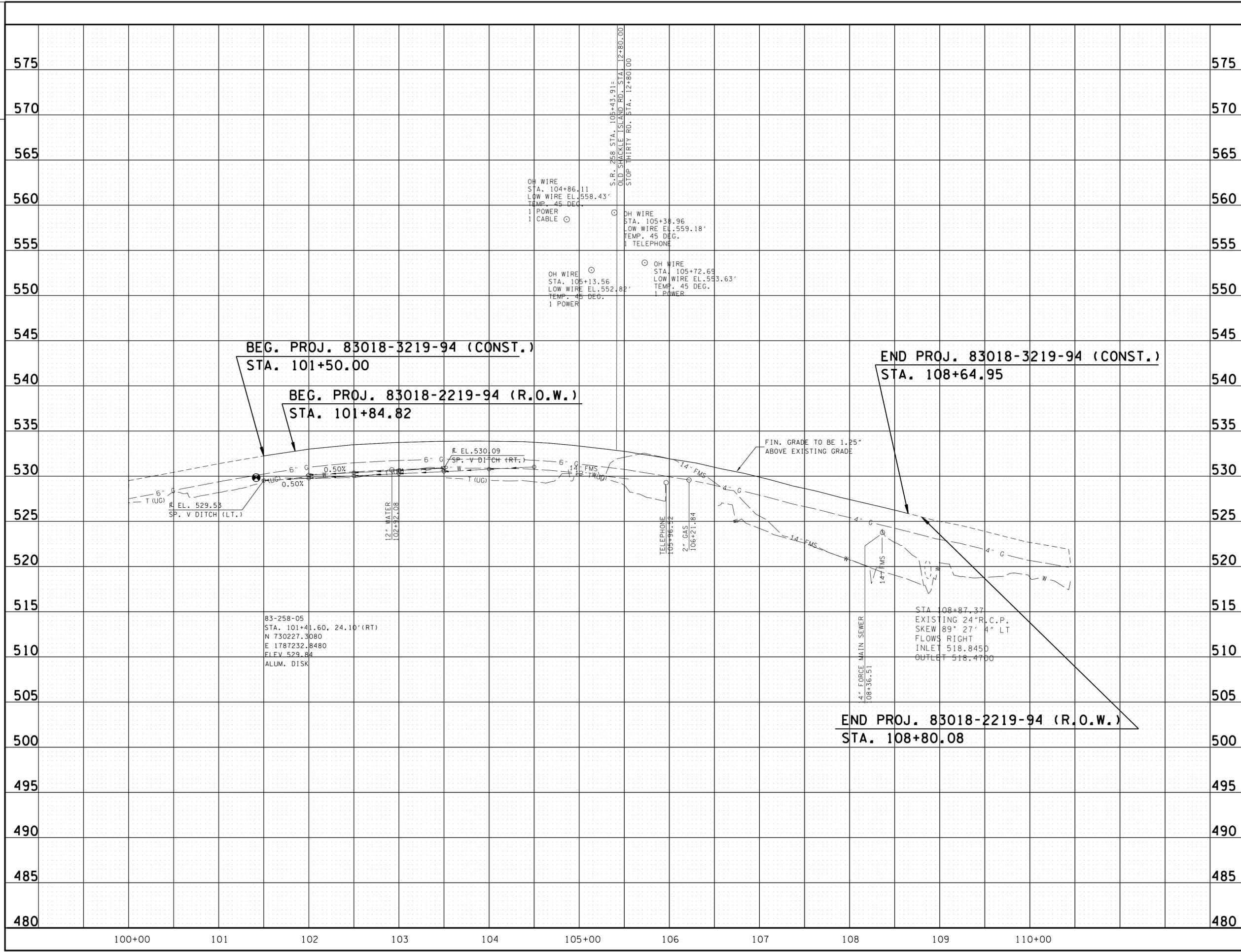
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COORDINATES ARE NAD/83(1995), ARE DATUM ADJUSTED BY THE FACTOR OF 1.00003 AND TIED TO THE TGRN. ALL ELEVATIONS ARE REFERENCED TO THE NAVD 1988.

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

**PROPOSED LAYOUT**

SCALE: 1" = 50'



TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2013	HSIP-258(8)	4C
CONST.	2013	HSIP-258(8)	4C

REV. 07-23-13: ADDED NEW UTILITY INFRASTRUCTURE AS DETERMINED BY A SURVEY UPDATE.

REV. 01-14-14: UPDATED STATION FOR BEG. PROJ.

**UNOFFICIAL SET**

**NOT FOR BIDDING**

SEALED BY

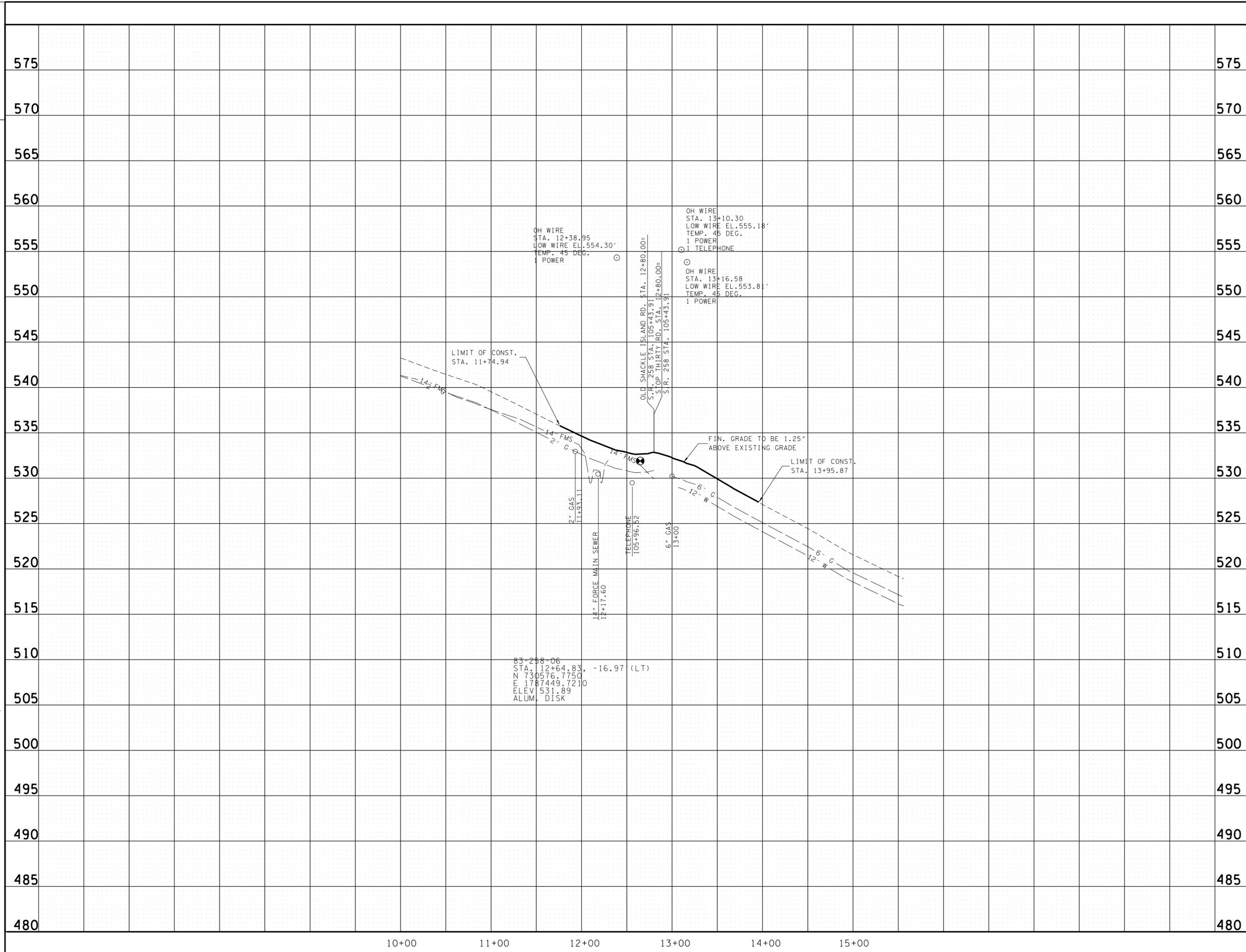
  

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

**PROFILE**

S.R. 258

SCALE: 1" = 50' HORIZ.  
1" = 5' VERT.



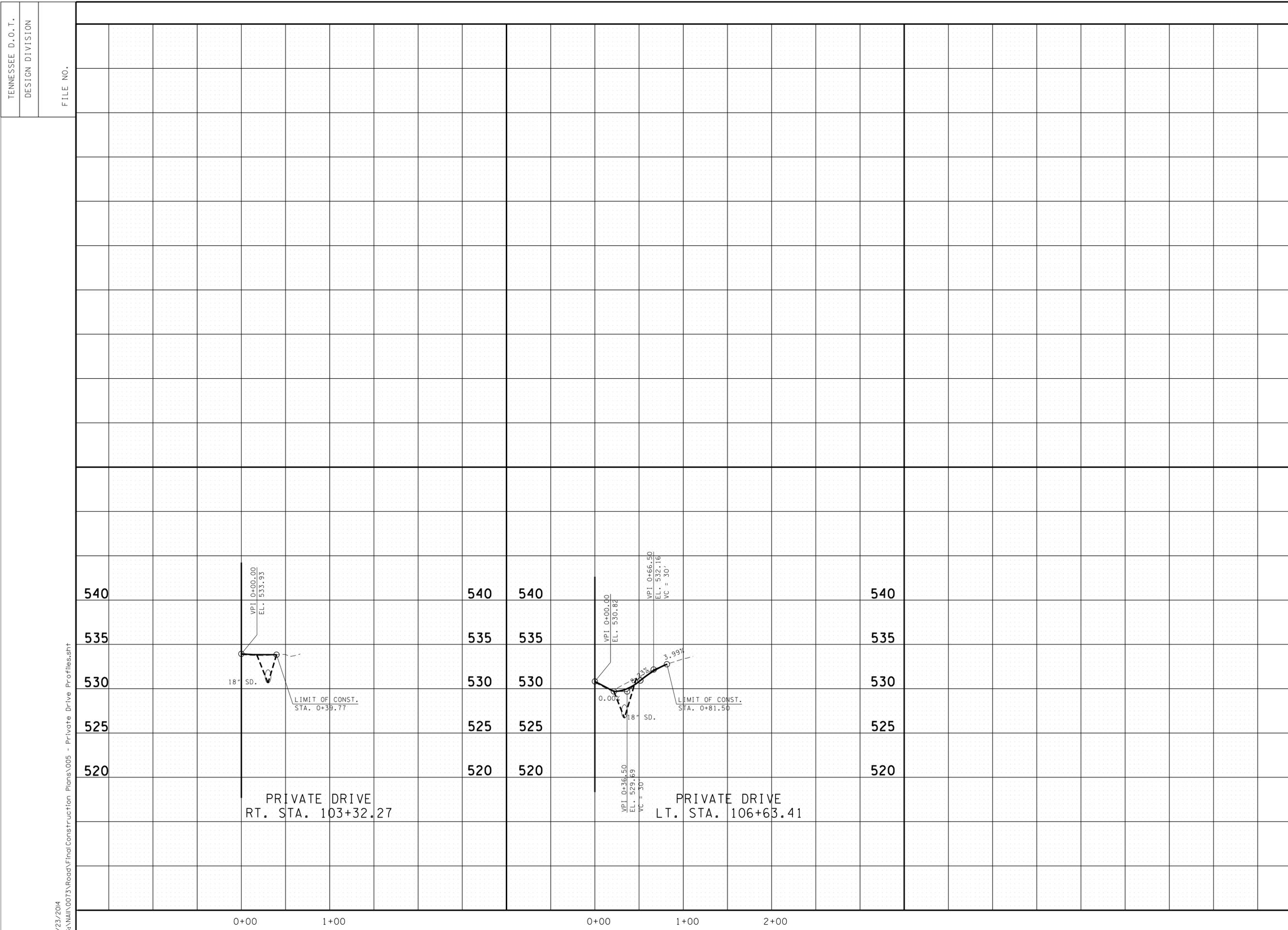
TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2013	HSIP-258(8)	4D
CONST.	2013	HSIP-258(8)	4D

REV. 07-23-13: ADDED NEW UTILITY INFRASTRUCTURE AS DETERMINED BY A SURVEY UPDATE.

**UNOFFICIAL SET**  
**NOT FOR BIDDING**

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STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
**PROFILE**  
OLD SHACKLE ISLAND ROAD & STOP THIRTY ROAD  
SCALE: 1" = 50' HORIZ.  
1" = 5' VERT.



TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2013	HSIP-258(B)	5
CONST.	2013	HSIP-258(B)	5

TENNESSEE D.O.T.  
DESIGN DIVISION  
FILE NO.

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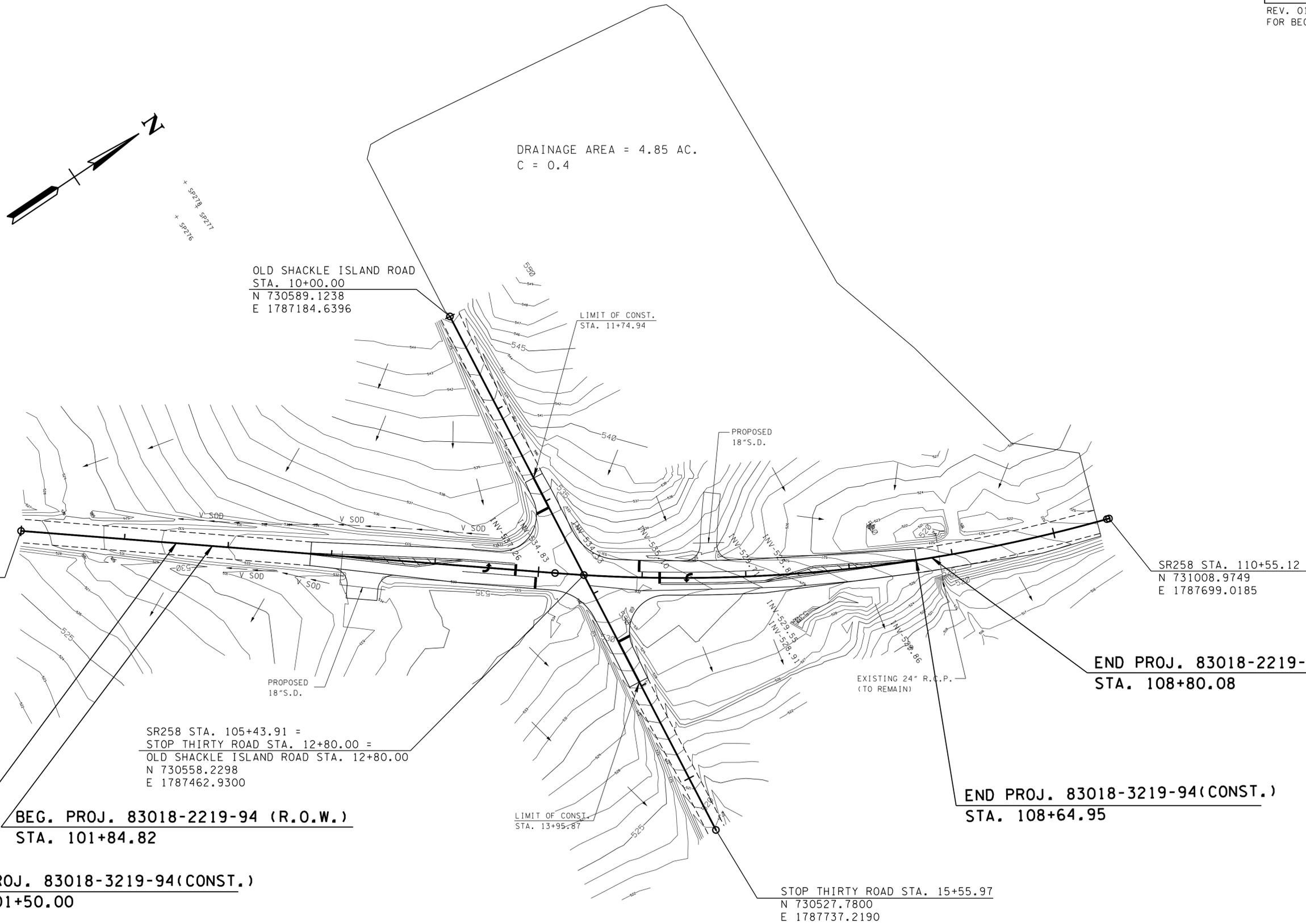
SEALED BY

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

**PROFILE OF PRIVATE DRIVES**  
SCALE: 1"=50' HORIZ.  
1"=5' VERT.

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2013	HSIP-258(B)	6
CONST.	2013	HSIP-258(B)	6

REV. 01-14-14: UPDATED STATION FOR BEG. PROJ.



**UNOFFICIAL SET**  
**NOT FOR BIDDING**

SEALED BY

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

**DRAINAGE MAP**

SCALE: 1"=50'

**EROSION, PREVENTION AND  
SEDIMENT CONTROL NOTES**

- (1) NO WORK SHALL BE STARTED UNTIL THE CONTRACTOR'S PLAN FOR THE STAGING OF THEIR OPERATIONS, INCLUDING THE PLAN FOR STAGING OF TEMPORARY AND PERMANENT EPSC MEASURES, HAS BEEN ACCEPTED BY THE ENGINEER. THE CONTRACTOR'S EPSC PLAN SHALL INCORPORATE AND SUPPLEMENT, AS ACCEPTABLE, THE BASIC EPSC DEVICES ON THE EPSC PLAN CONTAINED IN THE APPROVED SWPPP.
- (2) THE EPSC MEASURES AND/OR PLAN SHALL BE MODIFIED AS NECESSARY SO THAT THEY ARE EFFECTIVE AT ALL TIMES THROUGHOUT THE COURSE OF THE PROJECT.
- (3) THE ACCEPTED EPSC PLAN SHALL REQUIRE THAT EPSC MEASURES BE IN PLACE BEFORE CLEARING, GRUBBING, EXCAVATION, GRADING, CUTTING OR FILLING OCCURS, EXCEPT AS SUCH WORK MAY BE NECESSARY TO INSTALL EPSC MEASURES, INCLUDING WITHOUT LIMITATION AS FOLLOWS:
  - A. INITIAL CLEARING AND GRUBBING SHALL BE LIMITED TO THAT NECESSARY FOR THE INSTALLATION OF APPLICABLE EPSC MEASURES IN ACCORDANCE WITH THE ACCEPTED EPSC PLAN INCORPORATED INTO THE SWPPP.
  - B. NO OTHER CLEARING AND GRUBBING OPERATIONS SHALL BE STARTED BEFORE APPLICABLE EPSC MEASURES ARE IN PLACE IN ACCORDANCE WITH THE ACCEPTED EPSC PLAN INCORPORATED INTO THE SWPPP.
  - C. NO CULVERT OR BRIDGE CONSTRUCTION SHALL BE STARTED BEFORE APPLICABLE EPSC MEASURES ARE IN PLACE IN ACCORDANCE WITH THE ACCEPTED EPSC PLAN INCORPORATED INTO THE SWPPP.
  - D. NO GRADING, EXCAVATION, CUTTING, FILLING, OR OTHER EARTHWORK SHALL BE STARTED BEFORE EPSC MEASURES ARE IN PLACE IN ACCORDANCE WITH THE ACCEPTED EPSC PLAN INCORPORATED INTO THE SWPPP.
- (4) PERMANENT EPSC MEASURES SHALL BE INITIATED WITHIN 14 CALENDAR DAYS AFTER FINAL GRADING OF ANY SEQUENCE OR PHASE. TEMPORARY OR PERMANENT STABILIZATION SHALL BE INITIATED WITHIN 14 CALENDAR DAYS AFTER FINAL GRADING OR WHEN CONSTRUCTION ACTIVITIES ON A PORTION OF THE SITE ARE TEMPORARILY CEASED AND EARTH DISTURBING ACTIVITIES WILL NOT RESUME UNTIL AFTER 14 CALENDAR DAYS. PERMANENT STABILIZATION WITH PERENNIAL VEGETATION OR OTHER PERMANENTLY STABLE NON-ERODING SURFACE SHALL REPLACE ANY TEMPORARY MEASURES AS SOON AS PRACTICABLE. UNPACKED GRAVEL CONTAINING FINES (SILT AND CLAY SIZED PARTICLES) OR CRUSHER-RUN WILL NOT BE CONSIDERED A NON-ERODIBLE SURFACE.
- (5) STEEP SLOPES (A NATURAL OR CREATED SLOPE OF 35% GRADE (2.8H:1V) OR GREATER REGARDLESS OF HEIGHT) SHALL BE TEMPORARILY STABILIZED NO LATER THAN 7 CALENDAR DAYS AFTER CONSTRUCTION ACTIVITY ON THE SLOPE HAS TEMPORARILY OR PERMANENTLY CEASED.
- (6) FOR STORMWATER DISCHARGES ASSOCIATED WITH CONSTRUCTION SUPPORT ACTIVITIES; TDOT PROJECTS ARE COVERED UNDER THE "WASTE AND BORROW" MANUAL PER THE SSWMP.

**EROSION PREVENTION AND  
SEDIMENT CONTROL QUANTITIES**

ITEM NO.	DESCRIPTION	UNIT	QUANTITY
209-05	SEDIMENT REMOVAL	C.Y.	5
209-08.03	TEMPORARY SILT FENCE (WITHOUT BACKING)	L.F.	1700
209-08.07	ROCK CHECK DAM	EA.	8
209-08.08	ENHANCED ROCK CHECK DAM	EA.	1
1 303-10.01	MINERAL AGGREGATE (SIZE 57)	TON	15
2 709-05.05	MACHINED RIPRAP (CLASS A-3)	TON	125
2 740-10.03	GEOTEXTILE (TYPE III) (EROSION CONTROL)	S.Y.	185
3 740-11.02	TEMPORARY SEDIMENT TUBE 12IN (EPSC)	L.F.	500
801-01.07	TEMPORARY SEEDING (WITH MULCH)	UNIT	54
803-01	SODDING (NEW SOD)	S.Y.	183

- 1 FOR CULVERT PROTECTION (TYPE 1)
- 2 FOR CULVERT PROTECTION (TYPE 1) AND FOR TEMPORARY CONSTRUCTION EXITS (2 TOTAL), 50'X15'
- 3 ALL QUANTITIES AS DIRECTED BY THE ENGINEER

OUTFALL #	AREA (SF)	AREA (AC)	SLOPES(%)	"C" VALUE PROPOSED
1	75849.53	1.74	2.5	0.4
2	211266	4.85	8.1	0.4
3	6633.59	0.15	2.3	0.5
4	3185.04	0.07	4.3	0.7
5	10655.73	0.24	5.2	0.7
6	8482.97	0.19	2.2	0.7

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2013	HSIP-258(B)	7
CONST.	2013	HSIP-258(B)	7

REV. 01-14-14: UPDATED EPSC QUANTITIES AND ADDED OUTFALL TABLE

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SET  
NOT FOR  
BIDDING**

SEALED BY

**EROSION PREVENTION AND  
SEDIMENT CONTROL LEGEND**

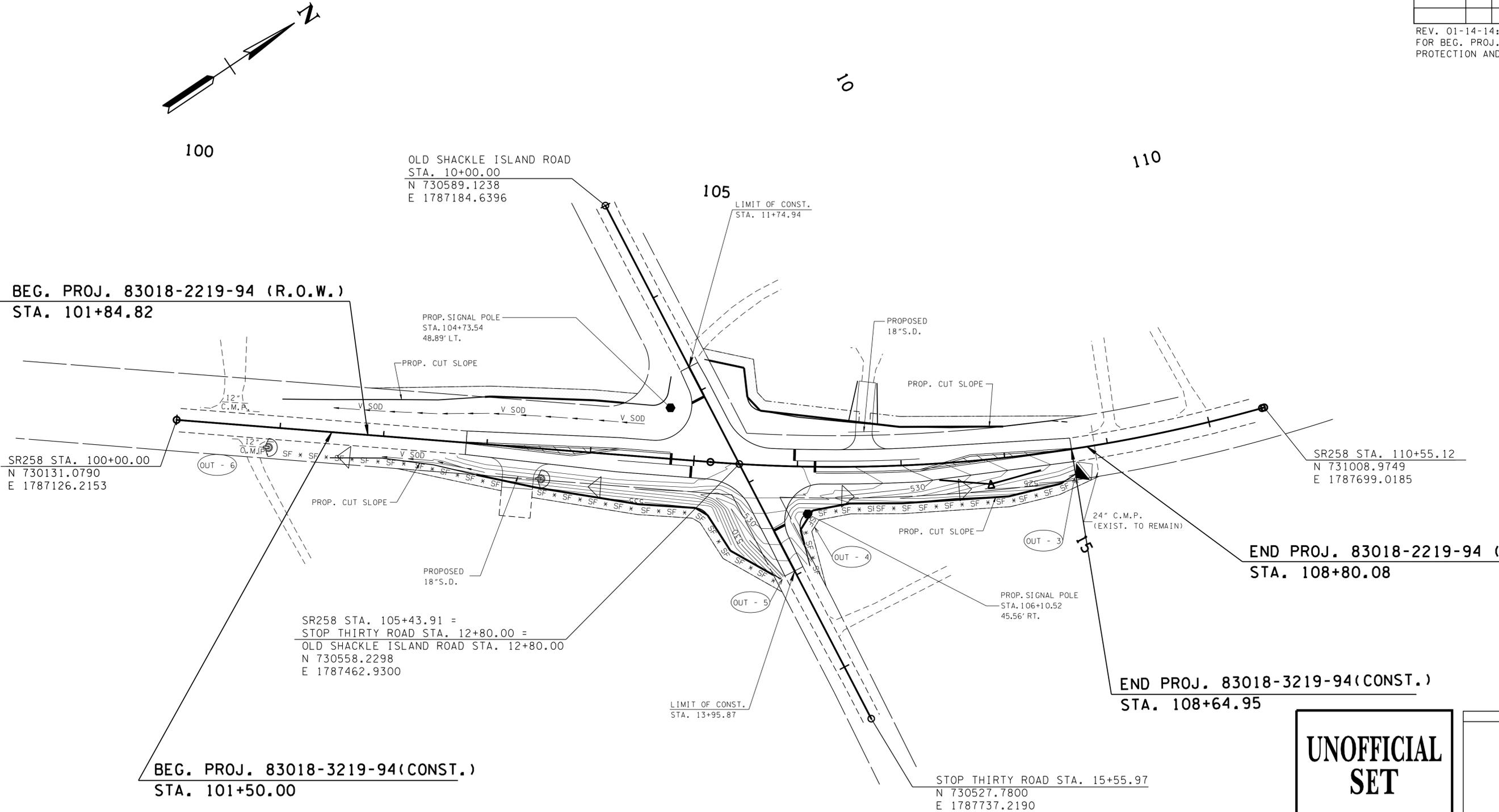
SYMBOL	ITEM	STD. DWG.
* SF * SF * SF *	SILT FENCE	EC-STR-3B
	ROCK CHECK DAM (V-DITCH)	EC-STR-6
	CULVERT PROTECTION (TYPE 1)	EC-STR-11
	ENHANCED ROCK CHECK DAM (V-DITCH)	EC-STR-6A

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

**EPSC NOTES  
AND  
LEGEND**

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2013	HSIP-258(8)	7A
CONST.	2013	HSIP-258(8)	7A

REV. 01-14-14: UPDATED STATION FOR BEG. PROJ. ADDED CULVERT PROTECTION AND OUTFALL LABELS.



BEG. PROJ. 83018-2219-94 (R.O.W.)  
STA. 101+84.82

SR258 STA. 100+00.00  
N 730131.0790  
E 1787126.2153

OLD SHACKLE ISLAND ROAD  
STA. 10+00.00  
N 730589.1238  
E 1787184.6396

SR258 STA. 105+43.91 =  
STOP THIRTY ROAD STA. 12+80.00 =  
OLD SHACKLE ISLAND ROAD STA. 12+80.00  
N 730558.2298  
E 1787462.9300

BEG. PROJ. 83018-3219-94 (CONST.)  
STA. 101+50.00

LIMIT OF CONST.  
STA. 13+95.87

STOP THIRTY ROAD STA. 15+55.97  
N 730527.7800  
E 1787737.2190

END PROJ. 83018-3219-94 (CONST.)  
STA. 108+64.95

END PROJ. 83018-2219-94 (R.O.W.)  
STA. 108+80.08

**UNOFFICIAL  
SET  
NOT FOR  
BIDDING**

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DEPARTMENT OF TRANSPORTATION

**EROSION  
PREVENTION  
AND SEDIMENT  
CONTROL PLAN**

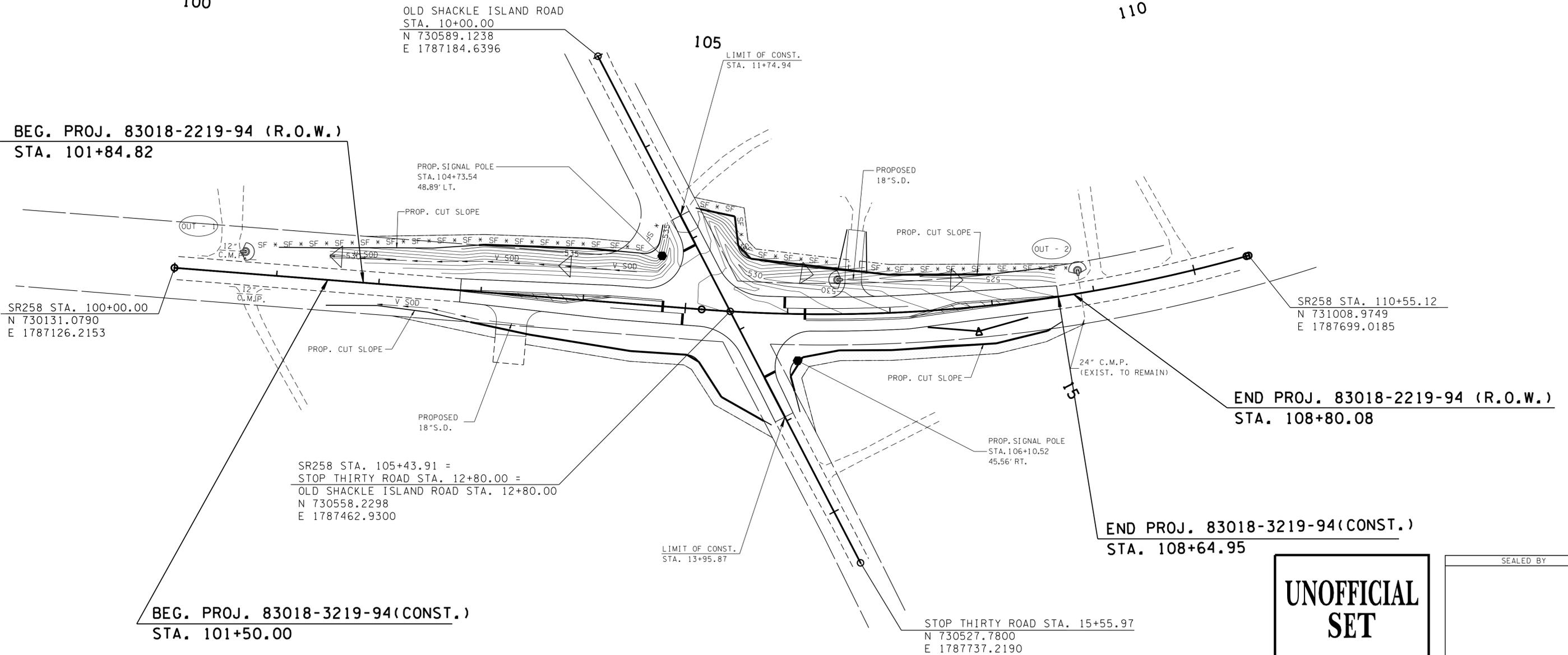
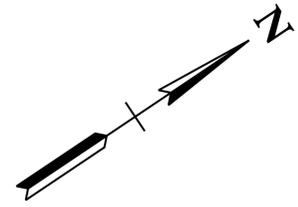
PHASE 1

SCALE: 1" = 50'

TENNESSEE D.O.T.  
DESIGN DIVISION  
FILE NO.

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2013	HSIP-258(8)	7B
CONST.	2013	HSIP-258(8)	7B

REV. 01-14-14: UPDATED STATION FOR BEG. PROJ. ADDED CULVERT PROTECTION AND OUTFALL LABELS.



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DEPARTMENT OF TRANSPORTATION

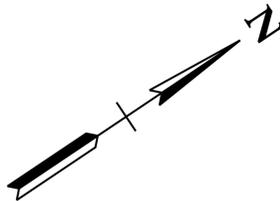
**EROSION  
PREVENTION  
AND SEDIMENT  
CONTROL PLAN**

PHASE 2

SCALE: 1" = 50'

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2013	HSIP-258(8)	8
CONST.	2013	HSIP-258(8)	8

REV. 01-14-14: UPDATED STATION FOR BEG. PROJ.



BEG. PROJ. 83018-2219-94 (R.O.W.)  
STA. 100+84.82

SR258 STA. 100+00.00  
N 730131.0790  
E 1787126.2153

SR258 STA. 110+55.12  
N 731008.9749  
E 1787699.0185

END PROJ. 83018-2219-94 (R.O.W.)  
STA. 108+80.08

END PROJ. 83018-3219-94 (CONST.)  
STA. 108+64.95

BEG. PROJ. 83018-3219-94 (CONST.)  
STA. 101+50.00

EXISTING 24" R.C.P.  
(TO REMAIN)

**UNOFFICIAL  
SET  
NOT FOR  
BIDDING**

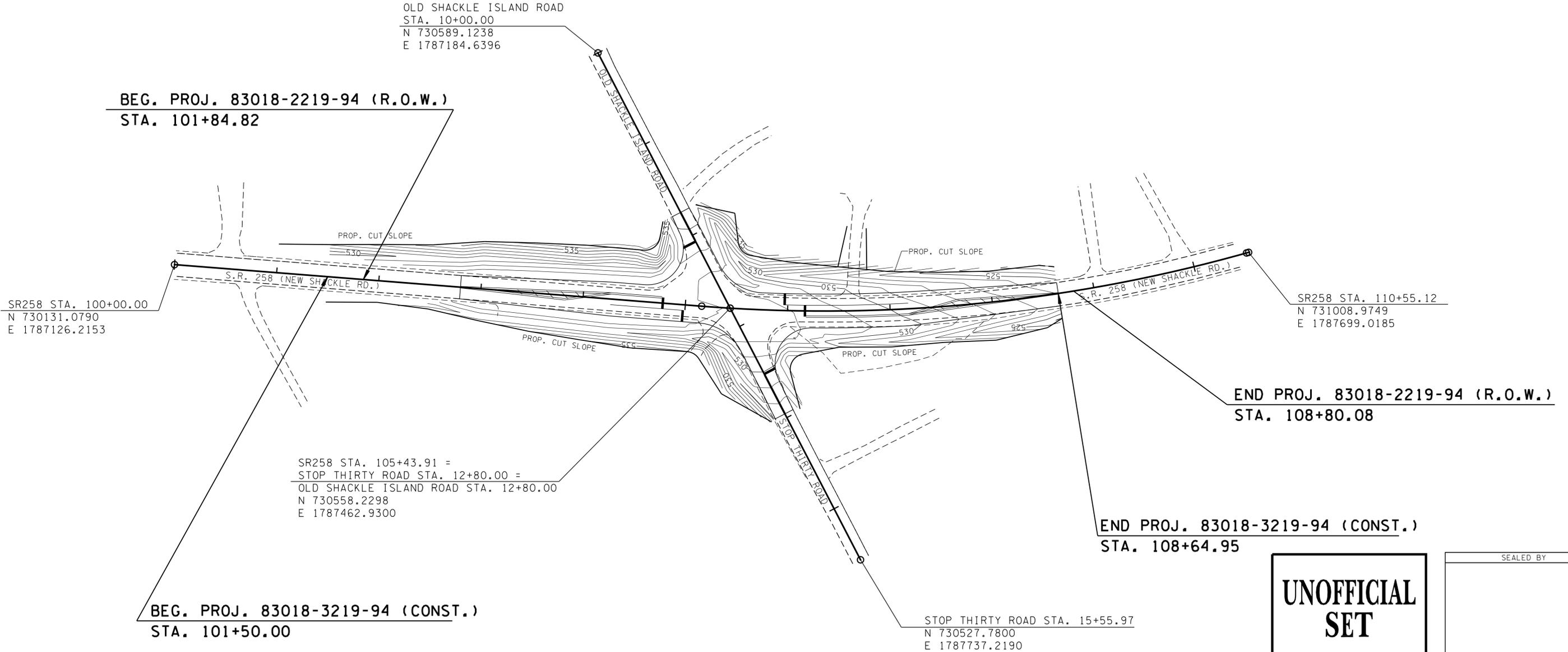
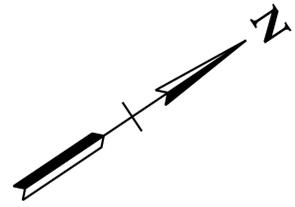
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DEPARTMENT OF TRANSPORTATION

**EXISTING  
CONTOURS**  
SCALE: 1" = 50'

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2013	HSIP-258(B)	9
CONST.	2013	HSIP-258(B)	9

REV. 01-14-14: UPDATED STATION FOR BEG. PROJ.



BEG. PROJ. 83018-2219-94 (R.O.W.)  
STA. 101+84.82

SR258 STA. 100+00.00  
N 730131.0790  
E 1787126.2153

OLD SHACKLE ISLAND ROAD  
STA. 10+00.00  
N 730589.1238  
E 1787184.6396

SR258 STA. 105+43.91 =  
STOP THIRTY ROAD STA. 12+80.00 =  
OLD SHACKLE ISLAND ROAD STA. 12+80.00  
N 730558.2298  
E 1787462.9300

SR258 STA. 110+55.12  
N 731008.9749  
E 1787699.0185

END PROJ. 83018-2219-94 (R.O.W.)  
STA. 108+80.08

END PROJ. 83018-3219-94 (CONST.)  
STA. 108+64.95

BEG. PROJ. 83018-3219-94 (CONST.)  
STA. 101+50.00

STOP THIRTY ROAD STA. 15+55.97  
N 730527.7800  
E 1787737.2190

**UNOFFICIAL  
SET  
NOT FOR  
BIDDING**

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STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

**PROPOSED  
CONTOURS**

SCALE: 1" = 50'

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2013	HSIP-258(B)	10
CONST.	2013	HSIP-258(B)	10

# PAVEMENT EDGE DROP-OFF TRAFFIC CONTROL NOTES

A. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES OR TRAFFIC LANE AND SHOULDER WHERE THE TRAFFIC LANE IS BEING USED BY TRAFFIC, CAUSED BY BASE PAVING OR RESURFACING:

1. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 0.75 INCH AND NOT EXCEEDING 2 INCHES:
  - a. WARNING SIGNS, UNEVEN PAVEMENT (W8-11) AND/OR SHOULDER DROP-OFF (W8-9A), SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.
  - b. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY ADDED PAVEMENT SHALL BE ELIMINATED WITHIN THREE WORKDAYS.
  - c. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY COLD PLANING SHALL BE ELIMINATED WITHIN THREE WORKDAYS.
  - d. WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE TRAFFIC LANE BEING UTILIZED BY TRAFFIC AND SHOULDER THE DIFFERENCE IN ELEVATION SHALL BE ELIMINATED WITHIN SEVEN WORKDAYS AFTER THE CONDITION IS CREATED.
2. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 2 INCHES AND NOT EXCEEDING 6 INCHES. TRAFFIC IS NOT TO BE ALLOWED TO TRAVERSE THIS DIFFERENCE IN ELEVATION.
  - a. SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:
    - (1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.
    - (2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.
  - b. IF THE DIFFERENCE IN ELEVATION IS ELIMINATED OR DECREASED TO 2 INCHES OR LESS BY THE END OF EACH WORKDAY, CONES MAY BE USED DURING DAYLIGHT HOURS IN LIEU OF DRUMS, BARRICADES OR OTHER APPROVED PROTECTIVE DEVICES MENTIONED IN PARAGRAPH a, PROVIDED WARNING SIGNS ARE ERECTED. WARNING SIGNS (UNEVEN PAVEMENT AND/OR LOW SHOULDER) SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.
  - c. WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE THROUGH TRAFFIC LANE AND THE SHOULDER AND THE ELEVATION DIFFERENCE IS LESS THAN 3.5 INCHES, THE CONTRACTOR MAY USE WARNING SIGNS AND/OR PROTECTIVE DEVICES AS APPLICABLE AND APPROVED BY THE ENGINEER. SEE PARAGRAPH a REGARDING USE OF DRUMS, BARRICADES OR OTHER APPROVED PROTECTIVE DEVICES. WARNING SIGNS (UNEVEN PAVEMENT AND/OR LOW SHOULDER) WILL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

IN THESE SITUATIONS, THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 2 MILES IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

3. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 6 INCHES BUT NOT EXCEEDING 18 INCHES, THE CONTRACTOR, WITH THE ENGINEER'S APPROVAL, MAY UTILIZE ONE OF THE FOLLOWING:

- a. THE CONTRACTOR SHALL ACCOMPLISH SEPARATION BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:
  - (1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.
  - (2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

IN ORDER TO USE THIS METHOD, THE CONTRACTOR MUST REDUCE THE DIFFERENCE IN ELEVATION TO 6 INCHES OR LESS BY THE END OF THE WORKDAY THAT THE CONDITION IS CREATED.

- b. THE CONTRACTOR SHALL PROVIDE DRUMS, BARRICADES OR OTHER APPROVED SEPARATION DEVICES AS SPECIFIED IN PARAGRAPH a, AND CONSTRUCT A STONE WEDGE WITH A 4:1 SLOPE, OR FLATTER, TO ELIMINATE THE VERTICAL OFFSET IF THE LOWER ELEVATION IS AT OR BELOW SUBGRADE AT THE END OF EACH DAY.
- c. THE CONTRACTOR SHALL PROVIDE DRUMS, BARRICADES OR OTHER APPROVED SEPARATION DEVICES AS SPECIFIED IN PARAGRAPH a AND IF THE LOWER ELEVATION IS BASE STONE OR ASPHALT PAVEMENT, PLACEMENT OF SUBSEQUENT LAYERS OF PAVEMENT MUST BEGIN THE NEXT WORK DAY AND PROGRESS CONTINUOUSLY UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED OR REDUCED TO SIX INCHES OR LESS.
- d. THE CONTRACTOR SHALL PROVIDE SEPARATION BY PORTABLE BARRIER RAIL.

FOR PRECEDING CONDITIONS a, b, AND c, THE CONTRACTOR SHALL USE THE SHOULDER DROP-OFF WARNING SIGN (W8-9A). IT SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN THE SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. IN THESE SITUATIONS, THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 1 MILE IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

4. FOR DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 18 INCHES.

SEPARATION WILL BE PROVIDED BY USE OF PORTABLE BARRIER RAIL.

IN THIS SITUATION THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING ONE MILE IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

B. IF THE DIFFERENCE IN ELEVATION IS WITHIN 30 FEET OF THE NEAREST TRAFFIC LANE BEING USED BY TRAFFIC CAUSED BY GRADING, EXCAVATION FOR UTILITIES, DRAINAGE STRUCTURES, UNDERCUTTING, ETC.:

1. IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 3/4 INCH AND NOT EXCEEDING 2 INCHES.

WARNING SIGNS (UNEVEN PAVEMENT AND/OR LOW SHOULDER) SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

2. IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 2 INCHES AND NOT EXCEEDING 6 INCHES:

- a. SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:
  - (1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.
  - (2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

3. IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 6 INCHES:

- a. SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:
  - (1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.
  - (2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.
- b. ELIMINATE VERTICAL OFFSET BY CONSTRUCTING A STONE WEDGE OR GRADING TO A 4:1 SLOPE, OR FLATTER, OR USE PORTABLE BARRIER RAIL.

THE CONTRACTOR SHALL SCHEDULE THE WORK SO AS TO MINIMIZE THE TIME TRAFFIC IS EXPOSED TO AN ELEVATION DIFFERENCE. ONCE THE CONTRACTOR BEGINS AN ACTIVITY THAT CREATES AN ELEVATION DIFFERENCE WITHIN 8 FEET OF A TRAFFIC LANE, THE ACTIVITY SHALL BE PURSUED AS A CONTINUOUS OPERATION UNTIL THE ELEVATION DIFFERENCE IS ELIMINATED.

C. IF THE DIFFERENCE IN ELEVATION IS FARTHER THAN 8 FEET FROM THE NEAREST TRAFFIC LANE BUT NOT MORE THAN 30 FEET FROM THE NEAREST TRAFFIC LANE:

SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

1. WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.
2. WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

THE CONTRACTOR SHALL SCHEDULE THE WORK SO AS TO MINIMIZE THE TIME TRAFFIC IS EXPOSED TO AN ELEVATION DIFFERENCE. ONCE THE CONTRACTOR BEGINS AN ACTIVITY THAT CREATES AN ELEVATION DIFFERENCE, THE ACTIVITY SHALL BE PURSUED AS A CONTINUOUS OPERATION UNTIL THE ELEVATION DIFFERENCE IS ELIMINATED.

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SET  
NOT FOR  
BIDDING**

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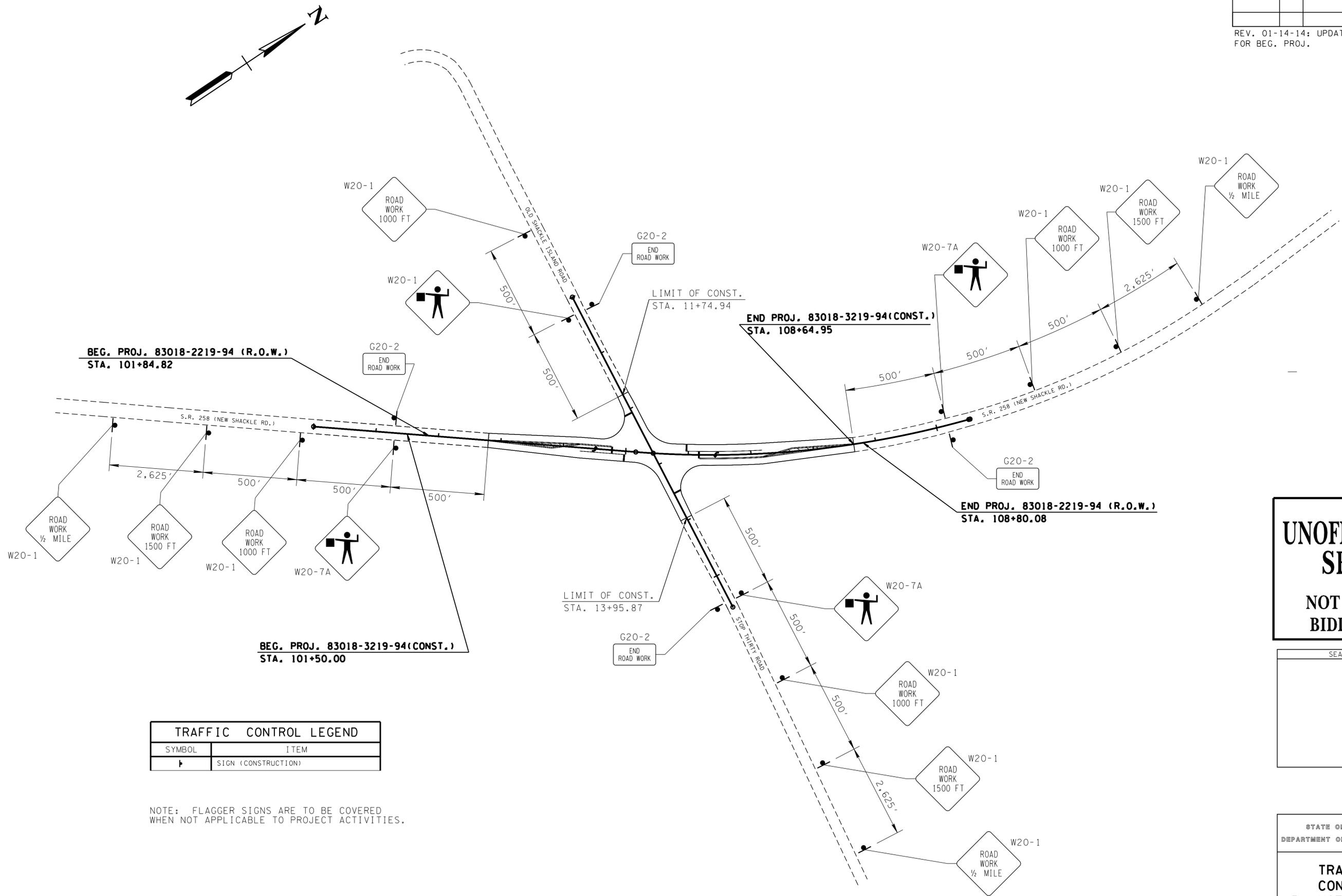
STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

**PAVEMENT EDGE  
DROP-OFF  
TRAFFIC CONTROL  
NOTES**



TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2013	HSIP-258(8)	10A
CONST.	2013	HSIP-258(8)	10B

REV. 01-14-14: UPDATED STATION FOR BEG. PROJ.



TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
	SIGN (CONSTRUCTION)

NOTE: FLAGGER SIGNS ARE TO BE COVERED WHEN NOT APPLICABLE TO PROJECT ACTIVITIES.

**UNOFFICIAL SET**  
**NOT FOR BIDDING**

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DEPARTMENT OF TRANSPORTATION

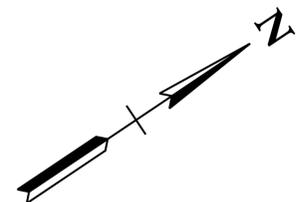
**TRAFFIC CONTROL ADVANCE SIGNING**

N.T.S.

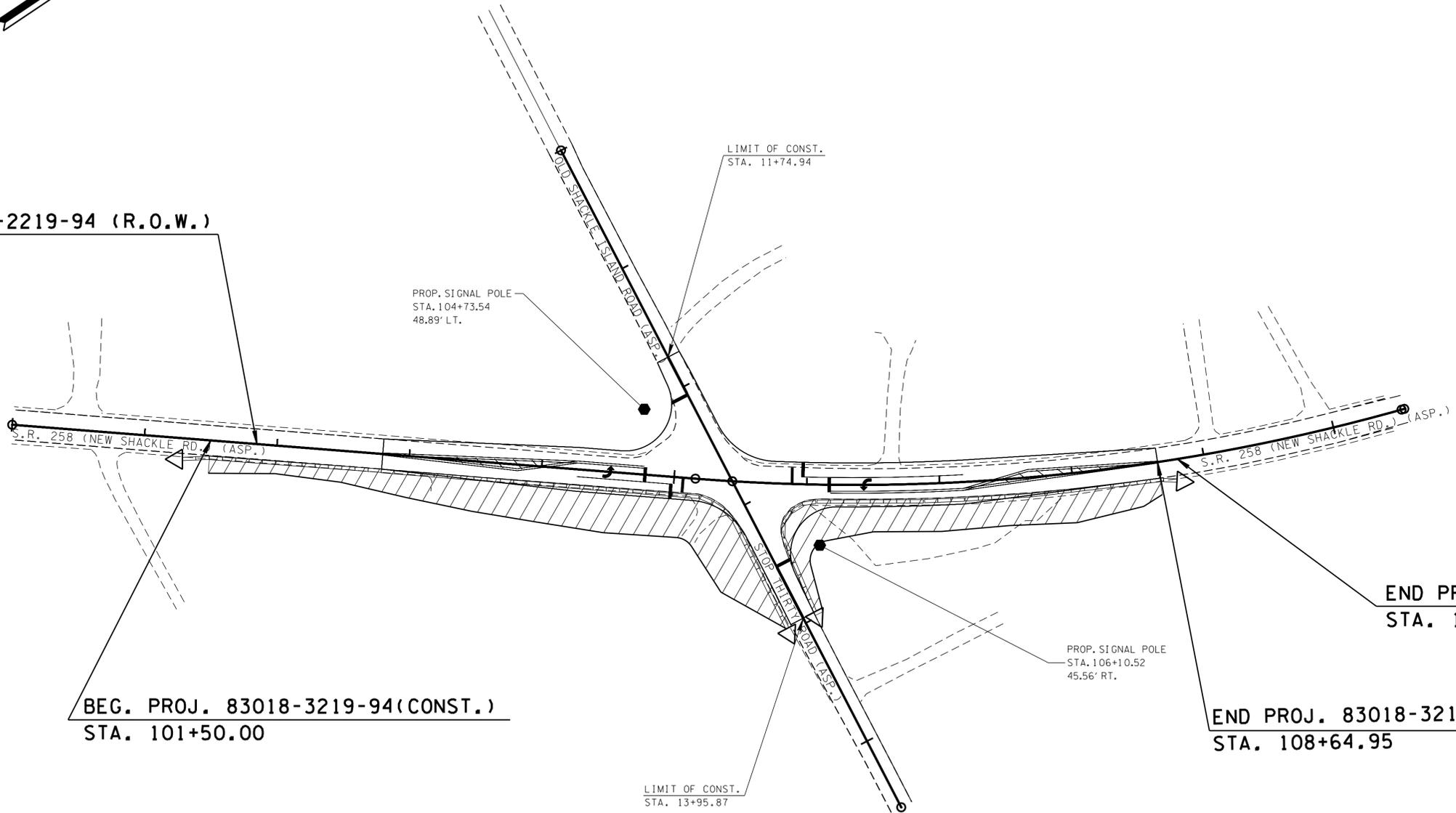
TENNESSEE D.O.T.  
 DESIGN DIVISION  
 FILE NO.

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2013	HSIP-258(8)	10B
CONST.	2013	HSIP-258(8)	10C

REV. 01-14-14: UPDATED STATION FOR BEG. PROJ.



BEG. PROJ. 83018-2219-94 (R.O.W.)  
 STA. 101+84.82



**UNOFFICIAL SET**  
**NOT FOR BIDDING**

TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
	PORTABLE BARRIER RAIL
	WORK ZONE
	TEMPORARY ATTENUATOR

SEALED BY

STATE OF TENNESSEE  
 DEPARTMENT OF TRANSPORTATION

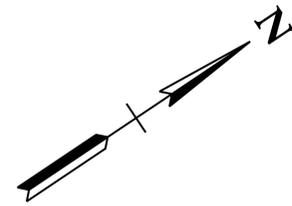
**TRAFFIC CONTROL PLAN PHASE 1**

SCALE: 1"=50'

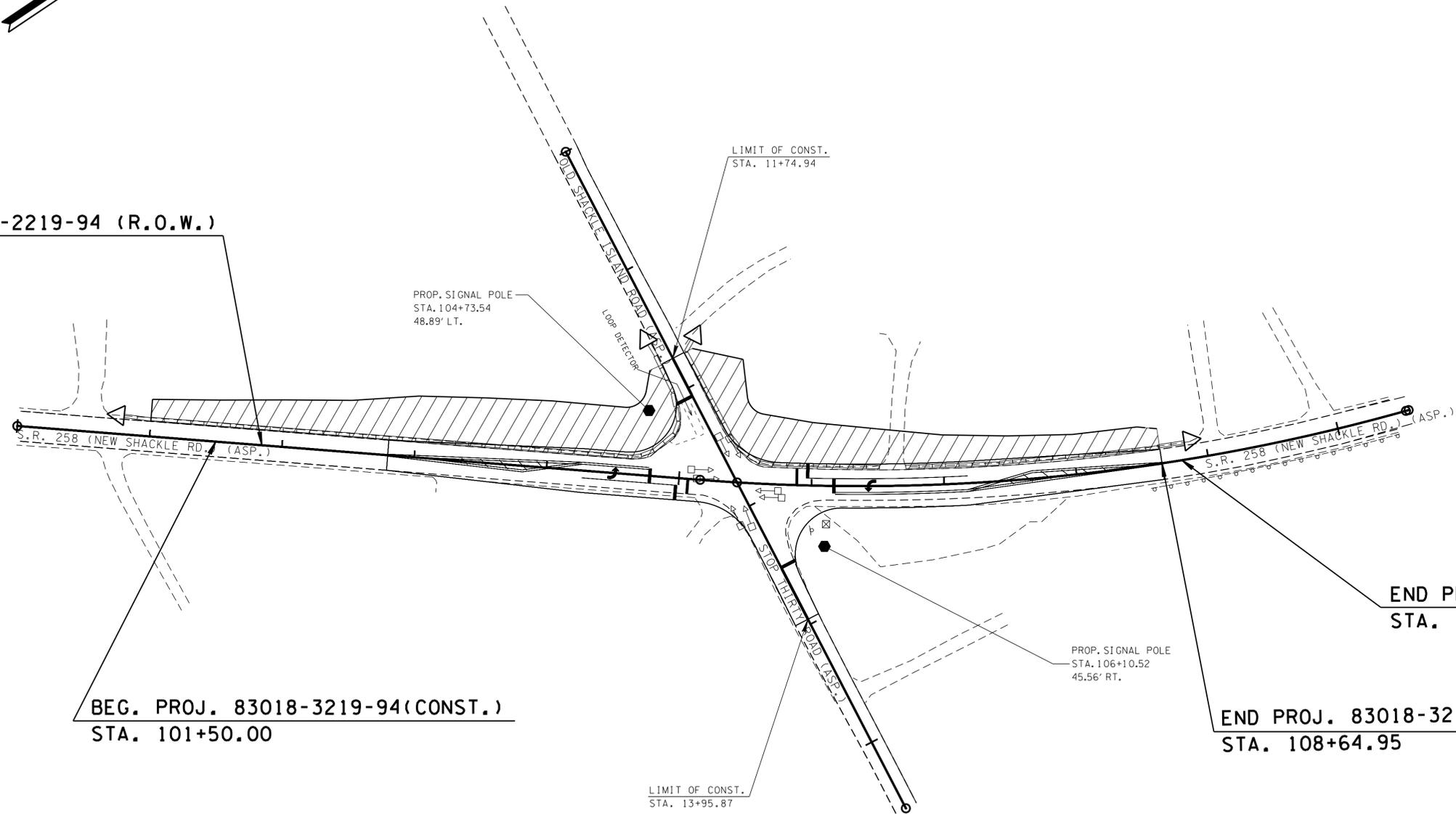
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TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2013	HSIP-258(8)	10C
CONST.	2013	HSIP-258(8)	10D

REV. 01-14-14: UPDATED STATION FOR BEG. PROJ.



BEG. PROJ. 83018-2219-94 (R.O.W.)  
STA. 101+84.82



END PROJ. 83018-2219-94 (R.O.W.)  
STA. 108+80.08

END PROJ. 83018-3219-94 (CONST.)  
STA. 108+64.95

**UNOFFICIAL SET**  
**NOT FOR BIDDING**

TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
	PORTABLE BARRIER RAIL
	WORK ZONE
	TEMPORARY ATTENUATOR

SEALED BY

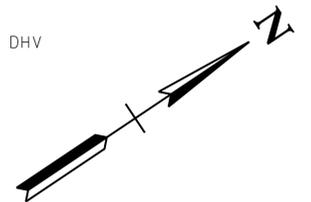
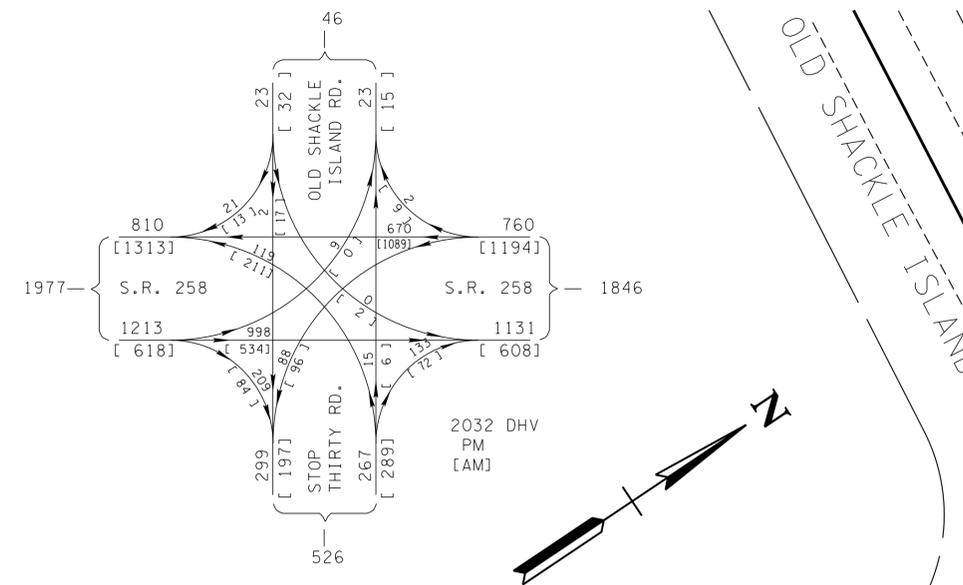
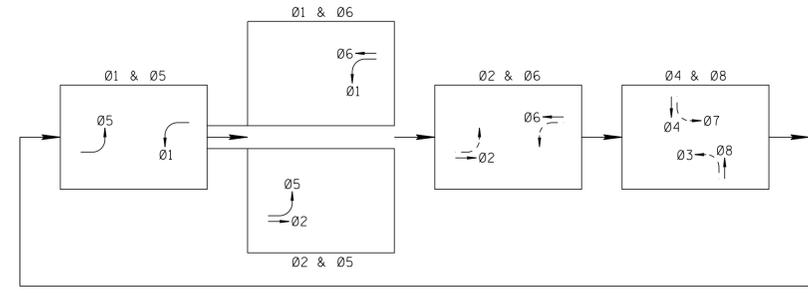
STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

**TRAFFIC CONTROL PLAN  
PHASE 2**

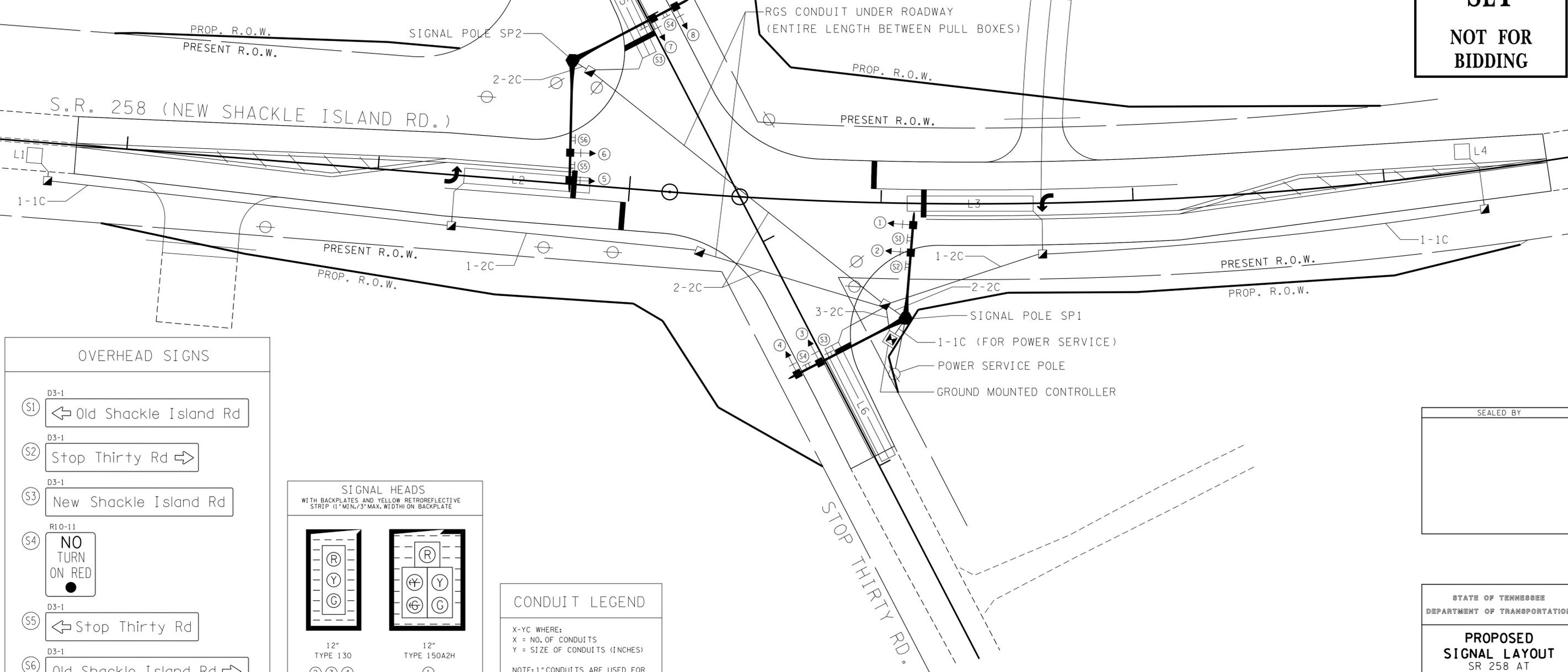
SCALE: 1"=50'

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2013	HSIP-258(8)	11
CONST.	2013	HSIP-258(8)	11

PHASING DIAGRAM



**UNOFFICIAL SET**  
**NOT FOR BIDDING**



**OVERHEAD SIGNS**

- (S1) D3-1 Old Shackle Island Rd
- (S2) D3-1 Stop Thirty Rd
- (S3) D3-1 New Shackle Island Rd
- (S4) R10-11 NO TURN ON RED
- (S5) D3-1 Stop Thirty Rd
- (S6) D3-1 Old Shackle Island Rd

**SIGNAL HEADS**  
WITH BACKPLATES AND YELLOW RETROREFLECTIVE STRIP (1" MIN./3" MAX. WIDTH) ON BACKPLATE

12"  
TYPE 130

12"  
TYPE 150A2H

**CONDUIT LEGEND**

X-YC WHERE:  
X = NO. OF CONDUITS  
Y = SIZE OF CONDUITS (INCHES)

NOTE: 1" CONDUITS ARE USED FOR LOOP WIRE FROM THE EDGE OF THE ROADWAY TO THE PULL BOX.

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DEPARTMENT OF TRANSPORTATION

**PROPOSED SIGNAL LAYOUT**  
SR 258 AT  
STOP THIRTY ROAD AND  
OLD SHACKLE ISLAND

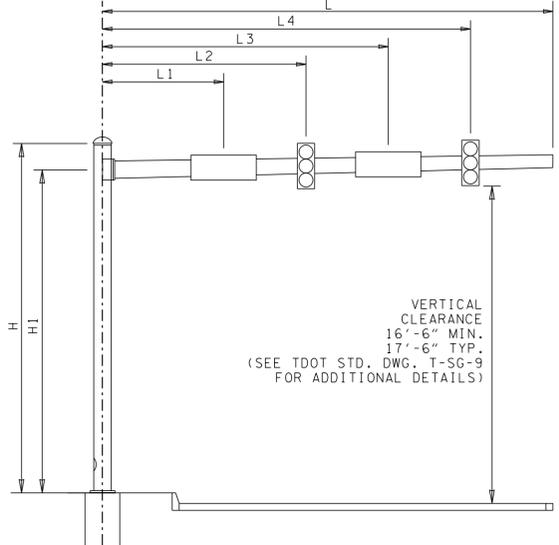
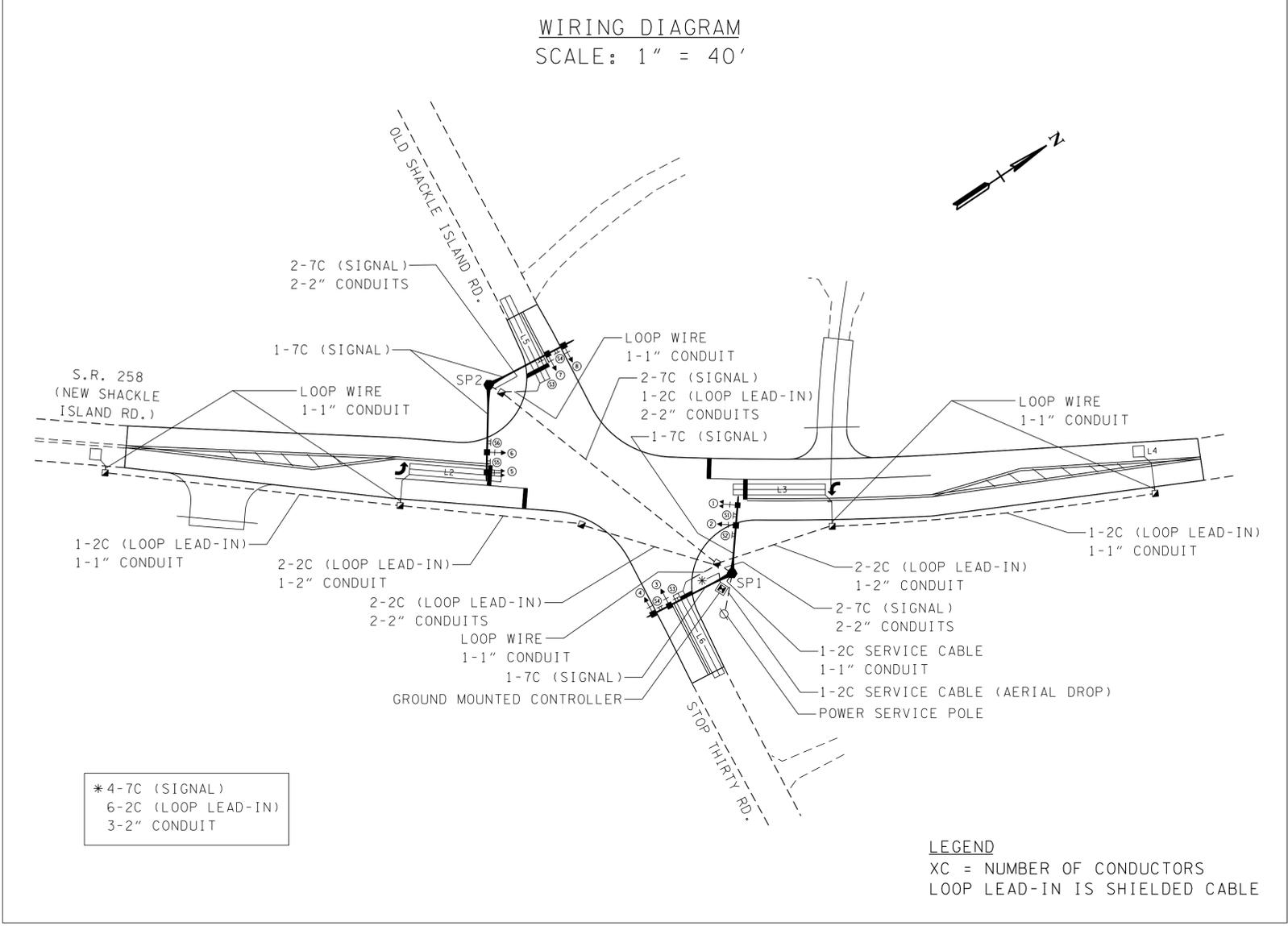
SCALE: 1"=20'

TRAFFIC SIGNAL TIMING CHART				
MOVEMENT PHASE	01 & 05	02 & 06	04 & 08	
MIN GREEN	8	15	10	
EXTENSION	4	4	4	
YELLOW CLEARANCE	4	4	4	
RED CLEARANCE	2	2	2	
MAX GREEN 1	20	40	30	
MAX GREEN 2	20	40	30	
MAX RECALL		X		
MIN RECALL				
NON-LOCK	X		X	
VEH OMIT				
PED OMIT	X	X	X	
FLASH		RED		

NOTE: EXISTING SIGNAL TIMINGS PROVIDED ARE BASIC TIMINGS.

DETECTION TABLE				
LOOP NO.	SIZE	TYPE	FUNCTION	CONTROLLER PHASE TERMINALS
L1	6' X 6'	LOOP	PULSE	02
L2	6' X 50'	QUADRAPOLE	PRESENCE	05
L3	6' X 50'	QUADRAPOLE	PRESENCE	01
L4	6' X 6'	LOOP	PULSE	06
L5	6' X 50'	QUADRAPOLE	PRESENCE	04
L6	6' X 50'	QUADRAPOLE	PRESENCE	08

SIGNAL HEAD PHASE WIRING TABLE	
SIGNAL HEAD	CONTROLLER PHASE TERMINALS
1	02 & 05
2	02
3	04
4	04
5	01 & 06
6	06
7	08
8	08



SIGNAL SUPPORT				POLE DETAILS				MAST ARM DETAILS						FOUNDATION				
I.D. NUMBER	STATION FROM S.R. 258 CENTERLINE	OFFSET FROM S.R. 258 CENTERLINE	COORDINATES	H (FT)	POLE HEIGHT	POLE BASE ELEVATION	MAST ARM (ROAD ARM IS OVER)	ANGLE BETWEEN TWIN MAST ARMS	H1 (FT)	MAST ARM ATTACHMENT HEIGHT	MAST ARM ATTACHMENT ELEVATION	L (FT)	L1 (FT)	L2 (FT)	L3 (FT)	L4 (FT)	DIAMETER	DEPTH
SP1	106+11	46' RT	N 730586.17 E 1787539.50	22'-0"	531.1		A (SR 258)	120°	21'-0"	552.1	40'	19'	24'	30'	35'		3'-0"	11'-0"
							B (STOP THIRTY)		19'-0"	550.1	50'	31'	36'	41'	46'			
SP2	104+74	49' LT	N 730533.12 E 1787381.16	22'-0"	536.8		A (SR 258)	120°	17'-0"	553.8	50'	29'	34'	40'	45'		3'-0"	11'-0"
							B (OLD SH. ISLAND)		18'-6"	555.3	50'	29'	34'	39'	44'			

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2013	HS1P-258(8)	11A
CONST.	2013	HS1P-258(8)	11A

**UNOFFICIAL SET**  
**NOT FOR BIDDING**

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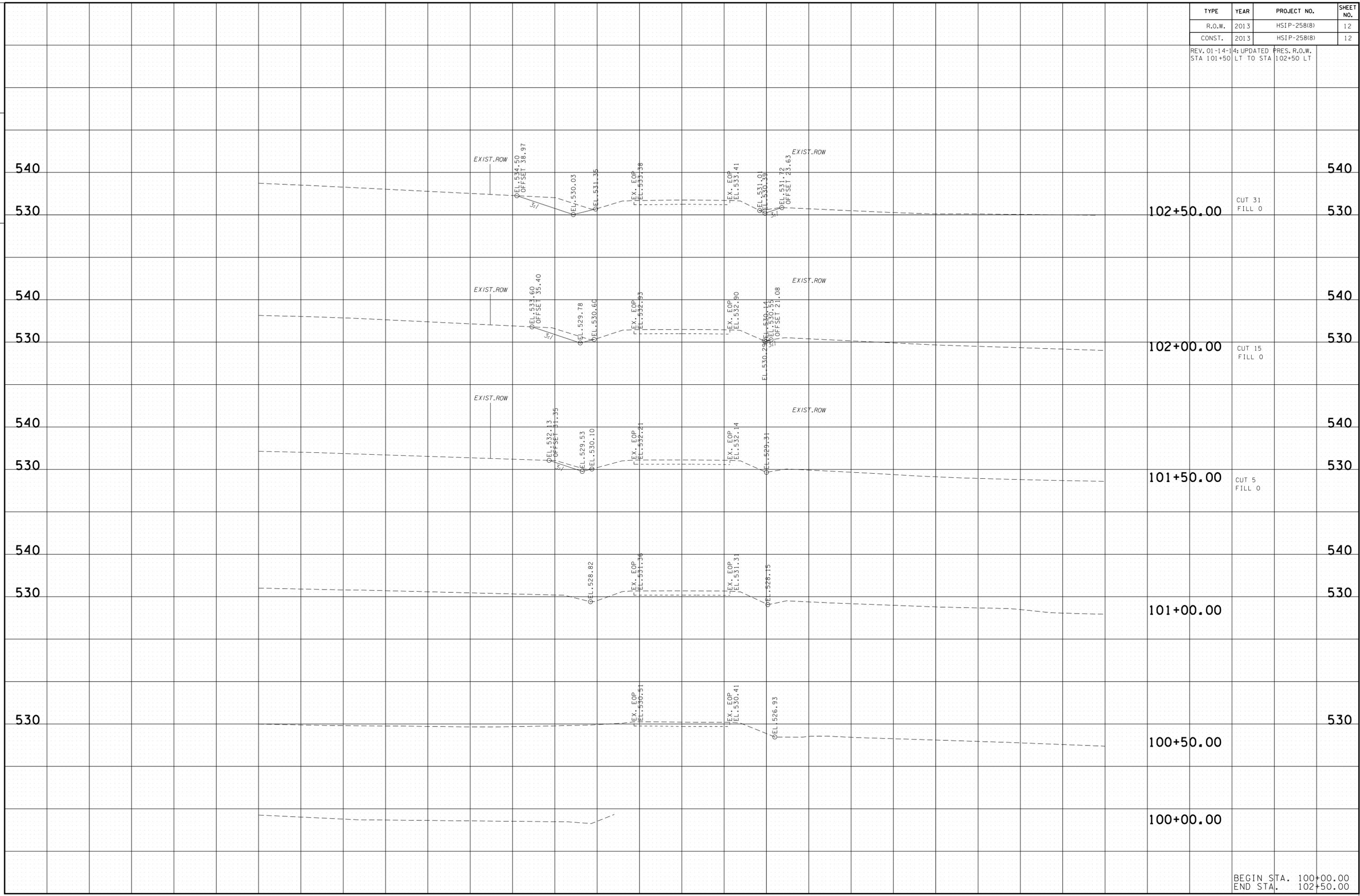
COORDINATES ARE NAD/83(1995), ARE DATUM ADJUSTED BY THE FACTOR OF 1.00003 AND TIED TO THE TGRN. ALL ELEVATIONS ARE REFERENCED TO THE NAVD 1988.

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

**PROPOSED SIGNAL DETAILS**  
SR 258 AT  
STOP THIRTY ROAD AND  
OLD SHACKLE ISLAND

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2013	HSIP-258(8)	12
CONST.	2013	HSIP-258(8)	12

REV. 01-14-14: UPDATED PRES. R.O.W. STA 101+50 LT TO STA 102+50 LT



102+50.00

CUT 31  
FILL 0

540

530

540

530

102+00.00

CUT 15  
FILL 0

540

530

540

530

101+50.00

CUT 5  
FILL 0

540

530

540

530

101+00.00

540

530

540

530

100+50.00

540

530

540

530

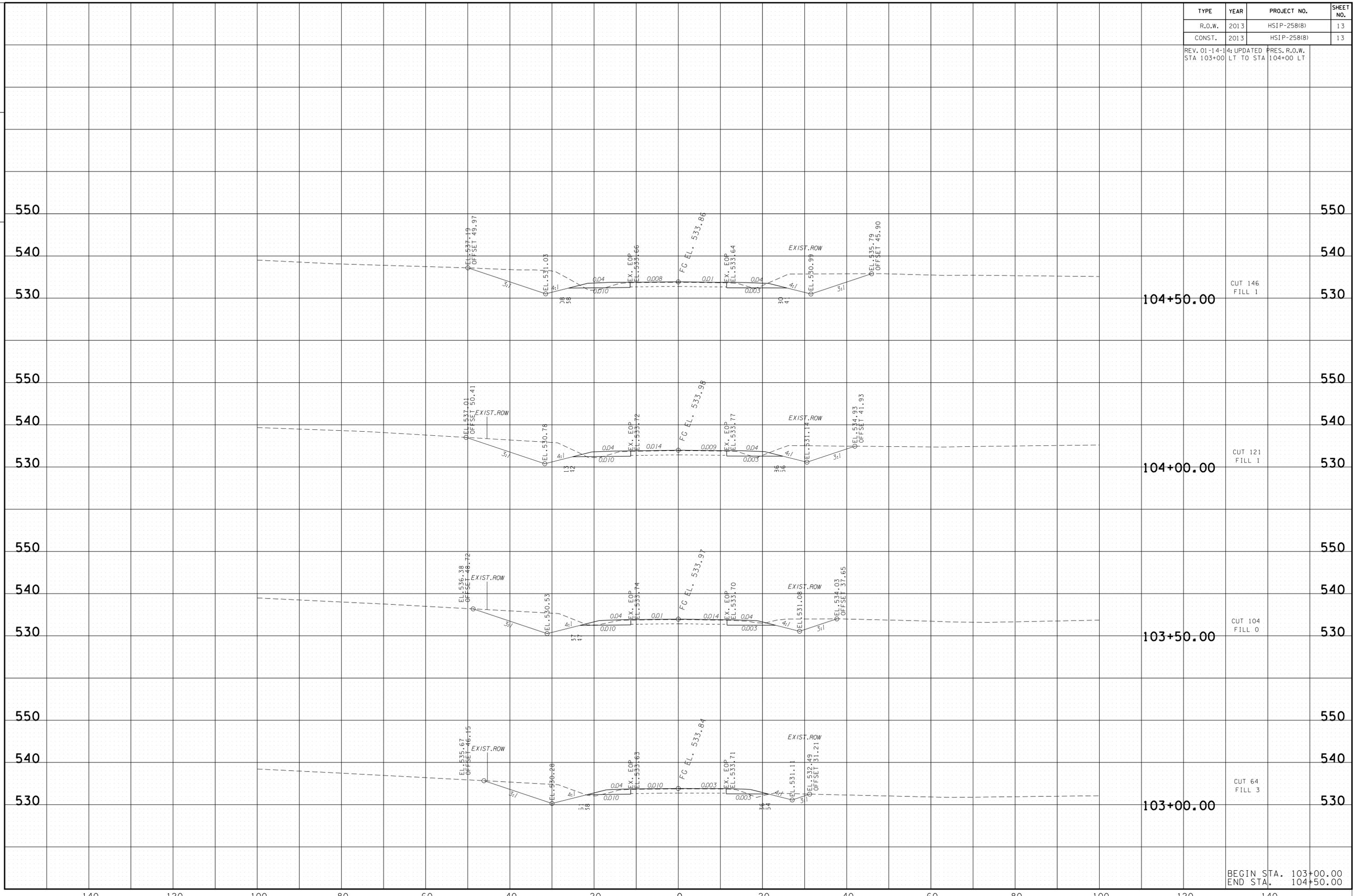
100+00.00

BEGIN STA. 100+00.00  
END STA. 102+50.00

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2013	HSIP-258(8)	13
CONST.	2013	HSIP-258(8)	13

REV. 01-14-14: UPDATED PRES. R.O.W. STA 103+00 LT TO STA 104+00 LT

1/23/2014  
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BEGIN STA. 103+00.00  
END STA. 104+50.00

CUT 146  
FILL 1

CUT 121  
FILL 1

CUT 104  
FILL 0

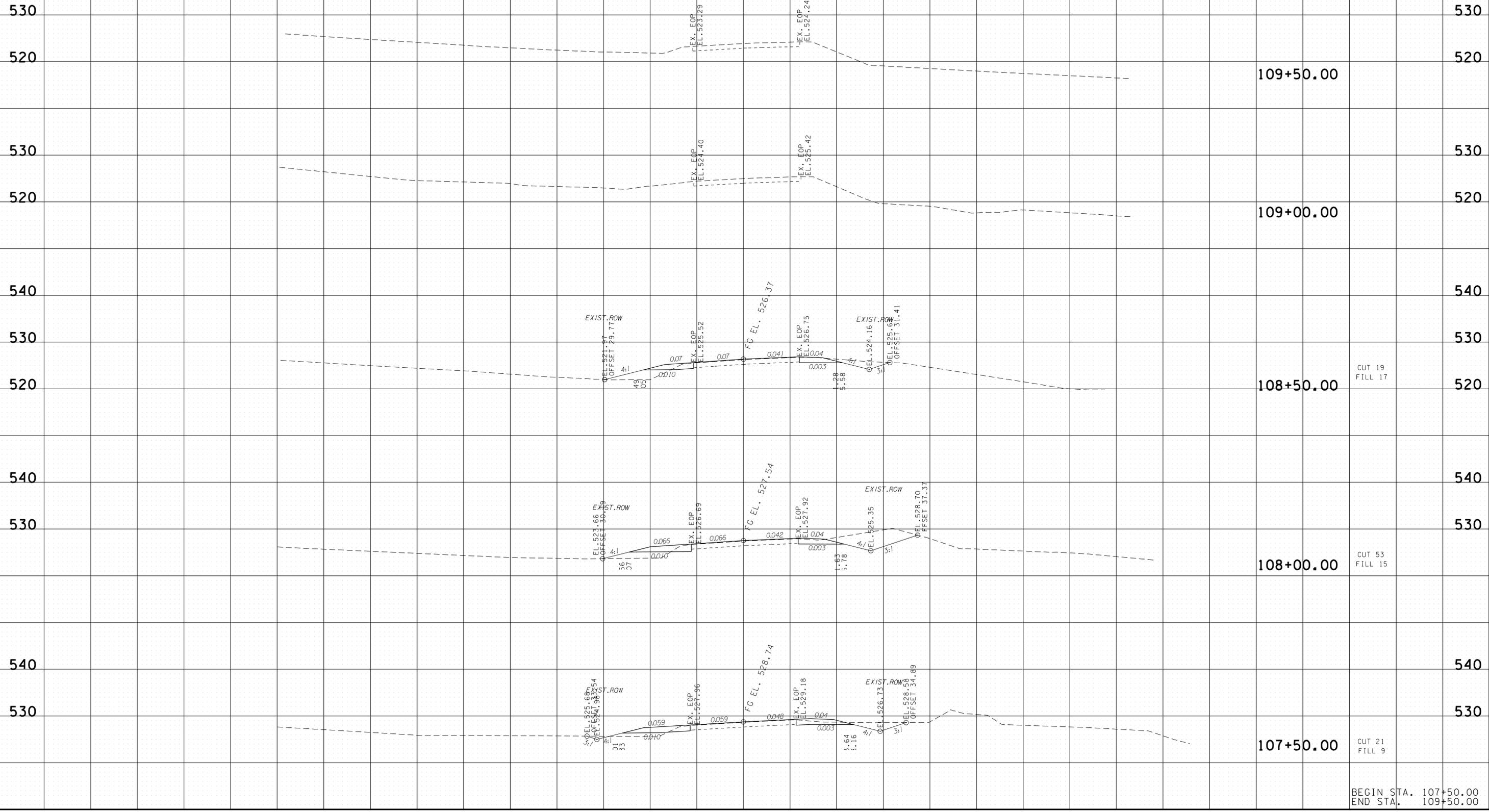
CUT 64  
FILL 3



TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2013	HSIP-258(8)	15
CONST.	2013	HSIP-258(8)	15

TENNESSEE D.O.T.  
DESIGN DIVISION

FILE NO.



1/23/2014  
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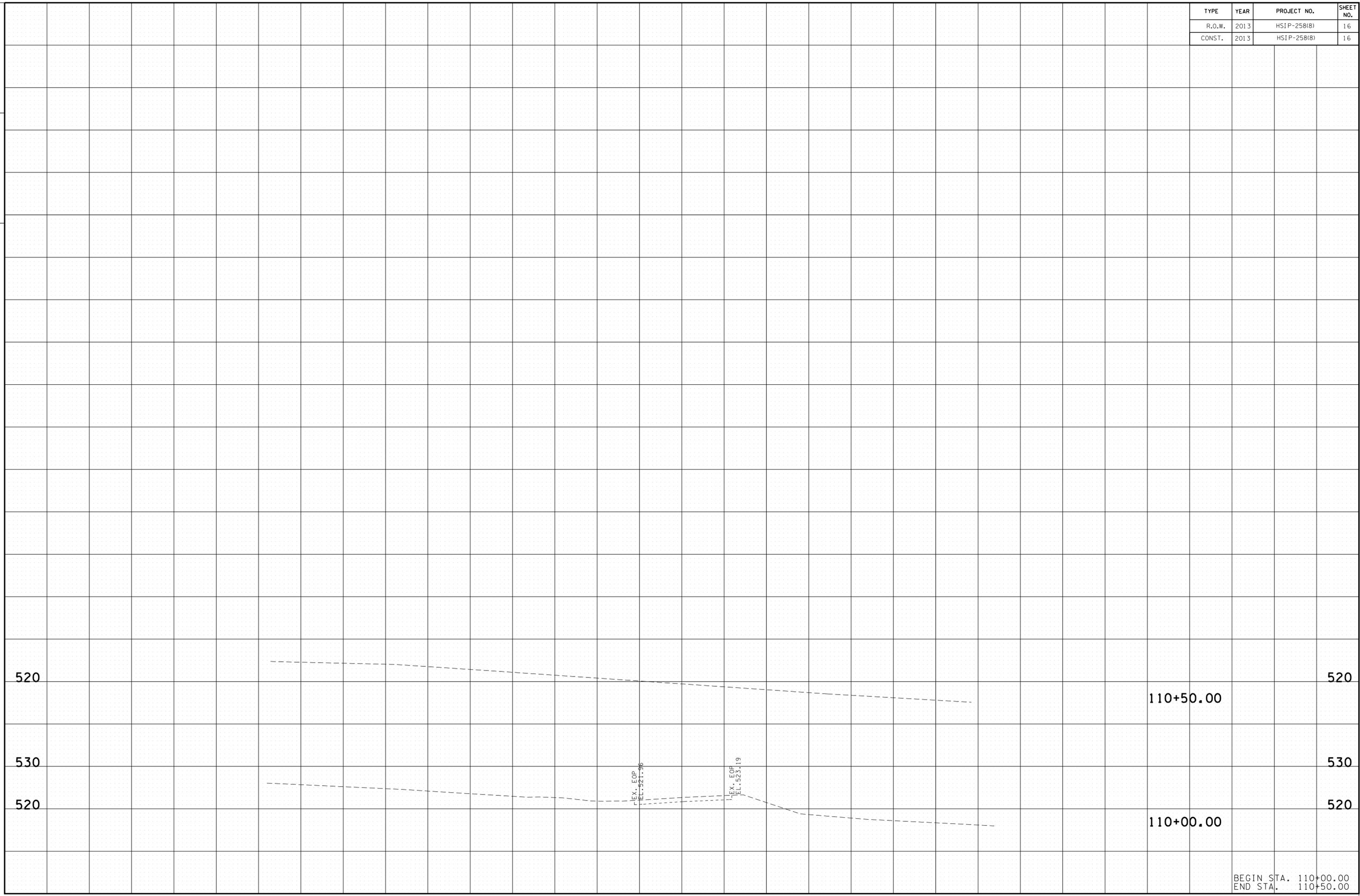
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TENNESSEE D. O. T.  
DESIGN DIVISION

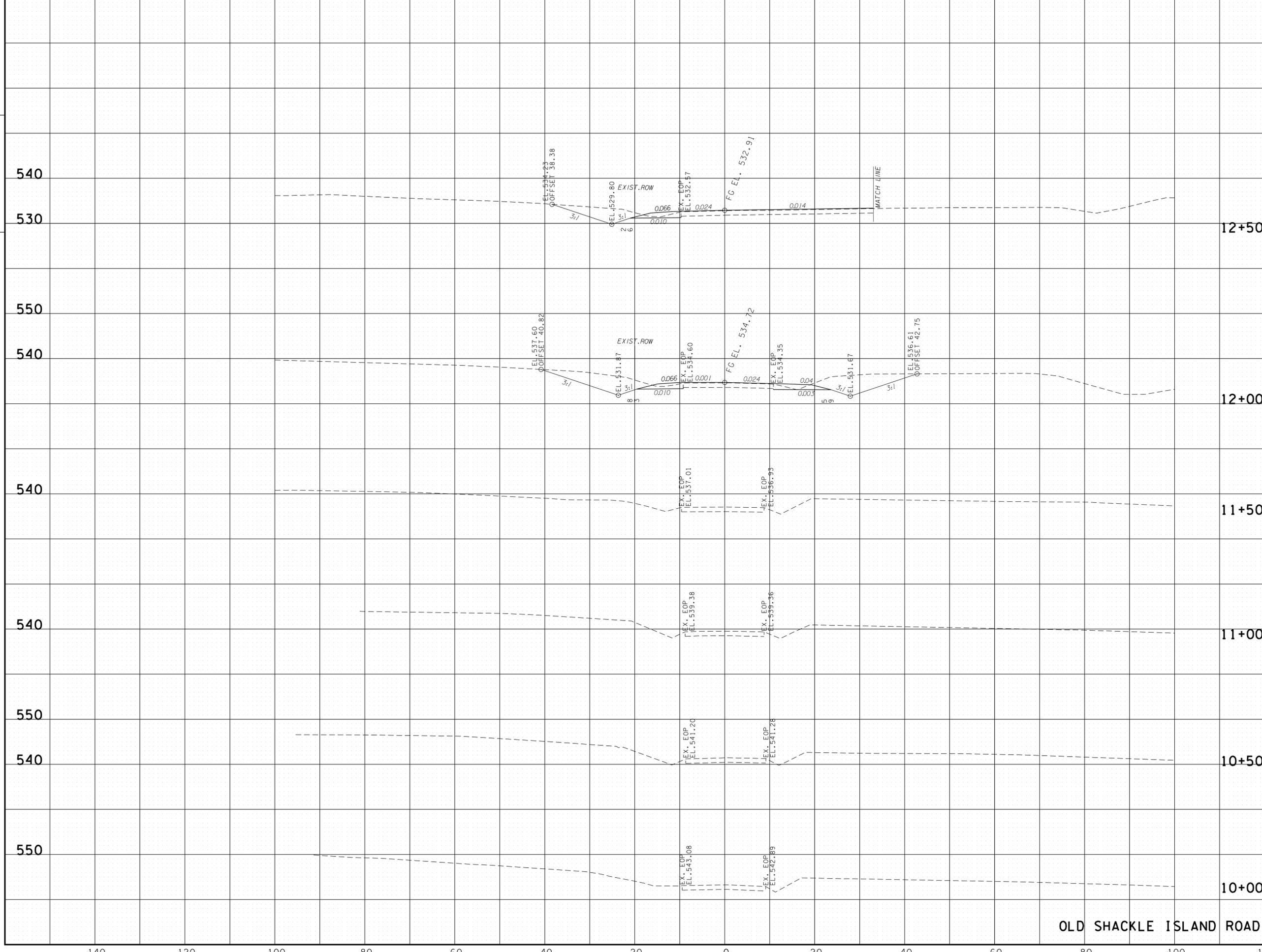
FILE NO.

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2013	HSIP-258(8)	16
CONST.	2013	HSIP-258(8)	16

1/23/2014  
G:\NAI\0073\Road\Final Construction Plans\02 - 016 - SR258 Cross Sections.sht



BEGIN STA. 110+00.00  
END STA. 110+50.00



TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2013	HSIP-258(8)	17
CONST.	2013	HSIP-258(8)	17

REV. 01-14-14: UPDATED PRES. R.O.W.  
 STA 12+00 RT

**OLD SHACKLE ISLAND ROAD**

BEGIN STA. 10+00.00  
 END STA. 12+50.00

