

Index of Sheets

SHEET NO.	SHEET NAME
1 ...	TITLE SHEET
2 ...	ESTIMATED ROADWAY QUANTITIES
3 ...	TYPICAL SECTIONS
4 ...	GENERAL NOTES
4A ...	SPECIAL NOTES
5 ...	TRAFFIC DETECTION LOOPS DETAIL
6 ...	SPECIAL TRAFFIC COUNTER SPECIFICATIONS

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF ENGINEERING

MADISON COUNTY

I - 40

FROM: L.M. 7.15 (W. OF LOWER BROWNSVILLE RD., MM 74.2)
TO: L.M. 11.75 (W. OF S.R. 20, MM 78.8)

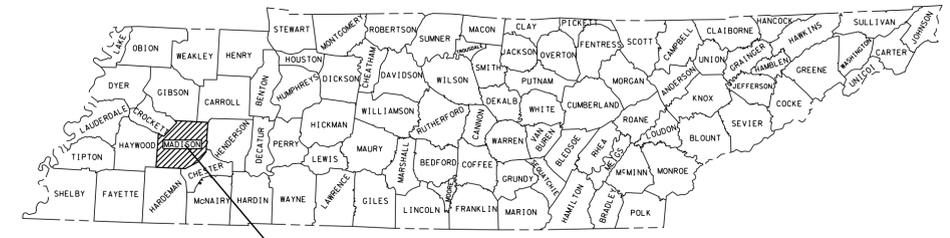
RESURFACING

STATE HIGHWAY NO. I-40 F.A.H.S. NO. N.A.

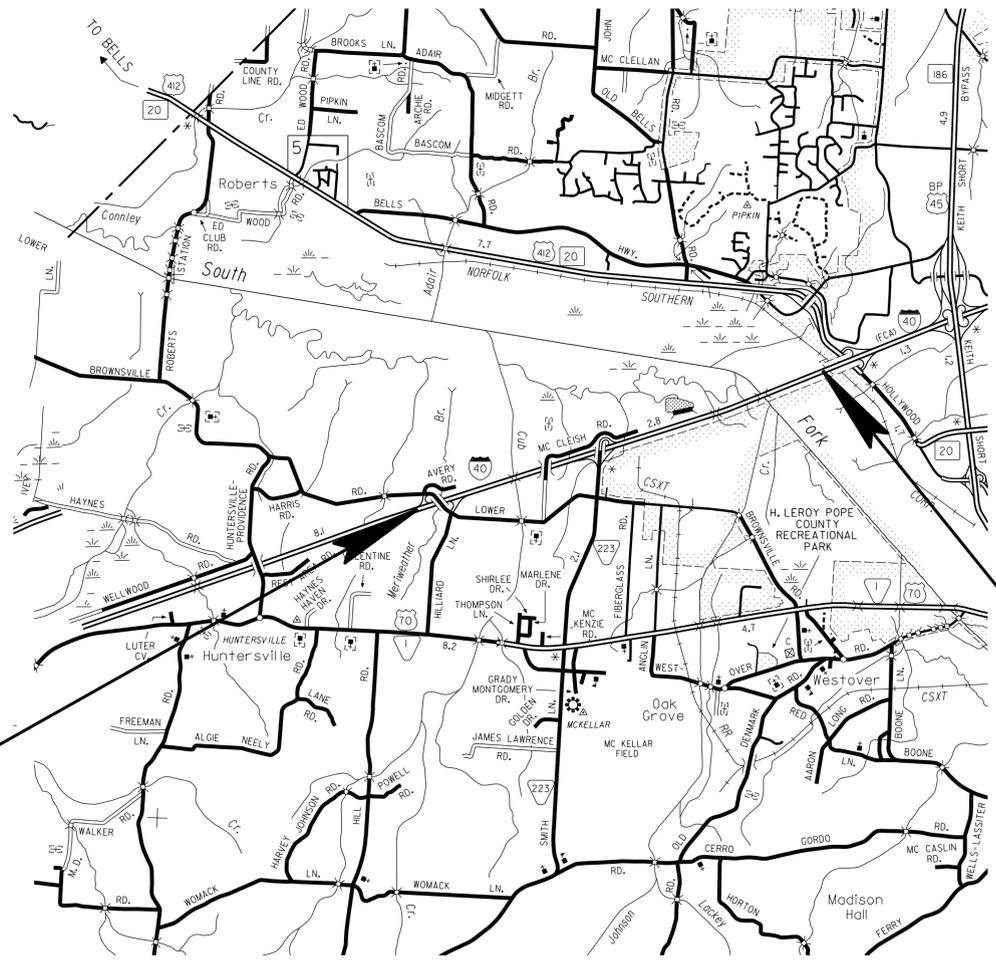
TENN.	YEAR	SHEET NO.
	2014	1
FED. AID PROJ. NO.	NH-1-40-1(345)	
STATE PROJ. NO.	57201-8147-44	

Standard Roadway Drawings

SHEET NO.	REV.	DESCRIPTION
RD-A-1	12-18-99	STANDARD ABBREVIATIONS
RD-L-1	10-26-94	STANDARD LEGEND
T-FAB-1	05-27-97	FLASHING YELLOW ARROW BOARD
T-M-1	11-01-11	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS AND MARKING ABBREVIATIONS
T-M-2	01-15-13	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS
T-M-5	04-23-13	MARKING DETAILS FOR EXPRESSWAYS & FREEWAYS
T-M-6	06-22-12	MARKING DETAIL FOR EXPRESSWAY & FREEWAY INTERCHANGES
T-M-7	01-12-12	GORE MARKING DETAILS FOR EXPRESSWAY AND FREEWAY INTERCHANGES
T-M-9	11-01-11	MARKING DETAILS FOR RAMP INTERSECTIONS
T-M-15		ASPHALT SHOULDER RUMBLE STRIP INSTALLATION DETAILS FOR INTERSTATE AND ACCESS CONTROLLED ROUTES
T-WZ-10	04-04-12	ADVANCE ROAD WORK SIGNING ON HIGHWAYS AND FREEWAYS
T-WZ-11	03-13-09	ONE LANE CLOSURE DETAIL ON DIVIDED HIGHWAYS
T-WZ-16	03-13-09	LANE SHIFT ON DIVIDED HIGHWAYS AND FREEWAYS
T-WZ-21	03-15-11	LANE CLOSURE WITH LEFT HAND MERGE AND LANE SHIFT
RD-L-3	04-15-04	STANDARD LEGEND FOR SIGNALIZATION AND LIGHTING
T-SG-2	07-29-04	LOOP LEAD-INS, CONDUIT AND PULL BOXES.
T-SG-3	11-11-04	STANDARD NOTES AND DETAILS OF INDUCTIVE LOOPS.



PROJECT SITE



**BEGIN PROJECT L.M. 7.15
(W. OF LOWER BROWNSVILLE RD., MM 74.2)**

**END PROJECT L.M. 11.75
(W. OF S.R. 20, MM 78.8)**

SPECIAL NOTES

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED MARCH 1, 2006 AND ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS AND IN THE PROPOSAL CONTRACT

TDOT C.E. MANAGER 1 TABITHA CAVANESS, P.E., REG. 4
DESIGNER LARRY BRASHER, REG. 4 CHECKED BY WILLIE COLEMAN, REG. 4

P.E. NO. 98043-4175-04
PIN: 040942.02



PROJECT LENGTH 4.60 MILES
TOTAL LANE MILES RESURFACED 18.40MILES

TRAFFIC DATA	
ADT (2014)	40064

**UNOFFICIAL
SET
NOT FOR
BIDDING**



APPROVED: Paul D. Decker
CHIEF ENGINEER

DATE: _____

APPROVED: [Signature]
JOHN SCHROER, COMMISSIONER

U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION

APPROVED: _____
DIVISION ADMINISTRATOR DATE

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2014	NH-I-40-1(345)	2

ESTIMATED ROADWAY QUANTITIES

ITEM NO.	DESCRIPTION	UNIT	57201-8147-44 QUANTITY	NH-I-40-1(345) QUANTITY	TOTAL QUANTITY
202-03.01	REMOVAL OF ASPHALT PAVEMENT	S.Y.	150		150
203-06	WATER	M.G.	19		19
① ② 303-02	MINERAL AGGREGATE, TYPE B BASE, GRADING (DESCRIPTION)	TON	2502		2502
③ 307-01.01	ASPHALT CONCRETE MIX (PG64-22) (BPMB-HM) GRADING A	TON	2497		2497
403-01	BITUMINOUS MATERIAL FOR TACK COAT (TC)	TON	223		223
④ ⑤ 411-03.10	ACS MIX(PG76-22) GRADING D	TON	455		455
⑥ ① 411-03.15	ASP CEM(PG76-22)FOR OGFC MIX	TON	713		713
⑦ ① 411-03.16	AGGREGATE FOR OGFC MIX	TON	11878		11878
⑧ ⑨ ⑩ ① 411-03.12	ACS MIX(PG64-22) THIN LIFT D ASPHALT	TON	9179		9179
411-12.01	SCORING SHOULDERS (CONTINUOUS) (16IN WIDTH)	L.M.		18.56	18.56
⑫ 415-01.01	COLD PLANING BITUMINOUS PAVEMENT	TON	2217		2217
415-01.02	COLD PLANING BITUMINOUS PAVEMENT	S.Y.	215964		215964
712-01	TRAFFIC CONTROL	LS	1		1
⑬ 712-06	SIGNS (CONSTRUCTION)	S.F.	3021		3021
712-08.03	ARROW BOARD (TYPE C)	EACH	4		4
712-04.01	FLEXIBLE DRUMS (CHANNELIZING)	EACH	810		810
712-05.03	WARNING LIGHTS (TYPE C)	EACH	100		100
716-01.23	Snwplwble Pvmt Mrkrs (Bi-Dir)(2 Color)	EACH		747	747
716-01.30	REMOVAL OF SNOWPLOWABLE REFLECTIVE MARKER	EACH	747		747
716-02.04	PLASTIC PAVEMENT MARKING(CHANNELIZATION STRIPING)	S.Y.		285	285
⑭ 716-05.20	PAINTED PAVEMENT MARKING (6" LINE)	L.M.	54.9		54.9
⑮ ⑯ 716-12.02	ENHANCED FLATLINE THERMO PVMT MRKNG (6IN LINE)	L.M.		21.95	21.95
⑰ 716-12.03	ENHANCED FLATLINE THERMO PVMT MRKNG (8IN BARRIER LINE)	L.F.		3418	3418
717-01	MOBILIZATION	LS	1		1
⑱ 730-03.21	INSTALL PULL BOX (TYPE B)	EACH	2		2
⑱ 730-12.02	CONDUIT 2" DIAMETER (PVC)	L.F.	24		24
⑱ 730-14.02	SAW SLOT	L.F.	300		300
⑰ ⑱ 730-14.03	LOOP WIRE	L.F.	1220		1220

FOOTNOTES

- ① INCLUDES 102 TONS FOR BREAKOUT.
- ② INCLUDES 20 TONS FOR RAMPS.
- ③ FOR REPLACEMENT OF QUANTITY REMOVED UNDER ITEM 202-03.01 ONLY; INCLUDES 2428 TONS FOR LONGITUDINAL JOINT REPAIRS.
- ④ INCLUDES 308 TONS FOR SPOT LEVELING.
- ⑤ INCLUDES 147 TONS FOR PAVING ON/OFF RAMPS.
- ⑥ INCLUDES 476 TONS FOR EXTRA-WIDTH COLD PLANING, PAVING GORE AREAS AND ACCELERATION LANES.
- ⑦ INCLUDES 7450 TONS FOR EXTRA-WIDTH COLD PLANING, PAVING GORE AREAS AND ACCELERATION LANES.
- ⑧ SEE SPECIAL PROVISION NUMBER 411B.
- ⑨ INCLUDES 431 TONS FOR EXTRA-WIDTH COLD PLANNING, PAVING FOR GORE AREAS, FOR PAVING ACCELERATION LANES AND ON/OFF RAMPS.
- ⑩ SEE SPECIAL PROVISION NUMBER 411TLD.
- ⑪ SEE SPECIAL PROVISION NUMBER 407G FOR MATERIAL TRANSFER DEVICE.
- ⑫ FOR LONGITUDINAL JOINT REPAIR.

- ⑬ QUANTITY INCLUDES
- | | | | |
|----|-----------------------------|---------|-------------|
| 4 | ROAD WORK NEXT 5 MI. | (G20-1) | (48" X 24") |
| 12 | END ROAD WORK | (G20-2) | (48" X 24") |
| 2 | MERGE LEFT | (W4-2) | (48" X 48") |
| 2 | MERGE RIGHT | (W4-2) | (48" X 48") |
| 49 | UNEVEN LANES | (W8-11) | (48" X 48") |
| 4 | ROAD WORK 1 MILE | (W20-1) | (48" X 48") |
| 8 | ROAD WORK AHEAD | (W20-1) | (48" X 48") |
| 5 | ROAD WORK 1500 FT. | (W20-1) | (48" X 48") |
| 5 | ROAD WORK 1000 FT. | (W20-1) | (48" X 48") |
| 5 | ROAD WORK 500 FT. | (W20-1) | (48" X 48") |
| 4 | RIGHT LANE CLOSED 1,500 FT. | (W20-5) | (48" X 48") |
| 4 | RIGHT LANE CLOSED 1,000 FT. | (W20-5) | (48" X 48") |
| 4 | RIGHT LANE CLOSED 500 FT. | (W20-5) | (48" X 48") |
| 4 | LEFT LANE CLOSED 1,500 FT. | (W20-5) | (48" X 48") |
| 4 | LEFT LANE CLOSED 1,000 FT. | (W20-5) | (48" X 48") |
| 4 | LEFT LANE CLOSED 500 FT. | (W20-5) | (48" X 48") |
| 28 | GROOVED PAVEMENT | (TN-64) | (48" X 48") |
| 4 | FRESH OIL | (W21-2) | (48" X 48") |
| 19 | SHOULDER WORK | (W21-5) | (48" X 48") |
| 49 | SHOULDER DROP-OFF | (W8-9) | (48" X 48") |

THIS CONSTRUCTION SIGNING IS TO BE AS A MINIMUM. OTHER SIGNS AS DIRECTED BY THE ENGINEER MAY BE REQUIRED DURING DIFFERENT PHASES.

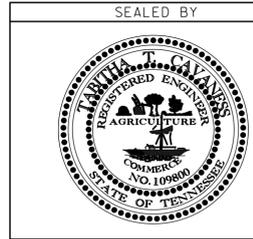
- ⑭ FOR USE AS TEMPORARY LINE MARKINGS.
- ⑮ CONTRACTOR SHALL USE THE RIBBON METHOD FOR APPLICATION.
- ⑯ FOR FINAL PAVEMENT MARKING ONLY.
- ⑰ ITEM INCLUDES AN EXTRA 160' (20' PER LOOP) THAT IS TO REMAIN IN PULL BOX FOR INSTALLATION BY TRAFFIC COUNTER PERSONNEL.

⑱

TRAFFIC COUNTER TABULATION				
INTERSECTION	LOOP WIRE 730-14.03 (LIN. FT.)	SAW SLOT 730-14.02 (LIN. FT.)	2" CONDUIT (PVC) 730-12.02 (LIN. FT.)	INSTALL PULL BOX (TYPE B) 730-03.21
I-40 @ L.M. 1,996 (APPROX.)	1220	300	24	2
TOTALS	1220	300	24	2

* ANY QUESTIONS PLEASE CONTACT STANLEY DUNN @ 615-350-4571

**UNOFFICIAL
SET
NOT FOR
BIDDING**

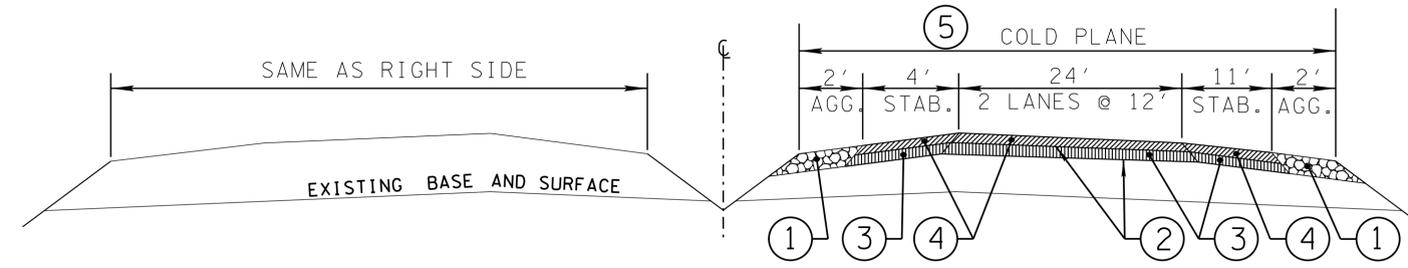


STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

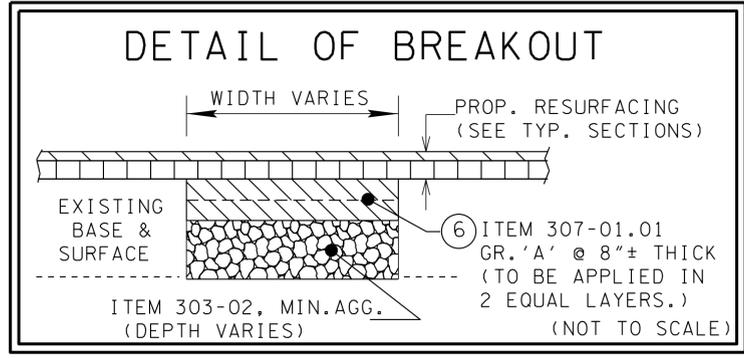
**ESTIMATED
ROADWAY
QUANTITIES**

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2014	NH-1-40-1(345)	3

TYPICAL SECTION



FROM: L.M. 7.15 TO L.M. 11.75



BRIDGE NOTE

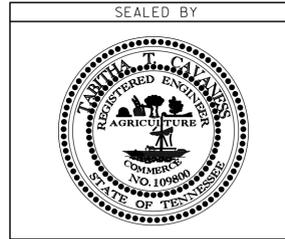
THE CONTRACTOR SHALL:
 COLD PLANE 1.25" DEPTH AND PAVE ACROSS
 EXISTING STRUCTURES WITH PLANS QUANTITIES
 ONLY @ L.M. 8.35 (144') WBL/EBL

BRIDGE NOTE

THE CONTRACTOR SHALL:
 "LEAVE AS IS"
 BRIDGES AT L.M. 10.45 (318') WBL/EBL
 BRIDGES AT L.M. 11.08 (430') WBL/EBL

PROPOSED PAVEMENT SCHEDULE	
①	MINERAL AGGREGATE BASE @ 2"± THICK FOR SHOULDERS ITEM 303-02 MINERAL AGGREGATE, TYPE "B" BASE, GRADING "C OR D"
②	TACK COAT (TC) ITEM 403-01 BITUMINOUS MATERIAL FOR TACK COAT (TC) (AT 0.07 GAL./S.Y. FOR GENERAL USE) (AT 0.10 GAL./S.Y. FOR MILLING-COLD PLANE)
③	ASPHALTIC CONCRETE SURFACE (ACS) @ 0.80"± THICK (APPROX. 85.00 LBS./S.Y.) ITEM 411-03.12 ACS MIX (PG64-22) THIN LIFT D ASPHALT
④	ASPHALTIC CONCRETE (ACS) @ 1.25"± THICK (APPROX. 110.00 LBS./S.Y.) ITEM 411-03.16 AGGREGATE FOR OGFC MIX ITEM 411-03.15 ASPHALT CEMENT (PG76-22) (ACS) FOR OGFC MIX
⑤	COLD PLANING @ 2.00"± THICK (APPROX. 210 LBS./S.Y.) ITEM 415-01.02 COLD PLANING BITUMINOUS PAVEMENT
⑥	BITUMINOUS BASE COURSE (BLACK BASE) @ 8"± THICK (APPROX. 920.0 LBS./S.Y.) ITEM 307-01.01 ASPHALT CONCRETE MIX (PG64-22) (BPMB-HM) GRADING "A" (THIS ITEM IS TO BE USED FOR BREAKOUT ONLY)
⑦	ASPHALTIC CONCRETE SURFACE (ACS) @ 1.25"± THICK (APPROX. 132.5 LBS./S.Y.) ITEM 411-03.10 ACS MIX (PG76-22) GRADING "D" (THIS ITEM IS TO BE USED FOR SPOT LEVELING AND RAMPS ONLY.)

**UNOFFICIAL
SET
NOT FOR
BIDDING**



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

**TYPICAL
SECTIONS**
(NOT TO SCALE)

26-SEP-2014 10:24 \\JJ04w\01.tdot.state.tn.us\04SHARED\Design\RESURF REG4 PROJ\MADISON\1-40\LM7-15-LMI-75\57140\MT-15 TYPICAL.dgn

GENERAL NOTES

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2014	NH-1-40-1(345)	4

GRADING

ANY AREA THAT IS DISTURBED OUTSIDE LIMITS OF CONSTRUCTION DURING THE LIFE OF THIS PROJECT SHALL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE.

THE CONTRACTOR SHALL NOT DISPOSE OF ANY MATERIAL EITHER ON OR OFF STATE-OWNED R.O.W. IN A REGULATORY FLOOD WAY AS DEFINED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY WITHOUT APPROVAL BY SAME. ALL MATERIAL SHALL BE DISPOSED OF IN UPLAND (NON-WETLAND) AREAS AND ABOVE ORDINARY HIGH WATER OF ANY ADJACENT WATERCOURSE. THIS DOES NOT ELIMINATE THE NEED TO OBTAIN ANY OTHER LICENSES OR PERMITS THAT MAY BE REQUIRED BY ANY OTHER FEDERAL, STATE, OR LOCAL AGENCY.

GUARDRAIL

THE CONTRACTOR SHALL NOT REMOVE ANY SECTIONS OF EXISTING GUARDRAIL TO REWORK SHOULDERS OR FLATTEN SLOPE UNTIL THE ENGINEER CONCURS IN THE NECESSITY OF REMOVAL DUE TO CONSTRUCTION REQUIREMENTS AND THE APPROPRIATE WARNING DEVICES ARE INSTALLED. THE PROPOSED GUARDRAIL, INCLUDING ANY ANCHOR SYSTEM, SHALL BE INSTALLED QUICKLY TO MINIMIZE TRAFFIC EXPOSURE TO ANY HAZARD. NO PAYMENT WILL BE MADE FOR A SECTION OF PROPOSED GUARDRAIL, INCLUDING ANCHORS, UNTIL IT IS COMPLETE IN PLACE.

IF ANY APPROACH END OF A SECTION OF GUARDRAIL OR BRIDGE RAIL MUST TEMPORARILY BE LEFT INCOMPLETE AND EXPOSED TO TRAFFIC, THE CONTRACTOR SHALL USE TWO (2) TEMPORARY BARRICADES OR DRUMS WITH TYPE A LIGHTS AND ROUNDED END ELEMENTS AS MINIMUM MEASURES TO PROTECT TRAFFIC FROM THE HAZARD OF AN EXPOSED END. ALL COST OF FURNISHING AND INSTALLING A TEMPORARY ROUNDED END ELEMENT SHALL BE INCLUDED IN THE COST OF THE PROPOSED GUARDRAIL.

UTILITIES

THE LOCATIONS OF UTILITIES SHOWN WITHIN THESE PLANS ARE APPROXIMATE ONLY. EXACT LOCATIONS SHALL BE DETERMINED IN THE FIELD BY CONTACTING THE UTILITY COMPANIES INVOLVED. NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC., AT 1-800-351-1111 AS REQUIRED BY TCA 65-31-106 WILL BE REQUIRED.

THE CONTRACTOR SHALL NOTIFY EACH INDIVIDUAL UTILITY OWNER OF HIS PLAN OF OPERATION IN THE AREA OF THE UTILITIES. PRIOR TO COMMENCING WORK, THE CONTRACTOR SHALL CONTACT THE UTILITY OWNERS AND REQUEST THEM TO PROPERLY LOCATE THEIR RESPECTIVE UTILITY ON THE GROUND. THIS NOTIFICATION SHALL BE GIVEN AT LEAST THREE (3) BUSINESS DAYS PRIOR TO COMMENCEMENT OF OPERATIONS AROUND THE UTILITY IN ACCORDANCE WITH TCA 65-31-106.

PAVEMENT MARKINGS

TEMPORARY PAVEMENT LINE MARKINGS ON INTERMEDIATE LAYERS OF PAVEMENT SHALL BE REFLECTIVE TAPE OR REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAYS WORK. SHORT, UNMARKED SECTIONS SHALL NOT BE ALLOWED. THESE MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-05.20 PAINTED PAVEMENT MARKINGS (6" LINE), L.M.

PERMANENT PAVEMENT LINE MARKINGS SHALL BE 6" SPRAY THERMOPLASTIC (60 mil) INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-12.02, SPRAY THERMO PVMT MRKNG (60 mil) (6IN LINE), L.M. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATIONS IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.

RESURFACING

IN ALL CASES, THE LENGTH OF THE PAVEMENT TRANSITION, THE THICKNESS AND WIDTH OF THE RESURFACING AND ANY ADDITIONAL PAVEMENT MATERIALS SHALL BE AS DIRECTED BY THE TDOT ENGINEER.

CONSTRUCTION WORK ZONE & TRAFFIC CONTROL NOTES

ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE WEEK BEFORE NEEDED, IF THE SIGN FACE IS FULLY COVERED.

IF THE CONTRACTOR MOVES OFF THE PROJECT, HE SHALL COVER OR REMOVE ALL UNNEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OF REMOVAL, COVERING, AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION) PER SQUARE FOOT.

A LONG TERM BUT SPORADIC USE WARNING SIGN, SUCH AS A FLAGGER SIGN, MAY REMAIN IN PLACE WHEN NOT REQUIRED PROVIDED THE SIGN FACE IS FULLY COVERED.

TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.

USE OF BARRICADES, PORTABLE BARRIER RAILS, VERTICAL PANELS, AND DRUMS SHALL BE LIMITED TO THE IMMEDIATE AREAS OF CONSTRUCTION WHERE A HAZARD IS PRESENT. THESE DEVICES SHALL NOT BE STORED ALONG THE ROADWAY WITHIN THIRTY (30) FEET OF THE EDGE OF THE TRAVELED WAY BEFORE OR AFTER USE UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL INCREASE TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF THE HORIZONTAL CURVE. THESE DEVICES SHALL BE REMOVED FROM THE CONSTRUCTION WORK ZONE WHEN THE ENGINEER DETERMINES THEY ARE NO LONGER NEEDED. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.

THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY, WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHEN THE LANE IS OPEN TO TRAFFIC UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO BE PARKED WITHIN THIRTY (30) FEET OF AN OPEN TRAFFIC LANE AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE.. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.

PAVING

THE CONTRACTOR SHALL BE REQUIRED TO COLD PLANE AND PAVE IN THE DIRECTION OF TRAFFIC.

MISCELLANEOUS

NOTHING IN THE GENERAL NOTES OR SPECIAL PROVISIONS SHALL RELIEVE THE CONTRACTOR FROM HIS RESPONSIBILITIES TOWARD THE SAFETY AND CONVENIENCE OF THE GENERAL PUBLIC AND THE RESIDENTS ALONG THE PROPOSED CONSTRUCTION AREA.

EROSION PREVENTION AND SEDIMENT CONTROL

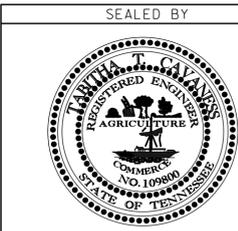
DISTURBED AREA

AREAS TO BE UNDISTURBED SHALL BE CLEARLY MARKED IN THE FIELD BEFORE CONSTRUCTION ACTIVITIES BEGIN.

PRE-CONSTRUCTION VEGETATIVE GROUND COVER SHALL NOT BE DESTROYED, REMOVED OR DISTURBED (I.E. CLEARING AND GRUBBING INITIATED) MORE THAN 15 CALENDAR DAYS PRIOR TO GRADING OR EARTH MOVING ACTIVITIES UNLESS THE AREA IS MULCHED, SEEDED WITH MULCH, OR OTHER TEMPORARY COVER IS INSTALLED.

NO MORE THAN 50 ACRES OF ACTIVE SOIL DISTURBANCE IS ALLOWED AT ANY TIME DURING THE CONSTRUCTION OF THE PROJECT. OFF-SITE BORROW OR WASTE AREAS ARE TO BE INCLUDED IN THE TOTAL DISTURBED AREA IF THE BORROW OR WASTE AREA IS EXCLUSIVE TO THE PROJECT PER TDOT'S WASTE AND BORROW MANUAL.

**UNOFFICIAL
SET
NOT FOR
BIDDING**



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

**GENERAL
NOTES**

SPECIAL NOTES

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2014	NH-1-40-1(345)	4A

SURFACE IS TO BE CROWNED AS DIRECTED BY THE ENGINEER.

ALL SIGNS WHICH INTERFERE WITH CONSTRUCTION WILL BE RELOCATED OUTSIDE LIMITS OF CONSTRUCTION BY THE CONTRACTOR. UPON COMPLETION OF CONSTRUCTION, THE CONTRACTOR WILL RESTORE THE SIGNS TO ORIGINAL LOCATION. COST TO BE INCLUDED IN ITEM NO. 712-06.

IF THE CONTRACTOR ELECTS TO UTILIZE SIGN POST ANCHORS (STUBS) FOR SIGN ERECTION. THESE SHALL BE REMOVED WHEN THE SIGNS ARE REMOVED TO AVOID FUTURE DAMAGE TO MOWERS OR OTHER MACHINERY.

UNDER THE DIRECTION OF THE ENGINEER, THE CONTRACTOR MAY BE REQUIRED TO APPLY PAINTED MARKINGS IN THE PAVEMENT AREAS NOT SPECIFICALLY DETAILED IN THE PLANS. PAYMENT WILL BE MADE AT THE UNIT PRICE BID FOR ITEM NO. 716-05.20.

UNDER THE DIRECTION OF THE ENGINEER, THE CONTRACTOR MAY BE REQUIRED TO APPLY PLASTIC MARKINGS IN THE PAVEMENT AREAS NOT SPECIFICALLY DETAILED IN THE PLANS. PAYMENT WILL BE MADE AT THE UNIT PRICE BID FOR ITEM NO. 716-12.02.

THE CONTRACTOR WILL BE REQUIRED TO CLEAN AND BLADE GRASS FROM SHOULDERS BEFORE PLACING ITEM 303-02. MATERIAL BLADED FROM SHOULDERS TO BE PICKED UP AND DISPOSED OF AS DIRECTED BY THE ENGINEER. COST TO BE INCLUDED IN ITEM 303-02.

MATERIAL TRANSFER DEVICE SHALL BE USED FOR ALL MIXES.

ONLY 4 ARROW BOARDS WILL BE PAID FOR DURING THE LIFE OF THIS PROJECT. IF THE CONTRACTOR'S WORK PHASES REQUIRED ADDITIONAL ARROW BOARDS, THEIR COST WILL BE INCLUDED IN THE COST OF OTHER ITEMS.

THE INSIDE SHOULDERS WILL BE PAVED CONCURRENTLY WITH THE INSIDE TRAFFIC LANE.

THE SHOULDER WILL BE PAVED CONCURRENTLY WITH THE TRAFFIC LANE.

THE BITUMINOUS MATERIAL DESIGNATED TO RESTORE THE COLD PLANING AREA WILL BE PLACED WITHIN 48 HOURS OF THE COMMENCEMENT OF COLD PLANING OPERATIONS. COLD PLANING OPERATIONS WILL BE LIMITED TO AN AREA EQUAL TO THAT WHICH CAN BE COVERED WITH BITUMINOUS MATERIAL WITHIN THE TIME LIMITS SPECIFIED. EVEN IF COLD PLANING OPERATIONS MUST BE SUSPENDED UNTIL PAVING CATCHES UP.

THE CONTRACTOR WILL BE REQUIRED TO PAVE IMMEDIATELY BEHIND COLD PLANING OPERATIONS TO ENSURE THAT THE MILLED SURFACE WILL BE COVERED WITH BITUMINOUS/CEMENT THE SAME WORK DAY, AND ABIDE BY DROP-OFF POLICY.

THE MILLING AND PAVING OF THE ROADWAY SHALL BE PERFORMED BETWEEN THE HOURS OF 6:00 P.M. AND 6:00 A.M. DURING WEEKDAYS. THE CONTRACTOR SHALL MAINTAIN ALL LANES OF TRAFFIC IN EACH DIRECTION BETWEEN 6:00 A.M. AND 6:00 P.M. MONDAY THROUGH FRIDAY.

THE DEPARTMENT SHALL RESERVE THE RIGHT TO REOPEN LANES AS NECESSARY WHEN TRAFFIC CONDITIONS ARE DEEMED UNACCEPTABLE (EXCESSIVE QUEUE LENGTHS AND DELAY TIMES). THE CONTRACTOR SHALL BE REQUIRED TO FULLY COOPERATE WITH THE PROJECT SUPERVISOR WHEN REQUESTED TO MAKE CHANGES TO THE TRAFFIC CONTROL.

THIS IS A NIGHT JOB ALL LANES OPEN TO TRAFFIC FROM 7:00 P.M. TO 6:00 A.M. MONDAY THRU FRIDAY.

THE CONTRACTOR SHALL COMPLY WITH SECTION 712 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION REGARDING TEMPORARY TRAFFIC CONTROL AND THE CURRENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

THE CONTRACTOR SHALL TAKE EXTREME CARE WHEN COLD PLANING THE EXISTING ASPHALT OFF BRIDGE DECK SO AS NOT TO DAMAGE THE EXISTING DECK SEALANT AND/OR EXPANSION JOINT MEMBERS (STEEL PLATES, BARS, AND/OR HEADERS). IF DAMAGE DOES OCCUR, THE CONTRACTOR SHALL BE RESPONSIBLE FOR NECESSARY REPAIRS TO ALL DAMAGED MEMBERS TO THE SATISFACTION OF THE PROJECT ENGINEER AT NO ADDITIONAL COST.

RESURFACING

TRAFFIC WILL BE ALLOWED TO TEMPORARILY DRIVE ON THE MILLED SURFACE OF THE ROADWAY UNDER THE FOLLOWING CONDITION ONLY:

- A: THE MILLED SURFACE IS FINE TEXTURED. THE FINE TEXTURE SHALL BE OBTAINED BY A MILLING MACHINE UTILIZING A MILLING HEAD WITH TEETH SPACING 3/8" OR LESS OPERATING AT LESS THAN 80 FEET PER MINUTE.
- B: THE SURFACE SHALL BE SWEEPED AND CLEANED ON ALL LOOSE MATERIALS.
- C: THE DIFFERENCE IN ELEVATION BETWEEN THE MILLED SURFACE AND THE ADJACENT LANE SHALL NOT EXCEED 1 1/2 INCHES.
- D: THE MILLED SURFACE SHALL BE PAVED WITHIN 48 HOURS.
- E: RAIN OR INCLEMENT WEATHER IS NOT EXPECTED OR FORECASTED WITHIN 48 HOURS AFTER MILLING.
- F: ALL APPLICABLE SIGNING IS INSTALLED IN ACCORDANCE WITH THE MUTCD SIGNING SHALL INCLUDE MOTORCYCLE WARNING SIGN (TN-64) PLACED IN ADVANCE OF ANY MILLED AREAS.
- G: IF RAVELING OR DETERIORATION OF THE MILLED SURFACE IS OCCURRING WHILE TRAFFIC IS DRIVING ON THE MILLED SURFACE, THEN THIS PRACTICE WILL NOT BE ALLOWED AND PAVING SHALL BE COMPLETED IMMEDIATELY AFTER MILLING.
- H: ONLY ONE LANE IN EACH DIRECTION SHALL HAVE A MILLED SURFACE AT ONE TIME.

PAVEMENT EDGE DROP-OFF TRAFFIC CONTROL

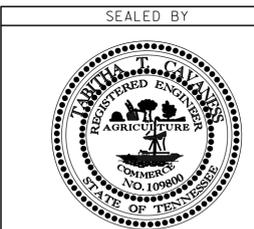
- A. DIFFERENCE IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES OR TRAFFIC LANE AND SHOULDER WHERE THE TRAFFIC LANE IS BEING USED BY TRAFFIC. CAUSED BY BASE, PAVING OR RESURFACING:
 - 1. DIFFERENCE IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 0.75 INCH AND NOT EXCEEDING 2 INCHES.
 - a. WARNING SIGNS, UNEVEN PAVEMENT (W8-11) AND/OR SHOULDER DROP-OFF (W8-9A), SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.
 - b. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY ADDED PAVEMENT SHALL BE ELIMINATED WITHIN THREE WORKDAYS.
 - c. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY COLD PLANING SHALL BE ELIMINATED WITHIN THREE WORKDAYS.
 - d. WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE TRAFFIC LANE BEING UTILIZED BY TRAFFIC AND SHOULDER, THE DIFFERENCE IN ELEVATION SHALL BE ELIMINATED WITHIN SEVEN WORKDAYS AFTER THE CONDITION IS CREATED.

SPECIAL EROSION PREVENTION AND SEDIMENT CONTROL

THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS, BEFORE CLEANING OUT OR REMOVAL/REPLACEMENT OF DITCHES, SIDE DRAINS, AND CROSS DRAINS, TO ENSURE THAT THE DRAINAGE FEATURES/ WATERCOURSES ARE NOT STREAMS OR WETLANDS. IF THE CONTRACTOR/ TDOT INSPECTOR IS UNSURE WHETHER THE DRAINAGE FEATURES ARE STREAMS OR WETLANDS, THE INSPECTOR SHALL CONTACT THE TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION TO OBTAIN THE APPROPRIATE PERMITS.

DUE TO THE TYPE OF WORK IT IS OUR UNDERSTANDING THAT THE PROJECT WILL NOT DISTURB MORE THAN ONE ACRE. IF THE DISTURBED AREA IS MORE THAN ONE ACRE, PLEASE CONTACT THE TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION BECAUSE A NPDES PERMIT WILL BE REQUIRED.

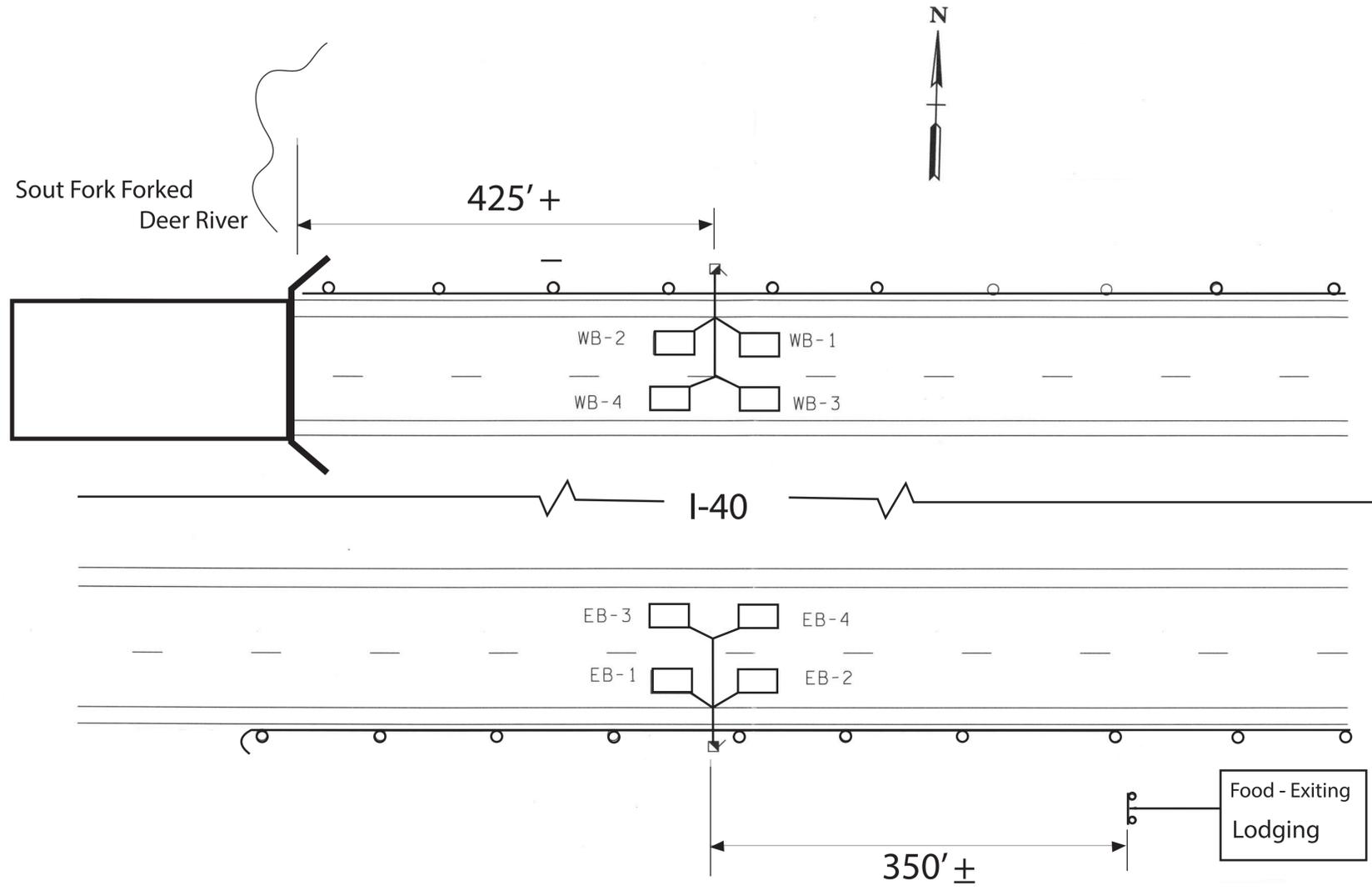
**UNOFFICIAL
SET
NOT FOR
BIDDING**



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

**SPECIAL
NOTES**

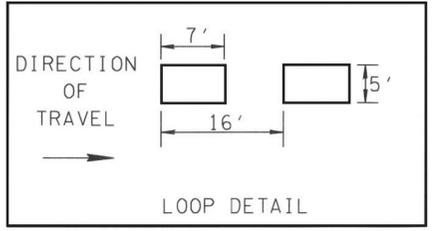
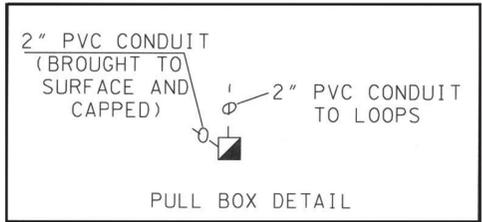
TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2014	NH-I-40-1(345)	5
.	.	.	.
.	.	.	.



I-40 Madison County at L. M. 10.184 EB (Approx.)

NOT TO SCALE L. M. 10.635 WB

**UNOFFICIAL
 SET
 NOT FOR
 BIDDING**



STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

**TRAFFIC
 DETECTION
 LOOPS DETAIL**

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2014	NH-1-40-1(345)	6

SPECIAL TRAFFIC COUNTER SPECIFICATIONS

THE CONTRACTOR SHALL SUPPLY AND INSTALL ALL RELATED EQUIPMENT. THE CONTRACTOR SHALL MAKE SURE EACH DETECTION LOOP IS OPERATIONAL AT THE COMPLETION OF THE PROJECT. THE STATE'S PROJECT PLANNING DIVISION'S TRAFFIC COUNTER PERSONNEL WILL MAKE THE TRAFFIC COUNTER FULLY OPERATIONAL.

ALL LOOPS SHALL BE FIVE FEET BY SEVEN FEET (5' X 7') AND TYPICAL IN SIZE WITH THREE (3) TURNS OF LOOP WIRE.

LEAD WIRES TO ALL LOOPS SHALL BE RUN THROUGH 2-INCH CONDUIT FROM THE SHOULDER OF THE ROADWAY TO AN IN-GROUND PULL BOX. THE PULL BOX SHOULD BE LOCATED AS CLOSE AS POSSIBLE AND BEHIND THE GUARDRAIL. LOOP LEAD-INS AND CONDUIT SHALL BE MARKED ON THE ROADWAY SURFACE SO THAT ANY GUARDRAIL POST INSTALLATION OR OTHER CONSTRUCTION WILL NOT DISTURB THIS UNDERGROUND WORK.

THE EXTRA TWENTY FEET (20') OF LOOP WIRE FOR EACH OF THE LOOP SHALL BE STORED IN THE PULL BOX FOR FUTURE INSTALLATION BY TRAFFIC COUNTER PERSONNEL. A BULL STRING SHALL BE INCLUDED IN THE STUB OUT CONDUIT.

EACH LOOP DETECTOR LEAD-IN SHALL BE MARKED WITH LANE IDENTIFICATION, DENOTING THE LANE AND POSITION IN THE LANE FOR EACH LOOP (SUCH AS NB-1 FOR NORTHBOUND 1, ECT.).

FOR QUESTIONS CONCERNING ANY OF THE INSTALLATION OF THE COUNTER EQUIPMENT, PLEASE CONTACT STANLEY DUNN AT 615-350-4571 OR AT STANLEY.DUNN@TN.GOV. CONTRACTOR SHALL NOTIFY STANLEY DUNN TO REPORT THAT WORK HAS BEEN COMPLETED SO THAT HE CAN FINISH THE INSTALLATION.

THE CONTRACTOR SHALL SUPPLY STANLEY DUNN WITH THE X/Y COORDINATED (BASED ON WGS COORDINATES) OF EACH PULL BOX.

LOCATE PULL BOXES APPROXIMATELY 1 FT. BEHIND THE GUARDRAIL BETWEEN THE POST.

EQUIPMENT AND INSTALLATION OF TRAFFIC SIGNAL ITEMS SHALL COMPLY WITH TDOT STANDARD SPECIFICATIONS, SECTION 730.

DETECTION LOOPS SHALL BE INSTALLED BEFORE THE FINAL SURFACE IS APPLIED.

**UNOFFICIAL
SET
NOT FOR
BIDDING**

SEALED BY

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

**SPECIAL
TRAFFIC COUNTER
SPECIFICATIONS**