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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF ENGINEERING

DECATUR COUNTY

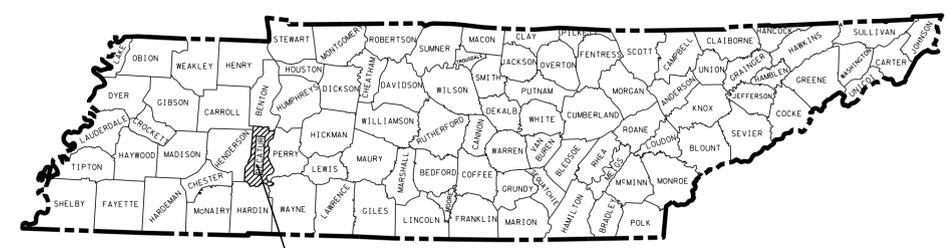
S.R. 20

FROM: L.M. 2.33 (WILKINSTOWN RD./BEAR CREEK RD.)
TO: L.M. 3.59 (JCT. S.R. 69)

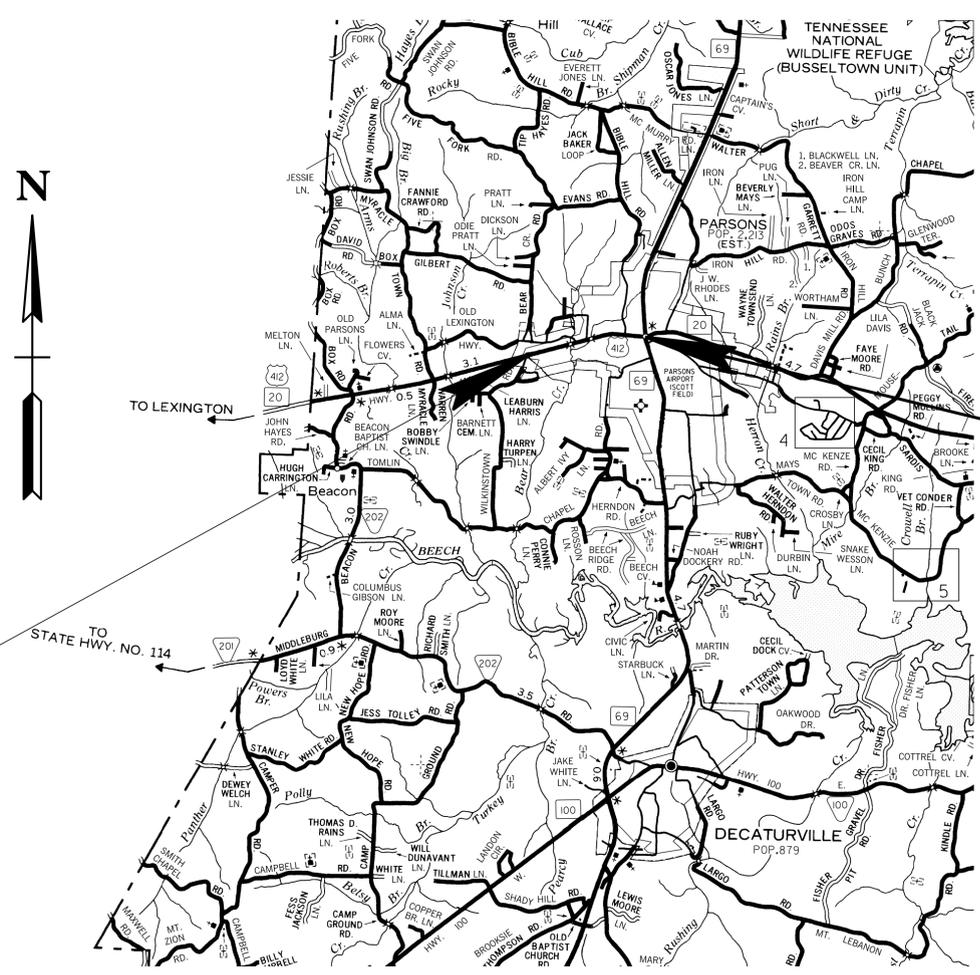
RESURFACING

STATE HIGHWAY NO. 20 F.A.H.S. NO. N.A.

TENN.	YEAR	SHEET NO.
	2014	1
FED. AID PROJ. NO.	NH/HSIP-20(53)	
STATE PROJ. NO.	20002-8240-14	



PROJECT SITE



UTILITY OWNERS	
UTILITY	UTILITY CONTACT
WATER SEWER GAS	PARSON CITY HALL LEE VILLAFLO (731) 847-6358 535 TENNESSEE AVE. S. PARSON, TN. 38363

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BEGIN PROJECT L.M. 2.33
(WILKINSTOWN RD./BEAR CREEK RD.)

END PROJECT L.M. 3.59
(JCT. S.R. 69)

SPECIAL NOTES

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED FEBRUARY 4, 2014 AND ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS AND IN THE PROPOSAL CONTRACT

TDOT C.E. MANAGER 1 TABITHA CAVANESS, P.E., REG. 4
DESIGNER ROSE WILLIAMS/WILLIE COLEMAN, REG. 4 CHECKED BY WILLIE COLEMAN, REG. 4

P.E. NO. 98043-4283-04
PIN: 119604.00



PROJECT LENGTH **1.26 MILES**
TOTAL LANE MILES RESURFACED **6.43 MILES**

TRAFFIC DATA	
ADT (2013)	7620

APPROVED: Paul D. Decker
CHIEF ENGINEER

DATE: _____

APPROVED: John Schroer
JOHN SCHROER, COMMISSIONER

U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION

APPROVED: _____
DIVISION ADMINISTRATOR DATE

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2014	NH/HSIP-20(53)	2

FOOTNOTES

ESTIMATED ROADWAY QUANTITIES						
ITEM NO.	DESCRIPTION	UNIT	20002-8240-14 QUANTITY	NH/HSIP-20(53) QUANTITY	TOTAL QUANTITY	
	202-03.01	REMOVAL OF ASPHALT PAVEMENT	S.Y.	155	0	155
①	203-01.05	EXCAVATION	C.Y.	8.36	0	8.36
	203-06	WATER	M.G.	1	0	1
②	303-02	MINERAL AGGREGATE, TYPE B BASE, GRADING (C OR D)	TON	105	0	105
③	307-01.01	ASPHALT CONCRETE MIX (PG64-22) (BPMB-HM) GRADING A	TON	72	0	72
④	307-01.15	ASC MIX (PG64-22) (BPMLC-HM) GRADING CS	TON	988	0	988
	403-01	BITUMINOUS MATERIAL FOR TACK COAT (TC)	TON	37	0	37
⑤⑥	411-02.10	ACS MIX(PG70-22) GRADING D	TON	2954	0	2954
⑦	415-01.01	COLD PLANING BITUMINOUS PAVEMENT	TON	9888	0	9888
	701-02.01	CONCRETE HANDICAP RAMP (RETROFIT)	S.F.	0	1596	1596
⑧	701-02.03	CONCRETE HANDICAP RAMP	S.F.	0	452	452
⑨	705-04.07	TAN ENERGY ABSORBING TERM (NCHRP 350, TL3)	EACH	0	7	7
	712-01	TRAFFIC CONTROL	LS	1	0	1
⑩	712-06	SIGNS (CONSTRUCTION)	S.F.	688	0	688
	712-08.03	ARROW BOARD (TYPE C)	EACH	2	0	2
	716-01.21	SNWPLWBLE PVMT MRKRS (BI-DIR)(1 COLOR)	EACH	0	295	295
⑪	716-02.03	PLASTIC PAVEMENT MARKING (CROSS-WALK)	L.F.	0	247	247
⑪	716-02.04	PLASTIC PAVEMENT MARKING(CHANNELIZATION STRIPING)	S.Y.	0	55	55
⑪	716-02.05	PLASTIC PAVEMENT MARKING (STOP LINE)	L.F.	0	139	139
⑪	716-02.06	PLASTIC PAVEMENT MARKING (TURN LANE ARROW)	EACH	0	10	10
⑪	716-03.01	PLASTIC WORD PAVEMENT MARKING (ONLY)	EACH	0	9	9
⑪	716-03.09	PLASTIC WORD PAVEMENT MARKING (NO PARKING)	EACH	0	6	6
⑪	716-04.02	PLASTIC PAVEMENT MARKING(DOUBLE TURNING ARROW)	EACH	0	1	1
⑪	716-04.05	PLASTIC PAVEMENT MARKING (STRAIGHT ARROW)	EACH	0	1	1
⑫	716-05.01	PAINTED PAVEMENT MARKING (4" LINE)	L.M.	12.5	0	12.5
⑫	716-05.05	PAINTED PAVEMENT MARKING (STOP LINE)	L.F.	150	0	150
⑪	716-05.08	PAINTED PAVEMENT MARKING (PARKING LINE)	L.F.	828	0	828
⑬	716-13.01	SPRAY THERMO PVMT MRKNG (60 mil) (4IN LINE)	L.M.	0	6.62	6.62
⑪	716-13.03	SPRAY THERMO PVMT MRKNG (60 mil) (8IN BARRIER LINE)	L.F.	0	550	550
⑪	716-13.05	SPRAY THERMO PVMT MRKNG (60 mil) (6IN DOTTED LINE)	L.F.	0	22	22
	717-01	MOBILIZATION	LS	1	0	1
⑭	730-12.01	CONDUIT 1" DIAMETER (PVC)	L.F.	50	0	50
⑭	730-14.01	SHIELDED DETECTOR CABLE	L.F.	50	0	50
⑭	730-14.02	SAW SLOT	L.F.	812	0	812
⑭	730-14.03	LOOP WIRE	L.F.	1624	0	1624

- ① FOR CONCRETE HANDICAP RAMP INSTALLATION.
- ② FOR BREAKOUT ONLY.
- ③ FOR REPLACEMENT OF QUANTITY REMOVED UNDER ITEM 202-03.01 ONLY.
- ④ INCLUDES 33 TONS FOR COUNTY ROADS, PRIVATE DRIVES, 119 TONS FOR GORE AREAS, SIDE ROADS AND INTERSECTIONS, AND 62 TONS FOR SHOULDERS.
- ⑤ INCLUDES 97 TONS FOR COUNTY ROADS, PRIVATE DRIVES, 349 TONS FOR GORE AREAS, SIDE ROADS AND INTERSECTIONS, 100 TONS FOR SPOT LEVELING AND 179 TONS FOR SHOULDER.
- ⑥ SEE SPECIAL PROVISION NO. 407G FOR MATERIAL TRANSFER DEVICE.
- ⑦ INCLUDES 346 TONS FOR GORE AREAS, SIDE ROADS AND INTERSECTIONS.
- ⑧ FOR USE AT INTERSECTIONS S.R. 20 LEXINGTON HWY AND NORTHWOOD DR.
- ⑨ FOR UPGRADE OF EXISTING ANCHORS AT ALL STRUCTURE LOCATIONS IN PROJECT LIMITS. THE COST OF REMOVAL OF EXISTING GUARDRAIL AND ANCHORS TO BE INCLUDED IN THIS ITEM.

⑩ QUANTITY INCLUDES

2	ROAD WORK NEXT 2 MI.	(G20-1)	(48" X 24")
4	END ROAD WORK	(G20-2)	(48" X 24")
6	UNEVEN LANES	(W8-11)	(48" X 48")
12	ROAD WORK AHEAD	(W20-1)	(48" X 48")
2	ROAD WORK 1 MILE	(W20-1)	(48" X 48")
2	ROAD WORK 1500 FT.	(W20-1)	(48" X 48")
2	ROAD WORK 1000 FT.	(W20-1)	(48" X 48")
2	ROAD WORK 500 FT.	(W20-1)	(48" X 48")
2	ONE LANE ROAD AHEAD	(W20-4)	(48" X 48")
2	FLAGGER AHEAD	(W20-7a)	(48" X 48")
2	FRESH OIL	(W21-2)	(48" X 48")
2	SHOULDER WORK	(W21-5)	(48" X 48")
6	MOTORCYCLE GROOVE SIGN	(TN-64)	(48" X 48")

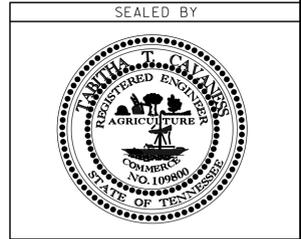
THIS CONSTRUCTION SIGNING IS TO BE AS A MINIMUM. OTHER SIGNS AS DIRECTED BY THE ENGINEER MAY BE REQUIRED DURING DIFFERENT PHASES.

- ⑪ FOR FINAL PAVEMENT MARKING ONLY.
- ⑫ FOR USE AS TEMPORARY LINE MARKINGS, INCLUDES 1.25 MILES FOR BREAKOUT, SPOT LEVELING, CITY STREETS AND INTERSECTIONS.
- ⑬ FOR FINAL PAVEMENT MARKING, INCLUDES 1 MILE FOR CITY STREETS AND INTERSECTIONS.

SIGNALIZATION TABULATION				
INTERSECTION	LOOP WIRE 730-14.03 (LIN. FT.)	SAW SLOT 730-14.02 (LIN. FT.)	SHIELDED CABLE 730-14.01 (LIN. FT.)	1" CONDUIT (PVC) 730-12.01 (LIN. FT.)
S.R.20 @ S.R. 69	1624	812	50	50
TOTALS	1624	812	50	50

CONTRACTOR NEEDS TO CONTACT THE TDOT TRAFFIC OFFICE FOR LOOP LAYOUT PLANS AND SPECIAL INSTRUCTIONS. 731-935-0186

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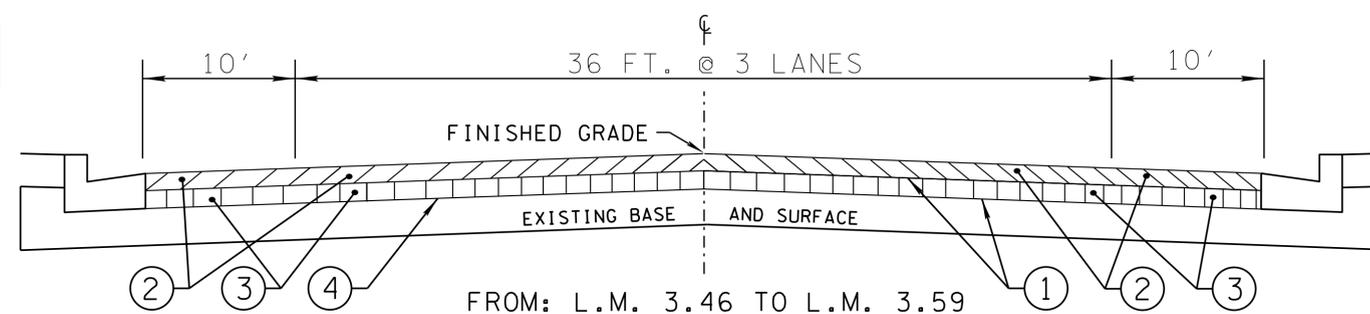
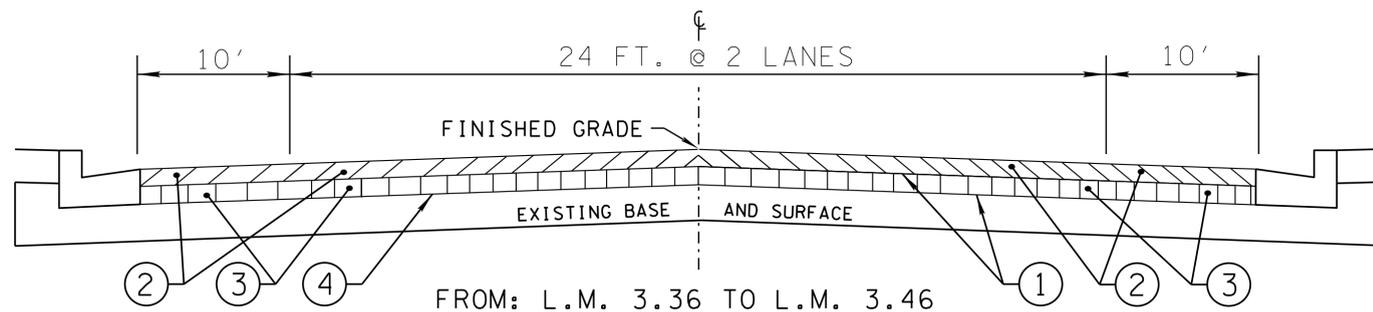
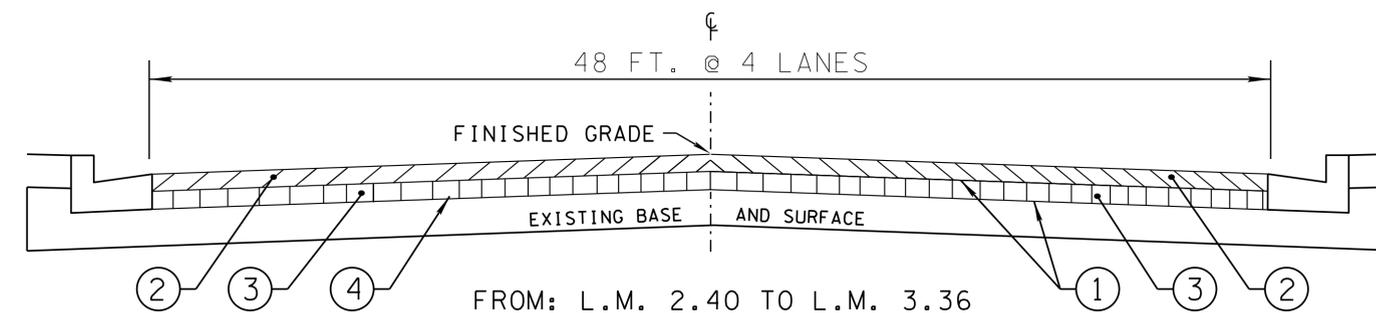
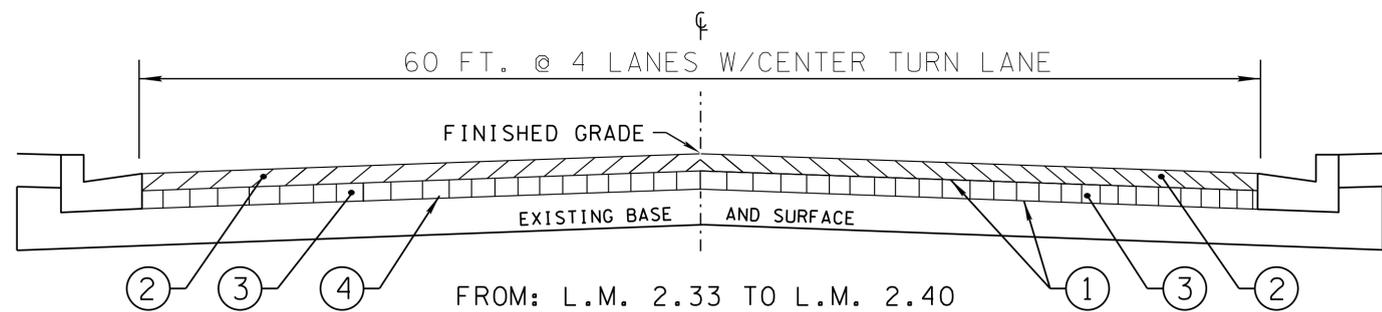
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

**ESTIMATED
ROADWAY
QUANTITIES**

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TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2014	NH/HSIP-20(53)	3

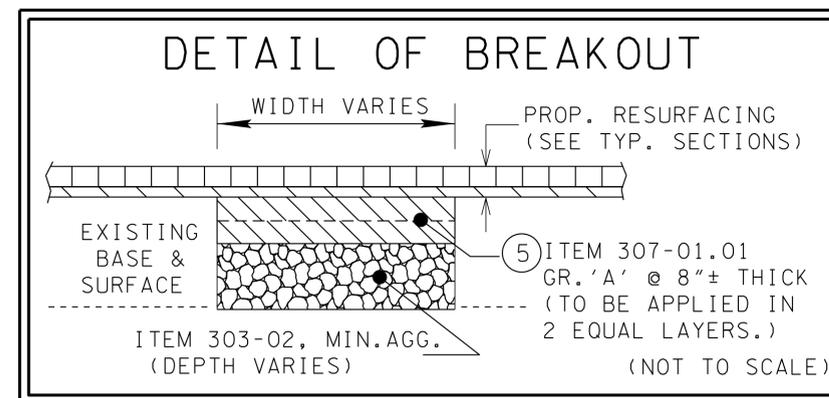
TYPICAL SECTION



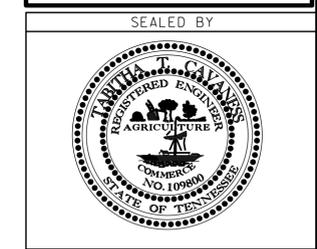
PROPOSED PAVEMENT SCHEDULE	
①	TACK COAT (TC) FOR MILLING - COLD PLANE ITEM 403-01 BITUMINOUS MATERIAL FOR TACK COAT (TC) (AT 0.10 GAL./S.Y.)
②	ASPHALTIC CONCRETE SURFACE (ACS) @ 1.25"± THICK (APPROX. 132.50 LBS./S.Y.) ITEM 411-02.10 ACS MIX (PG70-22) GRADING "D"
③	BITUMINOUS COURSE (LEVELING) @ 0.40"± THICK (APPROX. 46.00 LBS./S.Y.) ITEM 307-01.15 ACS MIX (PG64-22) (BPMLC-HM) GRADING CS
④	COLD PLANING @ 5"± THICK (APPROX. 525.00 LBS./S.Y.) ITEM 415-01.01 COLD PLANING BITUMINOUS PAVEMENT
⑤	BITUMINOUS BASE COURSE (BLACK BASE) @ 8"± THICK (APPROX. 920.0 LBS./S.Y.) ITEM 307-01.01 ASPHALT CONCRETE MIX (PG64-22) (BPMB-HM) GRADING "A" (THIS ITEM IS TO BE USED FOR BREAKOUT ONLY)

BRIDGE NOTE

THE CONTRACTOR SHALL:
PAVE ACROSS EXISTING STRUCTURES WITH
PLANS QUANTITIES ONLY @ L.M. 2.80
AND 2.99 (59')



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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS
(NOT TO SCALE)

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TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2014	NH/HS1P-20(53)	4

GENERAL NOTES

GRADING

ANY AREA THAT IS DISTURBED OUTSIDE LIMITS OF CONSTRUCTION DURING THE LIFE OF THIS PROJECT SHALL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE.

GUARDRAIL

THE CONTRACTOR SHALL NOT REMOVE ANY SECTIONS OF EXISTING GUARDRAIL TO REWORK SHOULDERS OR FLATTEN SLOPE UNTIL THE ENGINEER CONCURS IN THE NECESSITY OF REMOVAL DUE TO CONSTRUCTION REQUIREMENTS AND THE APPROPRIATE WARNING DEVICES ARE INSTALLED. THE PROPOSED GUARDRAIL, INCLUDING ANY ANCHOR SYSTEM, SHALL BE INSTALLED QUICKLY TO MINIMIZE TRAFFIC EXPOSURE TO ANY HAZARD. NO PAYMENT WILL BE MADE FOR A SECTION OF PROPOSED GUARDRAIL, INCLUDING ANCHORS, UNTIL IT IS COMPLETE IN PLACE.

IF ANY APPROACH END OF A SECTION OF GUARDRAIL OR BRIDGE RAIL MUST TEMPORARILY BE LEFT INCOMPLETE AND EXPOSED TO TRAFFIC, THE CONTRACTOR SHALL USE TWO (2) TEMPORARY BARRICADES OR DRUMS WITH TYPE A LIGHTS AND ROUNDED END ELEMENTS AS MINIMUM MEASURES TO PROTECT TRAFFIC FROM THE HAZARD OF AN EXPOSED END. ALL COST OF FURNISHING AND INSTALLING A TEMPORARY ROUNDED END ELEMENT SHALL BE INCLUDED IN THE COST OF THE PROPOSED GUARDRAIL.

UTILITIES

THE LOCATIONS OF UTILITIES SHOWN WITHIN THESE PLANS ARE APPROXIMATE ONLY. EXACT LOCATIONS SHALL BE DETERMINED IN THE FIELD BY CONTACTING THE UTILITY COMPANIES INVOLVED. NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC., AT 1-800-351-1111 AS REQUIRED BY TCA 65-31-106 WILL BE REQUIRED.

UNLESS OTHERWISE NOTED, ALL UTILITY ADJUSTMENTS WILL BE PERFORMED BY THE UTILITY OR IT'S REPRESENTATIVE. THE CONTRACTOR AND UTILITY OWNERS WILL BE REQUIRED TO COOPERATE WITH EACH OTHER IN ORDER TO EXPEDITE THE WORK REQUIRED BY THIS CONTRACT. ON CONTRACTS WHERE CONSTRUCTION STAKES, LINES, AND GRADES ARE CONTRACT ITEMS, THE CONTRACTOR WILL BE REQUIRED TO PROVIDE RIGHT-OF-WAY OR SLOPE STAKES, DITCH OR STREAM BED GRADES, OR OTHER ESSENTIAL SURVEY STAKING TO PREVENT CONFLICTS WITH THE HIGHWAY CONSTRUCTION. FREQUENTLY, THIS WILL BE REQUIRED AS THE FIRST ITEM OF WORK AND AT ANY LOCATION ON THE PROJECT DIRECTED BY THE ENGINEER.

THE CONTRACTOR WILL PROVIDE ALL NECESSARY PROTECTIVE MEASURES TO SAFEGUARD EXISTING UTILITIES FROM DAMAGE DURING CONSTRUCTION OF THIS PROJECT. IN THE EVENT THAT SPECIAL EQUIPMENT IS REQUIRED TO WORK OVER AND AROUND THE UTILITIES, THE CONTRACTOR WILL BE REQUIRED TO FURNISH SUCH EQUIPMENT. THE COST OF PROTECTING UTILITIES FROM DAMAGE AND FURNISHING SPECIAL EQUIPMENT WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.

PRIOR TO SUBMITTING HIS BID, THE CONTRACTOR WILL BE SOLELY RESPONSIBLE FOR CONTACTING OWNERS OF ALL AFFECTED UTILITIES IN ORDER TO DETERMINE THE EXTENT TO WHICH UTILITY RELOCATIONS AND/OR ADJUSTMENTS WILL HAVE UPON THE SCHEDULE OF WORK FOR THE PROJECT. WHILE SOME WORK MAY BE REQUIRED 'AROUND' UTILITY FACILITIES THAT WILL REMAIN IN PLACE, OTHER UTILITY FACILITIES MAY NEED TO BE ADJUSTED CONCURRENTLY WITH THE CONTRACTOR'S OPERATIONS. ADVANCE CLEAR CUTTING MAY BE REQUIRED BY THE ENGINEER AT ANY LOCATION WHERE CLEARING IS CALLED FOR IN THE SPECIFICATIONS AND CLEAR CUTTING IS NECESSARY FOR A UTILITY RELOCATION. ANY ADDITIONAL COST WILL BE INCLUDED IN THE UNIT PRICE BID FOR THE CLEARING ITEM SPECIFIED IN THE PLANS.

THE CONTRACTOR SHALL NOTIFY EACH INDIVIDUAL UTILITY OWNER OF HIS PLAN OF OPERATION IN THE AREA OF THE UTILITIES. PRIOR TO COMMENCING WORK, THE CONTRACTOR SHALL CONTACT THE UTILITY OWNERS AND REQUEST THEM TO PROPERLY LOCATE THEIR RESPECTIVE UTILITY ON THE GROUND. THIS NOTIFICATION SHALL BE GIVEN AT LEAST THREE (3) BUSINESS DAYS PRIOR TO COMMENCEMENT OF OPERATIONS AROUND THE UTILITY IN ACCORDANCE WITH TCA 65-31-106.

PAVEMENT MARKINGS

TEMPORARY PAVEMENT LINE MARKINGS ON INTERMEDIATE LAYERS OF PAVEMENT SHALL BE REFLECTIVE TAPE OR REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAYS WORK. SHORT, UNMARKED SECTIONS SHALL NOT BE ALLOWED. THESE MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-05.01 PAINTED PAVEMENT MARKINGS (4" LINE), L.M.

PERMANENT PAVEMENT LINE MARKINGS SHALL BE 4" SPRAY THERMOPLASTIC (60 ml) INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-13.01, SPRAY THERMO PVMT MRKNG (60 ml) (4IN LINE), L.M. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATIONS IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.

PAVING

THE CONTRACTOR SHALL BE REQUIRED TO COLD PLANE AND PAVE IN THE DIRECTION OF TRAFFIC.

RESURFACING

ON CURB AND GUTTER SECTIONS, PUBLIC ROAD INTERSECTIONS SHALL BE RESURFACED TO THE END OF RADIUS. A SATISFACTORY TRANSITION FROM THE NEW PAVEMENT TO THE EXISTING GRADE OF THE INTERSECTING PUBLIC ROAD SHALL BE PROVIDED.

ON URBAN TYPICAL SECTIONS, (CURB AND GUTTER), RESIDENTIAL DRIVEWAYS AND BUSINESS ENTRANCES SHALL HAVE A MINIMUM WIDTH OF MATERIAL NOT LESS THAN ONE FOOT USED IN THE TRANSITION TO FEATHER THE PAVEMENT EDGE.

IN ALL CASES, THE LENGTH OF THE PAVEMENT TRANSITION, THE THICKNESS AND WIDTH OF THE RESURFACING AND ANY ADDITIONAL PAVEMENT MATERIALS SHALL BE AS DIRECTED BY THE TDOT ENGINEER.

SIGNALIZATION

EQUIPMENT AND INSTALLATION OF TRAFFIC SIGNALS SHALL COMPLY WITH TDOT STANDARD SPECIFICATIONS, SECTION 730.

ANY SIGNAL HEADS, WHEN VISIBLE TO DRIVERS BUT NOT OPERATIONAL, SHALL BE COMPLETELY COVERED.

THE PROJECT ENGINEER SHALL NOTIFY THE LOCAL GOVERNMENTAL AGENCY RESPONSIBLE FOR TRAFFIC CONTROL MAINTENANCE AT LEAST ONE DAY IN ADVANCE OF THE COLD PLANING ACTIVITY AT SIGNALIZED INTERSECTIONS WHERE DETECTOR LOOPS ARE ON THE PAVEMENT. THE MAINTAINING AGENCY WILL THEN BE RESPONSIBLE FOR DISCONNECTING THE LOOP DETECTORS AND MARKING ANY NECESSARY TIMING ADJUSTMENTS IN THE SIGNAL CONTROLLER PRIOR TO THE CONSTRUCTION.

THE PROJECT ENGINEER SHALL BE RESPONSIBLE FOR SUPPLYING THE CONTRACTOR WITH AS BUILT SIGNAL PLANS AT THE PRE-CONSTRUCTION CONFERENCE. THESE PLANS WILL PROVIDE THE CONTRACTOR WITH THE DESIRED LOCATION FOR DETECTOR LOOP REPLACEMENT.

LOOPS SHALL BE INSTALLED IN THE LEVELING COURSE IF LEVELING COURSE IS PROVIDED.

LOOPS REPLACEMENT SHALL BE IN ACCORDANCE WITH SECTION 730 OF THE STANDARD SPECIFICATIONS.

CONSTRUCTION WORK ZONE & TRAFFIC CONTROL NOTES

ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE WEEK BEFORE NEEDED, IF THE SIGN FACE IS FULLY COVERED.

IF THE CONTRACTOR MOVES OFF THE PROJECT, HE SHALL COVER OR REMOVE ALL UNNEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OF REMOVAL, COVERING, AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION) PER SQUARE FOOT.

A LONG TERM BUT SPORADIC USE WARNING SIGN, SUCH AS A FLAGGER SIGN, MAY REMAIN IN PLACE WHEN NOT REQUIRED PROVIDED THE SIGN FACE IS FULLY COVERED.

TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.

THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY, WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHEN THE LANE IS OPEN TO TRAFFIC UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO BE PARKED WITHIN THIRTY (30) FEET OF AN OPEN TRAFFIC LANE AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE.. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.

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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

**GENERAL
NOTES**

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2014	NH/HS1P-20(53)	4A

SPECIAL NOTES

SURFACE IS TO BE CROWNED AS DIRECTED BY THE ENGINEER.

ALL SIGNS WHICH INTERFERE WITH CONSTRUCTION WILL BE RELOCATED OUTSIDE LIMITS OF CONSTRUCTION BY THE CONTRACTOR. UPON COMPLETION OF CONSTRUCTION, THE CONTRACTOR WILL RESTORE THE SIGNS TO ORIGINAL LOCATION. COST TO BE INCLUDED IN ITEM NO. 712-06.

IF THE CONTRACTOR ELECTS TO UTILIZE SIGN POST ANCHORS (STUBS) FOR SIGN ERECTION. THESE SHALL BE REMOVED WHEN THE SIGNS ARE REMOVED TO AVOID FUTURE DAMAGE TO MOWERS OR OTHER MACHINERY.

UNDER THE DIRECTION OF THE ENGINEER, THE CONTRACTOR MAY BE REQUIRED TO APPLY PLASTIC MARKINGS IN THE PAVEMENT AREAS NOT SPECIFICALLY DETAILED IN THE PLANS. PAYMENT WILL BE MADE AT THE UNIT PRICE BID FOR ITEM NO. 716-13.01.

ITEM 303-02 TO BE PLACED BEFORE PLACING SURFACING MATERIAL.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ANY SIGNS AND MAILBOXES DURING THE OPERATION, ANY SIGNS OR MAILBOXES DAMAGED AS A RESULT OF THE OPERATIONS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.

MATERIAL TRANSFER DEVICE SHALL BE USED FOR "D" MIX.

DRIVEWAYS AND FIELD ENTRANCES WILL BE PAVED THE SHOULDER WIDTH PLUS UP TO THREE FEET LIP OFF.

ONLY 2 ARROW BOARDS WILL BE PAID FOR DURING THE LIFE OF THIS PROJECT. IF THE CONTRACTOR'S WORK PHASES REQUIRED ADDITIONAL ARROW BOARDS, THEIR COST WILL BE INCLUDED IN THE COST OF OTHER ITEMS.

THE CONSTRUCTION OFFICE SHOULD CONTACT THE CITY ENGINEER FOR ADVICE ON LOCATING THE ADVANCE DETECTION LOOPS

THE CONTRACTOR SHALL NOT DISTURB EXISTING LOOP DETECTORS WHILE BREAKING OUT AND REMOVING EXISTING PAVEMENTS.

THE CONTRACTOR SHALL COMPLY WITH SECTION 712 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION REGARDING TEMPORARY TRAFFIC CONTROL AND THE CURRENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

RESURFACING

TRAFFIC WILL BE ALLOWED TO TEMPORARILY DRIVE ON THE MILLED SURFACE OF THE ROADWAY UNDER THE FOLLOWING CONDITION ONLY:

- A: THE MILLED SURFACE IS FINE TEXTURED. THE FINE TEXTURE SHALL BE OBTAINED BY A MILLING MACHINE UTILIZING A MILLING HEAD WITH TEETH SPACING 3/8" OR LESS OPERATING AT LESS THAN 80 FEET PER MINUTE.
- B: THE SURFACE SHALL BE SWEEPED AND CLEANED ON ALL LOOSE MATERIALS.
- C: THE DIFFERENCE IN ELEVATION BETWEEN THE MILLED SURFACE AND THE ADJACENT LANE SHALL NOT EXCEED 1 1/2 INCHES.
- D: THE MILLED SURFACE SHALL BE PAVED WITHIN 48 HOURS.
- E: RAIN OR INCLEMENT WEATHER IS NOT EXPECTED OR FORECASTED WITHIN 48 HOURS AFTER MILLING.
- F: ALL APPLICABLE SIGNING IS INSTALLED IN ACCORDANCE WITH THE MUTCD SIGNING SHALL INCLUDE MOTORCYCLE WARNING SIGN (TN-64) PLACED IN ADVANCE OF ANY MILLED AREAS.
- G: IF RAVELING OR DETERIORATION OF THE MILLED SURFACE IS OCCURRING WHILE TRAFFIC IS DRIVING ON THE MILLED SURFACE, THEN THIS PRACTICE WILL NOT BE ALLOWED AND PAVING SHALL BE COMPLETED IMMEDIATELY AFTER MILLING.
- H: ONLY ONE LANE IN EACH DIRECTION SHALL HAVE A MILLED SURFACE AT ONE TIME.

PAVEMENT EDGE DROP-OFF TRAFFIC CONTROL

- A. DIFFERENCE IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES OR TRAFFIC LANE AND SHOULDER WHERE THE TRAFFIC LANE IS BEING USED BY TRAFFIC. CAUSED BY BASE, PAVING OR RESURFACING:
 - 1. DIFFERENCE IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 0.75 INCH AND NOT EXCEEDING 2 INCHES.
 - a. WARNING SIGNS, UNEVEN PAVEMENT (W8-11) AND/OR SHOULDER DROP-OFF (W8-9A), SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.
 - b. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY ADDED PAVEMENT SHALL BE ELIMINATED WITHIN THREE WORKDAYS.
 - c. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY COLD PLANING SHALL BE ELIMINATED WITHIN THREE WORKDAYS.
 - d. WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE TRAFFIC LANE BEING UTILIZED BY TRAFFIC AND SHOULDER, THE DIFFERENCE IN ELEVATION SHALL BE ELIMINATED WITHIN SEVEN WORKDAYS AFTER THE CONDITION IS CREATED.

SPECIAL EROSION PREVENTION AND SEDIMENT CONTROL

THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS, BEFORE CLEANING OUT OR REMOVAL/REPLACEMENT OF DITCHES, SIDE DRAINS, AND CROSS DRAINS, TO ENSURE THAT THE DRAINAGE FEATURES/ WATERCOURSES ARE NOT STREAMS OR WETLANDS. IF THE CONTRACTOR/ TDOT INSPECTOR IS UNSURE WHETHER THE DRAINAGE FEATURES ARE STREAMS OR WETLANDS, THE INSPECTOR SHALL CONTACT THE TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION TO OBTAIN THE APPROPRIATE PERMITS.

THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS BEFORE CONSTRUCTING EARTH PADS FOR THE TYPE 38 TERMINALS TO ENSURE THAT THE ENVIRONMENTAL FEATURES (STREAMS, WETLANDS, OR SPRINGS) ARE NOT IMPACTED DURING CONSTRUCTION. IF THE CONTRACTOR/TDOT INSPECTOR IS UNSURE WHETHER THE ENVIRONMENTAL FEATURES ARE STREAMS, SPRINGS OR WETLANDS, THE INSPECTOR SHALL CONTACT THE TDOT ENVIRONMENTAL DIVISION ECOLOGY OR PERMITS SECTION SO AN ACCURATE DETERMINATION CAN BE ASSESSED. AND DURING CONSTRUCTION OF THE EARTH PADS FOR THE TYPE 38 GUARDRAIL TERMINALS, EXISTING VEGETATION WILL BE REQUIRED TO BE REMOVED FOR THE PROPOSED PAD AND SLOPES. IF DURING THE VEGETATION REMOVAL, ANY TREES GREATER THAN 3 INCHES IN DIAMETER ARE REQUIRED TO BE REMOVED, CONTACT THE TDOT ENVIRONMENTAL DIVISION ECOLOGY SECTION IMMEDIATELY. DUE TO RECENT MODIFICATION OF THE USFWS STANCE REGARDING THE INDIANA BAT NO TREES >3" DBH SHOULD BE CUT WITHOUT CLEARANCE FROM THE TDOT ENVIRONMENTAL DIVISION ECOLOGY SECTION.

DUE TO THE TYPE OF WORK IT IS OUR UNDERSTANDING THAT THE PROJECT WILL NOT DISTURB MORE THAN ONE ACRE. IF THE DISTURBED AREA IS MORE THAN ONE ACRE, PLEASE CONTACT THE TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION BECAUSE A NPDES PERMIT WILL BE REQUIRED.

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**SPECIAL
NOTES**