

Index of Sheets

SHEET NO.	SHEET NAME
1	... TITLE SHEET
2	... ESTIMATED ROADWAY QUANTITIES
3	... TYPICAL SECTIONS
4-4A	... GENERAL NOTES AND SPECIAL NOTES
5	... PRE-CONSTRUCTION RIDE NUMBERS
B1	... BRIDGE TABULATION AND ESTIMATED QUANTITIES
B2	... EPOXY OVERLAY NOTE

Standard Roadway Drawings

SHEET NO.	REV.	DESCRIPTION
RD-A-1	12-18-99	STANDARD ABBREVIATIONS
RD-L-1	10-26-94	STANDARD LEGEND
D-PG-3	04-15-97	FERROUS AND ALUMINUM CORRUGATED METAL PIPE
S-GR-38	08-16-12	DETAILS FOR CONSTRUCTION OF EARTH PAD FOR TYPE 38 GUARDRAIL END TERMINALS
S-GR-38A	06-30-05	DETAILS FOR CONSTRUCTION OF ALTERNATE EARTH PAD FOR TYPE 38 GUARDRAIL END TERMINALS
T-FAB-1	05-27-97	FLASHING YELLOW ARROW BOARD
T-M-1	11-01-11	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS AND MARKING ABBREVIATIONS
T-M-3	09-19-91	MARKING STANDARDS FOR TRAFFIC ISLANDS, MEDIANS AND PAVED SHOULDERS ON CONVENTIONAL ROADS
T-M-5	04-23-13	MARKING DETAILS FOR EXPRESSWAYS & FREEWAYS
T-M-6	06-22-12	MARKING DETAIL FOR EXPRESSWAY & FREEWAY INTERCHANGES
T-M-7	01-12-12	GORE MARKING DETAILS FOR EXPRESSWAY AND FREEWAY INTERCHANGES
T-M-15		ASPHALT SHOULDER RUMBLE STRIP INSTALLATION DETAILS FOR INTERSTATE AND ACCESS CONTROLLED ROUTES

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF ENGINEERING

WEAKLEY COUNTY

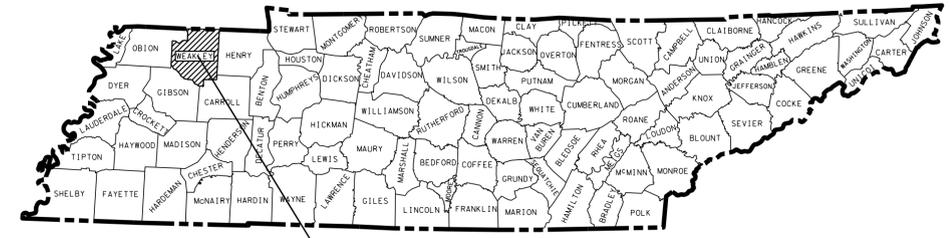
S.R. 22

FROM: L.M. 19.15 (RAMP FROM S.R. 431)
TO: L.M. 24.90 (W. OF RAMP FROM S.R. 43)

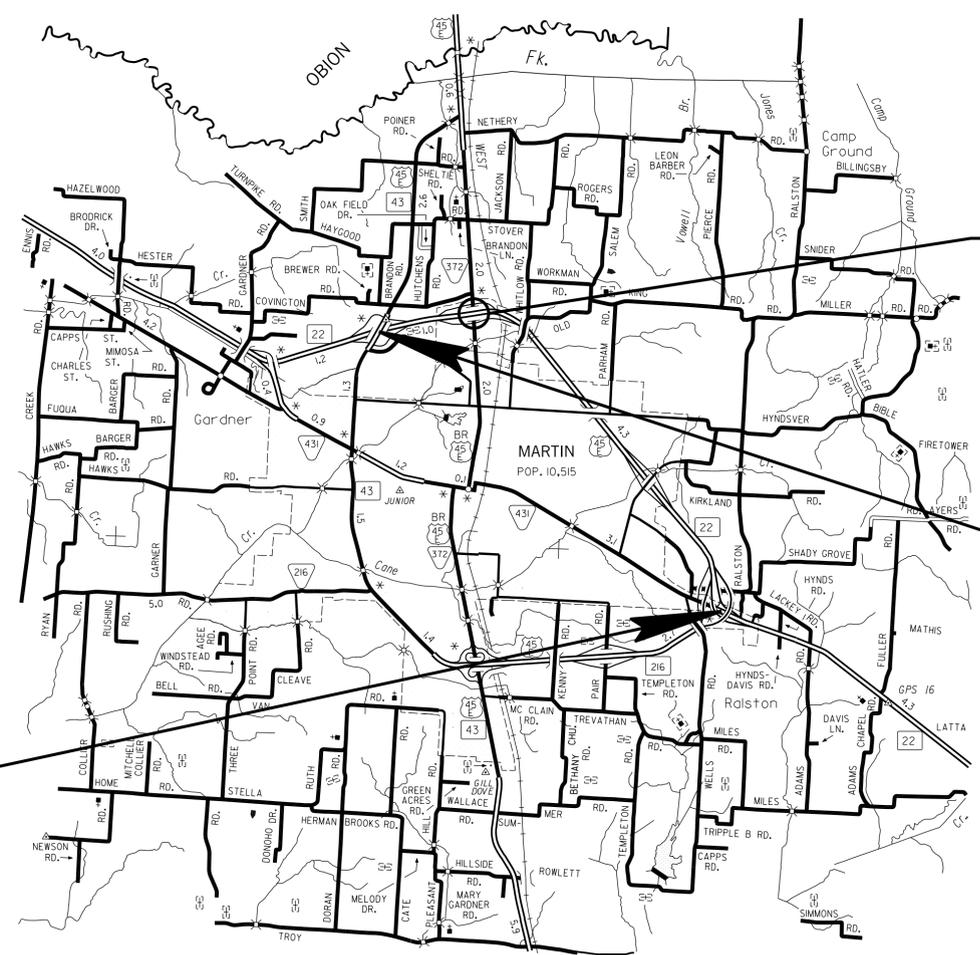
RESURFACING

STATE HIGHWAY NO. 22 F.A.H.S. NO. N.A.

TENN.	YEAR	SHEET NO.
	2014	1
FED. AID PROJ. NO.	NH/HSIP-22(75)	
STATE PROJ. NO.	92137-8221-14	



PROJECT SITE



RAILROAD NOTE:
WEST TN RAILROAD
OVERHEAD HWY BRIDGE CROSSING
#932966P @ L.M. 23.140
STRUCTURE NO.: 92SR02200431 & 92SR02200441

END PROJECT L.M. 24.90
(W. OF RAMP FROM S.R. 43)

BEGIN PROJECT L.M. 19.15
(RAMP FROM S.R. 431)

SPECIAL NOTES

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED FEBRUARY 4, 2014 AND ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS AND IN THE PROPOSAL CONTRACT.

TDOT C.E. MANAGER 1 TABITHA CAVANESS, REG. 4
DESIGNER RYAN PHILPOTT, REG. 4 CHECKED BY RAQUEL COOK/ANDREW NIPP, REG 4
P.E. NO. 98043-4283-04
PIN: 084890.01

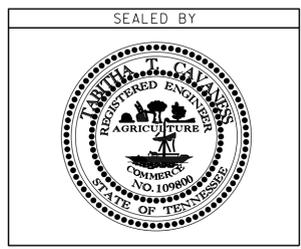
SCALE: 0 1 2 3 MILES



PROJECT LENGTH **5.75 MILES**
TOTAL LANE MILES RESURFACED **23.00 MILES**

UNOFFICIAL SET
NOT FOR BIDDING

TRAFFIC DATA	
ADT (2014)	6610



APPROVED: Paul D. Decker
CHIEF ENGINEER

DATE: _____

APPROVED: [Signature]
JOHN SCHROER, COMMISSIONER

NO EXCLUSIONS
NO EQUATIONS

U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION

APPROVED: _____
DIVISION ADMINISTRATOR DATE

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2014	NH/HSIP-22(75)	2

FOOTNOTES

ESTIMATED ROADWAY QUANTITIES						
ITEM NO.	DESCRIPTION	UNIT	92137-8221-14 QUANTITY	NH/HSIP-22(75) 92137-3221-94 QUANTITY	92137-4223-04 QUANTITY	TOTAL QUANTITY
202-03.01	REMOVAL OF ASPHALT PAVEMENT	S.Y.	500			500
203-06	WATER	M.G.	27			27
① 303-02	MINERAL AGGREGATE, TYPE B BASE, GRADING (TYPE D)	TON	3572			3572
② 307-01.01	ASPHALT CONCRETE MIX (PG64-22) (BPMB-HM) GRADING A	TON	230			230
③ 307-01.15	ASC MIX (PG64-22) (BPMLC-HM) GRADING CS	TON	6520			6520
403-01	BITUMINOUS MATERIAL FOR TACK COAT (TC)	TON	181			181
④ ⑤ 411-02.10	ACS MIX(PG70-22) GRADING D	TON	3005			3005
⑤ ⑥ ⑦ 411-03.13	ACS MIX(PG70-22) THIN LIFT D ASPHALT	TON	12047			12047
⑤ ⑥ ⑦ 411-12.01	SCORING SHOULDERS (CONTINUOUS) (16IN WIDTH)	L.M.		27.300		27.300
⑧ 415-01.02	COLD PLANING BITUMINOUS PAVEMENT	S.Y.	84765			84765
604-10.50	BRIDGE DECK REPAIRS (PARTIAL DEPTH OF SLAB)	S.Y.			50	50
617-04.02	TYPE 2 THIN EPOXY OVERLAY (LOW-MOD EPOXY)	S.Y.			4195	4195
⑨ 705-04.07	TAN ENERGY ABSORBING TERM (NCHRP 350, TL3)	EACH		33		33
712-01	TRAFFIC CONTROL	LS	1			1
⑩ 712-06	SIGNS (CONSTRUCTION)	S.F.	2240			2240
712-08.03	ARROW BOARD (TYPE C)	EACH	4			4
716-01.10	SNOWPLOWABLE REFLECTIVE MARKER	EACH		975		975
716-01.30	REMOVAL OF SNOWPLOWABLE REFLECTIVE MARKER	EACH		975		975
⑪ ⑫ 716-02.04	PLASTIC PAVEMENT MARKING(CHANNELIZATION STRIPING)	S.Y.		1869		1869
⑬ 716-05.01	PAINTED PAVEMENT MARKING (4" LINE)	L.M.	30.775			30.775
⑭ 716-05.02	PAINTED PAVEMENT MARKING (8" BARRIER LINE)	L.F.	13527			13527
⑮ 716-12.01	ENHANCED FLATLINE THERMO PVMT MRKNG (4IN LINE)	L.M.		29.875		29.875
⑯ 716-12.03	ENHANCED FLATLINE THERMO PVMT MRKNG (8IN BARRIER LINE)	L.F.		13527		13527
⑰ 716-12.05	ENHANCED FLATLINE THERMO PVMT MRKNG (6IN DOTTED LINE)	L.F.		1130		1130
717-01	MOBILIZATION	LS	1			1

- ① INCLUDES 339 TONS FOR BREAKOUT.
- ② FOR REPLACEMENT OF QUANTITY REMOVED UNDER ITEM 202-03.01 ONLY.
- ③ INCLUDES 583 TONS FOR COLD PLANING REPLACEMENT AT GORE AREAS, ON/OFF RAMPS, BRIDGE ENDS, AND RAMPS.
- ④ INCLUDES 1192 TONS FOR SPOT LEVELING, 2433 TONS FOR COLD PLANING REPLACEMENT AT GORE AREAS, ON/OFF RAMPS, BRIDGE ENDS, PAVEMENT BETWEEN BRIDGES IN CLOSE PROXIMITY, AND RAMPS.
- ⑤ SEE SPECIAL PROVISION NO. 407G FOR MATERIAL TRANSFER DEVICE.
- ⑥ INCLUDES 1077 TONS FOR GORE AREAS, ON/OFF RAMPS, BRIDGE ENDS, AND RAMPS.
- ⑦ SEE SPECIAL PROVISION NO. 411B FOR RIDEABILITY SPECIFICATIONS, AND SPECIAL PROVISION NO. 411TLD.
- ⑧ INCLUDES 15600 S.Y. FOR BRIDGE ENDS (TO BE MILLED AT A DEPTH OF ± 2"), 69165 S.Y. FOR ON/OFF RAMPS AND GORE AREAS (11752 S.Y. AT ± 2" AND 57413 S.Y. AT ± 1.25"). LATERAL ON/OFF RAMPS ARE TO BE MILLED ± 1.25" THE LENGTH OF THE RAMP ± 150' ON BOTH ENDS OF THE RAMP FOR GRADE TRANSITIONING, AND ARE TO BE REPLACED WITH PLANS QUANTITY.
- ⑨ FOR UPGRADE OF EXISTING ANCHORS AT ALL STRUCTURE LOCATIONS IN PROJECT LIMITS. THE COST OF REMOVAL OF EXISTING GUARDRAIL AND ANCHORS TO BE INCLUDED IN THIS ITEM.

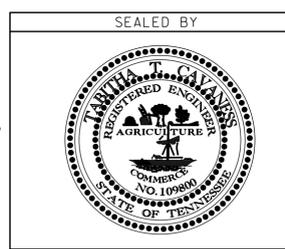
⑩ QUANTITY INCLUDES

4	ROAD WORK NEXT 6 MI.	(G20-1)	(48" X 24")
4	END ROAD WORK	(G20-2)	(48" X 24")
64	UNEVEN LANES	(W8-11)	(48" X 48")
24	ROAD WORK AHEAD	(W20-1)	(48" X 48")
4	ROAD WORK 1500 FT.	(W20-1)	(48" X 48")
4	ROAD WORK 1000 FT.	(W20-1)	(48" X 48")
4	ROAD WORK 500 FT.	(W20-1)	(48" X 48")
4	RIGHT LANE CLOSED 1,500 FT.	(W20-5)	(48" X 48")
4	RIGHT LANE CLOSED 1,000 FT.	(W20-5)	(48" X 48")
4	RIGHT LANE CLOSED 500 FT.	(W20-5)	(48" X 48")
4	LEFT LANE CLOSED 1,500 FT.	(W20-5)	(48" X 48")
4	LEFT LANE CLOSED 1,000 FT.	(W20-5)	(48" X 48")
4	LEFT LANE CLOSED 500 FT.	(W20-5)	(48" X 48")
4	FLAGGER AHEAD	(W20-7a)	(48" X 48")
4	FRESH OIL	(W21-2)	(48" X 48")
24	SHOULDER WORK	(W21-5)	(48" X 48")
18	GROOVED PAVEMENT	(TN-64)	(48" X 48")

THIS CONSTRUCTION SIGNING IS TO BE AS A MINIMUM. OTHER SIGNS AS DIRECTED BY THE ENGINEER MAY BE REQUIRED DURING DIFFERENT PHASES.

- ①① FOR FINAL PAVEMENT MARKING ONLY. SEE SPECIAL PROVISIONS 411RS & 716ST.
- ①② FOR USE AS TEMPORARY LINE MARKINGS. INCLUDES 4.9 MILES FOR BREAKOUT, SPOT LEVELING, ON/OFF RAMPS, AND COLD PLANING AT BRIDGE ENDS.
- ①③ FOR USE AS TEMPORARY LINE MARKINGS ONLY.
- ①④ INCLUDES 4 MILES FOR ON/OFF RAMPS AT EOP AND INTERSECTIONS.
- ①⑤ THE CONTRACTOR MAY ELECT TO SUBSTITUTE PREFORMED PLASTIC FOR THERMOPLASTIC. PREFORMED PLASTIC SHALL BE PAID FOR AT THE SAME UNIT PRICE AS BID FOR THERMOPLASTIC.

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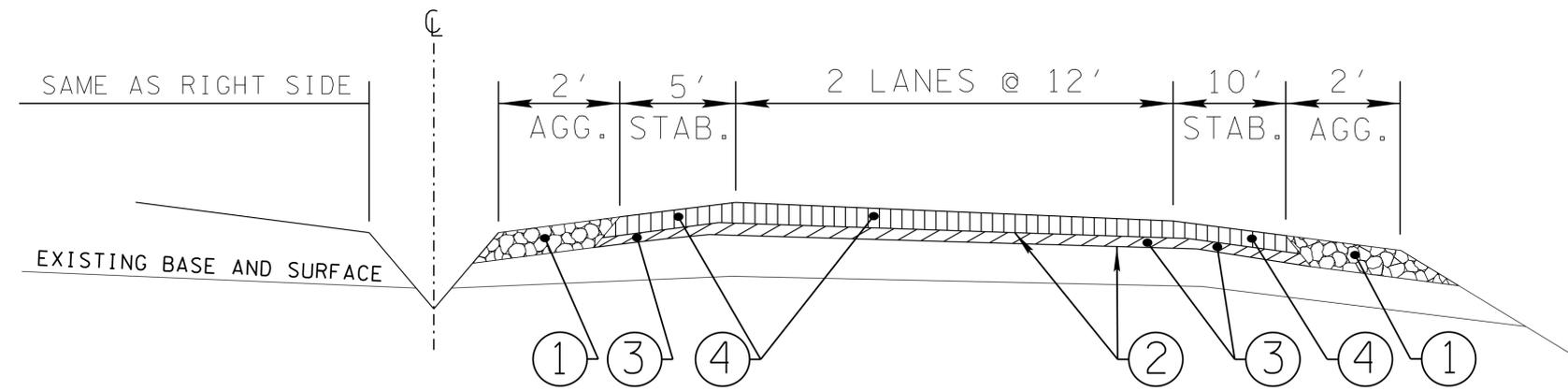
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

**ESTIMATED
ROADWAY
QUANTITIES**

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2014	NH/HSIP-22(75)	3

TENNESSEE D. O. T.
DESIGN DIVISION
FILE NO.

S. R. 22

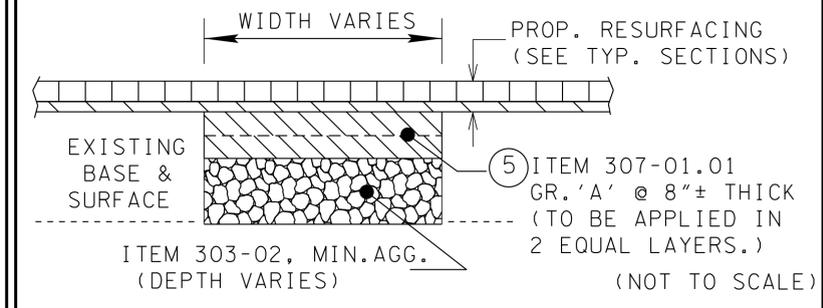


FROM: L.M. 19.15 TO L.M. 24.90

PROPOSED PAVEMENT SCHEDULE

①	MINERAL AGGREGATE BASE @ 2"± THICK FOR SHOULDERS ITEM 303-02 MINERAL AGGREGATE, TYPE "B" BASE, GRADING "D"
②	TACK COAT (TC) FOR GENERAL USE ITEM 403-01 BITUMINOUS MATERIAL FOR TACK COAT (TC) (AT 0.07 GAL./S.Y.)
③	BITUMINOUS COURSE (LEVELING) @ 0.40"± THICK (APPROX. 46.00 LBS./S.Y.) ITEM 307-01.15 ACS MIX (PG64-22) (BPMLC-HM) GRADING CS
④	ASPHALTIC CONCRETE SURFACE (ACS) @ 0.80"± THICK (APPROX. 85.00 LBS./S.Y.) ITEM 411-03.13 ACS MIX (PG70-22) THIN LIFT D ASPHALT
⑤	BITUMINOUS BASE COURSE (BLACK BASE) @ 8"± THICK (APPROX. 920.0 LBS./S.Y.) ITEM 307-01.01 ASPHALT CONCRETE MIX (PG64-22)(BPMB-HM) GRADING "A" (THIS ITEM IS TO BE USED FOR BREAKOUT ONLY)
	COLD PLANING @ 2"± THICK (APPROX. 210.00 LBS./S.Y.) COLD PLANING @ 1.25"± THICK (APPROX. 131.25 LBS./S.Y.) ITEM 415-01.02 COLD PLANING BITUMINOUS PAVEMENT (COLD PLANING AT BRIDGE ENDS, GORE AREAS, AND ON/OFF RAMPS ONLY)
	ASPHALTIC CONCRETE SURFACE (ACS) @ 1.25"± THICK (APPROX. 132.50 LBS./S.Y.) ITEM 411-02.10 ACS MIX (PG70-22) GRADING "D" (THIS ITEM IS TO BE USED FOR SPOT LEVELING AND COLD PLANING REPLACEMENT)

DETAIL OF BREAKOUT



BRIDGE NOTE

THE CONTRACTOR SHALL:

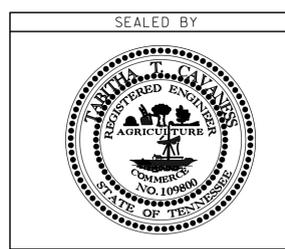
LEAVE STRUCTURE AS IS @ L.M. 23.14 (LT. & RT.)

PAVE ACROSS EXISTING STRUCTURE WITH PLANS QUANTITIES @ L.M. 22.46 (LT. & RT.)

COLD PLANE 2" DEPTH 300' OFF THE ENDS OF STRUCTURES @ L.M. 19.27, 23.14, & 23.39 (LT. & RT.)

FOR IMPROVEMENTS TO STRUCTURES @ L.M. 19.27 & 23.39 (LT. & RT.) SEE SHEETS B1 & B2.

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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS
(NOT TO SCALE)

D6-DEC-2013 10:44 \\JJ04w-f01.tdot.state.tn.us\04SHARED\Design\RESURF REG4 PROJ\WEAKLEY\SR22\LM9-15-LM24-90\92022LM9-15 TYPICAL.dgn

GENERAL NOTES

GRADING

ANY AREA THAT IS DISTURBED OUTSIDE LIMITS OF CONSTRUCTION DURING THE LIFE OF THIS PROJECT SHALL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE.

THE CONTRACTOR SHALL NOT DISPOSE OF ANY MATERIAL EITHER ON OR OFF STATE-OWNED R.O.W. IN A REGULATORY FLOOD WAY AS DEFINED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY WITHOUT APPROVAL BY SAME. ALL MATERIAL SHALL BE DISPOSED OF IN UPLAND (NON-WETLAND) AREAS AND ABOVE ORDINARY HIGH WATER OF ANY ADJACENT WATERCOURSE. THIS DOES NOT ELIMINATE THE NEED TO OBTAIN ANY OTHER LICENSES OR PERMITS THAT MAY BE REQUIRED BY ANY OTHER FEDERAL, STATE, OR LOCAL AGENCY.

GUARDRAIL

THE CONTRACTOR SHALL NOT REMOVE ANY SECTIONS OF EXISTING GUARDRAIL TO REWORK SHOULDERS OR FLATTEN SLOPE UNTIL THE ENGINEER CONCURS IN THE NECESSITY OF REMOVAL DUE TO CONSTRUCTION REQUIREMENTS AND THE APPROPRIATE WARNING DEVICES ARE INSTALLED. THE PROPOSED GUARDRAIL, INCLUDING ANY ANCHOR SYSTEM, SHALL BE INSTALLED QUICKLY TO MINIMIZE TRAFFIC EXPOSURE TO ANY HAZARD. NO PAYMENT WILL BE MADE FOR A SECTION OF PROPOSED GUARDRAIL, INCLUDING ANCHORS, UNTIL IT IS COMPLETE IN PLACE.

IF ANY APPROACH END OF A SECTION OF GUARDRAIL OR BRIDGE RAIL MUST TEMPORARILY BE LEFT INCOMPLETE AND EXPOSED TO TRAFFIC, THE CONTRACTOR SHALL USE TWO (2) TEMPORARY BARRICADES OR DRUMS WITH TYPE A LIGHTS AND ROUNDED END ELEMENTS AS MINIMUM MEASURES TO PROTECT TRAFFIC FROM THE HAZARD OF AN EXPOSED END. ALL COST OF FURNISHING AND INSTALLING A TEMPORARY ROUNDED END ELEMENT SHALL BE INCLUDED IN THE COST OF THE PROPOSED GUARDRAIL.

UTILITIES

THE LOCATIONS OF UTILITIES SHOWN WITHIN THESE PLANS ARE APPROXIMATE ONLY. EXACT LOCATIONS SHALL BE DETERMINED IN THE FIELD BY CONTACTING THE UTILITY COMPANIES INVOLVED. NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC., AT 1-800-351-1111 AS REQUIRED BY TCA 65-31-106 WILL BE REQUIRED.

THE CONTRACTOR SHALL NOTIFY EACH INDIVIDUAL UTILITY OWNER OF HIS PLAN OF OPERATION IN THE AREA OF THE UTILITIES. PRIOR TO COMMENCING WORK, THE CONTRACTOR SHALL CONTACT THE UTILITY OWNERS AND REQUEST THEM TO PROPERLY LOCATE THEIR RESPECTIVE UTILITY ON THE GROUND. THIS NOTIFICATION SHALL BE GIVEN AT LEAST THREE (3) BUSINESS DAYS PRIOR TO COMMENCEMENT OF OPERATIONS AROUND THE UTILITY IN ACCORDANCE WITH TCA 65-31-106.

PAVEMENT MARKINGS

TEMPORARY PAVEMENT LINE MARKINGS ON INTERMEDIATE LAYERS OF PAVEMENT SHALL BE REFLECTIVE TAPE OR REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAYS WORK. SHORT, UNMARKED SECTIONS SHALL NOT BE ALLOWED. THESE MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-05.01 PAINTED PAVEMENT MARKINGS (4" LINE), L.M.

WIDE (8 INCH) TEMPORARY PAVEMENT MARKING LINE WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-05.02 PAINTED PAVEMENT MARKING (8" BARRIER LINE), LIN. FT.

PERMANENT PAVEMENT LINE MARKINGS SHALL BE 4" ENHANCED FLATLINE THERMOPLASTIC INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-12.01, ENHANCED FLATLINE THERMO PVMT MRKNG (4IN LINE), L.M. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATIONS IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.

PAVING

THE CONTRACTOR SHALL BE REQUIRED TO COLD PLANE AND PAVE IN THE DIRECTION OF TRAFFIC.

RESURFACING

IN ALL CASES, THE LENGTH OF THE PAVEMENT TRANSITION, THE THICKNESS AND WIDTH OF THE RESURFACING AND ANY ADDITIONAL PAVEMENT MATERIALS SHALL BE AS DIRECTED BY THE TDOT ENGINEER.

CONSTRUCTION WORK ZONE & TRAFFIC CONTROL NOTES

ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE WEEK BEFORE NEEDED, IF THE SIGN FACE IS FULLY COVERED.

IF THE CONTRACTOR MOVES OFF THE PROJECT, HE SHALL COVER OR REMOVE ALL UNNEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OF REMOVAL, COVERING, AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION) PER SQUARE FOOT.

A LONG TERM BUT SPORADIC USE WARNING SIGN, SUCH AS A FLAGGER SIGN, MAY REMAIN IN PLACE WHEN NOT REQUIRED PROVIDED THE SIGN FACE IS FULLY COVERED.

TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.

THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY, WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHEN THE LANE IS OPEN TO TRAFFIC UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO BE PARKED WITHIN THIRTY (30) FEET OF AN OPEN TRAFFIC LANE AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE.. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.

MISCELLANEOUS

NOTHING IN THE GENERAL NOTES OR SPECIAL PROVISIONS SHALL RELIEVE THE CONTRACTOR FROM HIS RESPONSIBILITIES TOWARD THE SAFETY AND CONVENIENCE OF THE GENERAL PUBLIC AND THE RESIDENTS ALONG THE PROSPED CONSTRUCTION AREA.

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2014	NH/HSIP-22(75)	4

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SEALED BY



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

**GENERAL
NOTES**

SPECIAL NOTES

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2014	NH/HS1P-22(75)	4A

SURFACE IS TO BE CROWNED AS DIRECTED BY THE ENGINEER.

ALL SIGNS WHICH INTERFERE WITH CONSTRUCTION WILL BE RELOCATED OUTSIDE LIMITS OF CONSTRUCTION BY THE CONTRACTOR. UPON COMPLETION OF CONSTRUCTION, THE CONTRACTOR WILL RESTORE THE SIGNS TO ORIGINAL LOCATION. COST TO BE INCLUDED IN ITEM NO. 712-06.

IF THE CONTRACTOR ELECTS TO UTILIZE SIGN POST ANCHORS (STUBS) FOR SIGN ERECTION, THESE SHALL BE REMOVED WHEN THE SIGNS ARE REMOVED TO AVOID FUTURE DAMAGE TO MOWERS OR OTHER MACHINERY.

UNDER THE DIRECTION OF THE ENGINEER, THE CONTRACTOR MAY BE REQUIRED TO APPLY PAINTED MARKINGS IN THE PAVEMENT AREAS NOT SPECIFICALLY DETAILED IN THE PLANS. PAYMENT WILL BE MADE AT THE UNIT PRICE BID FOR ITEM NO. 716-05.01.

UNDER THE DIRECTION OF THE ENGINEER, THE CONTRACTOR MAY BE REQUIRED TO APPLY PLASTIC MARKINGS IN THE PAVEMENT AREAS NOT SPECIFICALLY DETAILED IN THE PLANS. PAYMENT WILL BE MADE AT THE UNIT PRICE BID FOR ITEM NO. 716-12.01.

ITEM 303-02 TO BE PLACED BEFORE PLACING SURFACING MATERIAL.

THE CONTRACTOR WILL BE REQUIRED TO CLEAN AND BLADE GRASS FROM SHOULDERS BEFORE PLACING ITEM 303-02. MATERIAL BLADED FROM SHOULDERS TO BE PICKED UP AND DISPOSED OF AS DIRECTED BY THE ENGINEER. COST TO BE INCLUDED IN ITEM 303-02.

MATERIAL TRANSFER DEVICE SHALL BE USED FOR D-MIX.

ONLY 4 ARROW BOARDS WILL BE PAID FOR DURING THE LIFE OF THIS PROJECT. IF THE CONTRACTOR'S WORK PHASES REQUIRED ADDITIONAL ARROW BOARDS, THEIR COST WILL BE INCLUDED IN THE COST OF OTHER ITEMS.

THE CONTRACTOR SHALL KEEP TWO TRAFFIC LANES, ONE IN EACH DIRECTION, OPEN TO TRAFFIC DURING NON-WORK HOURS / OR NON-WORK DAYS.

THE INSIDE SHOULDERS WILL BE PAVED CONCURRENTLY WITH THE INSIDE TRAFFIC LANE.

ALL EXISTING SURPLUS MATERIAL AND JOINT MATERIAL ABOVE THE ADJACENT SURFACE ELEVATION WILL BE REMOVED TO THE NORMAL SURFACE LEVEL. COST TO BE INCLUDED IN OTHER ITEMS.

THE BITUMINOUS MATERIAL DESIGNATED TO RESTORE THE COLD PLANING AREA WILL BE PLACED WITHIN 48 HOURS OF THE COMMENCEMENT OF COLD PLANING OPERATIONS. COLD PLANING OPERATIONS WILL BE LIMITED TO AN AREA EQUAL TO THAT WHICH CAN BE COVERED WITH BITUMINOUS MATERIAL WITHIN THE TIME LIMITS SPECIFIED. EVEN IF COLD PLANING OPERATIONS MUST BE SUSPENDED UNTIL PAVING CATCHES UP.

THE RAISED REFLECTOR SHALL BE REMOVED BEFORE COLD PLANING.

FEATHER "D"-MIX TO ENDS OF BRIDGES THAT ARE NOT TO BE PAVED.

SPECIAL EROSION PREVENTION AND SEDIMENT CONTROL

THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS, BEFORE CLEANING OUT OR REMOVAL/REPLACEMENT OF DITCHES, SIDE DRAINS, AND CROSS DRAINS, TO ENSURE THAT THE DRAINAGE FEATURES/ WATERCOURSES ARE NOT STREAMS OR WETLANDS. IF THE CONTRACTOR/ TDOT INSPECTOR IS UNSURE WHETHER THE DRAINAGE FEATURES ARE STREAMS OR WETLANDS, THE INSPECTOR SHALL CONTACT THE TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION TO OBTAIN THE APPROPRIATE PERMITS.

THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS BEFORE CONSTRUCTING EARTH PADS FOR THE TYPE 38 TERMINALS TO ENSURE THAT THE ENVIRONMENTAL FEATURES (STREAMS, WETLANDS, OR SPRINGS) ARE NOT IMPACTED DURING CONSTRUCTION. IF THE CONTRACTOR/TDOT INSPECTOR IS UNSURE WHETHER THE ENVIRONMENTAL FEATURES ARE STREAMS, SPRINGS OR WETLANDS, THE INSPECTOR SHALL CONTACT THE TDOT ENVIRONMENTAL DIVISION ECOLOGY OR PERMITS SECTION SO AN ACCURATE DETERMINATION CAN BE ASSESSED. AND DURING CONSTRUCTION OF THE EARTH PADS FOR THE TYPE 38 GUARDRAIL TERMINALS, EXISTING VEGETATION WILL BE REQUIRED TO BE REMOVED FOR THE PROPOSED PAD AND SLOPES. IF DURING THE VEGETATION REMOVAL, ANY TREES GREATER THAN 3 INCHES IN DIAMETER ARE REQUIRED TO BE REMOVED, CONTACT THE TDOT ENVIRONMENTAL DIVISION ECOLOGY SECTION IMMEDIATELY. DUE TO RECENT MODIFICATION OF THE USFWS STANCE REGARDING THE INDIANA BAT NO TREES >3" DBH SHOULD BE CUT WITHOUT CLEARANCE FROM THE TDOT ENVIRONMENTAL DIVISION ECOLOGY SECTION.

DUE TO THE TYPE OF WORK IT IS OUR UNDERSTANDING THAT THE PROJECT WILL NOT DISTURB MORE THAN ONE ACRE. IF THE DISTURBED AREA IS MORE THAN ONE ACRE, PLEASE CONTACT THE TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION BECAUSE A NPDES PERMIT WILL BE REQUIRED.

PAVEMENT EDGE DROP-OFF TRAFFIC CONTROL

- A. DIFFERENCE IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES OR TRAFFIC LANE AND SHOULDER WHERE THE TRAFFIC LANE IS BEING USED BY TRAFFIC. CAUSED BY BASE, PAVING OR RESURFACING:
 - 1. DIFFERENCE IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 0.75 INCH AND NOT EXCEEDING 2 INCHES.
 - a. WARNING SIGNS, UNEVEN PAVEMENT (W8-11) AND/OR SHOULDER DROP-OFF (W8-9A), SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.
 - b. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY ADDED PAVEMENT SHALL BE ELIMINATED WITHIN THREE WORKDAYS.
 - c. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY COLD PLANING SHALL BE ELIMINATED WITHIN THREE WORKDAYS.
 - d. WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE TRAFFIC LANE BEING UTILIZED BY TRAFFIC AND SHOULDER, THE DIFFERENCE IN ELEVATION SHALL BE ELIMINATED WITHIN SEVEN WORKDAYS AFTER THE CONDITION IS CREATED.

RAILROAD

THE CONTRACTOR IS TO CONDUCT HIS WORK SO AS TO PROTECT THE WEST TENNESSEE RAILROAD COMPANY TRACT FACILITIES AND PROPERTIES FROM ANY DAMAGE. THE WORK SHALL BE DONE IN ACCORDANCE WITH REGULATIONS STIPULATED BY WEST TENNESSEE RAILROAD COMPANY SO AS TO MAINTAIN CLEARANCE AND NOT TO INTERRUPT TRAIN TRAFFIC IN ANY MANNER. THE CONTRACTOR SHALL NOT STORE ANY OF HIS CONSTRUCTION EQUIPMENT ON THE RAILROAD'S RIGHT-OF-WAY.

THE CONTRACTOR SHALL NOTIFY AND COORDINATE HIS WORK AT THE RAILROAD CROSSING WITH THE FOLLOWING REPRESENTATIVES OF THE RAILROAD.

MR. BRUCE HOHORST, PRESIDENT
11 EAST CHURCH STREET
SEA BRIGHT, NJ 07760
PHONE: (732) 842-0912

AND

MR. BARRY CRABTREE - GENERAL MANAGER
WEST TENNESSEE RAILROAD, LLC
PHONE: (731) 432-4304
CELL: (731) 694-0272
E-MAIL: WTNRRR@AOL.COM

THE CONTRACTOR SHALL NOT PERFORM THE WORK OVER OR WITHIN 25 FT. OF THE RAILROAD TRACKS WITHOUT THE PRESENCE AND PERMISSION OF AN AUTHORIZED FLAGGER OR THE WEST TENNESSEE RAILROAD (SEE RAILROAD SPECIAL NOTES FOR THE RAILROAD CONTACT ADDRESSEES TO COORDINATE THIS WORK WITH THE RAILROAD AND REQUEST A RAILROAD FLAGGER BE PROVIDED FOR THE WORK).

SEE STATE CONTRACT SPECIAL PROVISION 105C(SL)(R) FOR RULES ON THE RAILROAD'S FLAGGING AND/OR WATCHMAN SERVICES. AND FOR WHAT INSURANCE POLICIES ARE REQUIRED BY THE WEST TENNESSEE RAILROAD.

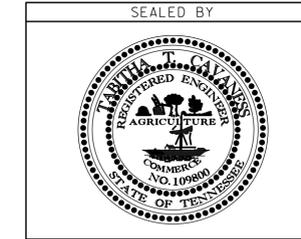
THE CONTRACTOR SHOULD TAKE EXTREME CARE WHEN COLD PLANING THE EXISTING ASPHALT OFF BRIDGE ENDS, SO AS NOT TO DAMAGE THE EXISTING DECK SEALANT AND/OR EXPANSION JOINT MEMBERS (STEEL PLATES, BARS, AND/OR HEADERS). IF DAMAGE DOES OCCUR, THE CONTRACTOR SHALL BE RESPONSIBLE FOR NECESSARY REPAIRS ON ALL DAMAGED MEMBERS TO THE SATISFACTION OF THE PROJECT ENGINEER AT NO ADDITIONAL COST.

RESURFACING

TRAFFIC WILL BE ALLOWED TO TEMPORARILY DRIVE ON THE MILLED SURFACE OF THE ROADWAY UNDER THE FOLLOWING CONDITION ONLY:

- A: THE MILLED SURFACE IS FINE TEXTURED. THE FINE TEXTURE SHALL BE OBTAINED BY A MILLING MACHINE UTILIZING A MILLING HEAD WITH TEETH SPACING ¾" OR LESS OPERATING AT LESS THAN 80 FEET PER MINUTE.
- B: THE SURFACE SHALL BE SWEEPED AND CLEANED ON ALL LOOSE MATERIALS.
- C: THE DIFFERENCE IN ELEVATION BETWEEN THE MILLED SURFACE AND THE ADJACENT LANE SHALL NOT EXCEED 1 ½ INCHES.
- D: THE MILLED SURFACE SHALL BE PAVED WITHIN 48 HOURS.
- E: RAIN OR INCLEMENT WEATHER IS NOT EXPECTED OR FORECASTED WITHIN 48 HOURS AFTER MILLING.
- F: ALL APPLICABLE SIGNING IS INSTALLED IN ACCORDANCE WITH THE MUTCD SIGNING SHALL INCLUDE MOTORCYCLE WARNING SIGN (TN-64) PLACED IN ADVANCE OF ANY MILLED AREAS.
- G: IF RAVELING OR DETERIORATION OF THE MILLED SURFACE IS OCCURRING WHILE TRAFFIC IS DRIVING ON THE MILLED SURFACE, THEN THIS PRACTICE WILL NOT BE ALLOWED AND PAVING SHALL BE COMPLETED IMMEDIATELY AFTER MILLING.
- H: ONLY ONE LANE IN EACH DIRECTION SHALL HAVE A MILLED SURFACE AT ONE TIME.

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DEPARTMENT OF TRANSPORTATION

**SPECIAL
NOTES**

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2014	NH/HSIP-22(75)	5

State of Tennessee
Department of Transportation

Report Number **PC** Division of Materials and Tests Date **December 4, 2013**
Page Number **1** Asphalt Roadway Surface Roughness Evaluation **F54**

Pre-Construction Ride Numbers

Project Ref. No.	NH/HSIP-22(75)		Contract No.	
Project No.	92137-3221-94,92137-8221-14		Region	4
Contractor	To Be Awarded		County	WEAKLEY
Weather / Temp.	CLOUDY / 66		Route	SR22
Standard Specs.	55.0	HC-IRI Number	Test Date	December 4, 2013
Equip. Calib. Date	December 4, 2013		Test No.	T-1
		Pre-Con Test Date	December 4, 2013	

Test Performed by TDOT Road Profiler									Average Roughness	
Test Limits		Direction and Measured Roughness (HC-IRI Number Per Mile)							Pre-Constuction	
Mile		NSEW	*Lane	Pre Ride Number	Payment Table	Max. # for Final Ride	Min. Required % Improvement	Percent Pay	HC-IRI Number Per Mile	
From	To									
19.15	20.15	N	1	82	Table 2	57.4	30	100	73.1	
20.15	21.15	N	1	66	Table 1	55.0		100		
21.15	22.15	N	1	63	Table 1	55.0		100		
22.15	23.15	N	1	76	Table 1	55.0		100		
23.15	24.15	N	1	67	Table 1	55.0		100		
24.15	24.90	N	1	84	Table 2	58.8	30	100		
19.15	20.15	N	2	75	Table 1	55.0		100	REMARKS	
20.15	21.15	N	2	65	Table 1	55.0		100		
21.15	22.15	N	2	52	Table 1	55.0		100		
22.15	23.15	N	2	56	Table 1	55.0		100		
23.15	24.15	N	2	65	Table 1	55.0		100		
24.15	24.90	N	2	87	Table 2	60.9	30	100		
24.90	23.90	S	1	90	Table 2	63.0	30	100		
23.90	22.90	S	1	70	Table 1	55.0		100		
22.90	21.90	S	1	76	Table 1	55.0		100		
21.90	20.90	S	1	71	Table 1	55.0		100		
20.90	19.90	S	1	64	Table 1	55.0		100		
19.90	19.15	S	1	67	Table 1	55.0		100		
24.90	23.90	S	2	96	Table 2	67.2	30	100		
23.90	22.90	S	2	86	Table 2	60.2	30	100		
22.90	21.90	S	2	74	Table 1	55.0		100		
21.90	20.90	S	2	80	Table 2	56.0	30	100		
20.90	19.90	S	2	74	Table 1	55.0		100		
19.90	19.15	S	2	69	Table 1	55.0		100		

*Lanes to be Left to Right each Direction - 1,2,3,4, etc. with 1 being the Inside Lane

Materials and Tests Engineer:	Gary Head	Inspector:	STEVE FESMIRE
Copies To :		Title:	TRANS. PROJ. SPEC.
Regional Materials Engineer:	MITCH BLANKENSHIP	Asphalt Roadway:	DT-1706
Project Engineer:			

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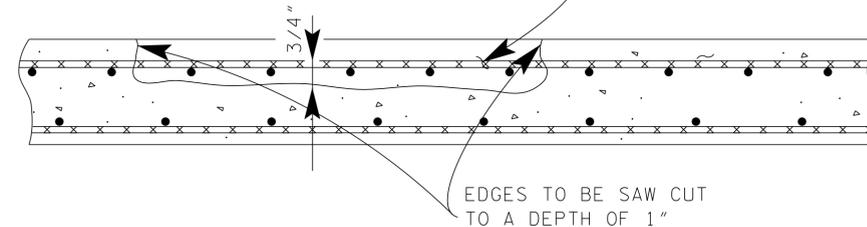
**PRE -
CONSTRUCTION
RIDE NUMBERS**

TYPE	YEAR	PROJECT NO.	SHEET NO.
FED. AID PROJ. NO.	2014	NH/HSIP-22(75)	B1
STATE PROJ. NO.	2014	92137-8221-14	
BRIDGE REPAIR	2014	92137-4223-04	

TABULATION OF BRIDGE RELATED WORK AND ESTIMATED QUANTITIES

LOCATION OF BRIDGE AND BRIDGE NUMBER	REFERENCE DRAWINGS TO BE PRINTED WITH CONTRACT DRAWINGS	TYPE OF WORK	604-10.50 BRIDGE DECK REPAIR (PARTIAL DEPTH OF SLAB) SY	617-04.02 TYPE 2 THIN OVERLAY (LOW-MOD EPOXY) SY
92-SR22-19.27 RT./ TRAVIS LANE (A104)		BRIDGE DECK REPAIR EPOXY THIN OVERLAY	10	773
92-SR22-19.27 LT./ TRAVIS LANE (A104)		BRIDGE DECK REPAIR EPOXY THIN OVERLAY	10	1238
92-SR22-19.27 (RAMP)/ TRAVIS LANE (A104)		BRIDGE DECK REPAIR EPOXY THIN OVERLAY	10	532
92-SR22-23.39 RT./ SR372 (HWY 45EBR)		BRIDGE DECK REPAIR EPOXY THIN OVERLAY	10	826
92-SR22-23.39 LT./ SR372 (HWY 45EBR)		BRIDGE DECK REPAIR EPOXY THIN OVERLAY	10	826
TOTAL			50	4195

CONCRETE FOR DECK REPAIR SHALL BE HIGH EARLY CONCRETE WITH A COMPRESSIVE STRENGTH OF 3,000 p.s.i. AT 18 HOURS



SKETCH SHOWING DECK REPAIR

NOTE: REMOVE CONCRETE IN ALL DELAMINATED AREAS TO A DEPTH OF 3/4" BELOW THE TOP BAR OF THE TOP MAT OF REINFORCING STEEL. ALL REINFORCING STEEL IN AREAS OF DECK REPAIR SHALL BE COMPLETELY CLEANED. CLEANING SHALL BE DONE PRIOR TO PLACING NEW CONCRETE OR INSTALLING PATCHING MATERIAL. AREAS OF CONCRETE REMOVAL SHALL BE DESIGNATED BY PERSONNEL FROM THE HEADQUARTERS, BRIDGE INSPECTION AND REPAIR OFFICE. INSPECTIONS TO DETERMINE AREAS OF DECK REPAIR SHALL BE SCHEDULED WITH THE BRIDGE REPAIR OFFICE AT LEAST THREE (3) DAYS IN ADVANCE.

NOTE: ITEM NO. 604-10.50, BRIDGE DECK REPAIR (PARTIAL DEPTH OF SLAB) SHALL BE BID WITH THE CONTINGENCY THAT THIS MAY BE INCREASED, DECREASED OR ELIMINATED AS DIRECTED BY THE ENGINEER.

NOTE: TRAFFIC CONTROL SHALL BE PROVIDED ON ROADWAY UNDERPASS DURING PARTIAL DEPTH DECK REPAIR OF OVERHEAD BRIDGE.

NOTE: ALL CONCRETE REPAIRS SHALL RECEIVE A WET CURE FOR THE DURATION OF THE CURE

NOTE: THE COST OF CONCRETE, CONCRETE REMOVAL, LABOR, CLEANING REBARS AND ANY MISCELLANEOUS MATERIAL REQUIRED SHALL BE PAID FOR UNDER ITEM NO. 604-10.50 BRIDGE DECK REPAIR (PARTIAL DEPTH OF SLAB), S.Y.

POWER DRIVEN HAND TOOLS:

POWER DRIVEN HAND TOOLS USED FOR THE REMOVAL OF UNSOUND CONCRETE IN MAKING PARTIAL AND FULL DEPTH REPAIRS ARE SUBJECT TO THE FOLLOWING RESTRICTIONS: (1) PARTIAL DEPTH REPAIRS; PNEUMATIC HAMMERS HEAVIER THAN NOMINAL 60 lb. CLASS SHALL NOT BE USED. ALSO TRAFFIC CONTROL SHALL BE SET UP DURING PARTIAL DEPTH REPAIRS OVER TRAFFIC. (2) FULL DEPTH REPAIRS; PNEUMATIC HAMMERS HEAVIER THAN NOMINAL 90 lb. CLASS SHALL NOT BE USED. ALSO ALL DECK REPAIR OVER BEAMS WILL BE RESTRICTED TO 60 lb. PNEUMATIC HAMMERS. (3) CHIPPING HAMMERS OF THE 15 lb. CLASS SHALL BE USED TO REMOVE CONCRETE FROM BENEATH ANY REINFORCING STEEL.

BRIDGE DECK REPAIR MATERIAL

IF OVERNIGHT LANE CLOSURES ARE PROVIDED FOR THIS PROJECT AND THE LANE CLOSURES ARE SUCH THAT TRAFFIC WILL BE KEPT OFF THE BRIDGE DECK WHERE DECK REPAIRS WILL BE MADE. THEN HIGH EARLY STRENGTH CONCRETE SHALL BE USED WITH A COMPRESSIVE STRENGTH OF 3,000 p.s.i. AT 18 HOURS. THE CONTRACTOR SHALL PROVIDE PROOF PRIOR TO BEGINNING WORK THAT PROPOSED CONCRETE MIX WILL OBTAIN REQUIRED PROPERTIES. PROOF SHALL BE PROVIDED BY AN INDEPENDENT TESTING COMPANY AND SUBMITTED TO THE MATERIAL AND TEST DIVISION OF TENNESSEE DEPARTMENT OF TRANSPORTATION FOR APPROVAL. THE MIX TO MEET THE REQUIREMENTS OF THE STANDARD SPECIFICATIONS CLASS "A". EXCEPT THE CEMENT SHALL BE A MINIMUM OF 714 lbs. A HIGH RANGE, WATER REDUCING ADMIXTURE (SUPERPLASTICIZER) SHALL BE USED SUBJECT TO THE APPROVAL OF AND THE CONCRETE SHALL ATTAIN A MINIMUM OF COMPRESSIVE STRENGTH OF 3,000 p.s.i. IN 18 HOURS. TRAFFIC SHALL NOT BE ALLOWED ON REPAIRED AREAS UNTIL THE CONCRETE HAS BEEN IN PLACE A MINIMUM OF 18 HOURS

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BRIDGE
TABULATION
AND ESTIMATED
QUANTITIES

THIN EPOXY OVERLAY NOTES

TYPE 2 THIN EPOXY OVERLAY SYSTEM - USE 2 LIFTS OF LOW MODULUS EPOXY AND AGGREGATE. TYPE 2 HAS THE OPTION OF USING THE APPLICATION EQUIPMENT DESCRIBE BELOW OR HAND MIXING.
THIN OVERLAY SYSTEM SHALL BE FROM THE QUALIFIED PRODUCTS LIST 23, SECTION D3. MINIMUM OVERLAY THICKNESS SHALL BE $\frac{3}{8}$ INCH.

APPLICATION EQUIPMENT SHOULD :

- A) BE CAPABLE OF METERING, MIXING AND DISTRIBUTING THE POLYMER AND PRETREATMENT TO MANUFACTURER'S RECOMMENDATION.
- B) USE AN APPLICATION MACHINE THAT FEATURES POSITIVE DISPLACEMENT VOLUMETRIC METERING PUMPS CONTROLLED BY A HYDRAULIC POWER UNIT.
- C) STORE COMPONENTS IN TEMPERATURE CONTROLLED RESERVOIRS CAPABLE OF MAINTAINING 100 DEGREES FAHRENHEIT (PLUS OR MINUS 10 DEGREES) TO ENSURE OPTIMAL MIXING.
- D) CHECK MIXING RATIO AT THE PUMP OUTLETS AS WELL AS CYCLE COUNTING CAPABILITIES TO MONITOR OUTPUT ON STANDARD FEATURES.
- E) USE MOTIONLESS IN-LINE MIXING SO AS TO NOT OVERLY SHEAR THE MATERIAL TO ENTRAP AIR IN THE MIX.
- F) MAXIMIZE MATERIAL WORKING TIME BY MIXING IT IMMEDIATELY BEFORE DISPENSING.

AGGREGATE SHALL BE ANGULAR, HAVING LESS THAN 0.2% MOISTURE AND FREE OF DIRT, CLAY, ASPHALT AND OTHER FOREIGN OR ORGANIC MATERIALS. AGGREGATE FOR ALL LAYERS SHALL BE BAUXITE OR FLINT ROCK PRODUCTS FLINT AND MEETS THE FOLLOWING GRADATION:

SIEVE SIZE	% PASSING
NO.6	95-100
NO.10	10-35
NO.20	0-3

FULL AND PARTIAL DEPTH DECK REPAIR SHALL CURE A MINIMUM OF 28 DAYS BEFORE THE OVERLAY IS PLACED. TRAFFIC SHALL BE ALLOWED TO USE THE BRIDGE DURING THE CURING PERIOD OF THE PATCHES BUT NOT AFTER SHOT BLASTING. MAGNESIUM PHOSPHATE BASED MATERIALS WILL NOT BE ALLOWED.

THE CONCRETE DECK SURFACE SHALL BE CLEANED BY SHOT BLASTING TO REMOVE ANY OIL, DIRT, RUBBER, TRAFFIC STRIPING, OR ANY OTHER POTENTIAL DETRIMENTAL MATERIAL SUCH AS CURING COMPOUND AND LAITANCES, WHICH THE MANUFACTURER AND ENGINEER'S OPINION WOULD PREVENT PROPER BONDING AND CURING OF THE MATERIAL. IN AREAS WHERE SHOTBLASTING EQUIPMENT CAN NOT REACH (I.E., ALONG CURBS AND BRIDGE RAILS) SANDBLASTING IS PERMITTED TO AN EXTENT TO THE ENGINEER'S AND MANUFACTURER'S APPROVAL. IMMEDIATELY BEFORE APPLICATION, ALL PREPARED SURFACES SHALL BE CLEANED WITH COMPRESSED AIR OR VACUUMED TO REMOVE DUST AND DEBRIS.

ALL SURFACES THAT ARE TREATED SHALL BE DRY AT THE TIME OF APPLICATION. THE OVERLAY SHALL NOT BE APPLIED WHEN IT HAS RAINED 24 HOURS PRIOR TO, OR RAIN IS FORECAST WITHIN 8 HOURS AFTER, APPLICATION. THE MOISTURE CONTENT IN THE DECK SUBSTRATE SHALL BE TESTED. MOISTURE IS NOT TO EXCEED 4.5 PERCENT WHEN MEASURED BY ELECTRONIC METER. IF THE TEST SHOWS EXCESS MOISTURE, THE DECK SHALL CONTINUE TO DRY BEFORE APPLICATION PROCEEDS.

BLUSHING (A WAXY SURFACE COATING ON THE EPOXY) IS CAUSED BY THE REACTION OF MOISTURE WITH THE HARDENING AGENT. BLUSHING CREATES A SURFACE THAT MAKES FUTURE LAYERS DIFFICULT TO ADHERE. LIFTS THAT SHOW SIGNS OF BLUSHING SHALL BE REMOVED AND REPLACED PRIOR TO APPLICATION OF THE NEXT. THE COST TO REMOVE AND REPLACE THESE AREAS SHALL BE AT THE CONTRACTOR'S EXPENSE.

TRAFFIC, OTHER THAN APPLICATION EQUIPMENT, SHALL NOT BE ALLOWED ON ANY PORTION OF THE DECK THAT HAS BEEN SHOT BLASTED OR WHERE PART OF THE APPLICATION HAS BEEN PLACED.

SEE MANUFACTURER'S RECOMMENDATIONS FOR REQUIRED AMBIENT AND SURFACE TEMPERATURES AND HUMIDITY LIMITS FOR APPLICATION.

THE MANUFACTURER SHALL HAVE A REPRESENTATIVE ON THE JOB SITE AT ALL TIMES DURING APPLICATION AND CURE TIME. THE REPRESENTATIVE, ALONG WITH CONSULTATION WITH ENGINEER, MAY SUSPEND ANY ITEM OF WORK THAT IS SUSPECT AND DOES NOT MEET THE REQUIREMENTS OF THE SPECIFICATIONS. WORK SHALL NOT RESUME UNTIL THE ENGINEER AND REPRESENTATIVE ARE SATISFIED THAT APPROPRIATE REMEDIAL ACTION HAS BEEN TAKEN BY THE CONTRACTOR.

ALL COSTS FOR AGGREGATE, EPOXY FOR MINIMUM OF TWO LIFTS, SURFACE PREPARATION, LABOR AND ANY OTHER MISCELLANEOUS MATERIALS REQUIRED TO PLACE THIN OVERLAY SHALL BE INCLUDED IN ITEM NO. 617-04.02, TYPE 2 THIN EPOXY OVERLAY (LOW-MOD EPOXY), SY, AS CALLED FOR ON THE QUANTITY SHEET.

THICKNESS VERIFICATION: THE PROJECT ENGINEER SHALL BE NOTIFIED OF THE NUMBER OF GALLONS USED ON THE PROJECT WITH NOTARIZED QUANTITY STATEMENTS FROM THE CONTRACTOR AND THE MANUFACTURER. THE CONTRACTOR SHALL VERIFY TO TDOT THAT THE OVERLAY IS AN AVERAGE OF AT LEAST $\frac{3}{8}$ INCH THICK AT THREE RANDOM LOCATIONS AGREED UPON BY THE PROJECT ENGINEER AND THE MATERIAL MANUFACTURER REPRESENTATIVE. IF $\frac{3}{8}$ INCH AVERAGE IS NOT ACHIEVED, A RETEST SHALL BE PERFORMED IN ADJOINING AREAS. THIN AREAS SHALL BE RE-COATED AS DESCRIBED ABOVE BY THE CONTRACTOR AND RE-VERIFIED AT NO ADDITIONAL COST TO TDOT. THIS VERIFICATION MAY CONSIST OF CORES, HOLES, E.T.C., BUT IN ALL CASES, ANY DESTRUCTIVELY TEST AREAS SHALL BE REPAIRED BY THE CONTRACTOR BEFORE FINAL ACCEPTANCE BY THE PROJECT ENGINEER.

TYPE	YEAR	PROJECT NO.	SHEET NO.
FED. AID PROJ. NO.	2014	NH/HSIP-22(75)	B2
STATE PROJ. NO.	2014	92137-8221-14	
BRIDGE REPAIR	2014	92137-4223-04	

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EPOXY OVERLAY
NOTE