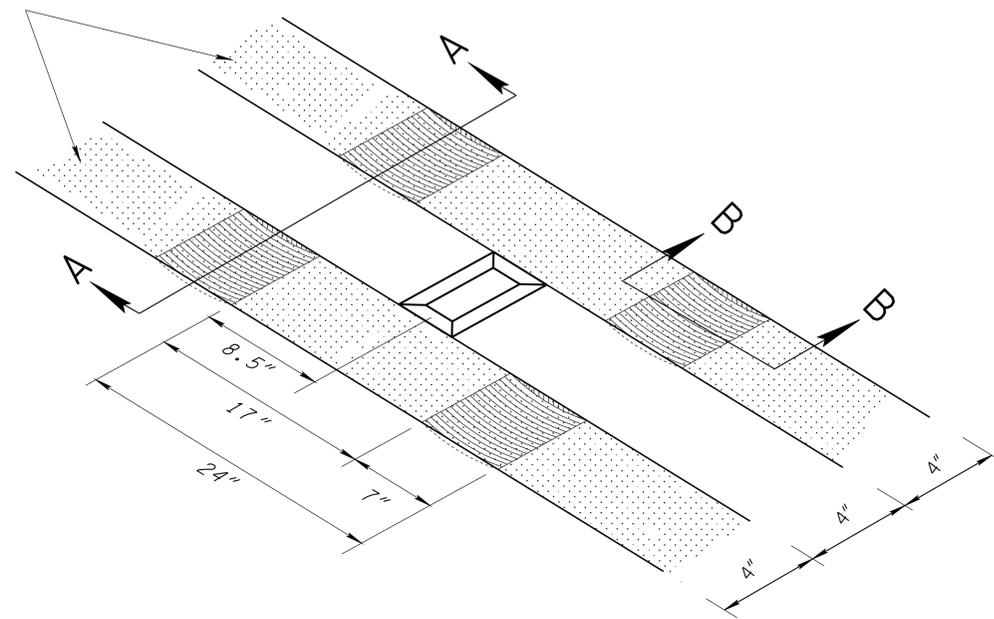


DETAIL "A"
FOR NO PASSING ZONES
OR
ONE WAY PASSING ZONES

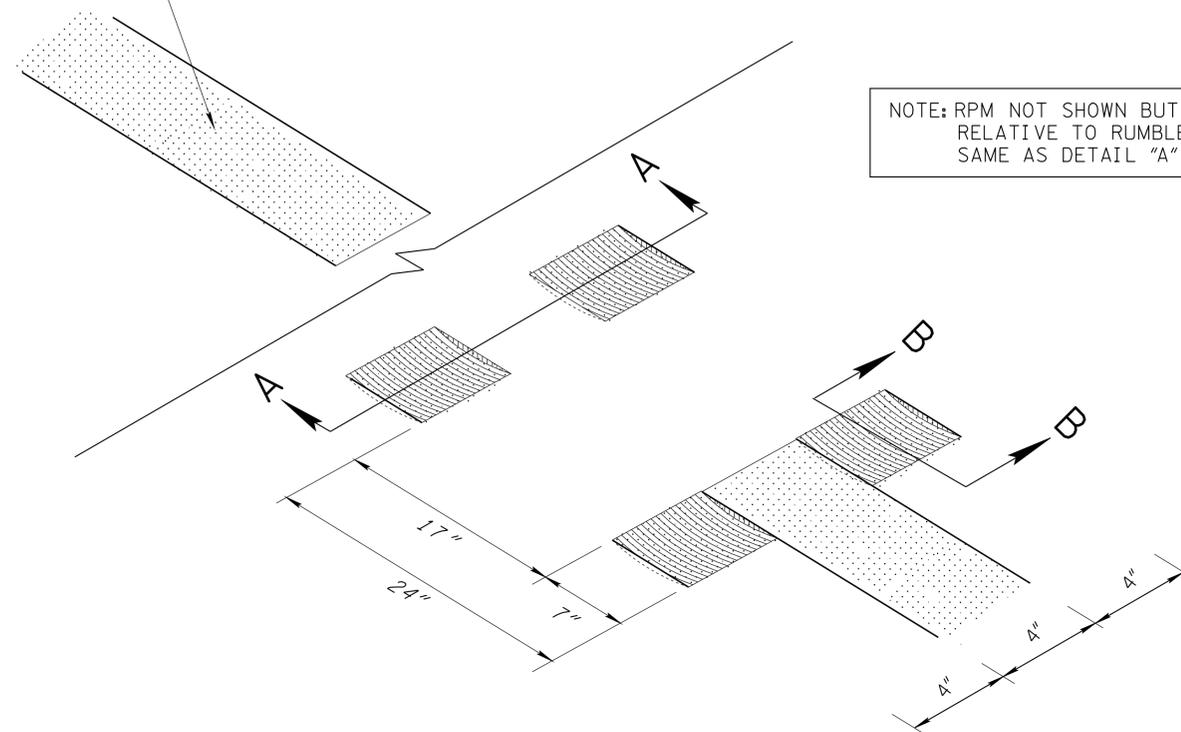
4" DOUBLE YELLOW LINES
(SOLID OR BROKEN)



ISOMETRIC VIEW

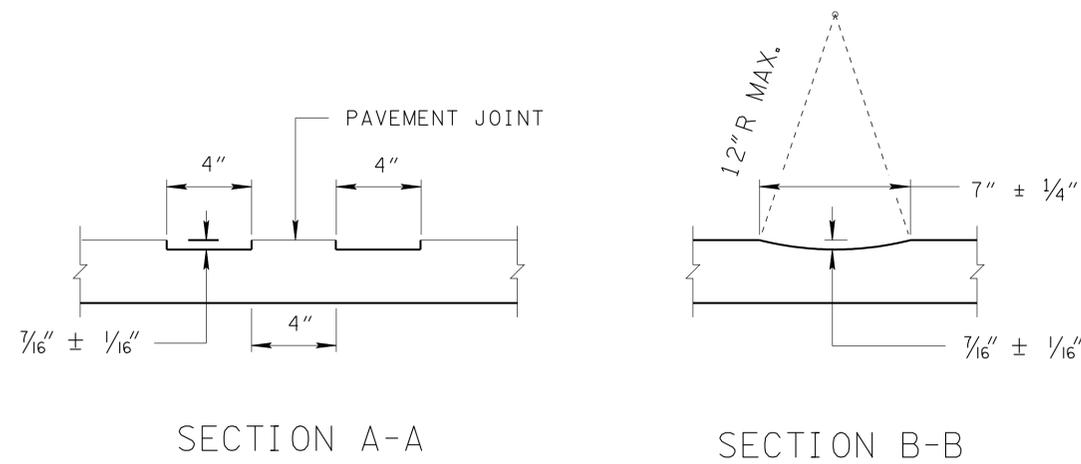
DETAIL "B"
FOR TWO WAY PASSING ZONES

4" BROKEN YELLOW LINE



NOTE: RPM NOT SHOWN BUT PLACEMENT
RELATIVE TO RUMBLES IS THE
SAME AS DETAIL "A"

ISOMETRIC VIEW



SECTION A-A

SECTION B-B

DESIGN NOTES

- (A) FOR IMPROVEMENTS OR RECONSTRUCTION OF EXISTING ROADS RUMBLE STRIPES MAY BE USED AS NEED DUE TO ACCIDENT HISTORY. FOR NEW CONSTRUCTION CENTERLINE RUMBLE STRIPES MAY BE SPECIFIED IF THE FOLLOWING CONDITIONS EXIST:
 - 1) DESIGN SPEED GREATER THAN 50 MPH
 - 2) ADT OF 1500 OR MORE
 - 3) TOTAL TRAVEL WAY WIDTH GREATER THAN OR EQUAL TO 20 FEET.
 - 4) ASPHALT PAVEMENT THAT IS AT LEAST 2.5 IN THICK.
- (B) WHEN RUMBLE STRIPES ARE SPECIFIED, TABLE 4-3 OF THE DESIGN GUIDELINES DOES NOT APPLY. ONLY SPRAY THERMOPLASTIC (60 MIL) 4 IN LINE (716-13.01) SHALL BE USED.
- (C) CENTERLINE RUMBLE STRIPES SHALL NOT BE USED ON BRIDGES.
- (D) THE PAVEMENT JOINT SHALL NOT BE MILLED.
- (E) RUMBLE STRIPE SHALL BE DISCONTINUED WHENEVER THE CENTERLINE MARKING IS ALSO DISCONTINUED.
- (F) RUMBLE STRIPE SHOULD NOT BE USED IN RESIDENTIAL OR COMMERCIAL AREAS.
- (G) SCORING FOR RUMBLE STRIPES TO PAID FOR UNDER ITEM NO. 411-12.05 (INCLUDES BOTH LEFT AND RIGHT SIDE PER LINEAR MILE).
- (H) FOR RPM SPACING SEE T-M-1. IN LOCATIONS WHERE RPMS ARE PRESENT STAGGER RUMBLES SUCH THAT RPMS ARE CENTERED BETWEEN RUMBLES.