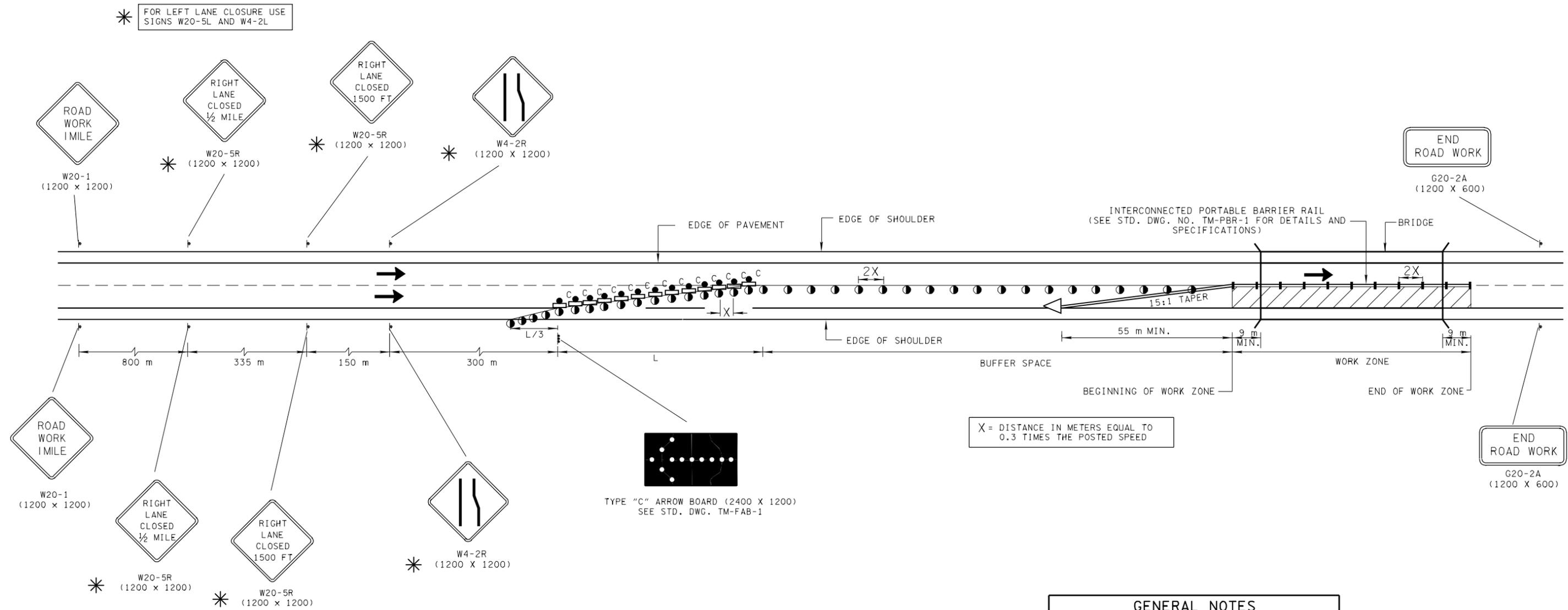


### TRAFFIC CONTROL FOR ONE LANE CLOSURE FOR BRIDGES ON DIVIDED HIGHWAYS



\* FOR LEFT LANE CLOSURE USE SIGNS W20-5L AND W4-2L

\* FOR LEFT LANE CLOSURE USE SIGNS W20-5L AND W4-2L

**COMPUTATION FOR DISTANCE L**

$L = W \times S$

L = TAPER LENGTH IN METERS  
 W = WIDTH OF OFFSET IN METERS  
 S = POSTED SPEED IN MPH

CHANNELIZATION DEVICE LEGEND	
	FLEXIBLE DRUMS
	SIGN SUPPORT
	FLEXIBLE DRUMS WITH TYPE "C" WARNING LIGHTS ATTACHED
	DIRECTION OF TRAFFIC
	WORK SITE
	VERTICAL PANEL (SEE STD. DWG. NO. TM-PBR-2 FOR DETAILS AND SPECIFICATIONS)
	FLASHING YELLOW ARROW BOARD (SEE STD. DWG. NO. TM-FAB-1, FOR DETAILS AND SPECIFICATIONS)
	ATTENUATOR (SEE STANDARD DRAWINGS)

- GENERAL NOTES**
- (A) THIS STANDARD CAN BE USED FOR THE CLOSURE OF AN INSIDE OR OUTSIDE LANE OF ANY MULTI-LANE DIVIDED HIGHWAY. FOR THE CLOSURE OF AN INTERIOR LANE, SEE STANDARD DRAWING NOS. TM-WZ-13, TM-WZ-14, OR TM-WZ-15 FOR DETAILS.
  - (B) PORTABLE BARRIER RAIL REQUIRED FOR BRIDGE DECK AND EXPANSION JOINT WORK.
  - (C) SEE STANDARD DRAWING NO. TM-WZ-10 FOR OTHER NEEDED ADVANCE SIGNING.
  - (D) SEE TABLE VI-1 OF PART VI OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) FOR RECOMMENDED LENGTHS OF BUFFER SPACE.
  - (E) TAPER LENGTH SHOWN FOR THE PORTABLE BARRIER RAIL IS A MINIMUM. PORTABLE BARRIER RAIL MAY BE EXTENDED BEYOND THE CLEAR ZONE TO ELIMINATE THE NEED FOR AN ATTENUATOR. SEE STANDARD DRAWING RDM-S-11 FOR CLEAR ZONE DISTANCE.
  - (F) A SECOND ARROW BOARD MAY BE USED WHEN GEOMETRIC CONDITIONS LIMITS THE VISIBILITY OF A LANE CLOSURE.

STATE OF TENNESSEE  
 METRIC  
 D. O. T.

ALL UNITS ARE IN MILLIMETERS UNLESS NOTED OTHERWISE.

MINOR REVISION -- FHWA APPROVAL NOT REQUIRED.

STATE OF TENNESSEE  
 DEPARTMENT OF TRANSPORTATION

**ONE LANE CLOSURE  
 DETAIL FOR BRIDGES  
 ON  
 DIVIDED HIGHWAYS**

5-27-98 TM-WZ-12