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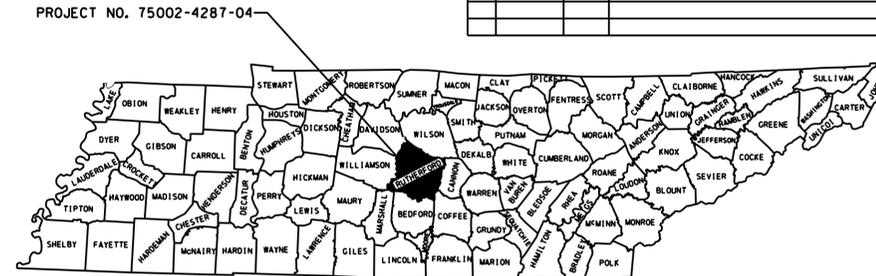
STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
BUREAU OF ENGINEERING

**RUTHERFORD COUNTY**

STATE ROUTE 1 OVER HARTS BRANCH  
BRIDGE NO. 75-SR1-4.76

TENN.	YEAR	SHEET NO.
	2016	1
FED. AID PROJ. NO.		
STATE PROJ. NO.	75002-4287-04	

REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



**BRIDGE REPAIR**



**UNOFFICIAL SET**  
NOT FOR BIDDING

APPROVED: *Paul D. Degges*  
PAUL D. DEGGES, CHIEF ENGINEER

DATE: \_\_\_\_\_

APPROVED: *John Schroer*  
JOHN SCHROER, COMMISSIONER

**SPECIAL NOTES**

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED JANUARY 1, 2015 AND ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS AND IN THE PROPOSAL CONTRACT.

TDOT C.E. MANAGER 1 OR  
TDOT DESIGN MANAGER 1 BRIAN EGLI

TDOT PROJECT MANAGER BRIAN EGLI

DESIGNED BY JAMES + ASSOCIATES, INC.

DESIGNER DAVID THOMPSON, P.E. CHECKED BY JAMIE GILLESPIE, P.E.

P.E. NO. 75002-4287-04

PIN NO. 121388.00

SCALE: 1" = 5,280'

TRAFFIC DATA	
ADT (2017)	23,490
ADT (2037)	30,070
DHV (2037)	3,007
D	60 - 40
T (ADT)	13 %
T (DHV)	9 %
V	40 MPH

ROADWAY LENGTH 0.1004 MILES  
BRIDGE LENGTH 0.0166 MILES  
PROJECT LENGTH 0.1170 MILES

U.S. DEPARTMENT OF TRANSPORTATION  
FEDERAL HIGHWAY ADMINISTRATION

APPROVED: \_\_\_\_\_  
DIVISION ADMINISTRATOR DATE

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## LIST OF SPECIAL PROVISIONS

DWG. NO.	LAST REVISION DATE	DESCRIPTION
105C (SL).....	09-29-14.....	PROTECTION OF RAILWAY INTEREST
107CS.....	02-13-12.....	NESTING SITES OF CLIFF SWALLOWS AND BARN SWALLOWS
202ACM.....	07-07-14.....	REMOVAL OF ASBESTOS CONTAINING MATERIALS
604CR.....	02-19-96.....	REPAIR OF BRIDGE DECK CRACKS

## ROADWAY DESIGN STANDARDS

DWG. NO.	REVISION DATE	DESCRIPTION
RD-A-1	12-18-99	STANDARD ABBREVIATIONS
RD-L-1	10-26-94	STANDARD LEGEND
RD-L-2	09-05-01	STANDARD LEGEND FOR UTILITY INSTALLATIONS
RD-L-5	05-01-08	STANDARD LEGEND FOR EROSION PREVENTION AND SEDIMENT CONTROL
RD-L-6	03-30-10	STANDARD LEGEND FOR EROSION PREVENTION AND SEDIMENT CONTROL
RD-L-7	05-24-12	STANDARD LEGEND FOR EROSION PREVENTION AND SEDIMENT CONTROL

## ROADWAY AND PAVEMENT APPURTENANCES

RP-H-3	01-30-15	CURB RAMP AND TRUNCATED DOME SURFACE DETAIL
RP-H-4	01-30-15	PERPENDICULAR CURB RAMP
RP-H-5	01-30-15	PARALLEL CURB RAMP
RP-H-7	01-30-15	PERPENDICULAR CURB RAMP IN CURVE
RP-H-8	02-05-16	PERPENDICULAR CURB RAMP PLACED OUTSIDE CURVE
RP-H-9	01-30-15	PARALLEL CURB RAMP IN CURVE
RP-S-7	02-05-16	DETAILS FOR CONCRETE SIDEWALKS

## SAFETY DEVICES AND FENCES

S-F-1	05-24-12	HIGH VISIBILITY FENCE
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## TRAFFIC CONTROL DESIGN STANDARD

T-FAB-1	05-27-97	FLASHING YELLOW ARROW BOARD
T-M-1	07-24-14	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS AND MARKING ABBREVIATIONS
T-M-2	07-24-14	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS
T-M-4	07-24-14	STANDARD INTERSECTION PAVEMENT MARKINGS
T-PBR-1	06-30-09	INTERCONNECTED PORTABLE BARRIER RAIL
T-WZ-10	04-02-12	ADVANCE ROAD WORK SIGNING ON HIGHWAYS AND FREEWAYS
T-WZ-11	03-13-09	ONE LANE CLOSURE DETAIL ON DIVIDED HIGHWAYS
T-WZ-14	03-13-09	TWO-OUTSIDE LANE CLOSURE ON INTERSTATES AND EXPRESSWAYS (PORTABLE BARRIER RAIL)
T-WZ-16	03-13-09	LANE SHIFT ON DIVIDED HIGHWAYS AND FREEWAYS
T-WZ-40	04-02-12	RIGHT LANE CLOSURES AT NEAR SIDE OF INTERSECTIONS
T-WZ-41	04-02-12	LEFT LANE CLOSURES AT NEAR SIDE OF INTERSECTIONS
T-WZ-42	04-02-12	CENTER LANE CLOSURES AT NEAR SIDE OF INTERSECTIONS

## TRAFFIC CONTROL - SIGNS

T-S-17	07-02-15	STANDARD GROUND MOUNTED SIGN USING PERFORATED/ KNOCKOUT SQUARE TUBE
T-S-19	07-19-15	STANDARD STEEL SIGN SUPPORTS
T-S-20	11-01-11	SIGN DETAILS

## EROSION PREVENTION AND SEDIMENT CONTROL

EC-STR-3C	08-01-12	SILT FENCE WITH WIRE BACKING
EC-STR-3E	04-01-08	SILT FENCE FABRIC JOINING DETAILS
EC-STR-19	04-01-08	CATCH BASIN PROTECTION
EC-STR-25	08-01-12	TEMPORARY CULVERT CROSSING, CONSTRUCTION EXIT, CONSTRUCTION FORD
EC-STR-37	06-10-14	SEDIMENT TUBE
EC-STR-39A	08-01-12	CURB INLET PROTECTION TYPE 3 AND 4

## BRIDGE APPURTENANCES (NEW STRUCTURES)

STD-1-2SS		STEEL SLIDER PLATE ASSEMBLIES FOR SINGLE SLOPE CONCRETE AND BRIDGE DECK DRAIN DETAILS - 2007
STD-1-2	03-28-08	STEEL SLIDER PLATE ASSEMBLIES FOR CONCRETE PARAPET AND BRIDGE DECK DRAIN DETAILS - 1993
STD-1-5	03-26-14	REINFORCED CONCRETE PAVEMENT AT BRIDGE ENDS - 1995
STD-6-1	11-01-10	STANDARD SEISMIC DETAILS - 1990
STD-10-1	04-08-05	MISCELLANEOUS ABUTMENT AND DRAINAGE DETAILS - 1971
STD-11-1	05-01-14	BRIDGE RAILING CONCRETE PARAPET WITH STRUCTURAL TUBING - 1988
STD-14-3	10-15-08	STANDARD DETAILS FOR PRESTRESSED BOX BEAMS - 1995

TYPE	YEAR	PROJECT NO.	SHEET NO.
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STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

BRIDGE NO. 75-SR1-4.76  
STATE ROUTE 1  
OVER HARTS BRANCH  
RUTHERFORD COUNTY  
2016

INDEX AND STANDARD DRAWINGS

TENNESSEE D.O.T.  
DESIGN DIVISION

FILE NO.

TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	2016	75002-4287-04	1B

**PROJECT COMMITMENTS**

COMMITMENT TO	SOURCE DIVISION	DESCRIPTION	STA./LOCATION
-----	ENVIRONMENTAL DIVISION, ENVIRONMENT	BRIDGE NO. 75SR0010005, SR-1 (US-41/70S, N. LOWRY STREET), OVER HARTS BRANCH, LM 4.76 IN SMYRNA HAS ASBESTOS CONTAINING MATERIAL (ACM) IN THE TRANSITE PIPES THAT ARE ATTACHED TO THE UNDERSIDE OF THE DECK AND IN THE FIBROUS BEAM PADS AT THE ABUTMENTS. ASBESTOS ABATEMENT WILL BE REQUIRED IN ACCORDANCE WITH SPECIAL PROVISION 202ACM, SPECIAL PROVISION REGARDING REMOVAL OF ASBESTOS CONTAINING MATERIAL, AND SHALL BE PAID FOR BY THE CONTRACTOR. STATE OF TENNESSEE ASBESTOS ACCREDITATION REQUIREMENTS (TCA 1200-01-20) MANDATE THAT ACM ABATEMENT WORK BE PERFORMED BY AN ACCREDITED FIRM (CONTRACTOR) USING ACCREDITED ABATEMENT WORKERS AND SUPERVISORS. ACM ABATEMENT SHALL BE COMPLETED PRIOR TO ACTIVITIES THAT WILL DISTURB THE ACM. A NOTICE TO THE TDEC, DIVISION OF AIR POLLUTION CONTROL IS REQUIRED TEN (10) DAYS IN ADVANCE OF ANY ACM ABATEMENT ACTIVITIES.	LM 4.76

**UNOFFICIAL SET**  
  
NOT FOR BIDDING

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

BRIDGE NO. 75-SR1-4.76  
STATE ROUTE 1  
OVER HARTS BRANCH  
RUTHERFORD COUNTY  
2016

**PROJECT COMMITMENTS**





**LIFTING DEVICES AND SUPPORT BRACKETS**

- (1) ALL LIFTING/HANDLING ACCESSORIES EMBEDDED WITHIN AND/OR PROJECTING FROM THE SURFACE OF THE BEAM OR FIELD CUT SHALL BE WEATHERIZED BY EPOXY COATING OR OTHER METHOD APPROVED BY THE ENGINEER.
- (2) LIFTING DEVICES AND SUPPORTS FOR BEAMS AND DECK PANELS WILL BE DESIGNED AND DETAILED IN ACCORDANCE WITH THE PCI DESIGN HANDBOOK, PRECAST AND PRESTRESSED CONCRETE, 5TH EDITION WITH ALL INTERIMS AND ERRATA.
- (3) THE DESIGN OF THE LIFTING DEVICES AND SUPPORTS FOR BEAMS AND DECK PANELS WILL BE THE RESPONSIBILITY OF THE CONTRACTOR. CONTRACTOR WILL SUBMIT PLANS AND HANDLING STRESS CALCULATIONS TO THE ENGINEER FOR APPROVAL PRIOR TO THE CONSTRUCTION OF ANY BEAM OR DECK PANEL.

**HANDLING AND STORAGE**

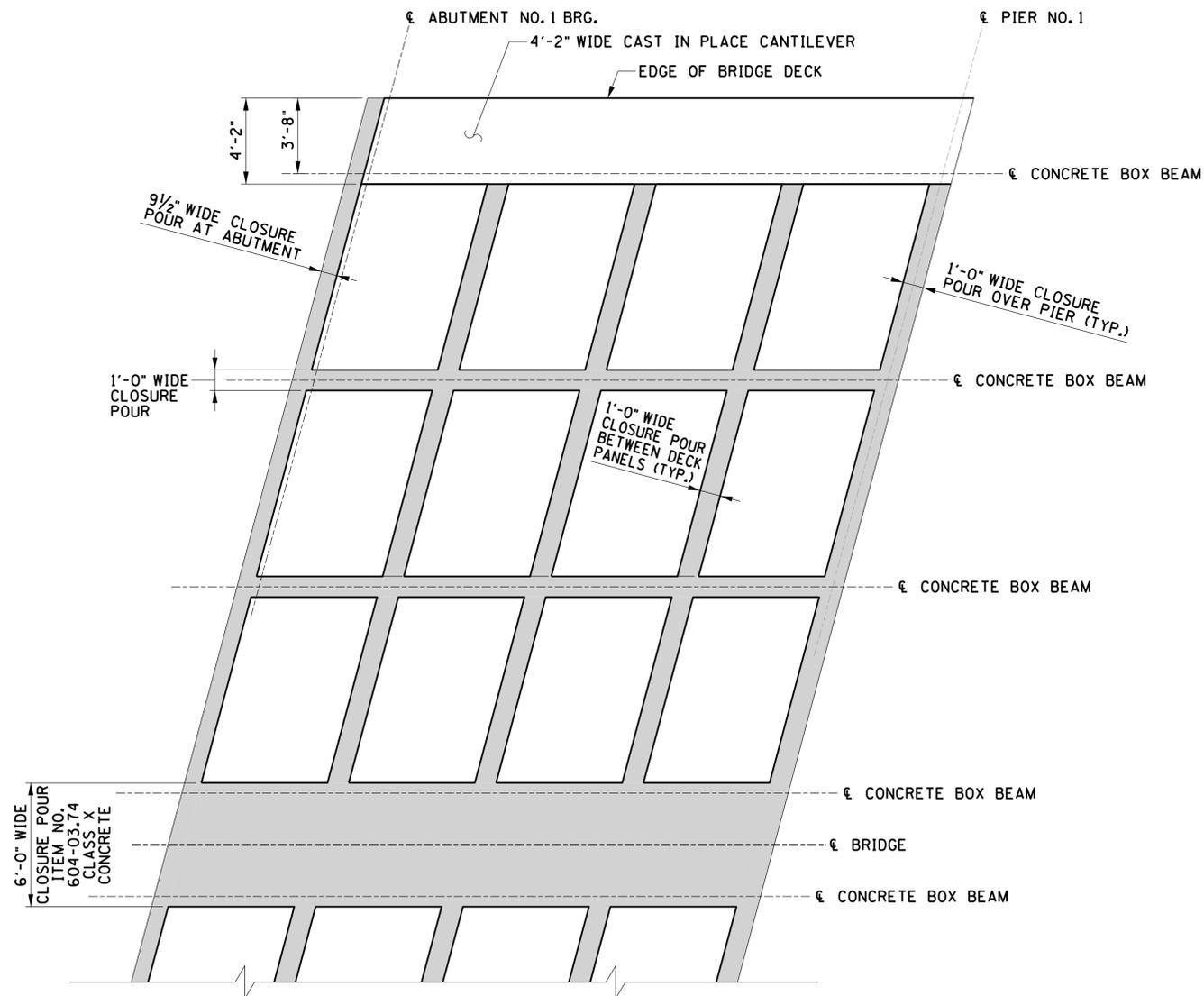
- (4) HANDLING AND STORAGE CONSIDERATIONS WILL BE DESIGNED AND DETAILED IN ACCORDANCE WITH THE PCI DESIGN HANDBOOK, PRECAST AND PRESTRESSED CONCRETE, 5TH EDITION WITH ALL INTERIMS AND ERRATA.
- (5) CONTRACTOR WILL BE RESPONSIBLE FOR THE HANDLING AND STORAGE OF BEAMS AND DECK PANELS IN SUCH A MANNER THAT DOES NOT CAUSE UNDUE STRESS IN THE MEMBER. CONTRACTOR WILL SUBMIT A HANDLING AND STORAGE PLAN TO THE ENGINEER FOR REVIEW PRIOR TO THE CONSTRUCTION OF ANY BEAM OR DECK PANEL. BEAMS AND DECK PANELS SHALL BE REJECTED DUE TO BUT NOT LIMITED TO THE FOLLOWING: BROKEN CORNERS, FULL DEPTH CRACKING, OR SIGNIFICANT DIMENSIONAL DEFORMITIES.

**GROUT AND MORTAR SPECIAL NOTES (ITEM NO. 615-04.10)**

- (6) STRUCTURAL CLOSURE POUR MATERIAL: F'c = 6,000 PSI (MIN.) (LEAN) AND 3,000 PSI (MIN.) (EXTENDED) IN < 8 HOURS.
- (7) ALL PANEL AND CLOSURE POUR GROUT AND MORTAR WILL HAVE A CORROSION INHIBITOR ADMIXTURE.
- (8) FOLLOW MANUFACTURER'S RECOMMENDATION FOR DOSAGE OF CORROSION INHIBITOR ADMIXTURE.
- (9) MIX, PLACE AND CURE PRODUCT FOLLOWING MANUFACTURER'S RECOMMENDATIONS, PREPARATION AND INSTALLATION.
- (10) CONTACT THE MANUFACTURER'S REPRESENTATIVE FOR ADVICE ON HOW TO REDUCE HEAT SUCH AS WET CURING OR ADDING RETARDING ADMIXTURE IF THE HEAT OF HYDRATION IS EXCESSIVE.
- (11) FINISH GROUT FLUSH WITH ADJACENT DECK PANEL.
- (12) MIX STRUCTURAL GROUT AND MORTAR JUST PRIOR TO USE IN ACCORDANCE WITH THE MANUFACTURER'S INSTRUCTIONS.
- (13) USE STRUCTURAL CLOSURE POUR MATERIAL GRAY IN COLOR AND CONTAINING NO CALCIUM CHLORIDE OR ADMIXTURE CONTAINING CALCIUM CHLORIDE OR OTHER INGREDIENT IN SUFFICIENT QUANTITY TO CAUSE CORROSION TO STEEL REINFORCEMENT.
- (14) BONDING FAILURES SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
- (15) REFER TO STRUCTURAL CLOSURE POUR MATERIAL PERFORMANCE CRITERIA TABLE THIS SHEET FOR GROUT AND MORTAR REQUIREMENTS. SUBMIT A CERTIFICATE OF COMPLIANCE OF STRUCTURAL CLOSURE POUR MATERIAL TO ENGINEER FOR APPROVAL.

STRUCTURAL CLOSURE POUR PERFORMANCE CRITERIA TABLE			
PERFORMANCE CHARACTERISTIC	TEST METHOD	PERFORMANCE CRITERIA	
COMPRESSIVE STRENGTH (CS), KSI	ASTM C39 MODIFIED	6.0 ≤ CS @ 7 DAYS (7 DAY CURE) @ 8 HOURS (OVERNIGHT CURE)	
SHRINKAGE (S), (CRACK AGE, DAYS)	AASHTO PP34 MODIFIED	20 < S	
BOND STRENGTH (BS), PSI	ASTM C882 MODIFIED	300 < BS	
CHLORIDE PENETRATION (CHP), (DEPTH FOR PERCENT CHLORIDE OF 0.2% BY MASS OF CEMENT AFTER 90-DAY PONDING, IN)	ASTM C1543 MODIFIED	Chp < 1.5	
FREEZING-AND-THAWING DURABILITY (F/T), (RELATIVE) MODULUS AFTER 300 CYCLES)	ASTM C666 PROCEDURE A MODIFIED	GRADE 1	GRADE 2
		70% ≤ F/T	80% ≤ F/T
		90% ≤ F/T	

CONST. NO.			
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DESIGNED BY D. THOMPSON/D. KEATON DATE \_\_\_\_\_  
 DRAWN BY ANGELA MOORE DATE \_\_\_\_\_  
 SUPERVISED BY DARRELL JAMES DATE \_\_\_\_\_  
 CHECKED BY JAMIE GILLESPIE DATE \_\_\_\_\_



**PARTIAL PLAN**  
(NOT TO SCALE)

**UNOFFICIAL SET**  
NOT FOR BIDDING



STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

BRIDGE SPECIAL NOTES

BRIDGE NO. 75-SR1-4.76  
STATE ROUTE 1  
OVER HARTS BRANCH  
RUTHERFORD COUNTY  
2016

BR-123-04

## ESTIMATED ROADWAY QUANTITIES

ITEM NO.	DESCRIPTION	UNIT	QUANTITY
① 105-01	CONSTRUCTION STAKES, LINES AND GRADES	LS	1
202-03	REMOVAL OF RIGID PAVEMENT, SIDEWALKS, ETC.	S.Y.	95
② 202-03.01	REMOVAL OF ASPHALT PAVEMENT	S.Y.	410
203-01	ROAD AND DRAINAGE EXCAVATION (UNCLASSIFIED)	C.Y.	67
④③ 209-05	SEDIMENT REMOVAL	C.Y.	5
④③ 209-08.02	TEMPORARY SILT FENCE (WITH BACKING)	L.F.	220
209-09.43	CURB INLET PROTECTION (TYPE 4)	EACH	2
④③ 209-40.33	CATCH BASIN PROTECTION (TYPE D)	EACH	1
303-01	MINERAL AGGREGATE, TYPE A BASE, GRADING D	TON	91
307-01.01	ASPHALT CONCRETE MIX (PG64-22) (BPMB-HM) GRADING A	TON	23
307-01.08	ASPHALT CONCRETE MIX (PG64-22) (BPMB-HM) GRADING B-M2	TON	93
402-01	BITUMINOUS MATERIAL FOR PRIME COAT (PC)	TON	0.22
403-01	BITUMINOUS MATERIAL FOR TACK COAT (TC)	TON	2
411-01.10	ACS MIX (PG64-22) GRADING D	TON	328
⑤ 415-01.02	COLD PLANING BITUMINOUS PAVEMENT	S.Y.	3,200
701-01.01	CONCRETE SIDEWALK (4")	S.F.	40
⑥ 705-08.51	PORTABLE IMPACT ATTENUATOR (NCHRP 350, TL-3)	EACH	2
707-08.11	HIGH VISIBILITY CONSTRUCTION FENCE	L.F.	200
⑦ 712-01	TRAFFIC CONTROL	LS	1
⑧ 712-02.02	INTERCONNECTED PORTABLE BARRIER RAIL	L.F.	830
⑨ 712-04.01	FLEXIBLE DRUMS (CHANNELIZING)	EACH	135
712-04.50	PORTABLE BARRIER RAIL DELINEATOR	EACH	24
712-05.01	WARNING LIGHTS (TYPE A)	EACH	2
712-05.03	WARNING LIGHTS (TYPE C)	EACH	51
⑩ 712-06	SIGNS (CONSTRUCTION)	S.F.	587
712-07.03	TEMPORARY BARRICADES (TYPE III)	L.F.	40
712-08.03	ARROW BOARD (TYPE C)	EACH	3
712-09.01	REMOVABLE PAVEMENT MARKING LINE	L.F.	10,150
712-09.04	REMOVABLE PAVEMENT MARKING (STOP LINE)	L.F.	70
712-09.05	REMOVABLE PAVEMENT MARKING (ARROW)	EACH	23
⑪ 713-16.01	CHANGEABLE MESSAGE SIGN	EACH	2
713-16.20	SIGNS (OM3-L)	EACH	2
713-16.21	SIGNS (OM3-R)	EACH	2
714-05.45	REMOVE AND RELOCATE UTILITY POLE	LS	1
714-08.30	REMOVE AND RELOCATE LIGHT STANDARD	EACH	1
716-01.21	SNWPLWBLE PVMT MRKRS (BI-DIR) (1 COLOR)	EACH	16
716-01.22	SNWPLWBLE PVMT MRKRS (MONO-DIR) (1 COLOR)	EACH	16
⑫ 716-02.06	PLASTIC PAVEMENT MARKING (TURN LANE ARROW)	EACH	4
⑬ 716-12.01	ENHANCED FLATLINE THERMO PVMT MRKNG (4" LINE)	L.M.	0.23
716-13.01	SPRAY THERMO PVMT MRKNG (60 MIL) (4" LINE)	L.M.	0.37
717-01	MOBILIZATION	LS	1
④③ 740-11.01	TEMPORARY SEDIMENT TUBE (8 INCH) (STRAW WATTLES)	L.F.	50
⑭ 801-01	SEEDING (WITH MULCH)	UNIT	1
⑭ 801-02	SEEDING (WITHOUT MULCH)	UNIT	1
⑮ 801-03	WATER (SEEDING AND SODDING)	M.G.	1

### FOOTNOTES

- ① ALL DIMENSIONAL DETAILS SHOWN ON PLANS, INCLUDING ELEVATIONS, SHALL BE CHECKED BY THE CONTRACTOR TO ASSURE ACCURACY OF THE LAYOUT PRIOR TO CONSTRUCTION. ALL BRIDGE SUBSTRUCTURES SHALL BE CHECKED AS TO LOCATION, DIMENSIONAL LAYOUTS AND ELEVATIONS, BY MEANS OF TWO INDEPENDENT LAYOUT METHODS. ANY ERRORS AND APPARENT DISCREPANCIES FOUND IN PREVIOUS SURVEYS, OR IN EITHER THE SPECIFICATIONS OR SPECIAL PROVISIONS, SHALL BE CALLED TO THE ENGINEER'S ATTENTION BY THE CONTRACTOR IMMEDIATELY AND PRIOR TO PROCEEDING WITH WORK.
- ② UNIT PRICE INCLUDES REMOVAL OF ALL MATERIALS NECESSARY TO INSTALL 15'-0" PAVEMENT AT BRIDGE END SLABS AND ADA SIDEWALKS AND RAMPS.
- ③ SEDIMENT SHALL BE REMOVED FROM BEHIND THE SILT FENCE WITH WIRE BACKING, CATCH BASIN PROTECTION OR SEDIMENT TUBE WHEN IT HAS ACCUMULATED TO ONE-HALF THE ORIGINAL HEIGHT OF THE STRUCTURE AND PAID FOR UNDER ITEM NO. 209-05, SEDIMENT REMOVAL, C.Y.
- ④ SEE SUBSECTION 209.07 OF THE STANDARD SPECIFICATIONS FOR MAINTENANCE REPLACEMENT. ALL QUANTITIES ARE TO BE USED AS DIRECTED BY THE ENGINEER.
- ⑤ DEPTH VARIES FROM 0" TO 1½" IN ALL AREAS NECESSARY FOR CONSTRUCTION OF ASPHALT TRANSITIONS. TWO SEPARATE PASSES ARE REQUIRED STARTING FROM THE 1½" SAWCUTS SHOWN IN THE PAVEMENT TRANSITION DETAIL ON SHEET 2H.
- ⑥ THIS ITEM SHALL BE A PORTABLE ENERGY ABSORBING TERMINAL MEETING THE REQUIREMENTS OF NCHRP 350 FOR TEST LEVEL 3. EXAMPLES WOULD BE A QUAD-GUARD, A REACT 350 OR A TRACC. THE PAY ITEM WILL INCLUDE FURNISHING AND INSTALLING ALL COMPONENTS AS SHOWN ON THE MANUFACTURERS DRAWING.
- ⑦ INCLUDES COST FOR REMOVAL OF EXISTING OR CONFLICTING PAVEMENT MARKINGS.
- ⑧ ITEM INCLUDES ALL COSTS NECESSARY FOR THE INITIAL INSTALLATION FOR PHASE I TRAFFIC CONTROL, REMOVAL AND RESET INTERMEDIATE TRAFFIC CONTROL, REMOVAL AND RESET FOR PHASE II TRAFFIC CONTROL.
- ⑨ INCLUDES INSTALLATION AND RELOCATION FOR EACH PHASE OF THE CONSTRUCTION SEQUENCE: PHASE I = 135, INTERMEDIATE = 16 AND PHASE II = 74 FOR STATE ROUTE 1.
- ⑩ BASED ON SECTION 712.10 OF STANDARD SPECIFICATIONS.
- ⑪ COORDINATE WITH T.D.O.T. CONSTRUCTION DIVISION FOR LOCATION AND MESSAGE FOR CHANGEABLE MESSAGE SIGN.
- ⑫ THE CONTRACTOR MAY ELECT TO SUBSTITUTE PREFORMED PLASTIC FOR THERMOPLASTIC. PREFORMED PLASTIC SHALL BE PAID FOR AT THE SAME UNIT PRICE AS BID FOR THERMOPLASTIC.
- ⑬ CONTRACTOR SHALL USE THE RIBBON METHOD FOR APPLICATION.
- ⑭ ITEM MAY BE INCREASED, DECREASED OR ELIMINATED AS DIRECTED BY THE ENGINEER.
- ⑮ INCLUDES 1 THOUSAND GALLONS FOR EROSION PREVENTION AND SEDIMENT CONTROL.

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DEPARTMENT OF TRANSPORTATION

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

**ESTIMATED  
ROADWAY  
QUANTITIES**

BRIDGE NO. 75-SR1-4.76  
STATE ROUTE 1  
OVER HARTS BRANCH  
RUTHERFORD COUNTY  
2016

## ROADWAY GENERAL NOTES

### GRADING

- (1) ANY AREA THAT IS DISTURBED OUTSIDE LIMITS OF CONSTRUCTION DURING THE LIFE OF THIS PROJECT SHALL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE.
- (2) THE CONTRACTOR SHALL NOT DISPOSE OF ANY MATERIAL EITHER ON OR OFF STATE-OWNED R.O.W. IN A REGULATORY FLOODWAY AS DEFINED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY WITHOUT APPROVAL BY SAME. ALL MATERIAL SHALL BE DISPOSED OF IN UPLAND (NON-WETLAND) AREAS AND ABOVE ORDINARY HIGH WATER OF ANY ADJACENT WATERCOURSE. THIS DOES NOT ELIMINATE THE NEED TO OBTAIN ANY OTHER LICENSES OR PERMITS THAT MAY BE REQUIRED BY ANY OTHER FEDERAL, STATE OR LOCAL AGENCY.

### SEEDING AND SODDING

- (3) ITEM NO. 801-01, SEEDING (WITH MULCH), SHALL BE USED WHERE EROSION CONTROL BLANKET OR SOD ARE NOT APPLIED.

### DRAINAGE

- (4) THE CONTRACTOR SHALL SHAPE DITCHES TO THE SPECIFIED DESIGN. THIS WORK WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COST WILL BE INCLUDED IN THE COST OF OTHER ITEMS.

### UTILITIES

- (5) THE LOCATIONS OF UTILITIES SHOWN WITHIN THESE PLANS ARE APPROXIMATE ONLY. EXACT LOCATIONS SHALL BE DETERMINED IN THE FIELD BY CONTACTING THE UTILITY COMPANIES INVOLVED. NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC., AT 1-800-351-1111 AS REQUIRED BY TCA 65-31-106 WILL BE REQUIRED.
- (6) UNLESS OTHERWISE NOTED, ALL UTILITY ADJUSTMENTS WILL BE PERFORMED BY THE UTILITY OR IT'S REPRESENTATIVE. THE CONTRACTOR AND UTILITY OWNERS WILL BE REQUIRED TO COOPERATE WITH EACH OTHER IN ORDER TO EXPEDITE THE WORK REQUIRED BY THIS CONTRACT. ON CONTRACTS WHERE CONSTRUCTION STAKES, LINES, AND GRADES ARE CONTRACT ITEMS, THE CONTRACTOR WILL BE REQUIRED TO PROVIDE RIGHT-OF-WAY OR SLOPE STAKES, DITCH OR STREAM BED GRADES, OR OTHER ESSENTIAL SURVEY STAKING TO PREVENT CONFLICTS WITH THE HIGHWAY CONSTRUCTION. FREQUENTLY, THIS WILL BE REQUIRED AS THE FIRST ITEM OF WORK AND AT ANY LOCATION ON THE PROJECT DIRECTED BY THE ENGINEER.
- (7) THE CONTRACTOR WILL PROVIDE ALL NECESSARY PROTECTIVE MEASURES TO SAFEGUARD EXISTING UTILITIES FROM DAMAGE DURING CONSTRUCTION OF THIS PROJECT. IN THE EVENT THAT SPECIAL EQUIPMENT IS REQUIRED TO WORK OVER AND AROUND THE UTILITIES, THE CONTRACTOR WILL BE REQUIRED TO FURNISH SUCH EQUIPMENT. THE COST OF PROTECTING UTILITIES FROM DAMAGE AND FURNISHING SPECIAL EQUIPMENT WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (8) PRIOR TO SUBMITTING HIS BID, THE CONTRACTOR WILL BE RESPONSIBLE FOR CONTACTING OWNERS OF ALL AFFECTED UTILITIES IN ORDER TO DETERMINE THE EXTENT TO WHICH UTILITY RELOCATIONS AND/OR ADJUSTMENTS WILL HAVE UPON THE SCHEDULE OF WORK FOR THE PROJECT. WHILE SOME WORK MAY BE REQUIRED 'AROUND' UTILITY FACILITIES THAT WILL REMAIN IN PLACE, OTHER UTILITY FACILITIES MAY NEED TO BE ADJUSTED CONCURRENTLY WITH THE CONTRACTOR'S OPERATIONS. ADVANCE CLEAR CUTTING MAY BE REQUIRED BY THE ENGINEER AT ANY LOCATION WHERE CLEARING IS CALLED FOR IN THE SPECIFICATIONS AND CLEAR CUTTING IS NECESSARY FOR A UTILITY RELOCATION. ANY ADDITIONAL COST WILL BE INCLUDED IN THE UNIT PRICE BID FOR THE CLEARING ITEM SPECIFIED IN THE PLANS.
- (9) THE CONTRACTOR SHALL NOTIFY EACH INDIVIDUAL UTILITY OWNER OF HIS PLAN OF OPERATION IN THE AREA OF THE UTILITIES. PRIOR TO COMMENCING WORK, THE CONTRACTOR SHALL CONTACT THE UTILITY OWNERS AND REQUEST THEM TO PROPERLY LOCATE THEIR RESPECTIVE UTILITY ON THE GROUND. THIS NOTIFICATION SHALL BE GIVEN AT LEAST THREE (3) BUSINESS DAYS PRIOR TO COMMENCEMENT OF OPERATIONS AROUND THE UTILITY IN ACCORDANCE WITH TCA 65-31-106.

### MISCELLANEOUS

- (10) ALL DETOUR, ACCESS, SERVICE AND FRONTAGE ROADS SHALL BE CONSTRUCTED WITH A MINIMUM OF ONE (1) COURSE OF BASE MATERIAL BEFORE TRAFFIC IS INTERRUPTED ON EXISTING ROADS.
- (11) THE CONTRACTOR SHALL BE REQUIRED TO REMOVE AND RESET MAILBOXES WHERE AND AS DIRECTED BY THE ENGINEER.
- (12) NOTHING IN THE GENERAL NOTES OR SPECIAL PROVISIONS SHALL RELIEVE THE CONTRACTOR FROM HIS RESPONSIBILITIES TOWARD THE SAFETY AND CONVENIENCE OF THE GENERAL PUBLIC AND THE RESIDENTS ALONG THE PROPOSED CONSTRUCTION AREA.

### ROAD CLOSURE

- (13) NO LESS THAN SEVEN (7) DAYS PRIOR TO THE CLOSURE OF THE ROAD, THE CONTRACTOR SHALL NOTIFY THE FOLLOWING INDIVIDUALS OR AGENCIES COMPLETELY DESCRIBING THE AFFECTED ROADS AND THE APPROXIMATE DURATION OF THE CONSTRUCTION; THESE PARTIES INCLUDE, BUT ARE NOT LIMITED TO: (1) LOCAL LAW ENFORCEMENT OFFICE, (2) LOCAL FIRE DEPARTMENT, (3) AMBULANCE SERVICE, (4) LOCAL SCHOOL SUPERINTENDENT, (5) UNITED STATES POSTAL SERVICE, AND (6) LOCAL ROAD SUPERINTENDENT.

### FINAL PAVEMENT MARKINGS

- (14) PERMANENT PAVEMENT LINE MARKINGS SHALL BE 4" ENHANCED FLATLINE THERMOPLASTIC INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-12.01, ENHANCED FLATLINE THERMO PVMT MRKNG (4IN LINE), L.M. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.
- (15) PERMANENT PAVEMENT LINE MARKINGS SHALL BE 4" SPRAY THERMOPLASTIC (60 MIL) INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-13.01, SPRAY THERMO PVMT MRKNG (60 MIL) (4IN LINE), L.M. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.

### PAVING

- (16) THE CONTRACTOR SHALL BE REQUIRED TO COLD PLANE AND PAVE IN THE DIRECTION OF TRAFFIC.

### RESURFACING

- (17) WHERE DIRECTED BY THE TDOT ENGINEER, THE CONTRACTOR SHALL BE REQUIRED TO SHAPE PUBLIC SIDE ROADS, BUSINESS ENTRANCES, AND PRIVATE DRIVES, AS WELL AS CLEANING OF EXISTING DRAINS BEFORE PLACING MATERIALS. ALL COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (18) ALL PUBLIC SIDE ROADS SHALL BE PAVED ONE PAVEMENT WIDTH THROUGH THE INTERSECTION AS A MINIMUM. A SATISFACTORY TRANSITION FROM THE NEW PAVEMENT TO THE EXISTING GRADE OF THE INTERSECTING PUBLIC ROAD OR BUSINESS ENTRANCE SHALL BE PROVIDED. SHOULD THE PAVEMENT OF THE INTERSECTING PUBLIC ROAD BE DISTRESSED, THE RESURFACING WIDTH MAY BE INCREASED TO THE NORMAL RIGHT OF WAY LINE.
- (19) PRIVATE DRIVEWAYS, FIELD ENTRANCES, AND BUSINESS ENTRANCES WILL BE RESURFACED A PAVEMENT WIDTH (LANE WIDTH) AS A MINIMUM. A PAVEMENT TAPER TO TRANSITION THE NEW PAVEMENT SHALL BE REQUIRED, IT SHALL BE BASED ON AN ADDITIONAL ONE FOOT OF WIDTH PER ONE INCH DEPTH OF PAVEMENT. IF THE SHOULDER IS NARROW ENOUGH THAT THE SUM OF THE SHOULDER AND THE TRANSITION ARE LESS THAN A PAVEMENT WIDTH, THE TRANSITION SHALL OCCUR WITHIN THE PAVEMENT WIDTH. IF THE SUM OF THE SHOULDER AND THE TRANSITION IS GREATER THAN A PAVEMENT WIDTH (LANE WIDTH), THE TRANSITION SHALL OCCUR OUTSIDE OF THE PAVEMENT WIDTH.
- (20) IN ALL CASES, THE LENGTH OF THE PAVEMENT TRANSITION, THE THICKNESS AND WIDTH OF THE RESURFACING AND ANY ADDITIONAL PAVEMENT MATERIALS SHALL BE AS DIRECTED BY THE TDOT ENGINEER.

### CONSTRUCTION WORK ZONE AND TRAFFIC CONTROL

- (21) ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE WEEK BEFORE NEEDED, IF THE SIGN FACE IS FULLY COVERED.
- (22) IF THE CONTRACTOR MOVES OFF THE PROJECT, HE SHALL COVER OR REMOVE ALL UNNEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OF REMOVAL, COVERING, AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION) PER SQUARE FOOT.
- (23) A LONG TERM BUT SPORADIC USE WARNING SIGN, SUCH AS A FLAGGER SIGN, MAY REMAIN IN PLACE WHEN NOT REQUIRED PROVIDED THE SIGN FACE IS FULLY COVERED.
- (24) TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.
- (25) USE OF BARRICADES, PORTABLE BARRIER RAILS, VERTICAL PANELS, AND DRUMS SHALL BE LIMITED TO THE IMMEDIATE AREAS OF CONSTRUCTION WHERE A HAZARD IS PRESENT. THESE DEVICES SHALL NOT BE STORED ALONG THE ROADWAY WITHIN THIRTY (30) FEET OF THE EDGE OF THE TRAVELED WAY BEFORE OR AFTER USE UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL INCREASE TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. THESE DEVICES SHALL BE REMOVED FROM THE CONSTRUCTION WORK ZONE WHEN THE ENGINEER DETERMINES THEY ARE NO LONGER NEEDED. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.

- (26) THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY, WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHEN THE LANE IS OPEN TO TRAFFIC UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO PARK WITHIN THIRTY (30) FEET OF AN OPEN TRAFFIC LANE AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.

- (27) ALL DETOUR AND CONSTRUCTION SIGNING SHALL BE IN STRICT ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

### LIGHTING

- (28) INSTALLATION AND MATERIALS SHALL COMPLY WITH SECTIONS 714 AND 917 OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION DATED MARCH 1, 2006 AND WITH THE LATEST REVISIONS TO THE NATIONAL ELECTRIC CODE, NFPA 70.
- (29) ALL WIRING SHALL BE CONCEALED UNDERGROUND IN 2-INCH SCHEDULE 40 PVC RIGID CONDUIT.
- (30) THE GROUND WIRE SHALL BE RUN INSIDE CONDUIT WITHIN STRUCTURES, SHALL BE COLORED GREEN AND HAVE THW INSULATION.
- (31) EXISTING FOUNDATIONS TO BE REMOVED A MINIMUM OF SIX INCHES BELOW GRADE.
- (32) ALL INCIDENTAL EQUIPMENT AND MATERIAL REQUIRED FOR THE SUCCESSFUL EXECUTION OF THIS WORK SHALL BE FURNISHED IN 714 ITEMS WHETHER SPECIFICALLY NOTED OR NOT.
- (33) LIGHT STANDARDS SHALL BE ROUND TAPERED POLES. LENGTH SHALL BE DETERMINED BY REQUIRED MOUNTING HEIGHT.
- (34) STANDARDS SHALL BE DESIGNED IN ACCORDANCE WITH THE REQUIREMENTS OF THE LATEST EDITION OF THE STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORT FOR HIGHWAY SIGNS, LUMINARIES AND TRAFFIC SIGNALS PUBLISHED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS.
- (35) STANDARDS SHALL BE DESIGNED FOR 80-MPH WIND PRESSURE AND SHALL SUPPORT A 62-POUND LUMINAIRE ON A 15-FOOT ARM.
- (36) ALL NEW ROADWAY LIGHT STANDARDS SHALL BE MOUNTED ON BASES WITH ACCESS DOOR, TRANSFORMER BASES SHALL MEET AASHTO SPECIFICATIONS AND HAVE FHWA APPROVAL. STANDARDS SHALL BE ALUMINUM WITH TRANSFORMER BASES.
- (37) BRACKET ARMS SHALL BE ROUND TAPERED TRUSS TYPE WITH STRAP MOUNTING AND LENGTHS AS SCHEDULED.
- (38) BRACKET ARM UPSWEEP SHALL BE THE SAME FOR ALL LIGHT STANDARDS OF THE SAME TYPE.

### EROSION PREVENTION AND SEDIMENT CONTROL DISTURBED AREA

- (39) THE TOTAL AREA TO BE DISTURBED NOT INCLUDING EXCLUSIVE BORROW/WASTE AREAS AND STAGING AREAS IS 0.023 ACRES. SINCE THE DISTURBED ACREAGE IS LESS THAN AN ACRE, A SWPPP IS NOT BEING PREPARED. IF DISTURBED ACREAGE EXCEEDS ONE ACRE, PLEASE CONTACT TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION AS SOON AS POSSIBLE.
- (40) AREAS TO BE UNDISTURBED SHALL BE CLEARLY MARKED IN THE FIELD BEFORE CONSTRUCTION ACTIVITIES BEGIN.
- (41) UNLESS OTHERWISE NOTED IN THE PLANS, THE CONTRACTOR SHALL NOT CLEAR/DIS ANY AREA BEYOND 15 FEET FROM SLOPE LINES.
- (42) PRE-CONSTRUCTION VEGETATIVE GROUND COVER SHALL NOT BE DESTROYED, REMOVED OR DISTURBED (I.E. CLEARING AND GRUBBING INITIATED) MORE THAN 15 CALENDAR PRIOR TO GRADING OR EARTH MOVING ACTIVITIES UNLESS THE AREA IS MULCHED, WITH MULCH, OR OTHER TEMPORARY COVER IS APPLIED.
- (43) CLEARING, GRUBBING, AND OTHER DISTURBANCE TO RIPARIAN VEGETATION SHALL BE LIMITED TO THE MINIMUM NECESSARY FOR SLOPE CONSTRUCTION AND EQUIPMENT OPERATIONS. EXISTING VEGETATION, INCLUDING STREAM AND WETLAND BUFFERS (UNLESS PERMITTED), SHOULD BE PRESERVED TO THE MAXIMUM EXTENT POSSIBLE. UNNECESSARY VEGETATION REMOVAL IS PROHIBITED.
- (44) ALL DISTURBED AREAS SHALL BE PROPERLY STABILIZED AS SOON AS PRACTICABLE. PRIORITY SHALL BE GIVEN TO FINISHING OPERATIONS AND PERMANENT EPSC MEASURES OVER TEMPORARY EPSC MEASURES ON ALL PROJECTS.

TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	2016	75002-4287-04	2D

**UNOFFICIAL SET**  
NOT FOR BIDDING

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

BRIDGE NO. 75-SR1-4.76  
STATE ROUTE 1  
OVER HARTS BRANCH  
RUTHERFORD COUNTY  
2016

ROADWAY  
GENERAL  
NOTES

# ROADWAY GENERAL NOTES (CONT'D)

## EROSION PREVENTION AND SEDIMENT CONTROL DISTURBED AREA (CONT'D)

- (45) CONSTRUCTION SHALL BE SEQUENCED AND STAGED TO MINIMIZE THE EXPOSURE TIME OF GRADED OR DENUDED SOIL AREAS, PRESERVE TOPSOIL, AND MINIMIZE SOIL COMPACTION.
- (46) IF OFFSITE BORROW AND WASTE AREAS BECOME NECESSARY DURING THE LIFE OF THE PROJECT, THIS SUPPORT ACTIVITY SHALL BE ADDRESSED PER THE TDOT WASTE AND BORROW MANUAL.

### SEDIMENT CONTROL

- (47) EPSC MEASURES SHALL BE INSTALLED AND FUNCTIONAL PRIOR TO ANY EARTH MOVING OPERATIONS, AND SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD EXCEPT AS SUCH WORK MAY BE NECESSARY TO INSTALL EPSC MEASURES.
- (48) TEMPORARY EPSC MEASURES MAY BE REMOVED AT THE BEGINNING OF THE WORKDAY, BUT MUST BE REINSTALLED AT THE END OF THE WORKDAY OR BEFORE/DURING A PRECIPITATION EVENT.
- (49) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT THE OFFSITE MIGRATION OR DEPOSIT OF SEDIMENT OFF THE PROJECT LIMITS (E.G. R.O.W., EASEMENTS, ETC.), INTO WATERS OF THE STATE/U.S., OR ONTO ROADWAYS USED BY THE GENERAL PUBLIC. IF SEDIMENT ESCAPES THE CONSTRUCTION SITE, OFFSITE ACCUMULATIONS OF SEDIMENT THAT HAVE NOT REACHED A STREAM MUST BE REMOVED AT A FREQUENCY SUFFICIENT TO MINIMIZE OFFSITE IMPACTS (E.G., FUGITIVE SEDIMENT THAT HAS ESCAPED THE CONSTRUCTION SITE AND HAS COLLECTED IN A STREET MUST BE REMOVED SO THAT IT IS NOT SUBSEQUENTLY WASHED INTO STORM SEWERS AND STREAMS BY THE NEXT RAIN AND/OR SO THAT IT DOES NOT POSE A SAFETY HAZARD TO USERS OF PUBLIC STREETS). ARRANGEMENTS CONCERNING REMOVAL OF SEDIMENT ON ADJOINING PROPERTY MUST BE SETTLED WITH THE ADJOINING PROPERTY OWNER BEFORE REMOVAL OF SEDIMENT.
- (50) THE DEWATERING OF WORK AREAS, TRENCHES, FOUNDATIONS, EXCAVATIONS, ETC. THAT HAVE COLLECTED STORMWATER, WATER FROM VEHICLE WASH AREAS, OR GROUNDWATER SHALL BE EITHER HELD IN SETTLING BASINS OR TREATED BY FILTRATION AND/OR CHEMICAL TREATMENT PRIOR TO ITS DISCHARGE. ALL PHYSICAL AND/OR CHEMICAL TREATMENT WILL BE APPLIED IN ACCORDANCE WITH THE MANUFACTURER'S GUIDELINES AND FULLY DESCRIBED IN THE EPSC PLANS. WATER DISCHARGED SHALL NOT CAUSE AN OBJECTIONABLE COLOR CONTRAST WITHIN THE RECEIVING NATURAL RESOURCE. WATER MUST BE HELD IN SETTLING BASINS UNTIL AT LEAST AS CLEAR AS THE RECEIVING WATERS. SETTLING BASINS SHALL NOT BE LOCATED CLOSER THAN 20 FEET FROM THE TOP BANK OF A STREAM. SETTLING BASINS AND SEDIMENT TRAPS SHALL BE PROPERLY DESIGNED ACCORDING TO THE SIZE OF THE DRAINAGE AREAS OR VOLUME OF WATER TO BE TREATED. TREATED WATER MUST BE DISCHARGED THROUGH A PIPE OR WELL-VEGETATED OR LINED CHANNEL, SO THAT THE DISCHARGE DOES NOT CAUSE EROSION OR SEDIMENT TRANSPORT. DISCHARGES FROM BASINS AND IMPOUNDMENTS SHALL UTILIZE OUTLET STRUCTURES THAT ONLY WITHDRAW WATER FROM NEAR THE SURFACE OF THE BASIN OR IMPOUNDMENT. DISCHARGES MUST NOT CAUSE AN OBJECTIONABLE COLOR CONTRAST WITH THE RECEIVING STREAM.
- (51) CHECK DAMS SHALL BE USED WHERE RUNOFF IS CONCENTRATED. CLEAN ROCK, BRUSH, GABION, OR SANDBAG CHECK DAMS SHALL BE PROPERLY CONSTRUCTED TO REDUCE VELOCITY AND CONTROL EROSION.
- (52) DELAYING PLANTING OF PERMANENT COVER VEGETATION UNTIL WINTER MONTHS OR DRY MONTHS SHOULD BE AVOIDED, IF POSSIBLE.
- (53) OFFSITE VEHICLE TRACKING OF SEDIMENTS AND THE GENERATION OF DUST SHALL BE MINIMIZED. A STABILIZED CONSTRUCTION EXIT (A POINT OF ENTRANCE/EXIT TO THE CONSTRUCTION PROJECT) SHALL BE PROVIDED TO REDUCE THE TRACKING OF MUD AND DIRT ONTO PUBLIC ROADS BY CONSTRUCTION VEHICLES.
- (54) TEMPORARY EPSC MEASURES MAY BE REMOVED AT THE BEGINNING OF THE WORKDAY, BUT MUST BE REPLACED AT THE END OF THE WORKDAY.
- (55) ALL PHYSICAL AND/OR CHEMICAL TREATMENT WILL BE APPLIED IN ACCORDANCE WITH MANUFACTURER'S GUIDELINES AND DESCRIBED ON THE EPSC PLANS FOR ALL PROJECTS REQUIRING ADDITIONAL PHYSICAL OR CHEMICAL TREATMENT OF STORMWATER RUNOFF.

### NATURAL RESOURCES

- (56) SOIL MATERIALS MUST BE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. EPSC MEASURES TO PROTECT NATURAL RESOURCES AND WATER QUALITY SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD. APPROPRIATE EPSC MEASURES MUST BE INSTALLED ALONG THE BASE OF ALL FILLS AND CUTS, ON THE DOWNHILL SIDE OF STOCKPILED SOIL, AND ALONG NATURAL RESOURCES IN CLEARED AREAS TO PREVENT SEDIMENT MIGRATION INTO STREAMS, WETLANDS OR OTHER NATURAL FEATURES IN ACCORDANCE WITH TDOT STANDARDS. EPSC MEASURES SHALL BE INSTALLED ON THE CONTOUR, ENTRENCHED AND STAKED, AND EXTEND THE WIDTH OF THE AREA TO BE CLEARED.
- (57) INSTREAM EPSC DEVICES REQUIRE THE TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION REVIEW AND MUST BE PROCESSED BY THE PERMITS SECTION TO OBTAIN WATER QUALITY PERMITS.
- (58) THE OPERATION OF EQUIPMENT IN WATERS OF THE STATE/U.S., INCLUDING WETLANDS AND EPHEMERAL, INTERMITTENT, AND PERENNIAL STREAMS, SHALL BE ONLY AS DEPICTED ON THE CONSTRUCTION PLANS AND/OR AS SO SPECIFIED IN THE WATER QUALITY PERMITS, IF APPLICABLE. ANY DISCREPANCIES BETWEEN PLANS AND PERMITS SHALL BE BROUGHT TO THE ATTENTION OF THE TDOT RESPONSIBLE PARTY AS SOON AS POSSIBLE. ADDITIONAL PERMITS REQUIRED BY THE CONTRACTOR'S METHOD OF OPERATION SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO OBTAIN AFTER RECEIVING THE APPROVAL OF THE TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION.

- (59) STREAM BEDS SHALL NOT BE USED AS TRANSPORTATION ROUTES FOR CONSTRUCTION EQUIPMENT. TEMPORARY CULVERT CROSSING SHALL BE LIMITED TO ONE POINT PER STREAM AND EPSC MEASURES SHALL BE USED WHERE THE STREAM BANKS ARE DISTURBED. WHERE THE STREAMBED IS NOT COMPOSED OF BEDROCK, A PAD OF CLEAN ROCK SHALL BE USED AT THE CROSSING POINT AND CULVERTED TO PREVENT THE IMPOUNDMENT OF WATER FLOW. CLEAN ROCK IS ROCK OF VARIOUS TYPE AND SIZE, DEPENDING UPON APPLICATION, WHICH CONTAINS NO FINES, SOILS, OR OTHER WASTES OR CONTAMINANTS. OTHER MATERIALS USED FOR ALL TEMPORARY FILLS SHALL BE COMPLETELY REMOVED IN THEIR ENTIRETY AFTER THE WORK IS COMPLETED AND THE AFFECTED AREAS RETURNED TO THEIR PREEXISTING ELEVATION. ALL TEMPORARY CROSSINGS SHALL BE CONSTRUCTED IN ACCORDANCE WITH STD. DWG. EC-STR-25 UNLESS SPECIFICALLY ADDRESSED IN THE EPSC PLANS. ALTERNATIVELY, PLACING A TEMPORARY BRIDGE (E.G. BAILEY BRIDGE OR EQUIVALENT, TIMBERS, ETC.) FROM TOP OF BANK TO TOP OF BANK OR THE APPROPRIATE USE OF BARGES AT THE CROSSING TO AVOID DISTURBANCE OF THE STREAMBED IS AN ACCEPTABLE OPTION.
- (60) HEAVY EQUIPMENT WORKING IN WETLANDS WITH PERMITTED TEMPORARY IMPACTS SHALL BE PLACED ON MATS, OR OTHER MEASURES MUST BE TAKEN TO MINIMIZE SOIL DISTURBANCE AND COMPACTION UNLESS SPECIFICALLY ADDRESSED IN THE CONSTRUCTION PLANS. ANY MATS AND OTHER MEASURES USED FOR HEAVY EQUIPMENT SHALL BE REMOVED IN THEIR ENTIRETY AFTER THE WORK IS COMPLETED. ALL AFFECTED AREAS SHOULD BE RETURNED TO PRE-EXISTING CONDITIONS.
- (61) WETLANDS SHALL NOT BE USED AS EQUIPMENT STORAGE, STAGING, OR TRANSPORTATION AREAS, UNLESS SPECIFICALLY PROVIDED FOR IN THE CONSTRUCTION PLANS AND PERMITS.

### SPECIES

- (62) NO ACTIVITY MAY SUBSTANTIALLY DISRUPT THE MOVEMENT OF THOSE SPECIES OF AQUATIC LIFE INDIGENOUS TO THE WATER BODY, INCLUDING THOSE SPECIES THAT NORMALLY MIGRATE THROUGH THE AREA.

### INSPECTION, MAINTENANCE AND REPAIR

- (63) THE TDOT CONSTRUCTION SUPERVISOR (OR THEIR DESIGNEE) AND THE CONTRACTOR'S RESPONSIBLE PARTY ARE RESPONSIBLE FOR INSPECTIONS, MAINTENANCE AND REPAIR ACTIVITIES ARE THE RESPONSIBILITY OF THE CONTRACTOR. THE TDOT CONSTRUCTION SUPERVISOR OR THEIR DESIGNEE SHALL COMPLETE THE EPSC INSPECTION REPORTS AND DISTRIBUTE COPIES PER THE CONTRACT.
- (64) TDOT CONSULTANTS AND CONTRACTOR STAFF RESPONSIBLE FOR THE INSPECTION, IMPLEMENTATION, MAINTENANCE, AND/OR REPAIR OF EPSC MEASURES SHALL SUCCESSFULLY COMPLETE THE "TDEC 'LEVEL 1 - FUNDAMENTALS OF EROSION PREVENTION AND SEDIMENT CONTROL FOR CONSTRUCTION SITES' COURSE AND ANY REFRESHER COURSES AS REQUIRED TO MAINTAIN CERTIFICATION. TDOT STAFF AND SUPERVISORS RESPONSIBLE FOR THE INSPECTION, IMPLEMENTATION, MAINTENANCE, AND/OR REPAIR OF EPSC MEASURES SHALL SUCCESSFULLY COMPLETE THE "TDEC 'FUNDAMENTALS OF EROSION AND SEDIMENT CONTROL' CLASS AND ANY REFRESHER COURSES AS REQUIRED TO MAINTAIN CERTIFICATION.
- (65) EPSC CONTROLS SHALL BE INSPECTED ACCORDING TO PERMIT REQUIREMENTS TO VERIFY MEASURES HAVE BEEN INSTALLED AND MAINTAINED IN ACCORDANCE WITH TDOT STANDARD DRAWINGS, SPECIFICATIONS, AND GOOD ENGINEERING PRACTICES. EPSC INSPECTIONS SHALL BE DOCUMENTED ON THE TDOT EPSC INSPECTION REPORT.
- (66) DISCHARGE POINTS SHALL BE INSPECTED TO ASCERTAIN WHETHER EPSC MEASURES ARE EFFECTIVE IN PREVENTING EROSION AND CONTROLLING SEDIMENT INCLUDING SIGNIFICANT IMPACTS TO SURROUNDING NATURAL RESOURCES AND ADJACENT PROPERTY OWNERS. WHERE DISCHARGE LOCATIONS ARE INACCESSIBLE, NEARBY DOWN GRADIENT LOCATIONS SHALL BE INSPECTED. LOCATIONS WHERE VEHICLES ENTER AND EXIT THE SITE SHALL BE INSPECTED FOR EVIDENCE OF OFFSITE ROADWAY SEDIMENT TRACKING.
- (67) UPON CONCLUSION OF THE INSPECTIONS, EPSC MEASURES FOUND TO BE INEFFECTIVE SHALL BE REPAIRED, REPLACED, OR MODIFIED BEFORE THE NEXT RAIN EVENT, IF POSSIBLE, BUT IN NO CASE MORE THAN 24 HOURS AFTER THE INSPECTION OR WHEN THE CONDITION IS IDENTIFIED. IF THE REPAIR, REPLACEMENT OR MODIFICATION IS NOT PRACTICAL WITHIN THE 24 HOUR TIMEFRAME, WRITTEN DOCUMENTATION SHALL BE PROVIDED IN THE FIELD DIARY AND EPSC INSPECTION REPORT. AN ESTIMATED REPAIR, REPLACEMENT OR MODIFICATION SCHEDULE SHALL BE DOCUMENTED WITHIN 24 HOURS AFTER IDENTIFICATION.
- (68) INSPECTION, REPAIR, AND MAINTENANCE OF EPSC MEASURES SHALL BE PERFORMED ON A REGULAR BASIS. SEDIMENT SHALL BE REMOVED FROM SEDIMENT CONTROL STRUCTURES WHEN THE DESIGN CAPACITY HAS BEEN REDUCED BY FIFTY PERCENT (50%). DURING SEDIMENT REMOVAL, THE CONTRACTOR SHALL TAKE STEPS TO ENSURE THAT STRUCTURAL COMPONENTS OF EPSC MEASURES ARE NOT DAMAGED AND THUS MADE INEFFECTIVE. IF DAMAGE DOES OCCUR, THE CONTRACTOR SHALL REPAIR THE EPSC MEASURES AT THE CONTRACTOR'S OWN EXPENSE.
- (69) THE EPSC PLAN SHALL BE UPDATED WHENEVER EPSC INSPECTIONS INDICATE, OR WHERE STATE OR FEDERAL OFFICIALS DETERMINE EPSC MEASURES ARE PROVING INEFFECTIVE IN ELIMINATING OR SIGNIFICANTLY MINIMIZING POLLUTANT SOURCES OR ARE OTHERWISE NOT ACHIEVING THE GENERAL OBJECTIVES OR CONTROLLING POLLUTANTS IN STORM WATER DISCHARGES ASSOCIATED WITH THE CONSTRUCTION ACTIVITY.
- (70) SEDIMENT REMOVED FROM SEDIMENT CONTROL STRUCTURES SHALL BE PLACED AND BE TREATED IN A MANNER SO THAT THE SEDIMENT IS CONTAINED WITHIN THE PROJECT LIMITS AND DOES NOT MIGRATE ONTO ADJACENT PROPERTIES AND INTO WATERS OF THE STATE/U.S. COST FOR THIS TREATMENT SHALL BE INCLUDED IN PRICE BID FOR ITEM NO. 209-05 SEDIMENT REMOVAL, C.Y.
- (71) THE TDOT PROJECT SUPERVISOR (OR THEIR DESIGNEE) AND THE CONTRACTOR'S SITE SUPERINTENDENT ARE RESPONSIBLE FOR INSPECTIONS, MAINTENANCE AND REPAIR ACTIVITIES ARE THE RESPONSIBILITY OF THE CONTRACTOR.

### EROSION PREVENTION

- (72) CONSTRUCTION SHALL BE SEQUENCED AND STAGED TO MINIMIZE THE EXPOSURE TIME OF GRADED OR DENUDED SOIL AREAS, PRESERVE TOPSOIL, AND MINIMIZE SOIL COMPACTION.
- (73) THE ACCEPTED EPSC PLAN SHALL REQUIRE THAT EPSC MEASURES BE IN PLACE BEFORE CLEARING, GRUBBING, EXCAVATION, GRADING, CULVERT OR BRIDGE CONSTRUCTION, CUTTING, FILLING, OR ANY OTHER EARTHWORK OCCURS, EXCEPT AS SUCH WORK MAY BE NECESSARY TO INSTALL EPSC MEASURES.

- (74) NO WORK SHALL BE STARTED UNTIL THE CONTRACTOR'S PLAN FOR THE STAGING OF OPERATIONS, INCLUDING THE PLAN FOR STAGING OF TEMPORARY AND PERMANENT EPSC MEASURES, HAS BEEN ACCEPTED BY THE TDOT RESPONSIBLE PARTY. THE CONTRACTOR'S EPSC PLAN SHALL INCORPORATE AND SUPPLEMENT, AS ACCEPTABLE, THE BASIC EPSC DEVICES ON THE EPSC PLAN.

- (75) TEMPORARY STABILIZATION SHALL BE INITIATED WITHIN 14 CALENDAR DAYS WHEN CONSTRUCTION ACTIVITIES ON A PORTION OF THE SITE ARE TEMPORARILY CEASED AND EARTH DISTURBING ACTIVITIES WILL NOT RESUME UNTIL AFTER 14 CALENDAR DAYS. PERMANENT STABILIZATION MEASURES IN DISTURBED AREAS SHALL BE INITIATED WITHIN 14 CALENDAR DAYS AFTER FINAL GRADING OF ANY PHASE OF CONSTRUCTION.

- (76) STEEP SLOPES SHALL BE TEMPORARILY STABILIZED NOT LATER THAN 7 DAYS AFTER CONSTRUCTION ACTIVITY ON THE SLOPE HAS TEMPORARILY OR PERMANENTLY CEASED. STEEP SLOPES ARE DEFINED AS A NATURAL OR CREATED SLOPE OF 35% GRADE OR GREATER REGARDLESS OF HEIGHT.

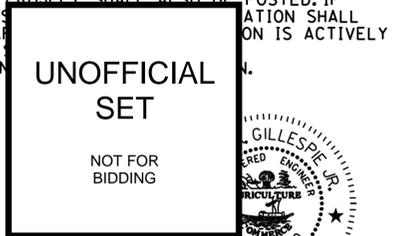
- (77) PERMANENT STABILIZATION WILL REPLACE TEMPORARY MEASURES AS SOON AS PRACTICABLE. PRIORITY SHALL BE GIVEN TO FINISHING OPERATIONS AND PERMANENT EPSC MEASURES OVER TEMPORARY EPSC MEASURES ON ALL PROJECTS.

- (78) TEMPORARY OR PERMANENT STABILIZATION MUST BE FREE OF FINES (SILT AND CLAY SIZED PARTICLES). UNPACKED GRAVEL CONTAINING FINES OR CRUSHER-RUN WILL NOT BE CONSIDERED SUFFICIENT STABILIZATION.

- (79) DELAYING THE PLANTING OF COVER VEGETATION UNTIL WINTER MONTHS OR DRY MONTHS SHOULD BE AVOIDED.

### PERMITS, PLANS AND RECORDS

- (80) THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR AND OBTAIN ANY NECESSARY ENVIRONMENTAL PERMITS OR APPROVALS, INCLUDING BUT NOT LIMITED TO ARCHAEOLOGY, ECOLOGY, HISTORICAL, HAZARDOUS MATERIALS, AIR AND NOISE, TDEC ARAP/401, USACE SECTION 404, TVA SECTION 26A, AND TDEC NPDES PERMITS, FROM FEDERAL, STATE AND/OR LOCAL AGENCIES REGARDING ANY MATERIAL AND STAGING AREAS AND THE OPERATION OF ANY PROJECT-DEDICATED ASPHALT AND/OR CONCRETE PLANTS TO BE USED. ANY SUCH PERMITS SHALL BE SUPPLIED TO THE TDOT PROJECT RESPONSIBLE PARTY PRIOR TO THE USE OF THE PERMITTED AREAS).
- (81) ANY DISAGREEMENT BETWEEN THE CONSTRUCTION PLANS, THE PROJECT AS CONSTRUCTED, AND THE PERMIT(S) ISSUED FOR THE PROJECT, SHALL BE BROUGHT TO THE ATTENTION OF THE TDOT PROJECT RESPONSIBLE PARTY. THE ENVIRONMENTAL DIVISION, DESIGN DIVISION, AND HEADQUARTERS CONSTRUCTION OFFICE SHALL BE CONTACTED IN THESE INSTANCES AND DECIDE WHICH HAS PRECEDENCE AND WHETHER PERMIT OR PLANS REVISIONS ARE NEEDED. IN GENERAL, PERMIT CONDITIONS WILL PREVAIL.
- (82) IF A CHANGE IN PROJECT SCOPE OCCURS DURING CONSTRUCTION, INCLUDING VALUE ENGINEERING, THE TDOT PERMIT SECTION SHALL BE CONTACTED TO DETERMINE WHETHER PERMIT REVISIONS ARE NEEDED. THE ROADWAY DESIGN DIVISION SHALL BE CONTACTED TO DETERMINE IF ANY PLAN REVISIONS ARE NEEDED.
- (83) THE CONTRACTOR SHALL REVIEW ALL EXISTING PERMITS TO ENSURE THAT WORK AT PERMITTED SITES DOES NOT EXCEED EXPIRATION DATE. IF WORK IS GOING TO BE CONTINUED AFTER EXPIRATION DATES, THE CONTRACTOR SHALL CONTACT THE TDOT PROJECT RESPONSIBLE PARTY TO COMMENCE PERMIT RENEWAL PROCESS.
- (84) ALL WATER QUALITY PERMITS SHALL BE POSTED NEAR THE MAIN ENTRANCE OF THE CONSTRUCTION SITE ACCESSIBLE TO THE PUBLIC. THE NAME, COMPANY NAME, EMAIL ADDRESS, TELEPHONE NUMBER AND ADDRESS OF THE PROJECT SITE OWNER, OPERATOR, OR A LOCAL CONTACT PERSON WITH A BRIEF DESCRIPTION OF THE PROJECT SHALL ALSO BE POSTED. IF POSTING THIS INFORMATION NEAR A MAIN ENTRANCE IS NOT PRACTICAL, POSTING SHALL BE PLACED IN A PUBLICLY ACCESSIBLE LOCATION NEAR THE MAIN ENTRANCE. THIS LOCATION IS UNDERWAY AND MOVED AS NECESSARY. THIS LOCATION IS TO BE MAINTAINED THROUGHOUT CONSTRUCTION. ALL POSTINGS SHALL BE MAINTAINED THROUGHOUT CONSTRUCTION.



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ROADWAY  
GENERAL  
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# ROADWAY GENERAL AND SPECIAL NOTES (CONT'D)

## EROSION PREVENTION AND SEDIMENT CONTROL

### PERMITS, PLANS AND RECORDS (CONT'D)

- (85) THE EPSC PLAN IS TO SERVE AS AN INITIAL GUIDE FOR SITE PERSONNEL AS THE CONSTRUCTION PROCESS DEVELOPS. IT MUST BE AMENDED, MODIFIED, AND UPDATED WHENEVER A CHANGE IN THE DESIGN OR CONSTRUCTION OF THE PROJECT OCCURS. THE STAGES DEPICTED IN THE EPSC PLANS MAY NOT COINCIDE WITH THE ACTUAL PHASES OF CONSTRUCTION ESTABLISHED BY THE CONTRACTOR DURING CONSTRUCTION, THUS MODIFICATIONS WILL BE REQUIRED TO ENSURE THE EPSC PLAN IS MAINTAINED TO DEPICT CURRENT SITE CONDITIONS. IT SHOULD BE MAINTAINED SUCH THAT IT WILL ALWAYS REFLECT THE MEASURES THAT ARE INSTALLED DURING THE VARIOUS PHASES OF CONSTRUCTION. IT IS IMPRACTICAL TO DETERMINE ALL THE INTERMEDIATE PHASES OF CONSTRUCTION THAT WILL OCCUR, THUS THESE DOCUMENTS WILL HAVE TO BE UPDATED THROUGHOUT THE LIFE OF THE CONSTRUCTION PROJECT.
- (86) ALL PROJECT RELATED ENVIRONMENTAL PERMITS SHALL BE MAINTAINED AT OR NEAR THE PROJECT SITE.
- (87) THE EPSC PLAN SHALL BE UPDATED WHENEVER EPSC INSPECTIONS INDICATE, OR WHERE STATE OR FEDERAL OFFICIALS DETERMINE EPSC MEASURES ARE PROVING INEFFECTIVE IN ELIMINATING OR SIGNIFICANTLY MINIMIZING POLLUTANT SOURCES OR ARE OTHERWISE NOT ACHIEVING THE GENERAL OBJECTIVES OF CONTROLLING POLLUTANTS IN STORM WATER DISCHARGES ASSOCIATED WITH THE CONSTRUCTION ACTIVITY.
- (88) THE TDOT PROJECT RESPONSIBLE PARTY (INCLUDING TDOT STAFF, CONSULTANTS AND CONTRACTOR STAFF) RESPONSIBLE FOR THE IMPLEMENTATION AND MAINTENANCE OF EPSC PLANS SHALL SUCCESSFULLY COMPLETE THE TDEC "LEVEL 1 - FUNDAMENTALS OF EROSION PREVENTION AND SEDIMENT CONTROL FOR CONSTRUCTION SITES" COURSE AND ANY REFRESHER COURSES AS REQUIRED TO MAINTAIN CERTIFICATION. A COPY OF CERTIFICATION RECORDS FOR THE COURSES SHALL BE KEPT ON SITE AND AVAILABLE UPON REQUEST.

### GOOD HOUSEKEEPING MEASURES AND WASTE DISPOSAL

- (89) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT LITTER AND CONSTRUCTION WASTES FROM ENTERING WATERS OF THE STATE/U.S. THESE MATERIALS SHALL BE REMOVED FROM STORMWATER EXPOSURE PRIOR TO ANTICIPATED STORM EVENTS OR BEFORE BEING CARRIED OFFSITE BY WIND, OR OTHERWISE PREVENTED FROM BECOMING A POLLUTANT SOURCE FOR STORMWATER DISCHARGES. AFTER USE, MATERIALS USED FOR EPSC SHALL BE REMOVED FROM THE SITE.
- (90) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS TO ENSURE THAT PETROLEUM PRODUCTS OR OTHER CHEMICAL POLLUTANTS ARE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. ALL EQUIPMENT REFUELING, SERVICING, AND STAGING AREAS SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL LAWS, RULES, REGULATIONS, AND ORDINANCES, INCLUDING THOSE OF THE NATIONAL FIRE PROTECTION ASSOCIATION. APPROPRIATE CONTAINMENT MEASURES FOR THESE AREAS SHALL BE USED.
- (91) CONTRACTORS SHALL PROVIDE DESIGNATED TRUCK WASHOUT AREAS ON THE SITE. THESE AREAS MUST BE SELF CONTAINED, NOT CONNECTED TO ANY STORMWATER OUTLET OF THE SITE, AND PROPERLY SIGNED. WASH DOWN OR WASTE DISCHARGE OF CONCRETE TRUCKS SHALL NOT BE PERMITTED ONSITE UNLESS PROPER SETTLEMENT AREAS HAVE BEEN PROVIDED IN ACCORDANCE WITH BOTH STATE AND FEDERAL REGULATIONS.
- (92) WHEEL WASH WATER SHALL BE COLLECTED AND ALLOWED TO SETTLE OUT SUSPENDED SOLIDS PRIOR TO DISCHARGE. WHEEL WASH WATER SHALL NOT BE DISCHARGED DIRECTLY INTO ANY STORMWATER SYSTEM OR STORMWATER TREATMENT SYSTEM.
- (93) IF PORTABLE SANITARY FACILITIES ARE PROVIDED ON CONSTRUCTION SITES, SANITARY WASTE WILL BE COLLECTED FROM THE PORTABLE UNITS IN A TIMELY MANNER BY A LICENSED WASTE MANAGEMENT CONTRACTOR OR AS REQUIRED BY ANY REGULATIONS. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF SANITARY WASTE.
- (94) ONLY CONSTRUCTION PRODUCTS NEEDED SHALL BE STORED ONSITE BY THE CONTRACTOR. THE CONTRACTOR SHALL STORE ALL MATERIALS UNDER COVER AND IN APPROPRIATE CONTAINERS. PRODUCTS MUST BE STORED IN ORIGINAL CONTAINERS AND LABELED. MATERIAL MIXING SHALL BE CONDUCTED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS. THE CONTRACTOR'S RESPONSIBLE PARTY SHALL INSPECT MATERIALS STORAGE AREAS REGULARLY TO ENSURE PROPER USE AND DISPOSAL.
- (95) WHEN POSSIBLE, ALL PRODUCTS WILL BE USED COMPLETELY BEFORE PROPERLY DISPOSING OF THE CONTAINER OFFSITE. THE MANUFACTURER'S DIRECTIONS FOR DISPOSAL OF MATERIALS AND CONTAINERS WILL BE FOLLOWED.
- (96) ALL PAINT CONTAINERS WILL BE TIGHTLY SEALED AND STORED WHEN NOT REQUIRED FOR USE. EXCESS PAINT WILL BE DISPOSED OF ACCORDING TO THE MANUFACTURER'S INSTRUCTIONS AND APPLICABLE STATE AND LOCAL REGULATIONS.
- (97) ALL HAZARDOUS WASTE MATERIALS SHALL BE DISPOSED OF IN A MANNER WHICH IS COMPLIANT WITH LOCAL OR STATE REGULATIONS. SITE PERSONNEL SHALL BE INSTRUCTED IN THESE PRACTICES, AND THE INDIVIDUAL DESIGNATED AS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR SEEING THAT THESE PRACTICES ARE FOLLOWED. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF HAZARDOUS MATERIAL.
- (98) OPEN BURNING IS PROHIBITED UNLESS IT IS SPECIFICALLY ALLOWED BY LAW. IF ALLOWED, NATURAL VEGETATION, TREES, AND UNTREATED LUMBER SHALL BE THE ONLY MATERIALS THAT CAN BE OPEN BURNED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL APPLICABLE STATE AND LOCAL PERMITS PRIOR TO ANY BURNING.
- (99) DISPOSAL OF ONSITE VEGETATION AND TREES BY CHIPPING THEM INTO MULCH IS PREFERABLE TO OPEN BURNING. THIS MULCH MAY BE USED AS AN ONSITE SOIL STABILIZATION MEASURE WHERE APPROPRIATE.

(100) WASTE MATERIAL (EARTH, ROCK, ASPHALT, CONCRETE, ETC.) NOT REQUIRED FOR THE CONSTRUCTION OF THE PROJECT WILL BE DISPOSED OF BY THE CONTRACTOR. IMPACTS TO WATERS OF THE STATE/U.S. SHALL BE AVOIDED IF POSSIBLE. IF UNAVOIDABLE, THE CONTRACTOR WILL OBTAIN ANY AND ALL NECESSARY PERMITS INCLUDING, BUT NOT LIMITED TO NPDES, AQUATIC RESOURCES ALTERATION PERMIT(S), CORPS OF ENGINEERS SECTION 404 PERMITS, AND TVA SECTION 26A PERMITS TO DISPOSE OF WASTE MATERIALS.

### SUPPORT ACTIVITIES

- (101) MATERIALS AND STAGING AREAS SHALL NOT AFFECT ANY WATERS OF THE STATE/U.S. UNLESS THESE AREAS ARE SPECIFICALLY COVERED BY ENVIRONMENTAL PERMITS. OBTAINED SOLELY BY THE CONTRACTOR. THE CONTRACTOR SHALL REVIEW ALL EXISTING PERMITS TO ENSURE THAT WORK AT PERMITTED SITES DOES NOT EXCEED EXPIRATION DATES. IF WORK IS GOING TO BE CONTINUED AFTER EXPIRATION DATES, THE CONTRACTOR SHALL CONTACT THE TDOT PROJECT RESPONSIBLE PARTY TO COMMENCE PERMIT RENEWAL PROCESS.
- (102) IF OFFSITE BORROW AND WASTE AREAS BECOME NECESSARY DURING THE LIFE OF THE PROJECT, THIS SUPPORT ACTIVITY SHALL BE ADDRESSED PER THE TDOT WASTE AND BORROW MANUAL.
- (103) MATERIALS AND STAGING AREAS SHALL BE LOCATED IN NON-WETLAND AREAS AND ABOVE THE 100-YEAR, FEDERAL EMERGENCY MANAGEMENT AGENCY FLOODPLAIN.
- (104) IT WILL BE THE RESPONSIBILITY OF THE CONTRACTOR TO SUPPLY EPSC PLANS FOR THE MATERIAL AND STAGING AREAS TO THE ENVIRONMENTAL DIVISION COMPLIANCE AND FIELD SERVICES OFFICE FOR REVIEW.

### SPILL PREVENTION, MANAGEMENT AND NOTIFICATION

- (105) ALL ON-SITE VEHICLES WILL BE MONITORED FOR LEAKS AND RECEIVE REGULAR PREVENTIVE MAINTENANCE TO REDUCE THE CHANCE OF LEAKAGE AND SPILLS.
- (106) FOR ALL HAZARDOUS MATERIALS STORED ONSITE, THE MANUFACTURER'S RECOMMENDED METHODS FOR SPILL CLEAN UP SHALL BE CLEARLY POSTED. SITE PERSONNEL SHALL BE MADE AWARE OF THE PROCEDURES AND THE LOCATIONS OF THE INFORMATION AND CLEANUP SUPPLIES.
- (107) APPROPRIATE CLEANUP MATERIALS AND EQUIPMENT WILL BE MAINTAINED BY THE CONTRACTOR IN THE MATERIALS STORAGE AREA ONSITE AND UNDER COVER. SPILL RESPONSE EQUIPMENT SHALL BE INSPECTED AND MAINTAINED BY THE CONTRACTOR AS NECESSARY TO REPLACE ANY MATERIALS USED IN SPILL RESPONSE ACTIVITIES.
- (108) ALL SPILLS SHALL BE CLEANED IMMEDIATELY AFTER DISCOVERY AND THE MATERIALS DISPOSED OF PROPERLY. THE SPILL AREA SHALL BE KEPT WELL VENTILATED AND PERSONNEL WILL WEAR APPROPRIATE PROTECTIVE CLOTHING TO PREVENT INJURY FROM CONTACT WITH A HAZARDOUS SUBSTANCE.
- (109) THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE THE SPILL PREVENTION AND CLEANUP COORDINATOR. THE CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT THE SITE SUPERINTENDENT HAS HAD APPROPRIATE TRAINING FOR HAZARDOUS MATERIALS HANDLING, SPILL MANAGEMENT, AND CLEANUP.
- (110) IF AN OIL SHEEN IS OBSERVED ON SURFACE WATER (E.G. SETTLING PONDS, DETENTION PONDS, SWALES), ACTION SHALL BE TAKEN IMMEDIATELY TO REMOVE THE MATERIAL CAUSING THE SHEEN. THE CONTRACTOR SHALL USE APPROPRIATE MATERIALS TO CONTAIN AND ABSORB THE SPILL. THE SOURCE OF THE OIL SHEEN WILL ALSO BE IDENTIFIED AND REMOVED OR REPAIRED AS NECESSARY TO PREVENT FURTHER RELEASES.
- (111) FERTILIZERS SHALL BE APPLIED ONLY IN THE AMOUNTS SPECIFIED. ONCE APPLIED, FERTILIZERS SHALL BE WORKED INTO THE SOIL TO LIMIT THE EXPOSURE TO STORMWATER.
- (112) IF A SPILL OCCURS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR COMPLETING THE SPILL REPORTING FORM AND FOR REPORTING THE SPILL TO THE TDOT PROJECT RESPONSIBLE PARTY. ALL SPILLS MUST BE REPORTED TO THE APPROPRIATE AGENCY, AND MEASURES SHALL BE TAKEN IMMEDIATELY TO PREVENT THE POLLUTION OF WATERS OF THE STATE/U.S., INCLUDING GROUNDWATER, SHOULD A SPILL OCCUR.
- (113) WHERE A RELEASE CONTAINING A HAZARDOUS SUBSTANCE IN AN AMOUNT EQUAL TO OR IN EXCESS OF A REPORTABLE QUANTITY ESTABLISHED UNDER EITHER 40 CFR 117 OR 40 CFR 302 OCCURS DURING A 24 HOUR PERIOD, SEE THE LATEST TENNESSEE GENERAL PERMIT NO. TNRI00000 STORMWATER DISCHARGES FROM CONSTRUCTION ACTIVITIES SECTION 5.1 FOR REPORTING REQUIREMENTS.
- (114) CONTRACTOR'S BULK FUEL AND PETROLEUM PRODUCTS STORED ONSITE OR ADJACENT TO THE R.O.W. IN ABOVE GROUND STORAGE TANKS IN EXCESS OF 1,320 GALLONS SHALL HAVE SECONDARY CONTAINMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREPARING A SPILL PREVENTION CONTROL AND COUNTERMEASURE (SPCC) PLAN FOR THE BULK STORAGE AND BE SOLELY RESPONSIBLE FOR OBTAINING ANY NECESSARY LOCAL, STATE, AND FEDERAL PERMITS. THE SPCC PLAN AND/OR PERMITS SHALL BE KEPT ONSITE AND A COPY PROVIDED TO THE TDOT PROJECT RESPONSIBLE PARTY AT THE PRE-CONSTRUCTION MEETING.

### SPECIAL NOTES

#### DEMOLITION, REPAIR, OR REHABILITATION OF BRIDGES

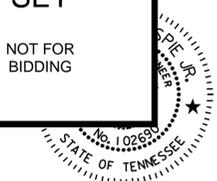
- (115) THE CONTRACTOR SHALL VERIFY THAT AN ASBESTOS SURVEY HAS BEEN COMPLETED PRIOR TO ANY DEMOLITION, REPAIR OR REHABILITATIONS ACTIVITIES (NOT INCLUDING ASPHALT MILLING OR OVERLAY).

- (116) ASBESTOS-CONTAINING MATERIALS (ACM) ABATEMENT IS THE RESPONSIBILITY OF THE CONTRACTOR AND SHALL BE COMPLETED PRIOR TO ANY DEMOLITION, REPAIR OR REHABILITATION OF BRIDGE(S). ABATEMENT SHOULD BE ACCOMPLISHED PER SP202ACM SPECIAL PROVISION REGARDING REMOVAL OF ASBESTOS-CONTAINING MATERIALS. STATE OF TENNESSEE ASBESTOS ACCREDITATION REQUIREMENTS (TCA 1200-01-20) MANDATE THAT ACM ABATEMENT WORK BE PERFORMED BY AN ACCREDITED FIRM (CONTRACTOR) USING ACCREDITED ABATEMENT WORKERS AND SUPERVISORS.
- (117) THE CONTRACTOR SHALL BE RESPONSIBLE FOR SUBMITTING A NOTICE TO THE TDEC, DIVISION OF AIR POLLUTION CONTROL TEN (10) DAYS IN ADVANCE OF ANY ACM ABATEMENT, DEMOLITION, OR MAJOR REPAIR INVOLVING THE REMOVAL/REPLACEMENT OF A STRUCTURAL COMPONENT.

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**UNOFFICIAL SET**

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ROADWAY  
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# ROADWAY SPECIAL NOTES (CONT'D)

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## ENVIRONMENTAL

(118) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION COMPREHENSIVE INSPECTION OFFICE SHALL BE INVITED TO ALL PRE-CONSTRUCTION MEETINGS.

## ECOLOGY

- (119) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION OR A DESIGNEE SHALL ADVISE THE CONTRACTOR DURING THE PRE-CONSTRUCTION MEETING WHEN ENVIRONMENTAL DIVISION PERSONNEL OR DESIGNATED CONSULTANT WILL NEED TO BE ON-SITE FOR WORK BEING DONE WHICH COULD AFFECT WATERS OF THE STATE/U.S. OR SPECIES.
- (120) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION OR A DESIGNEE SHALL ATTEND THE PRE-CONSTRUCTION MEETING FOR ALL PROJECTS WHICH HAVE THREATENED OR ENDANGERED SPECIES OR CRITICAL HABITAT PROXIMAL TO SCHEDULED WORK. THIS WILL PROVIDE THE OPPORTUNITY TO ENSURE THAT PERSONNEL INCLUDING THE CONTRACTOR'S PERSONNEL AND SUBCONTRACTORS ARE MADE AWARE OF THE NECESSARY PRECAUTIONS THAT MUST BE FOLLOWED.
- (121) ALL PROJECTS WITH LEGALLY PROTECTED SPECIES OR CRITICAL HABITAT IDENTIFIED SHALL HAVE MEASURES IN PLACE TO CONTAIN CONCRETE DUST, CEMENT DUST AND ALL OTHER MATERIALS. THESE MATERIALS ARE NOT ALLOWED TO ENTER WATERS OF THE STATE/U.S.

## PROJECT COMMITMENTS

(122) SEE PROJECT COMMITMENTS, SHEET 1B, FOR DETAILS RELATING TO SPECIAL ENVIRONMENTAL COMMITMENTS REQUIRED BY THIS PROJECT.

## STREAMS, WETLANDS AND BUFFER ZONES

- (123) FOR PROJECTS THAT DISCHARGE INTO KNOWN EXCEPTIONAL TENNESSEE WATERS OR WATERS IMPAIRED BY SILTATION, A 60 FOOT NATURAL RIPARIAN BUFFER ZONE ADJACENT TO AND ON BOTH SIDES OF THE RECEIVING STREAM WITH THIS DESIGNATION SHALL BE PRESERVED TO THE MAXIMUM EXTENT PRACTICABLE DURING CONSTRUCTION ACTIVITIES AT THE SITE. THE 60 FOOT CRITERION FOR THE WIDTH OF THE BUFFER ZONE CAN BE ESTABLISHED ON AN AVERAGE WIDTH BASIS AT A PROJECT, AS LONG AS THE MINIMUM WIDTH OF THE BUFFER ZONE IS MORE THAN 30 FEET AT ANY MEASURED LOCATION.
- (124) A 30 FOOT NATURAL RIPARIAN BUFFER ZONE ADJACENT TO AND ON BOTH SIDES OF THE RECEIVING STREAM SHALL BE PRESERVED TO THE MAXIMUM EXTENT PRACTICABLE DURING CONSTRUCTION ACTIVITIES AT THE SITE. THE 30 FOOT CRITERION FOR THE WIDTH OF THE BUFFER ZONE CAN BE ESTABLISHED ON AN AVERAGE WIDTH BASIS AT A PROJECT, AS LONG AS THE MINIMUM WIDTH OF THE BUFFER ZONE IS MORE THAN 15 FEET AT ANY MEASURED LOCATION. EVERY ATTEMPT SHALL BE MADE FOR CONSTRUCTION ACTIVITIES NOT TO TAKE PLACE WITHIN THE BUFFER ZONES.
- (125) BUFFER ZONES ARE NOT SEDIMENT CONTROL MEASURES AND MUST NOT BE RELIED UPON AS PRIMARY SEDIMENT CONTROL MEASURES. THE RIPARIAN BUFFER ZONE SHALL BE ESTABLISHED BETWEEN THE TOP OF THE STREAM BANK AND THE DISTURBED CONSTRUCTION AREA. EVERY ATTEMPT SHALL BE MADE FOR CONSTRUCTION ACTIVITIES NOT TO TAKE PLACE WITHIN THE BUFFER ZONES. BEST MANAGEMENT PRACTICES (BMPs) PROVIDING EQUIVALENT PROTECTION AS THE NATURAL RIPARIAN ZONE MAY BE USED. A JUSTIFICATION FOR USE AND DESIGN EQUIVALENCY SHALL BE DOCUMENTED WITHIN THE SWPPP. THE ENVIRONMENTAL AND ROADWAY DESIGN DIVISIONS SHALL REVIEW AND APPROVE THIS REVISION OF THE EPSC PLANS BEFORE DISTURBANCE OF THE SITE PROCEEDS, UNLESS PREVIOUSLY EXEMPT IN THE NPDES CONSTRUCTION GENERAL PERMIT. WHERE ISSUED, ARAP/401 REQUIREMENTS WILL PREVAIL IF IN CONFLICT WITH THESE BUFFER ZONE REQUIREMENTS.

## CLIFF AND BARN SWALLOW

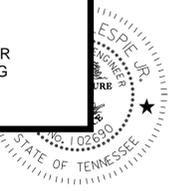
(126) CLIFF AND BARN SWALLOW: NO DISTURBANCE OF CLIFF AND BARN SWALLOW NESTS, EGGS, OR BIRDS (YOUNG OR ADULTS) IS PERMITTED DURING APRIL 15 THROUGH JULY 31. NEST REMOVAL OR DESTRUCTION AND THE IMPLEMENTATION OF MEASURES PREVENTING FUTURE NEST BUILDING (E.G., OBSTRUCTING A STRUCTURE USING NETTING) ARE PERMITTED DURING AUGUST 1 THROUGH APRIL 14.

EXCEPTIONS ARE AS FOLLOWS:

1. TDOT MAY REMOVE OR DESTROY NESTS AND PREVENT NEST BUILDING PROVIDED NO EGGS ARE PRESENT PRIOR TO APRIL 15. ABSENCE OF EGGS MUST BE DOCUMENTED USING APPROPRIATE MEANS FOR DETERMINATION, INCLUDING SITE VISITS AND PHOTOGRAPHS.
2. TDOT MAY REMOVE OR DESTROY NESTS PROVIDED NO BIRDS (YOUNG OR ADULTS) ARE PRESENT IN ANY NESTS PRIOR TO JULY 31. ABSENCE OF BIRDS MUST BE DOCUMENTED USING APPROPRIATE MEANS FOR DETERMINATION, INCLUDING SITE VISITS, PHOTOGRAPHS, AND OBSERVATIONS OF NO BIRDS USING NESTS.

## SPECIAL NOTE TO CONTRACTOR

(127) NO CONSTRUCTION EQUIPMENT SHALL ENTER STR-1 FOR VEGETATION REMOVAL.

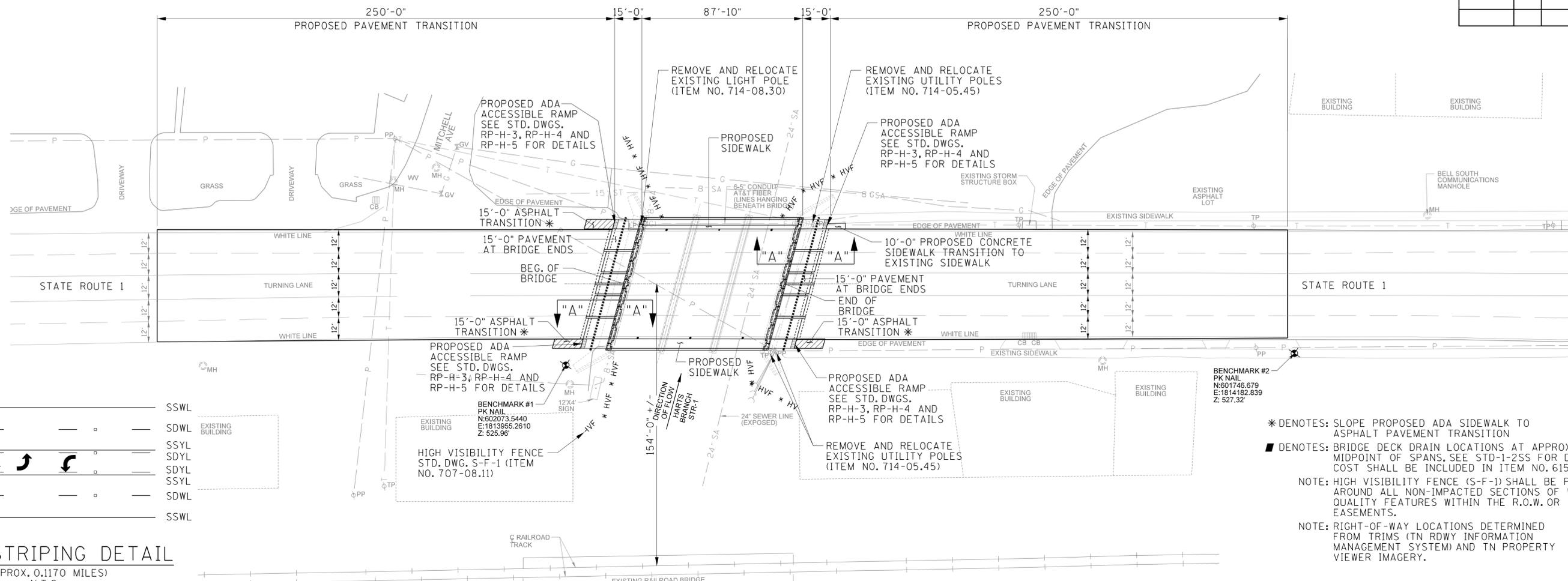


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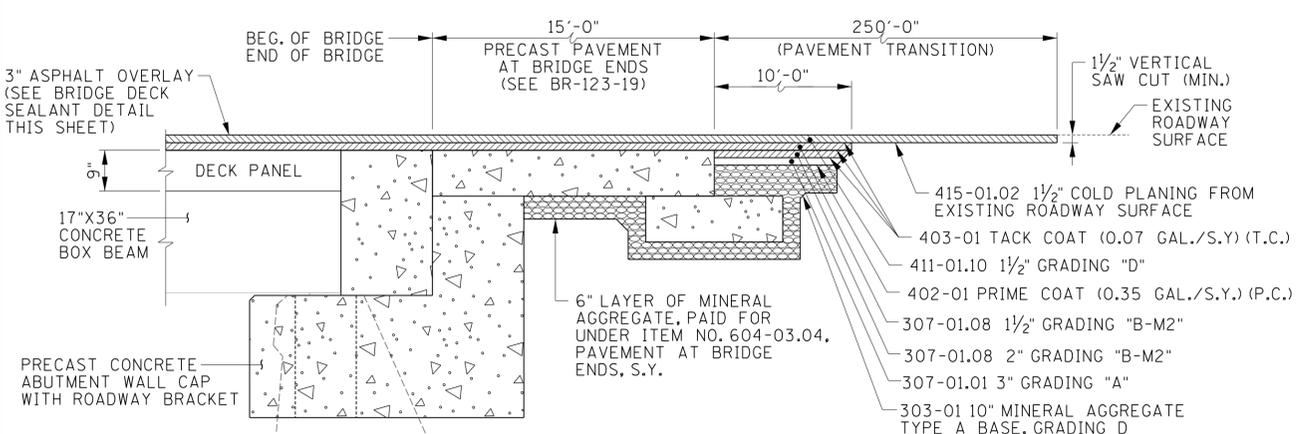


PLAN

**FINAL STRIPING DETAIL**

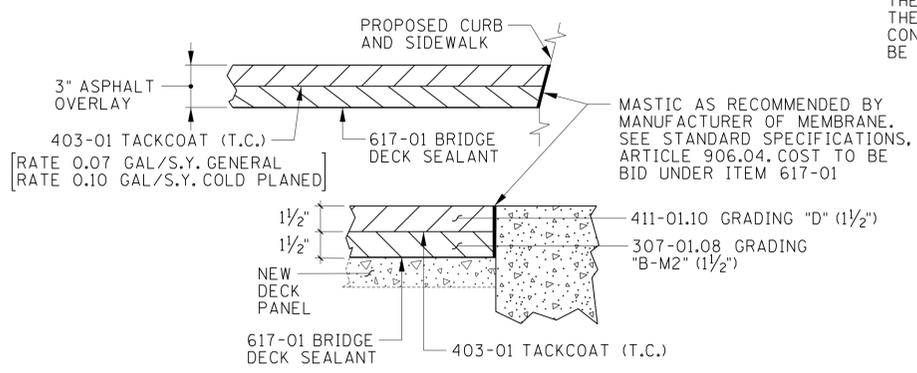
(APPROX. 0.1170 MILES)  
N.T.S.

NOTE: INCLUDES PROPOSED PAVEMENT TRANSITION, PAVEMENT AT BRIDGE ENDS AND BRIDGE DECK.



**SECTION "A-A"**

(P.A.B.E AND PAVEMENT TRANSITION)  
(AT ABUTMENT NO. 1 AND NO. 2)  
N.T.S.



**BRIDGE DECK SEALANT DETAIL**

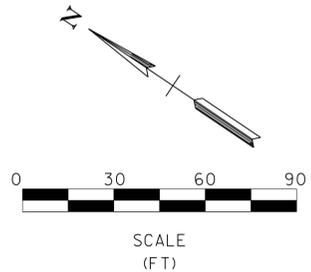
N.T.S.  
NOTE: SEE SECTIONS 617 AND 906.04 OF THE TENNESSEE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.

- \* DENOTES: SLOPE PROPOSED ADA SIDEWALK TO ASPHALT PAVEMENT TRANSITION
- DENOTES: BRIDGE DECK DRAIN LOCATIONS AT APPROXIMATE MIDPOINT OF SPANS, SEE STD-1-2SS FOR DETAILS. COST SHALL BE INCLUDED IN ITEM NO. 615-04.10
- NOTE: HIGH VISIBILITY FENCE (S-F-1) SHALL BE PLACED AROUND ALL NON-IMPACTED SECTIONS OF WATER QUALITY FEATURES WITHIN THE R.O.W. OR EASEMENTS.
- NOTE: RIGHT-OF-WAY LOCATIONS DETERMINED FROM TRIMS (TN RDWY INFORMATION MANAGEMENT SYSTEM) AND TN PROPERTY VIEWER IMAGERY.

**SPECIAL NOTE TO CONTRACTOR:**

CONTRACTOR SHALL USE EXTREME CARE AND TAKE ANY MEASURE NECESSARY TO ENSURE THAT NO DEBRIS IS DROPPED INTO THE STREAM. THIS SHALL BE ACCOMPLISHED BY THE USE OF BASKETS, NETTING, WRAPPING, WORK PLATFORM, OR OTHER SIMILARLY EFFECTIVE MEANS. ANY DEBRIS WHICH IS ALLOWED TO DROP ON THE BANKS BELOW THE BRIDGE SHALL BE REMOVED BY THE CONTRACTOR. COST OF REMOVING AND BE INCLUDED IN ITEMS BID ON.

**UNOFFICIAL SET**  
NOT FOR BIDDING



STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

BRIDGE NO. 75-SR1-4.76  
STATE ROUTE 1  
OVER HARTS BRANCH  
RUTHERFORD COUNTY  
2016

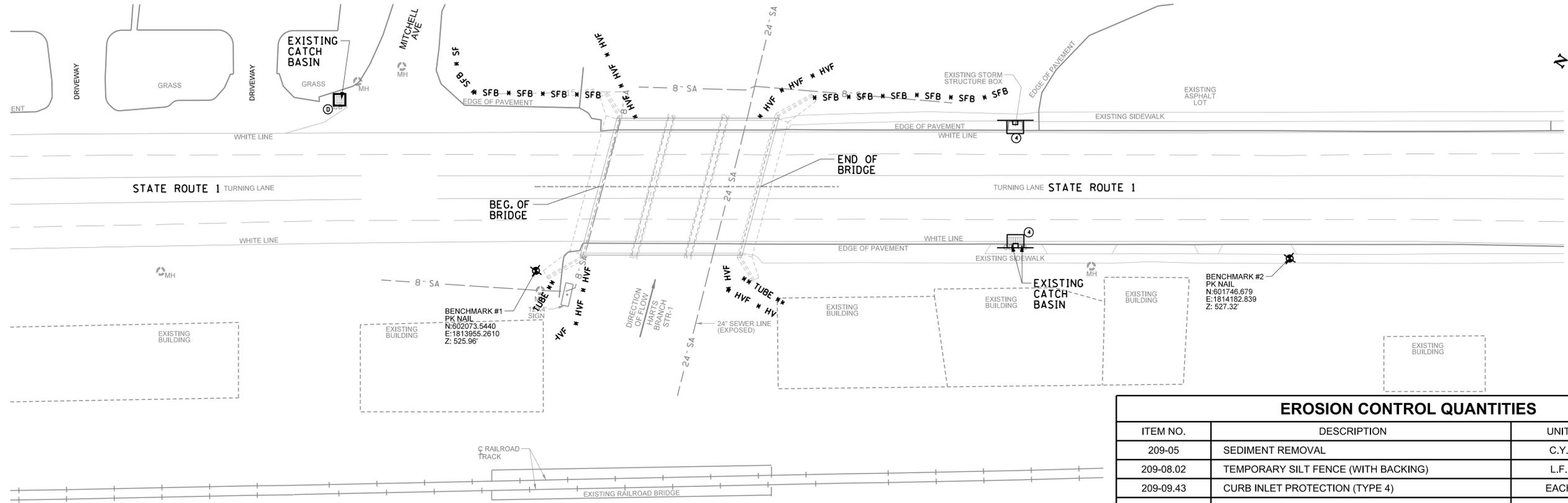
ROADWAY  
DETAILS

### SPECIAL NOTE TO CONTRACTOR:

CONTRACTOR SHALL USE EXTREME CARE AND TAKE ANY MEASURE NECESSARY TO ENSURE THAT NO DEBRIS IS DROPPED INTO THE WATER. THIS SHALL BE ACCOMPLISHED BY THE USE OF BASKETS, NETTING, WRAPPING, WORK PLATFORM, OR OTHER SIMILARLY EFFECTIVE MEANS. ANY DEBRIS WHICH IS ALLOWED TO DROP ON THE BANKS BELOW THE BRIDGE SHALL NOT BE ALLOWED TO ENTER THE WATER AND SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR. COST OF REMOVING AND DISPOSING OF DEBRIS SHALL BE INCLUDED IN ITEMS BID ON.

EPSC LEGEND		
•SFB•SFB•	SILT FENCE WITH WIRE BACKING (SFB)	EC-STR-3C
☐	CATCH BASIN PROTECTION (TYPE D)	EC-STR-19
•TUBE•TUBE•	SEDIMENT TUBE	EC-STR-37
☐	CURB INLET PROTECTION (TYPE 4)	EC-STR-39A
•HVF•HVF•	HIGH VISIBILITY FENCE	S-F-1

TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	2016	75002-4287-04	2J



### PLAN

AREA OF DISTURBANCE = 1,004 S.F. (0.023 ACRES)

- NOTE: RIGHT-OF-WAY LOCATIONS DETERMINED FROM TRIMS (TN RDWY INFORMATION MANAGEMENT SYSTEM) AND TN PROPERTY VIEWER IMAGERY.
- NOTE: ALL WORK AND EQUIPMENT TO REMAIN INSIDE EXISTING RIGHT OF WAY.
- NOTE: THE CONTRACTOR SHALL USE ANY MEASURE NECESSARY TO ENSURE THAT CONSTRUCTION EQUIPMENT WILL NOT ENTER ANY PORTION OF HARTS BRANCH AND THAT THE STREAM AND SURROUNDING VEGETATION WILL NOT BE DISTURBED AND ARE PROTECTED FROM SEDIMENT AND OTHER POLLUTANTS EXCEPT AT PERMITTED LOCATIONS.
- NOTE: SILT FENCE SHALL BE INSTALLED ON EXISTING GROUND CONTOURS WITH A 3' MINIMUM UPHILL TURN ON EACH END.
- NOTE: SEEDING (WITH MULCH) SHALL BE PLACED WITH A 4' MINIMUM WIDTH AFTER REMOVAL OF THE SILT FENCE.

- NOTE: SEE SHEET NO. 2C (ESTIMATED ROADWAY QUANTITIES) FOR FOOTNOTES TO EACH ITEM.
- NOTE: THE CONTRACTOR SHALL TAKE APPROPRIATE MEASURES BELOW THE BRIDGE DURING THE REMOVAL OF CONCRETE FROM SUBSTRUCTURES AND DECK TO PREVENT ANY DEBRIS FROM FALLING INTO THE WATER. COST FOR CONTAINMENT WILL NOT BE PAID DIRECTLY BUT SHALL BE INCLUDED IN ITEMS BID ON.
- NOTE: THE CONTRACTOR SHALL REMOVE ANY DEBRIS THAT FALLS ONTO THE AREAS BELOW THE BRIDGE, AND ANY PAINT CHIPS SHALL BE HANDLED AND DISPOSED OF PER THE SPECIFICATIONS SET FORTH IN THE GENERAL NOTES.
- NOTE: HIGH VISIBILITY FENCE (S-F-1) SHALL BE PLACED AROUND ALL NON-IMPACTED SECTIONS OF WATER QUALITY FEATURES WITHIN THE R.O.W. OR EASEMENTS.

EROSION CONTROL QUANTITIES			
ITEM NO.	DESCRIPTION	UNIT	QUANTITY
209-05	SEDIMENT REMOVAL	C.Y.	5
209-08.02	TEMPORARY SILT FENCE (WITH BACKING)	L.F.	220
209-09.43	CURB INLET PROTECTION (TYPE 4)	EACH	2
209-40.33	CATCH BASIN PROTECTION (TYPE D)	EACH	1
707-08.11	HIGH VISIBILITY CONSTRUCTION FENCE	L.F.	200
740-11.01	TEMPORARY SEDIMENT TUBE (8 INCH) (STRAW WATTLES)	L.F.	50
801-01	SEEDING (WITH MULCH)	UNIT	1
801-02	SEEDING (WITHOUT MULCH)	UNIT	1
801-03	WATER (SEEDING AND SODDING)		

**UNOFFICIAL SET**  
NOT FOR BIDDING



BRIDGE NO. 75-SR1-4.76  
STATE ROUTE 1  
OVER HARTS BRANCH  
RUTHERFORD COUNTY  
2016

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

**EROSION CONTROL PLAN AND SPECIAL NOTES**

### TRAFFIC CONTROL QUANTITIES

ITEM NO.	DESCRIPTION	UNIT	QUANTITY	SIZE	M.U.T.C.D. NO.	REMARKS
705-08.51	PORTABLE IMPACT ATTENUATOR (NCHRP 350, TL-3)	EACH	2			
712-01	TRAFFIC CONTROL	LS	1			
712-02.02	INTERCONNECTED PORTABLE BARRIER RAIL	L.F.	830			
712-04.01	FLEXIBLE DRUMS (CHANNELIZING)	EACH	135			
712-04.50	PORTABLE BARRIER RAIL DELINEATOR	EACH	24			
712-05.01	WARNING LIGHTS (TYPE A)	EACH	2			
712-05.03	WARNING LIGHTS (TYPE C)	EACH	51			
712-06	SIGNS (CONSTRUCTION)	S.F.	587			
712-07.03	TEMPORARY BARRICADES (TYPE III)	L.F.	40			
712-08.03	ARROW BOARDS (TYPE C)	EACH	3			
712-09.01	REMOVABLE PAVEMENT MARKING LINE	L.F.	10,150			
712-09.04	REMOVABLE PAVEMENT MARKING (STOP LINE)	L.F.	70			
712-09.05	REMOVABLE PAVEMENT MARKING (ARROW)	EACH	23			
713-16.01	CHANGEABLE MESSAGE SIGN	EACH	2			
713-16.20	SIGN (OM3-L)	EACH	2			
713-16.21	SIGN (OM3-R)	EACH	2			
716-01.21	SNWPLWBLE PVMT MRKRS (BI-DIR) (1 COLOR)	EACH	16			
716-01.22	SNWPLWBLE PVMT MRKRS (MONO-DIR) (1 COLOR)	EACH	16			
716-02.06	PLASTIC PAVEMENT MARKING (TURN LANE ARROW)	EACH	4			
716-12.01	ENHANCED FLATLINE THERMO PVMT MRKNG (4" LINE)	L.M.	0.23			
716-13.01	SPRAY THERMO PVMT MRKNG (60 MIL) (4" LINE)	L.M.	0.37			
717-01	MOBILIZATION	LS	1			

NOTE: SEE SHEET NO. 2C (ESTIMATED ROADWAY QUANTITIES) FOR FOOTNOTES TO EACH ITEM.

### ITEM NO. 712-06 SIGNS (CONSTRUCTION)

DESCRIPTION	QUANTITY	ITEM NO. 712-06 (S.F.)	SIZE	M.U.T.C.D. NO.	REMARKS
END ROAD WORK	10	80	48" x 24"	G20-2	
NO LEFT TURN	3	12	24" x 24"	R3-2	
SIDEWALK CLOSED	2	8	30" x 18"	R9-9	
ROAD CLOSED	1	10	48" x 30"	R11-2	
ROAD CLOSED TO THRU TRAFFIC	1	13	60" x 30"	R11-4	
LANE SHIFT RIGHT	1	16	48" x 48"	W1-4AR	
REVERSE CURVE 2 LANES	2	32	48" x 48"	W1-4BR	
LEFT LANE CLOSURE	1	16	48" x 48"	W4-2L	
RIGHT LANE CLOSURE	2	32	48" x 48"	W4-2R	
ROAD NARROWS	2	32	48" x 48"	W5-1	
ROAD WORK AHEAD	9	144	48" x 48"	W20-1	
ROAD WORK 1 MILE	2	32	48" x 48"	W20-1	
ROAD WORK 1500 FT	2	32	48" x 48"	W20-1	
CENTER LANE CLOSED 1000 FT	2	32	48" x 48"	W20-5	
CENTER LANE CLOSED 500 FT	2	32	48" x 48"	W20-5	
LEFT LANE CLOSED 1/2 MILE	1	16	48" x 48"	W20-5L	
LEFT LANE CLOSED 1500 FT	1	16	48" x 48"	W20-5L	
RIGHT TWO LANES CLOSED 1/2 MILE	1	16	48" x 48"	W20-5R	
RIGHT TWO LANES CLOSED 1500 FT	1	16	48" x 48"	W20-5R	
<b>TOTAL</b>		<b>587</b>			

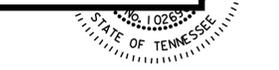
### TRAFFIC CONTROL SPECIAL NOTES

- (1) THESE TRAFFIC CONTROL PLANS DO NOT RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY OF INSTALLING TRAFFIC CONTROL DEVICES IN ACCORDANCE WITH THE CURRENT EDITION OF THE "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES."
- (2) EACH PHASE OF THE SUGGESTED SEQUENCE OF CONSTRUCTION FOR WHICH THESE TRAFFIC CONTROL PLANS WERE DESIGNED, REQUIRES THE TIMELY COMPLETION OF THE PRECEDING PHASE. ANY VARIATIONS IN THE PROPOSED PHASING SHALL REQUIRE A REVIEW AND APPROVAL OF THE SIGNING AND TRAFFIC CONTROL DEVICES BY THE ENGINEER.
- (3) THE CONTRACTOR IS REQUIRED TO PROVIDE LANE SHIFTS WHERE NECESSARY TO ROUTE TRAFFIC AROUND CONSTRUCTION.
- (4) NO TRAFFIC SHALL BE DETOURED OR ROADWAY CLOSED, ABANDONED OR REMOVED WITHOUT THE PRIOR APPROVAL OF THE ENGINEER.
- (5) CONSTRUCTION SIGNING SHOWN IN THESE PLANS IS TO SERVE AS A GUIDE ONLY. OTHER SIGNS MAY BE REQUIRED DURING VARIOUS PHASES OF CONSTRUCTION.
- (6) PERMANENT SIGNS AND PERMANENT PAVEMENT MARKINGS SHALL BE IN PLACE BEFORE COMPLETED ROADWAYS ARE OPEN TO TRAFFIC.
- (7) TYPE "C" WARNING LIGHTS SHALL BE USED ON ALL CHANNELIZING DRUMS IN TAPERS ON STATE ROUTE 1.
- (8) THE CONTRACTOR SHALL MAINTAIN ALL TRAFFIC CONTROL DEVICES IN PROPER CONDITION THROUGHOUT THE DURATION OF THE PROJECT.
- (9) EXISTING SIGNS THAT CONFLICT WITH TEMPORARY TRAFFIC CONTROL SIGNING SHALL BE COVERED OR REMOVED AND STOCKPILED AS DIRECTED BY THE ENGINEER.
- (10) THE CONTRACTOR WILL BE REQUIRED TO HAVE A RESPONSIBLE PERSON ON CALL (WITHIN ONE HOUR), AT ALL TIMES, FOR TRAFFIC CONTROL DURING THE CONSTRUCTION OF THIS PROJECT.
- (11) AT ALL TIMES THE ROADWAY MUST BE MAINTAINED IN A CONDITION TO ALLOW THE PASSAGE OF EMERGENCY VEHICLES.
- (12) FOR TRAFFIC CONTROL DETAILS, REFER TO STD. DWG. NOS. T-WZ-10, T-WZ-11, T-WZ-14, T-WZ-16 AND T-WZ-40 THRU T-WZ-42 THROUGHOUT THE DURATION OF THE PROJECT.
- (13) IF ADDITIONAL SIGNS ARE DEEMED NECESSARY BY THE ENGINEER, THEY SHALL BE FURNISHED AND INSTALLED AT THE BID PRICE FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION), S.F.
- (14) ALL LOCAL EMERGENCY AGENCIES AND RESIDENCES WITHIN THE IMMEDIATE AREA PROJECT AREA SHALL BE NOTIFIED NOT LESS THAN 48 HOURS IN ADVANCE OF BRIDGE CONSTRUCTION WHICH MAY AFFECT ACCESS TO THESE AREAS.
- (15) NO LESS THAN SEVEN (7) DAYS PRIOR TO THE CLOSURE OF LANES, THE CONTRACTOR SHALL NOTIFY THE FOLLOWING INDIVIDUALS OR AGENCIES COMPLETELY DESCRIBING THE AFFECTED ROADS AND THE APPROXIMATE DURATION OF THE CONSTRUCTION: THESE PARTIES INCLUDE, BUT ARE NOT LIMITED TO: (1) RUTHERFORD COUNTY SHERIFF'S DEPARTMENT, (2) RUTHERFORD COUNTY FIRE DEPARTMENT, (3) LOCAL AMBULANCE SERVICE, (4) RUTHERFORD COUNTY SCHOOL SUPERINTENDENT, (5) LOCAL POST OFFICE, (6) TDOT REGION 3 TRAFFIC ENGINEERING OFFICE.
- (16) BEFORE OPENING THE LANE SHIFTS TO TRAFFIC, THE TRANSITIONAL MARKINGS ON THE EXISTING ROADWAY MUST BE IN PLACE. THESE MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 712-09.01, REMOVABLE PAVEMENT MARKING LINE, LIN. FT. ALL EXISTING MARKINGS IN THE AREA OF THESE TRANSITIONAL MARKINGS SHALL BE OBLITERATED TO ELIMINATE CONFLICTING MARKINGS. REMOVAL OF THE EXISTING CONFLICTING MARKINGS WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COST WILL BE INCLUDED IN ITEM NO. 712-01, TRAFFIC CONTROL, LS.
- (17) ALL LANE CLOSURES SHALL BE APPROVED BY THE ENGINEER. STATE ROUTE 1 PHASED ROAD CLOSURE WILL BE RESTRICTED TO WEEKENDS ONLY, BETWEEN 9:00 P.M. FRIDAY TO 5:00 A.M. MONDAY, EXCLUDING HOLIDAY WEEKENDS. BRIDGE CONSTRUCTION SHALL BE COMPLETED AS A CONTINUOUS OPERATION ON THE WEEKENDS.
- (18) THE CONTRACTOR WILL BE ALLOWED 4 WEEKENDS FOR PHASED SUPERSTRUCTURE REMOVAL AND RECONSTRUCTION OF THE BRIDGE. ONE WEEKEND SHALL BE USED FOR INSTALLING BRIDGE PEDESTRIAN RAIL AND NEW CONCRETE SIDEWALKS WITH ADA RAMPS. ONE WEEKEND SHALL BE USED FOR DECK SEAL MEMBRANE, PAVING AND STRIPING. A WEEKEND IS DEFINED AS BETWEEN FRIDAY NIGHT AT 9:00 PM AND MONDAY AT 5:00 AM.

TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	2016	75002-4287-04	2K

**UNOFFICIAL SET**

NOT FOR BIDDING



STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

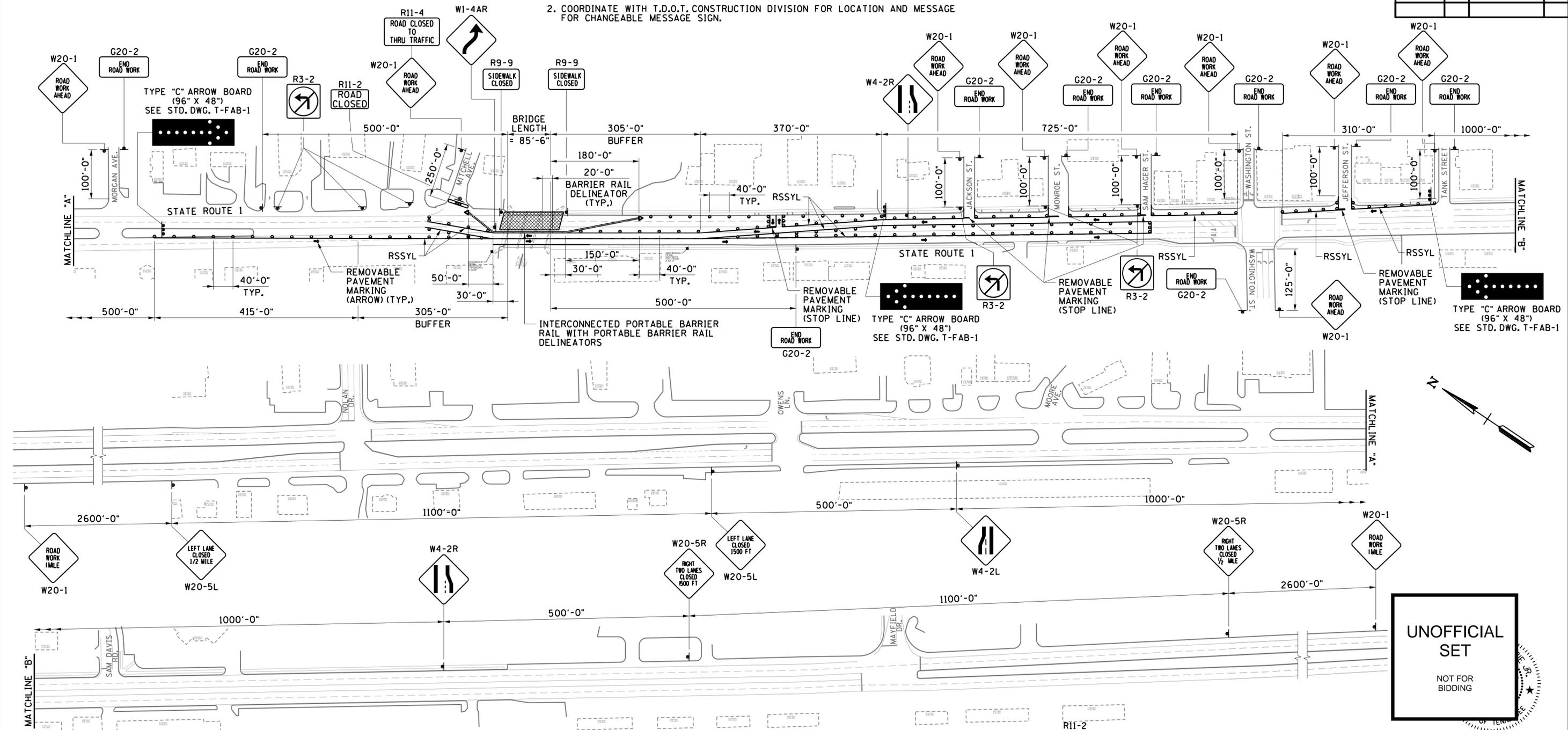
BRIDGE NO. 75-SR1-4.76  
STATE ROUTE 1  
OVER HARTS BRANCH  
RUTHERFORD COUNTY  
2016

TRAFFIC CONTROL  
SPECIAL NOTES  
AND  
QUANTITIES

**SPECIAL NOTES**

1. CONTRACTOR SHALL REMOVE OR COVER ANY EXISTING STRIPING THAT CONFLICTS WITH TEMPORARY TRAFFIC CONTROL STRIPING. CONTRACTOR TO REPLACE STRIPING TO PRECONSTRUCTION DISPOSITION PRIOR TO OPENING ROAD TO NORMAL TRAFFIC.
2. COORDINATE WITH T.D.O.T. CONSTRUCTION DIVISION FOR LOCATION AND MESSAGE FOR CHANGEABLE MESSAGE SIGN.

TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	2016	75002-4287-04	2L

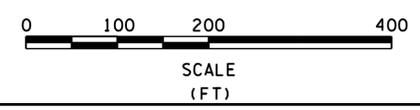


**TRAFFIC CONTROL LEGEND**

	FLEXIBLE DRUM (CHANNELIZING)		WORK ZONE
	SIGN (CONSTRUCTION)		DIRECTION OF TRAVEL (REMOVABLE PAVEMENT MARKING - ARROW)
	INTERCONNECTED PORTABLE BARRIER (WITH BARRIER RAIL DELINEATORS)		FLASHING YELLOW ARROW BOARD/CHANGEABLE MESSAGE SIGN
	ATTENUATOR (SEE STANDARD DRAWINGS)		EXISTING BUILDING
	REMOVABLE SINGLE SOLID WHITE LINE		
	REMOVABLE SINGLE SOLID YELLOW LINE		

**PHASE I TRAFFIC CONTROL PLAN**

(WEEKEND 1 - PHASE I CONSTRUCTION)



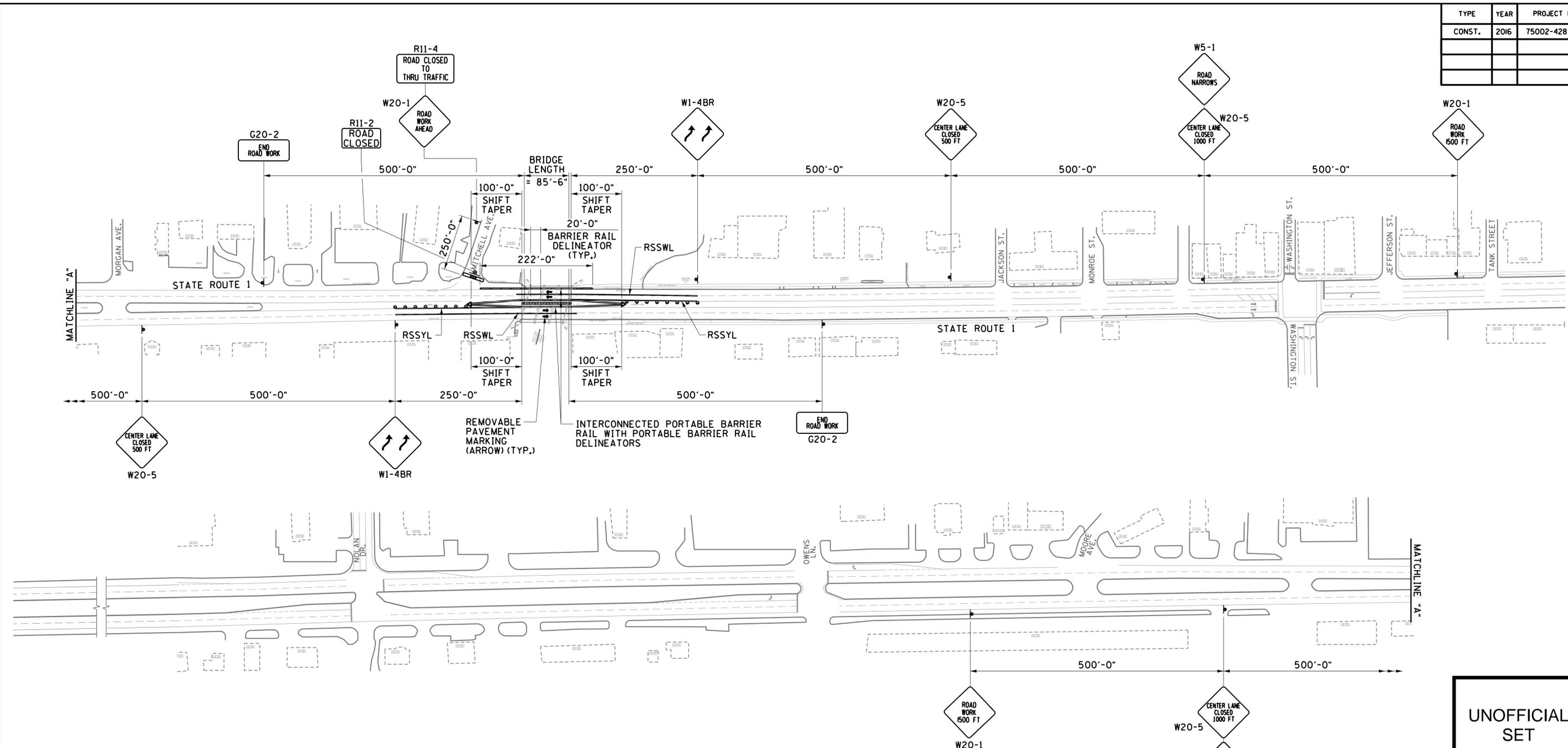
BRIDGE NO. 75-SR1-4.76  
STATE ROUTE 1  
OVER HARTS BRANCH  
RUTHERFORD COUNTY  
2016

**UNOFFICIAL SET**  
NOT FOR BIDDING

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLAN - PHASE I  
(WEEKEND 1)  
(STATE ROUTE 1)

TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	2016	75002-4287-04	2M



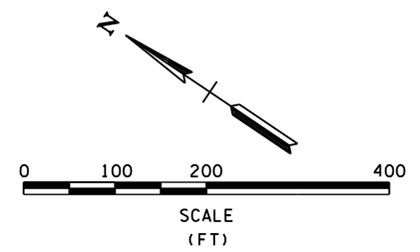
**INTERMEDIATE TRAFFIC CONTROL PLAN**  
 (TIME PERIOD BETWEEN PHASE I AND PHASE II - NO MAJOR BRIDGE CONSTRUCTION)

**UNOFFICIAL SET**  
 NOT FOR BIDDING

	FLEXIBLE DRUM (CHANNELIZING)		WORK ZONE
	SIGN (CONSTRUCTION)		DIRECTION OF TRAVEL (REMOVABLE PAVEMENT MARKING - ARROW)
	INTERCONNECTED PORTABLE BARRIER (WITH BARRIER RAIL DELINEATORS)		FLASHING YELLOW ARROW BOARD/CHANGEABLE MESSAGE SIGN
	ATTENUATOR (SEE STANDARD DRAWINGS)		EXISTING BUILDING
	REMOVABLE SINGLE SOLID WHITE LINE		
	REMOVABLE SINGLE SOLID YELLOW LINE		

**SPECIAL NOTES**

- CONTRACTOR SHALL REMOVE OR COVER ANY EXISTING STRIPING THAT CONFLICTS WITH TEMPORARY TRAFFIC CONTROL STRIPING. CONTRACTOR TO REPLACE STRIPING TO PRECONSTRUCTION DISPOSITION PRIOR TO OPENING ROAD TO NORMAL TRAFFIC.
- COORDINATE WITH T.D.O.T. CONSTRUCTION DIVISION FOR LOCATION AND MESSAGE FOR CHANGEABLE MESSAGE SIGN.



BRIDGE NO. 75-SR1-4.76  
 STATE ROUTE 1  
 OVER HARTS BRANCH  
 RUTHERFORD COUNTY  
 2016

STATE OF TENNESSEE  
 DEPARTMENT OF TRANSPORTATION

**TRAFFIC CONTROL PLAN - INTERMEDIATE (BETWEEN PHASE I AND PHASE II) (STATE ROUTE 1)**



**REQUIRED LOCATION REQUEST INFORMATION**

NAME OF CALLER  
 TELEPHONE NUMBER  
 BEST TIME TO CALL  
 COUNTY  
 TOWN  
 STREET ADDRESS  
 START DATE START TIME  
 TYPE OF WORK  
 BLASTING ?  
 WORK BEING DONE BY  
 WORK BEING DONE FOR

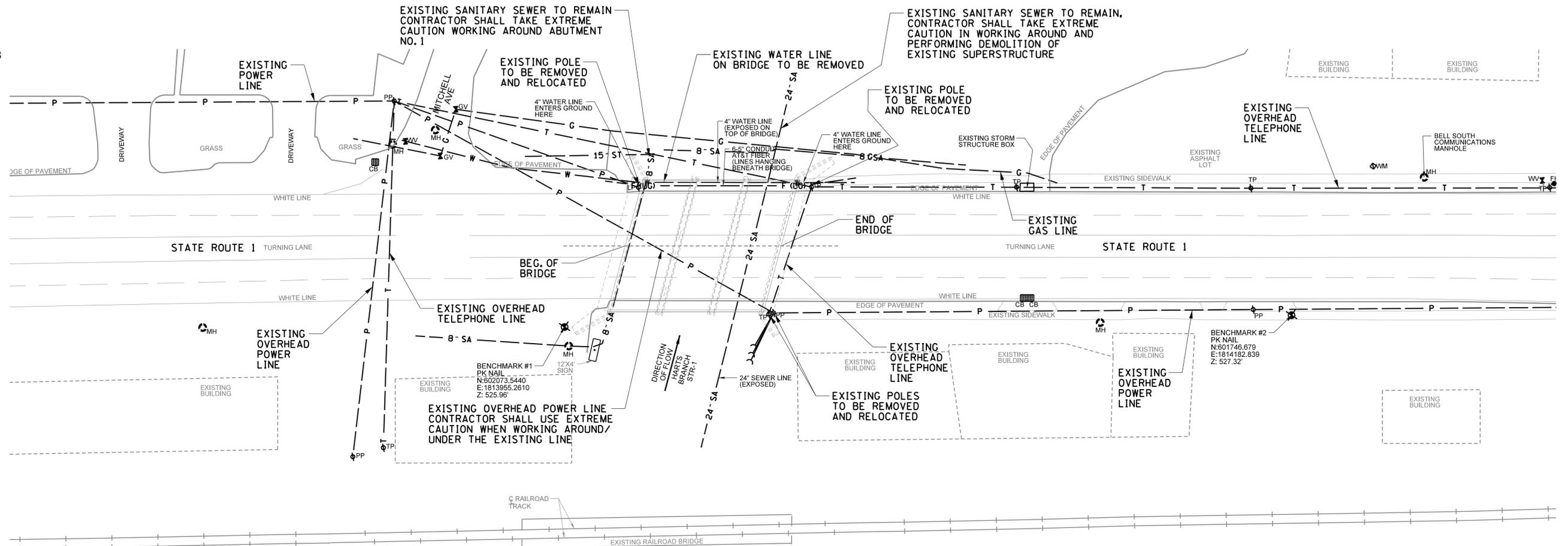
CALL THREE WORKING DAYS BEFORE YOU DIG  
 1-800-351-1111  
 IN NASHVILLE : 366-1987  
  
 SYSTEM INCORPORATED

**UTILITY CONTACTS**

ATMOS ENERGY (UNITED CITIES GAS) 334 W. LOKEY AVENUE MURFREESBORO, TN 37130 615-890-6749	AT&T 333 COMMERCE ST 23C142 NASHVILLE, TN 37201 DAVID HUFFAKER 615-214-4871	COMCAST - RUTHERFORD CO. 660 MAINSTREAM DRIVE NASHVILLE, TN 37228 615-244-5900	CONSOLIDATED UTILITY DISTRICT 709 NEW SALEM HWY. MURFREESBORO, TN 37129 MARK ELGIN - MAINTENANCE 615-225-3305	TOWN OF SMYRNA-SMYRNA UTILITIES 315 SOUTH LOWRY STREET SMYRNA, TN 37167 PAUL WILSON - DIRECTOR OF UTILITIES 615-459-9742 EXT. 2111
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PORTIONS OF THIS DRAWING DEPICTS EXISTING CONDITIONS. LIMITS OF PROPOSED BRIDGE CONSTRUCTION MAY VARY FROM EXISTING CONDITIONS.

TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	2016	75002-4287-04	2P



**PLAN**

**LEGEND**

**MAPPING SYMBOLS AND CODES**

	○ IRON PIPE OLD (IP(O))	
	⊗ IRON SPIKE OLD (IS(O))	
	⊙ LIGHT STANDARD METAL/WOOD (LS/LP)	
	□ MAIL BOX (MB)	
	○ MAN HOLE (MH)	
	□ MONUMENT CONCRETE OLD (MON(O))	
	□ MONUMENT CONCRETE NEW (MON(N))	
	○ P.K. NAIL OLD (PK(O))	
	● P.K. NAIL NEW (PK(N))	
	○ LIGHT POLE WITH POWER (PP)	
	○ LIGHT POLE WITH TELEPHONE (T)	
	○ POWER MAN HOLE (PMH)	
	○ POWER POLE (P)	
	○ POWER AND TELEPHONE POLE (P/T)	
○ IRON PIN NEW (IR(N))	□ RIGHT-OF-WAY MONUMENT (RWM)	
○ IRON PIN OLD (IR(O))	⊗ SPRINKLER HEAD/VALVE (SPH, SPV)	

**LINE STYLES**

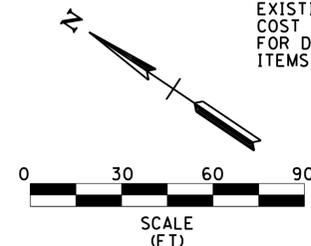
CABLE LINE	— C —
CENTER LINE	— P —
CONCRETE AREA	
EDGE OF WOODS LINE	
FENCE LINE	— X — X — X —
GAS LINE	— X* — G —
OVERHEAD POWER LINE	— P —

OVERHEAD TELEPHONE LINE	— T —
OVERHEAD POWER AND TELEPHONE LINE	— P — T —
PROPERTY LINE	— P —
SANITARY SEWER	— X* — SA —
STORM WATER	— X* — ST —
FIBER OPTIC CABLE	— F —
UNDERGROUND FIBER OPTIC CABLE	— F(UG) —
UNDERGROUND POWER LINE	— P(UG) —
UNDERGROUND POWER AND TELEPHONE LINE	— UG — P — T —
UNDERGROUND TELEPHONE LINE	— T(UG) —
WATER LINE	— X* — W —

NOTE: UTILITY INFORMATION SHOWN WAS DERIVED FROM FIELD MEASUREMENT AND OBSERVATION. THE CONTRACTOR SHALL FIELD VERIFY ALL EXISTING UTILITY LOCATIONS AND SERVICES PRIOR TO ROADWAY AND BRIDGE CONSTRUCTION.

NOTE: CONTRACTOR SHALL USE EXTREME CAUTION WHEN OPERATING IN ANY AREA WHERE UTILITIES ARE PRESENT AND AVOID WORKING IN THESE AREAS AS MUCH AS PRACTICALLY POSSIBLE. DAMAGES TO ANY UTILITIES ARE TO BE REPAIRED BY THE CONTRACTOR AT HIS OWN EXPENSE.

NOTE: THE CONTRACTOR SHALL VERIFY THE LOCATION OF EXISTING POLES, GUY WIRES AND ROADWAY SIGNS PRIOR TO COMMENCEMENT OF CONSTRUCTION. THE CONTRACTOR SHALL COORDINATE THE MOVEMENT OF SUCH SIGNS, POLES AND GUY WIRES WITH THEIR RESPECTIVE OWNERS. THE CONTRACTOR SHALL RE-SET EXISTING SIGNAGE IN ITS ORIGINAL LOCATION. THE COST OF RE-SETTING SIGNAGE WILL NOT BE PAID FOR DIRECTLY BUT WILL BE INCLUDED IN OTHER ITEMS OF CONSTRUCTION.



BRIDGE NO. 75-SR1-4.76  
 STATE ROUTE 1  
 OVER HARTS BRANCH  
 RUTHERFORD COUNTY  
 2016

**UNOFFICIAL SET**  
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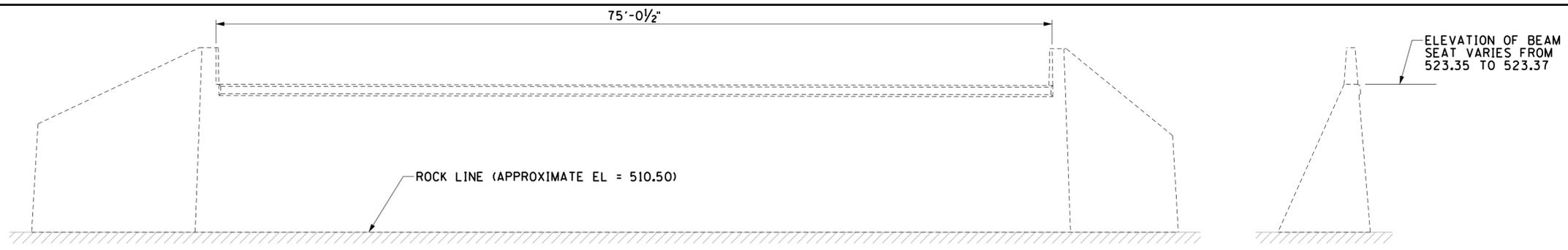
STATE OF TENNESSEE  
 DEPARTMENT OF TRANSPORTATION

UTILITIES

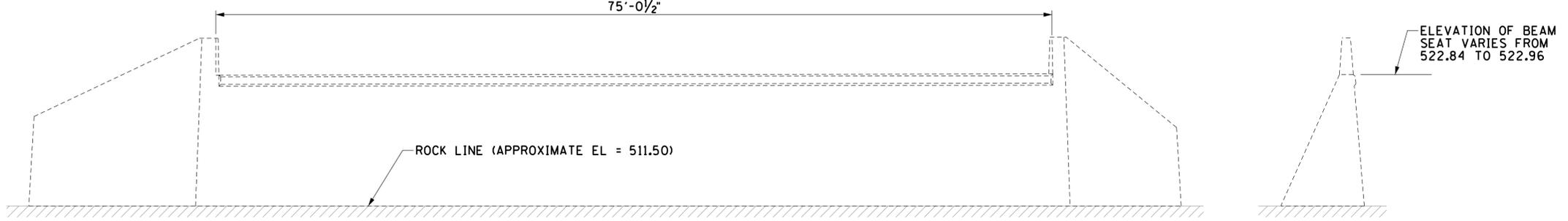




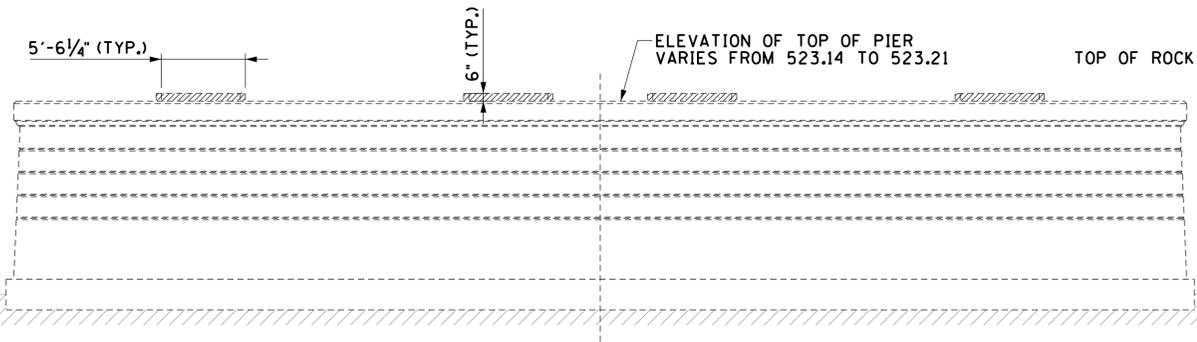




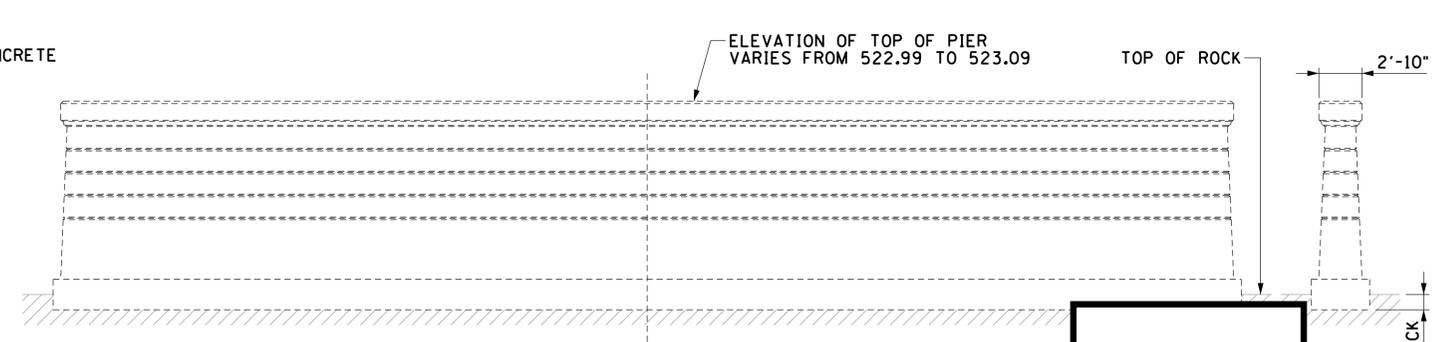
**ELEVATION - ABUTMENT NO. 1**  
(LOOKING BACK ON SURVEY)



**ELEVATION - ABUTMENT NO. 2**  
(LOOKING FORWARD ON SURVEY)



**ELEVATION - PIER NO. 1**



**ELEVATION - PIER NO. 2**

CONST. NO.			
PROJECT NO.	YEAR	SHEET NO.	
75002-4287-04	2016		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

- DENOTES: AREAS OF CONCRETE TO BE REMOVED IN PHASES AND TO BE INCLUDED IN ITEM NO. 202-04.01, REMOVAL OF STRUCTURE (SEE BR-123-05 AND BR-123-06 FOR ADDITIONAL INFORMATION ON PHASING).
- NOTE: REINFORCING STEEL THAT DOES NOT IMPEDE OR PROTRUDE BEYOND NEW CONSTRUCTION IS TO REMAIN. CONTRACTOR SHALL TAKE EXTREME CARE NOT TO DAMAGE THIS REINFORCING STEEL DURING CONCRETE REMOVAL. EXISTING STEEL TO BE INCORPORATED INTO NEW CONSTRUCTION SHALL BE COMPLETELY CLEANED BEFORE POURING NEW ABUTMENTS OR PIER.
- NOTE: THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE STABILITY OF THE STRUCTURE DURING DEMOLITION. DESIGN CALCULATIONS AND DETAILS OF TEMPORARY SUPPORT SYSTEM OR FALSEWORK REQUIRED SHALL BE SUBMITTED TO THE ENGINEER FOR REVIEW AND SHALL MEET WITH THE FULL SATISFACTION OF THE ENGINEER BEFORE ANY DEMOLITION IS BEGUN. COST OF STABILIZING THE STRUCTURE SHALL BE INCLUDED IN ITEM NO. 602-10.05, BRACING REPAIRS, L.S.
- NOTE: 1" DEEP SAW CUTS SHALL BE MADE ALONG THE LINE OF REMOVAL PRIOR TO REMOVING DEMOLITION AREA.
- NOTE: PRIOR TO DEMOLITION, THE CONTRACTOR SHALL FIELD VERIFY DIMENSIONS AND ELEVATIONS OF ABUTMENTS AND PIERS.

DESIGNED BY D. THOMPSON/D. KEATON DATE \_\_\_\_\_  
 DRAWN BY ANGELA MOORE DATE \_\_\_\_\_  
 SUPERVISED BY DARRELL JAMES DATE \_\_\_\_\_  
 CHECKED BY JAMIE GILLESPIE DATE \_\_\_\_\_



**UNOFFICIAL SET**  
 NOT FOR BIDDING

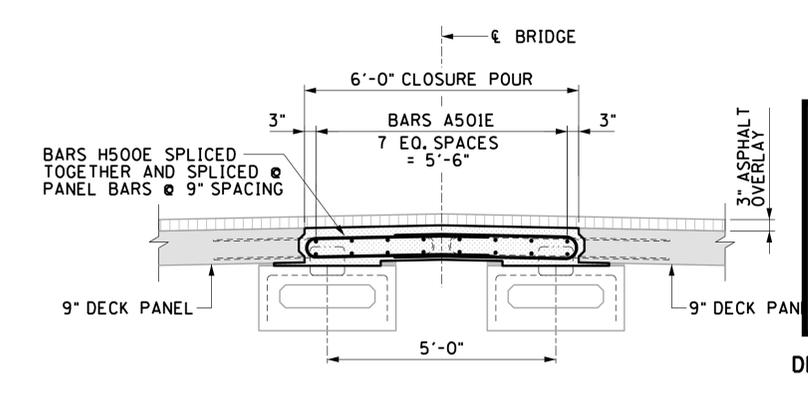
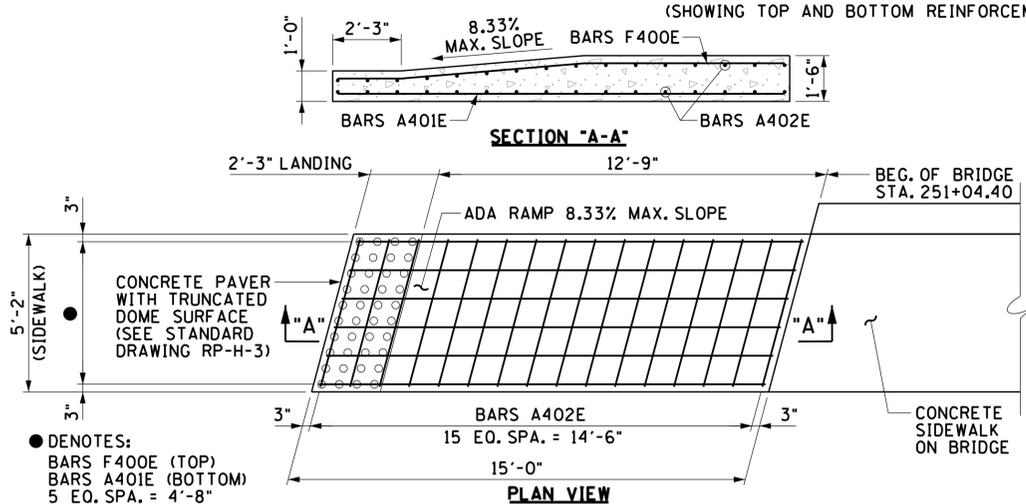
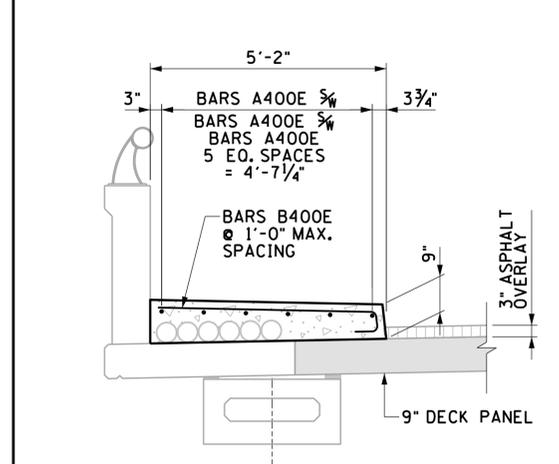
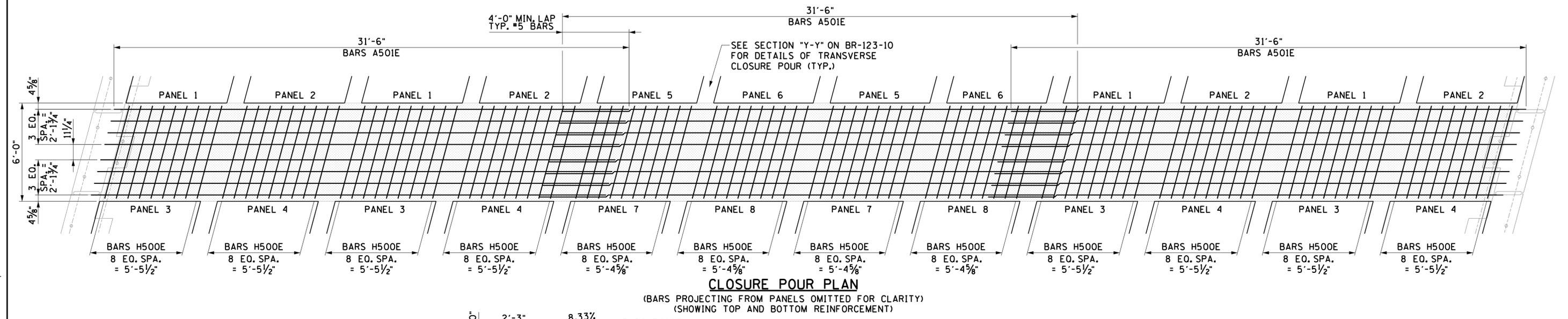
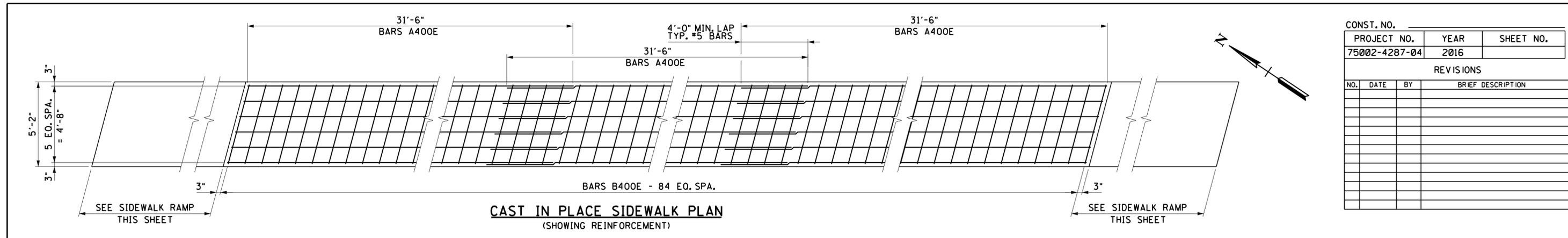
STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

DEMOLITION DETAILS

BRIDGE NO. 75-SR1-4.76  
STATE ROUTE 1  
OVER HARTS BRANCH  
RUTHERFORD COUNTY  
2016



CONST. NO.			
PROJECT NO.	YEAR	SHEET NO.	
75002-4287-04	2016		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



**UNOFFICIAL SET**  
NOT FOR BIDDING  
STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

**CAST IN PLACE SIDEWALK SECTION**  
(NOT TO SCALE)

DESIGNED BY D. THOMPSON/D. KEATON  
DRAWN BY ANGELA MOORE  
SUPERVISED BY DARRELL JAMES  
CHECKED BY JAMIE GILLESPIE

DATE \_\_\_\_\_  
DATE \_\_\_\_\_  
DATE \_\_\_\_\_  
DATE \_\_\_\_\_

**JA**  
JAMES ASSOCIATES

**SIDEWALK RAMP**  
(CAST IN PLACE ADA RAMP ADJACENT TO PRECAST PAVEMENT AT BRIDGE END SLABS SHOWING REINFORCING STEEL) (NOT TO SCALE)

NOTE: ALL WORK SHALL COMPLY WITH THE AMERICANS WITH DISABILITIES ACT (ADA).  
NOTE: COST OF SIDEWALK REINFORCING STEEL TO BE INCLUDED IN ITEM NO. 604-03.04, PAVEMENT AT BRIDGE ENDS, S.Y.  
NOTE: SIDEWALK RAMP WILL BE POURED MONOLITHICALLY WITH THE PAVEMENT AT BRIDGE END SLABS.  
NOTE: CONTRACTOR SHALL PROVIDE AS-BUILT SURVEY OF ADA RAMP (ALL 4 CORNERS) TO CONFIRM COMPLIANCE WITH ADA. COST TO BE INCLUDED IN OTHER ITEMS BID ON.

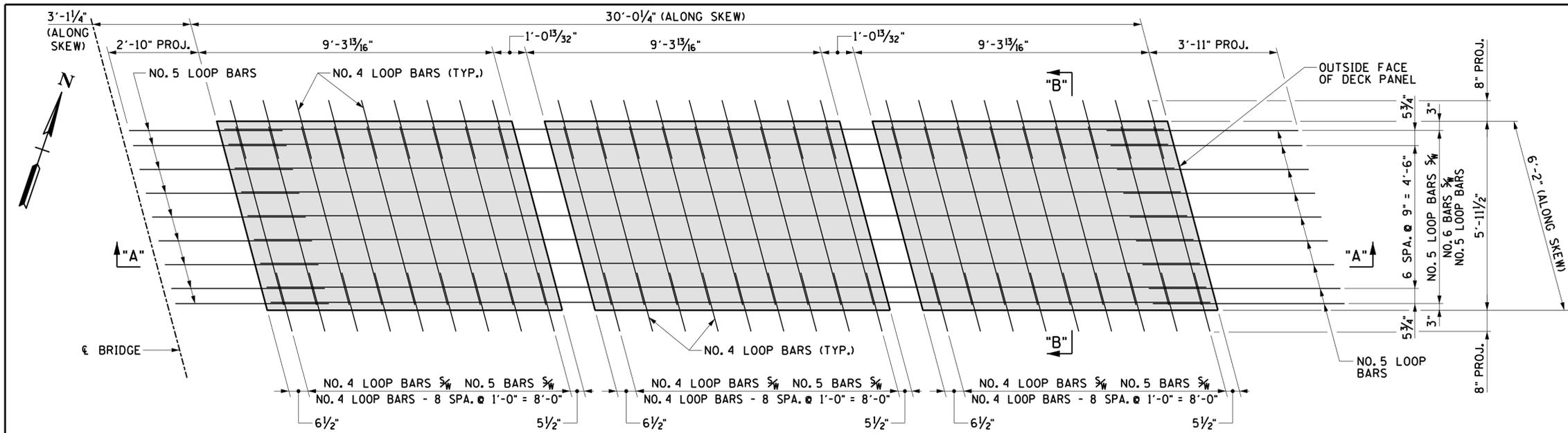
**CROSS-SECTION OF CLOSURE POUR AT CENTERLINE OF BRIDGE**  
(BETWEEN PHASE I AND PHASE II CONSTRUCTION) (NOT TO SCALE)

SUPERSTRUCTURE

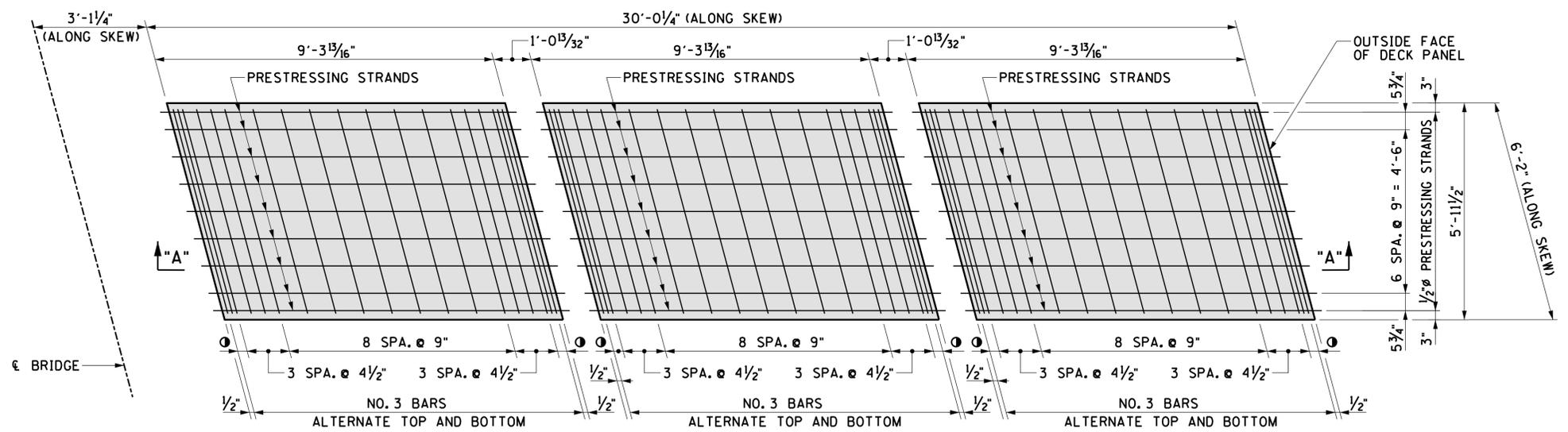
BRIDGE NO. 75-SR1-4.76  
STATE ROUTE 1  
OVER HARTS BRANCH  
RUTHERFORD COUNTY  
2016

BR-123-09

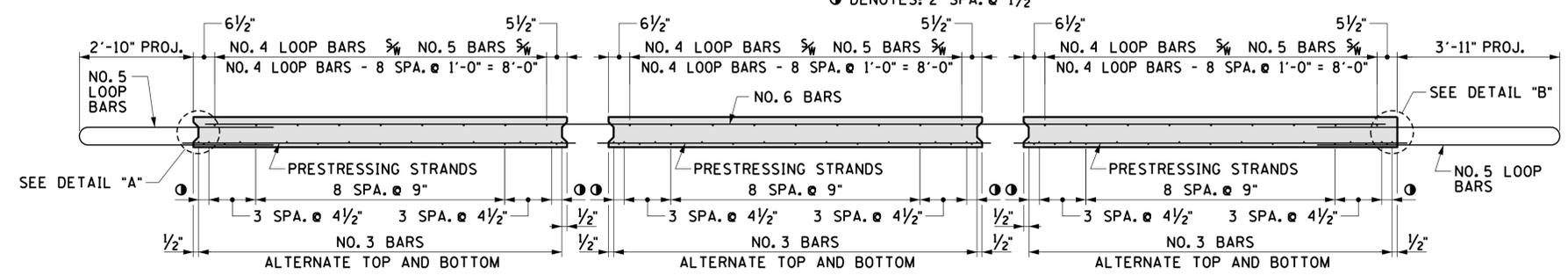




**PLAN VIEW OF THE PRECAST PANEL NO. 1 SHOWING TOP REINFORCEMENT**  
(4 PANELS REQUIRED)

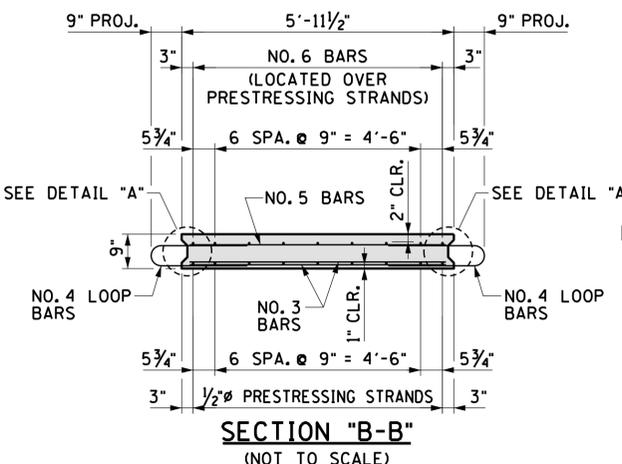


**PLAN VIEW OF THE PRECAST PANEL NO. 1 SHOWING BOTTOM REINFORCEMENT AND PRESTRESSING STRANDS**  
(4 PANELS REQUIRED)  
● DENOTES: 2 SPA. @ 1 1/2"



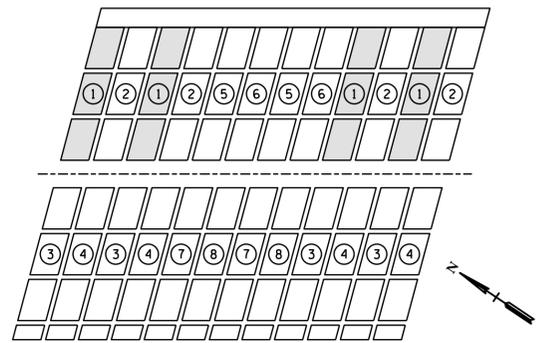
**SECTION "A-A"**  
(NOT TO SCALE)  
● DENOTES: 2 SPA. @ 1 1/2"

NOTE: ALL REINFORCING STEEL IS EPOXY COATED.

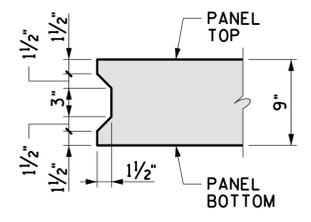


**SECTION "B-B"**  
(NOT TO SCALE)

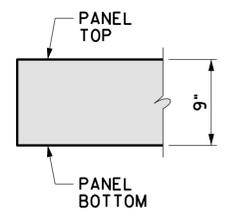
CONST. NO.			
PROJECT NO.	YEAR	SHEET NO.	
75002-4287-04	2016		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



**KEY MAP**



**DETAIL "A"**  
(REINFORCING STEEL NOT SHOWN FOR CLARITY)



**DETAIL "B"**  
(REINFORCING STEEL NOT SHOWN FOR CLARITY)

**UNOFFICIAL SET**  
NOT FOR BIDDING  
DEPARTMENT OF TRANSPORTATION

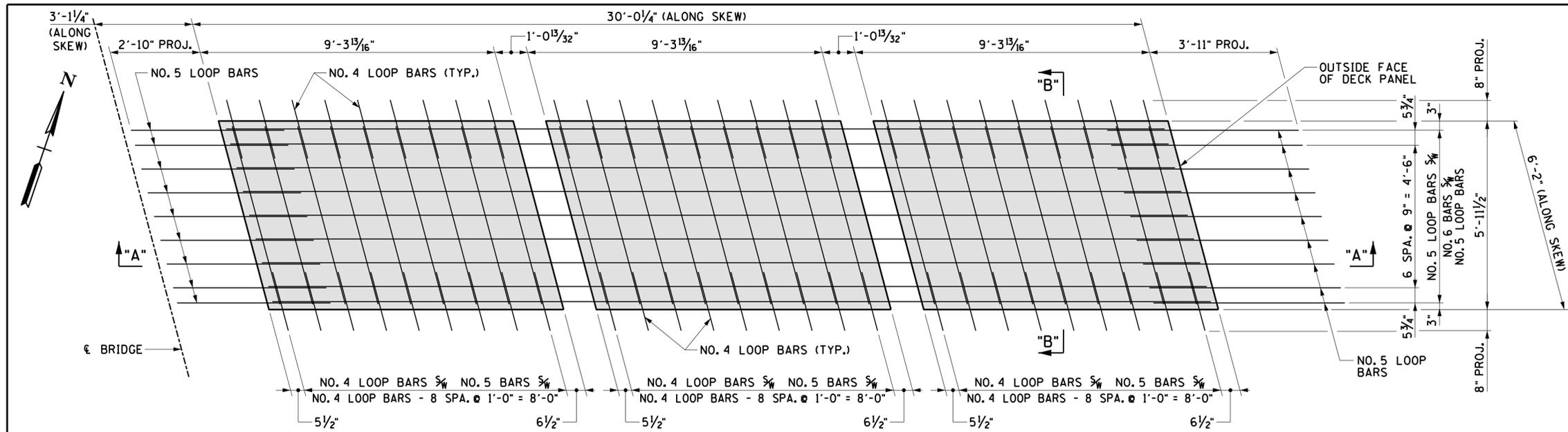
SUPERSTRUCTURE DETAILS  
PRECAST DECK PANEL NO. 1

BRIDGE NO. 75-SR1-4.76  
STATE ROUTE 1  
OVER HARTS BRANCH  
RUTHERFORD COUNTY  
2016

DESIGNED BY D. THOMPSON/D. KEATON  
DRAWN BY ANGELA MOORE  
SUPERVISED BY DARRELL JAMES  
CHECKED BY JAMIE GILLESPIE

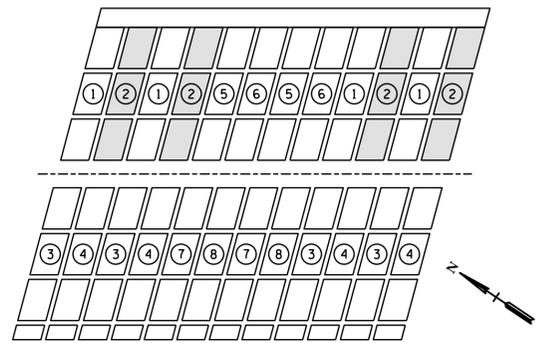
DATE \_\_\_\_\_  
DATE \_\_\_\_\_  
DATE \_\_\_\_\_  
DATE \_\_\_\_\_



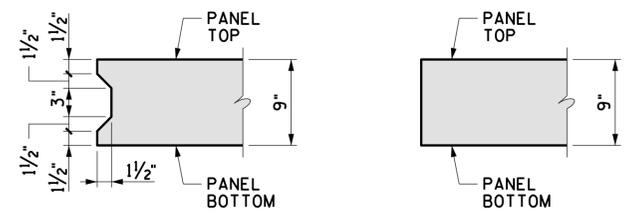


**PLAN VIEW OF THE PRECAST PANEL NO. 2 SHOWING TOP REINFORCEMENT**  
(4 PANELS REQUIRED)

CONST. NO.			
PROJECT NO.	YEAR	SHEET NO.	
75002-4287-04	2016		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

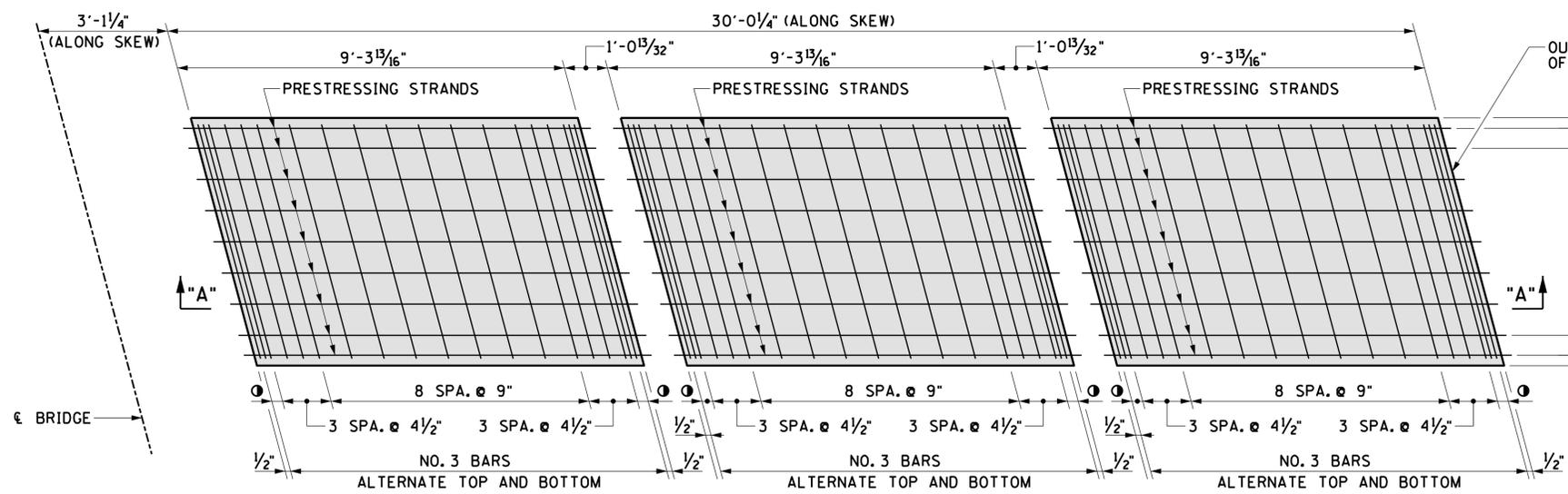


**KEY MAP**



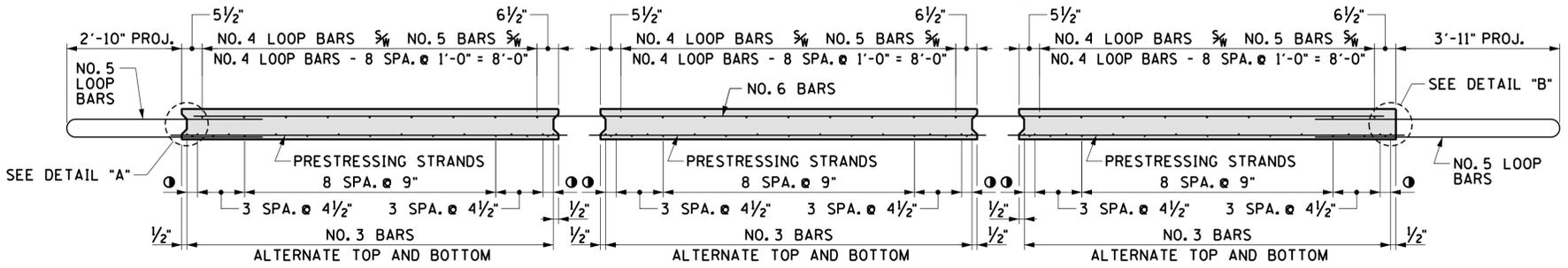
**DETAIL "A"**  
(REINFORCING STEEL NOT SHOWN FOR CLARITY)

**DETAIL "B"**



**PLAN VIEW OF THE PRECAST PANEL NO. 2 SHOWING BOTTOM REINFORCEMENT AND PRESTRESSING STRANDS**  
(4 PANELS REQUIRED)

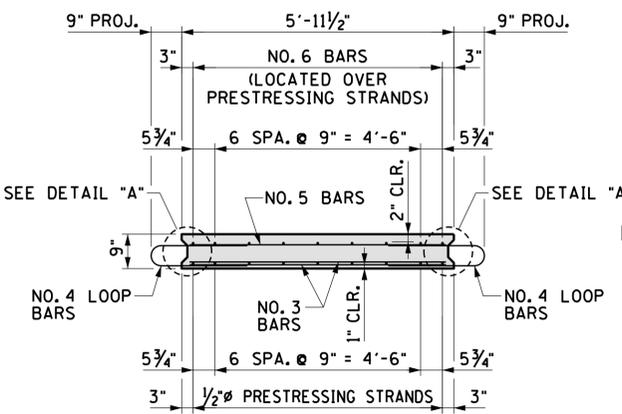
● DENOTES: 2 SPA. @ 1 1/2"



**SECTION "A-A"**  
(NOT TO SCALE)

● DENOTES: 2 SPA. @ 1 1/2"

NOTE: ALL REINFORCING STEEL IS EPOXY COATED.



**SECTION "B-B"**  
(NOT TO SCALE)

**UNOFFICIAL SET**  
NOT FOR BIDDING

DEPARTMENT OF TRANSPORTATION  
**SUPERSTRUCTURE DETAILS  
PRECAST DECK PANEL NO. 2**

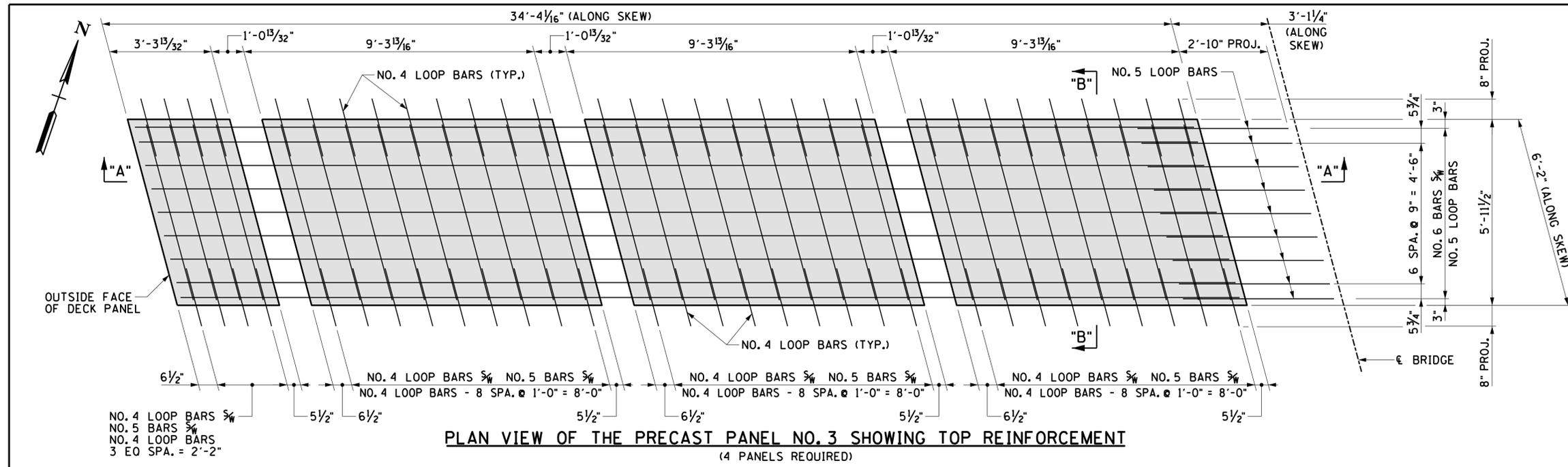
BRIDGE NO. 75-SR1-4.76  
STATE ROUTE 1  
RUTHERFORD COUNTY  
2016

DESIGNED BY D. THOMPSON/D. KEATON  
DRAWN BY ANGELA MOORE  
SUPERVISED BY DARRELL JAMES  
CHECKED BY JAMIE GILLESPIE

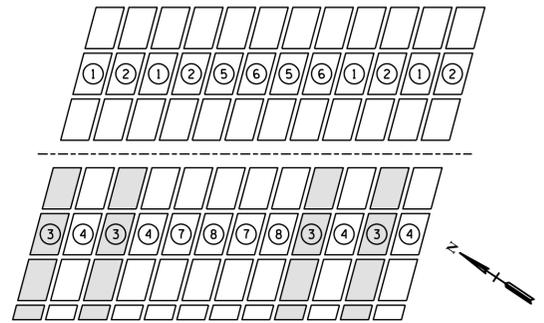
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DATE \_\_\_\_\_  
DATE \_\_\_\_\_



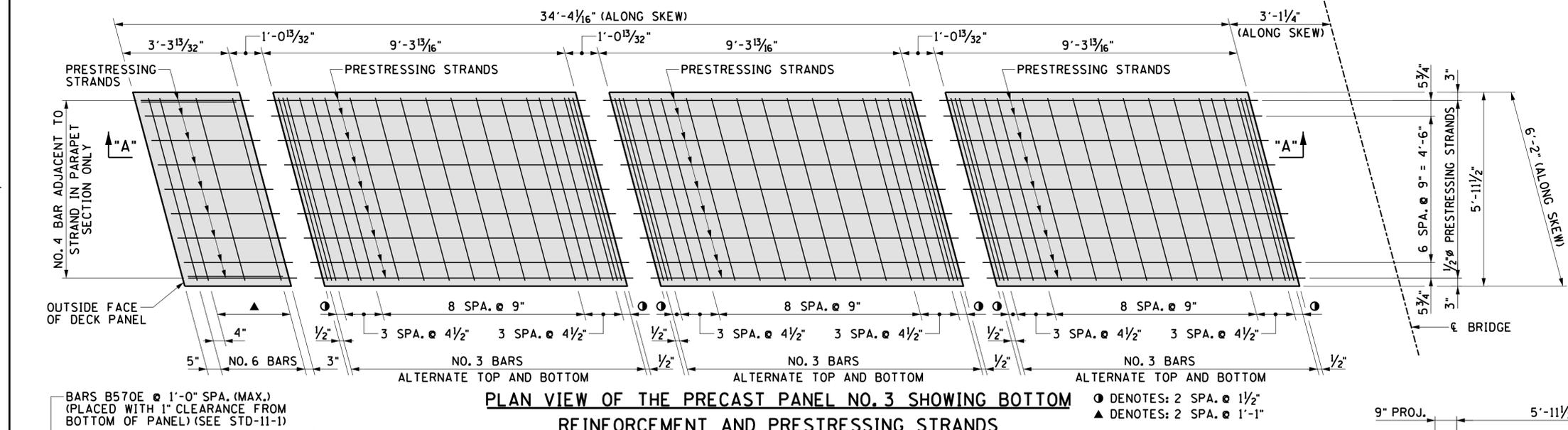
CONST. NO.			
PROJECT NO.	YEAR	SHEET NO.	
75002-4287-04	2016		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



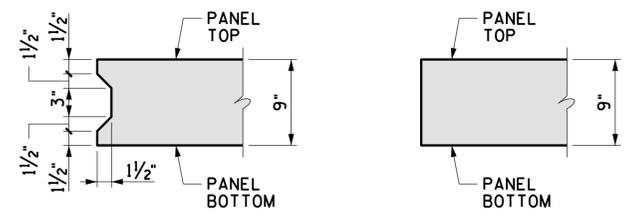
**PLAN VIEW OF THE PRECAST PANEL NO. 3 SHOWING TOP REINFORCEMENT**  
(4 PANELS REQUIRED)



**KEY MAP**

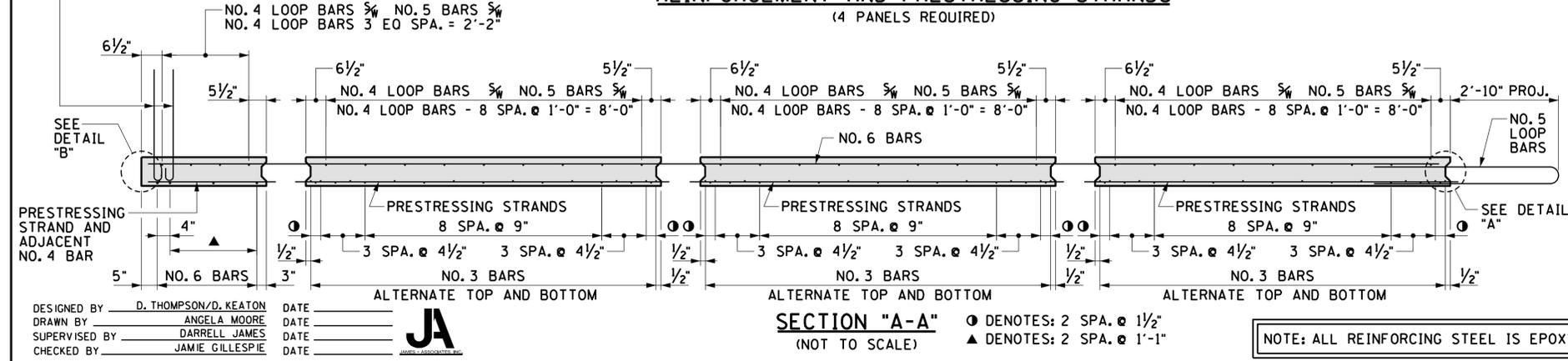


**PLAN VIEW OF THE PRECAST PANEL NO. 3 SHOWING BOTTOM REINFORCEMENT AND PRESTRESSING STRANDS**  
(4 PANELS REQUIRED)

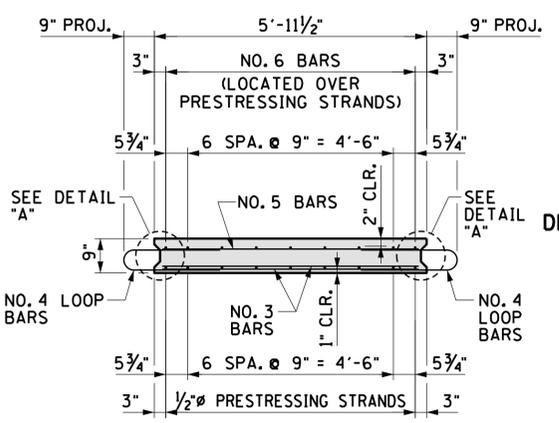


**DETAIL "A"**  
(REINFORCING STEEL NOT SHOWN FOR CLARITY)

**DETAIL "B"**



**SECTION "A-A"**  
(NOT TO SCALE)



**SECTION "B-B"**  
(NOT TO SCALE)

**UNOFFICIAL SET**  
NOT FOR BIDDING

**SUPERSTRUCTURE DETAILS  
PRECAST DECK PANEL NO. 3**

BRIDGE NO. 75-SR1-4.76  
STATE ROUTE 1  
OVER HARTS BRANCH  
RUTHERFORD COUNTY  
2016

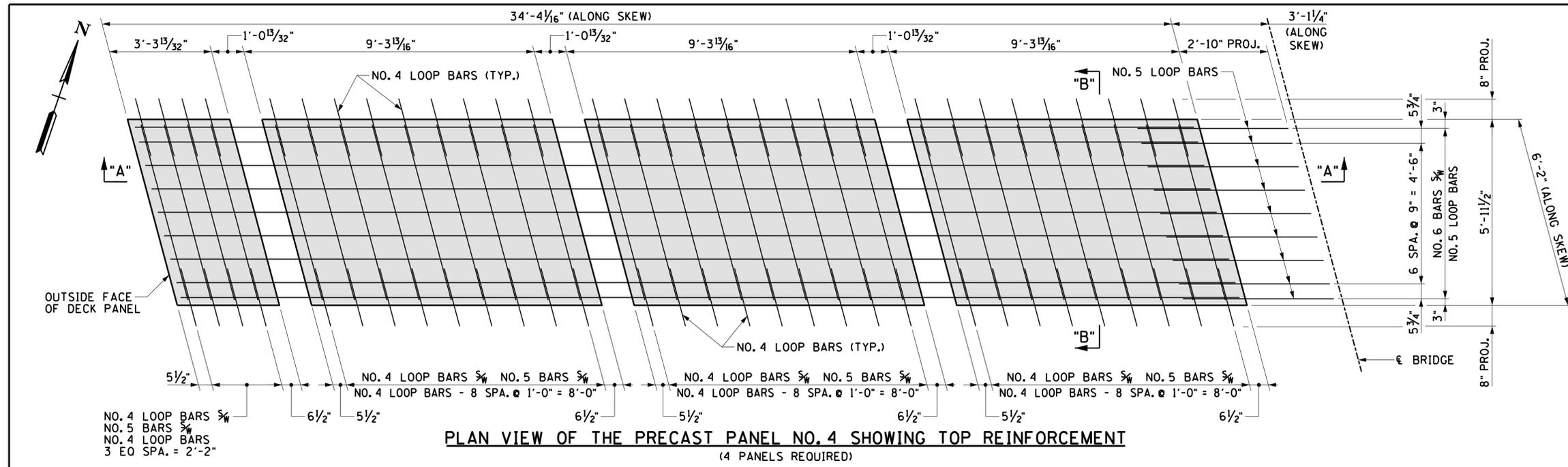
DESIGNED BY D. THOMPSON/D. KEATON  
DRAWN BY ANGELA MOORE  
SUPERVISED BY DARRELL JAMES  
CHECKED BY JAMIE GILLESPIE

DATE  
DATE  
DATE  
DATE

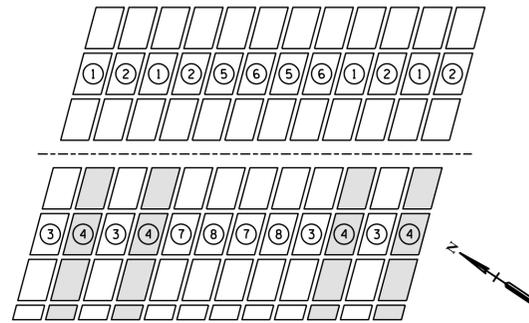


NOTE: ALL REINFORCING STEEL IS EPOXY COATED.

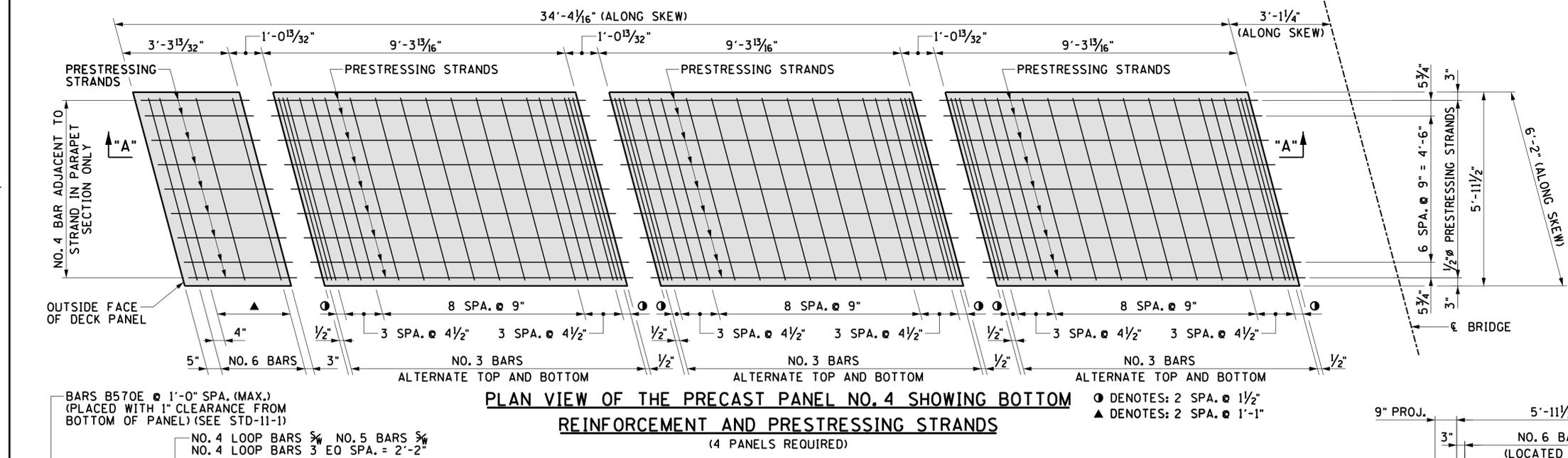
CONST. NO.			
PROJECT NO.	YEAR	SHEET NO.	
75002-4287-04	2016		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



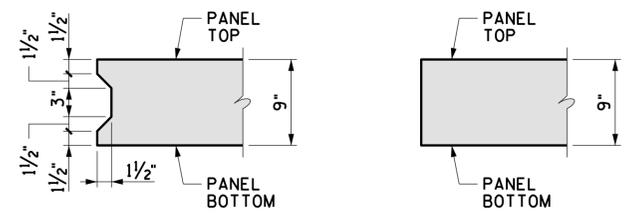
**PLAN VIEW OF THE PRECAST PANEL NO. 4 SHOWING TOP REINFORCEMENT**  
(4 PANELS REQUIRED)



**KEY MAP**



**PLAN VIEW OF THE PRECAST PANEL NO. 4 SHOWING BOTTOM REINFORCEMENT AND PRESTRESSING STRANDS**  
(4 PANELS REQUIRED)



**DETAIL "A"**  
(REINFORCING STEEL NOT SHOWN FOR CLARITY)

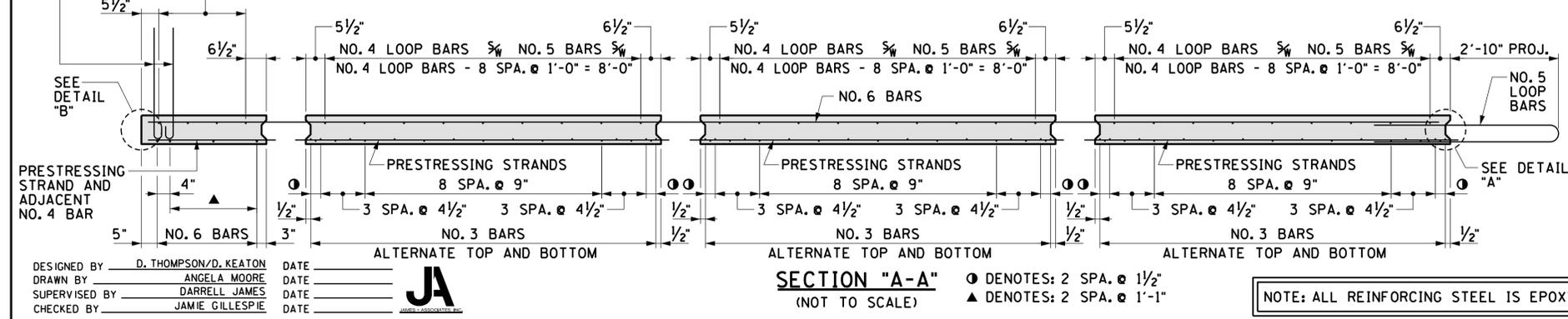
**DETAIL "B"**

**UNOFFICIAL SET**  
NOT FOR BIDDING

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

SUPERSTRUCTURE DETAILS  
PRECAST DECK PANEL NO. 4

BRIDGE NO. 75-SR1-4.76  
STATE ROUTE 1  
OVER HARTS BRANCH  
RUTHERFORD COUNTY  
2016



**SECTION "A-A"**  
(NOT TO SCALE)

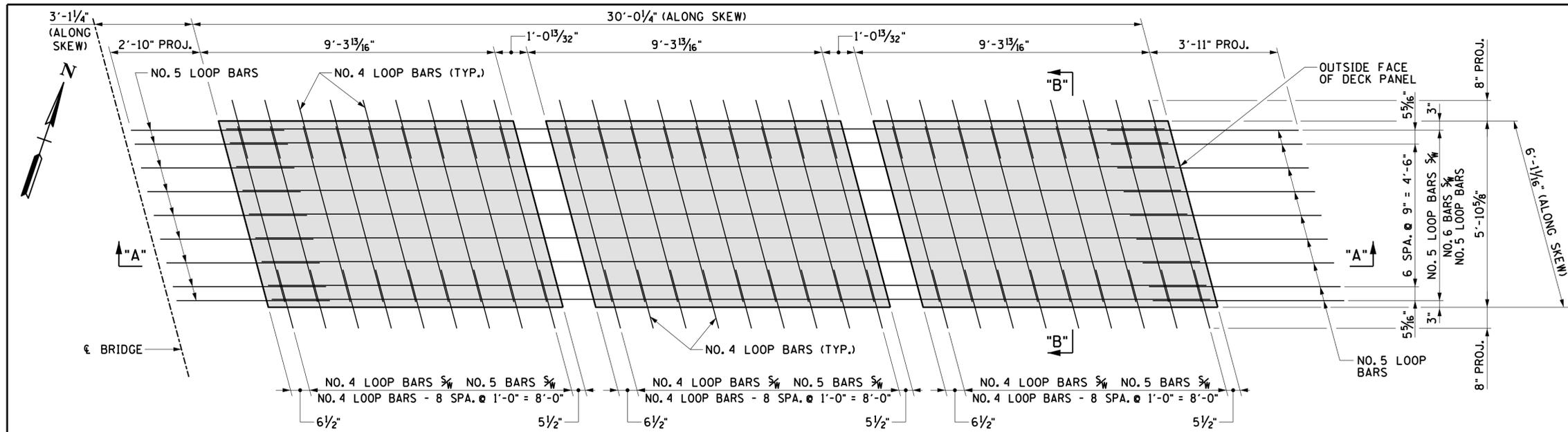
**SECTION "B-B"**  
(NOT TO SCALE)

NOTE: ALL REINFORCING STEEL IS EPOXY COATED.

DESIGNED BY D. THOMPSON/D. KEATON  
DRAWN BY ANGELA MOORE  
SUPERVISED BY DARRELL JAMES  
CHECKED BY JAMIE GILLESPIE

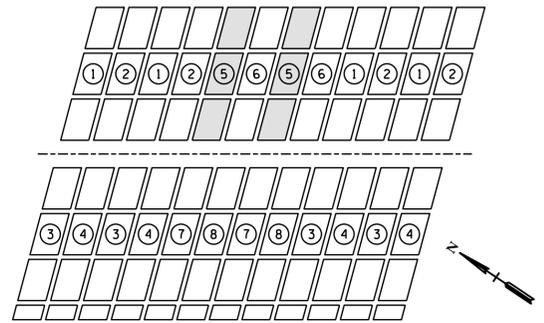
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DATE \_\_\_\_\_



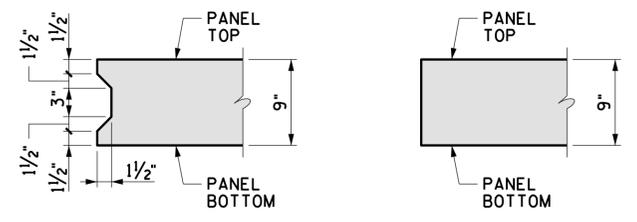


**PLAN VIEW OF THE PRECAST PANEL NO. 5 SHOWING TOP REINFORCEMENT**  
(2 PANELS REQUIRED)

CONST. NO.			
PROJECT NO.	YEAR	SHEET NO.	
75002-4287-04	2016		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

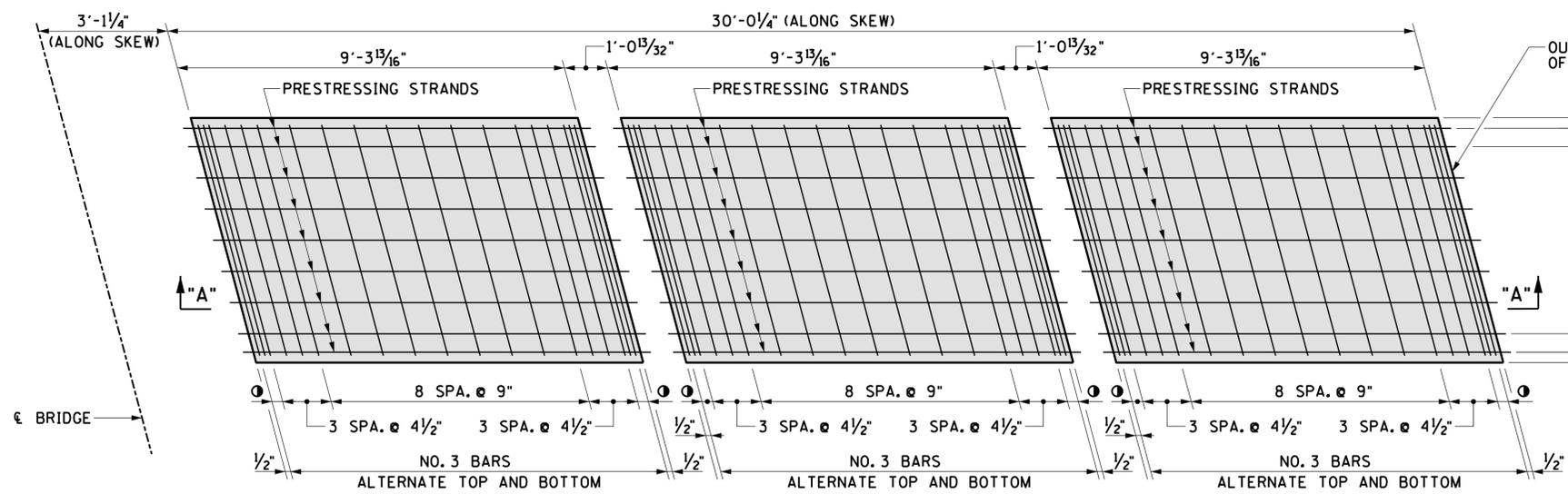


**KEY MAP**

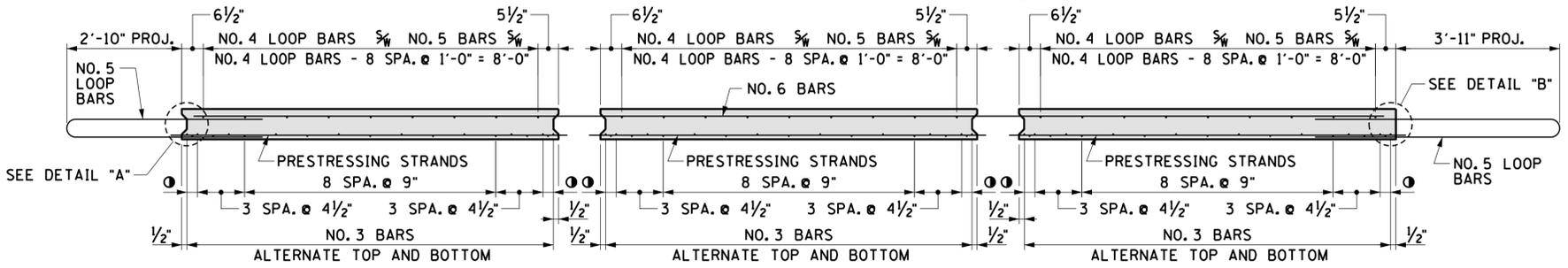


**DETAIL "A"**  
(REINFORCING STEEL NOT SHOWN FOR CLARITY)

**DETAIL "B"**  
(REINFORCING STEEL NOT SHOWN FOR CLARITY)

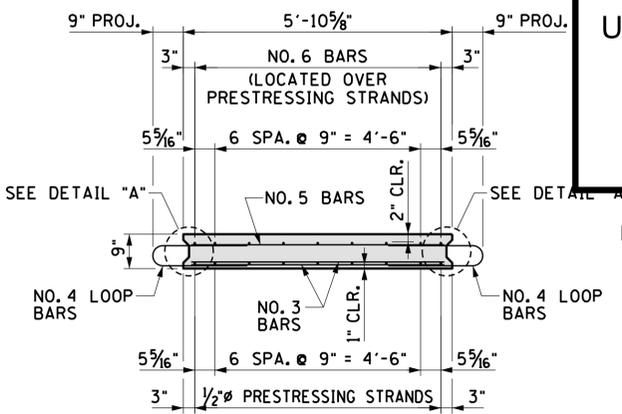


**PLAN VIEW OF THE PRECAST PANEL NO. 5 SHOWING BOTTOM REINFORCEMENT AND PRESTRESSING STRANDS**  
(2 PANELS REQUIRED)  
● DENOTES: 2 SPA. @ 1 1/2"



**SECTION "A-A"**  
(NOT TO SCALE)  
● DENOTES: 2 SPA. @ 1 1/2"

NOTE: ALL REINFORCING STEEL IS EPOXY COATED.



**SECTION "B-B"**  
(NOT TO SCALE)

**UNOFFICIAL SET**  
NOT FOR BIDDING



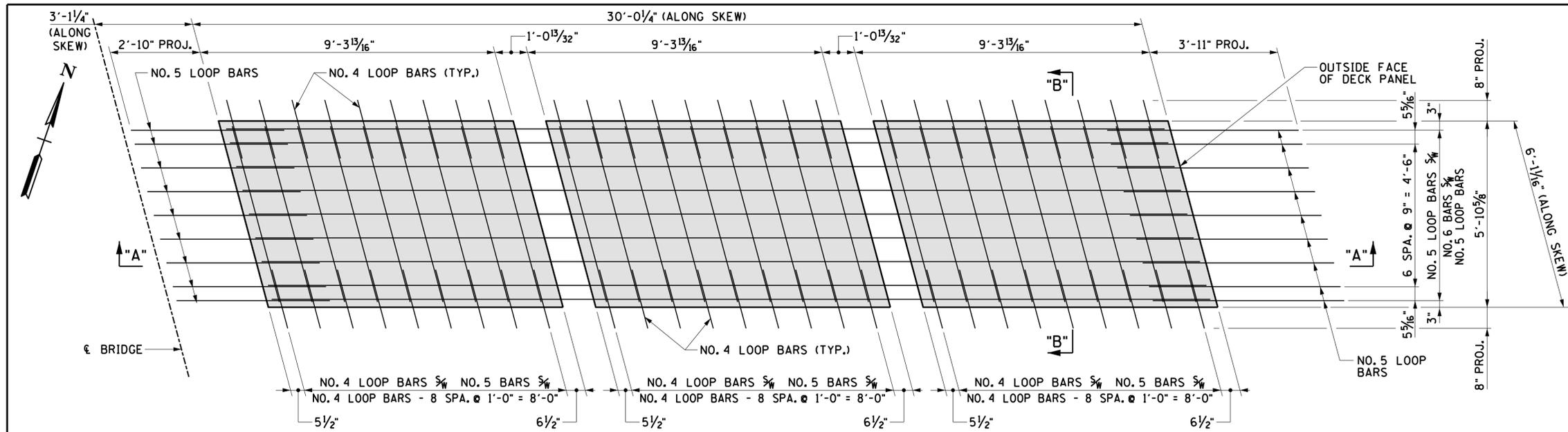
STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

SUPERSTRUCTURE DETAILS  
PRECAST DECK PANEL NO. 5

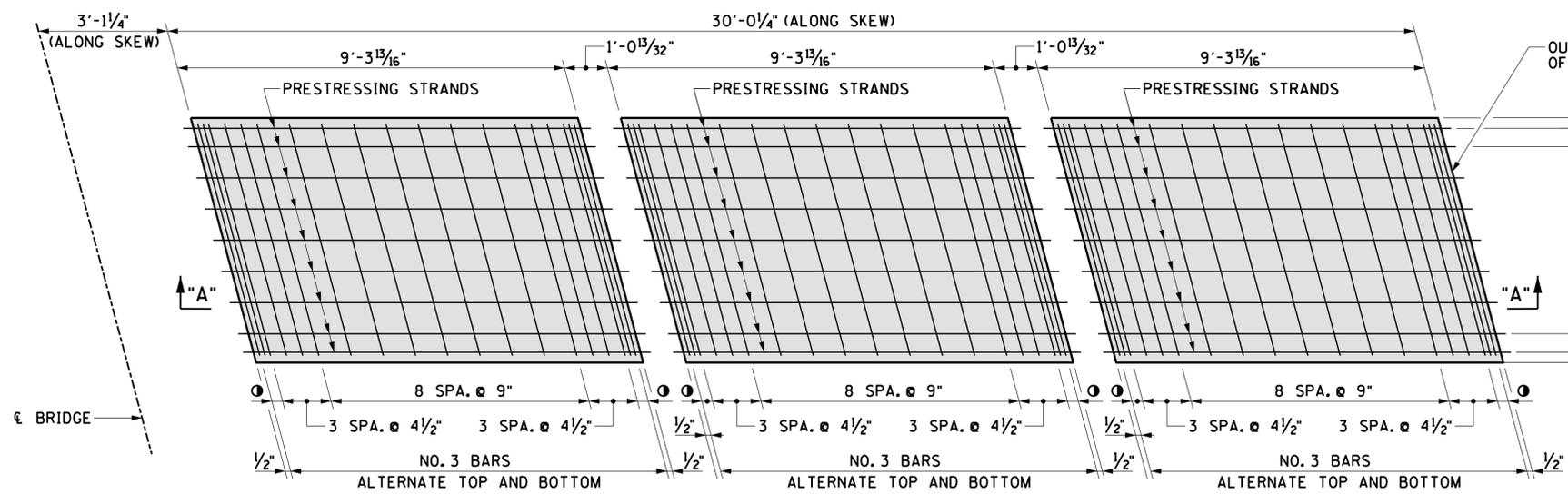
BRIDGE NO. 75-SR1-4.76  
STATE ROUTE 1  
OVER HARTS BRANCH  
RUTHERFORD COUNTY  
2016

DESIGNED BY D. THOMPSON/D. KEATON  
DRAWN BY ANGELA MOORE  
SUPERVISED BY DARRELL JAMES  
CHECKED BY JAMIE GILLESPIE

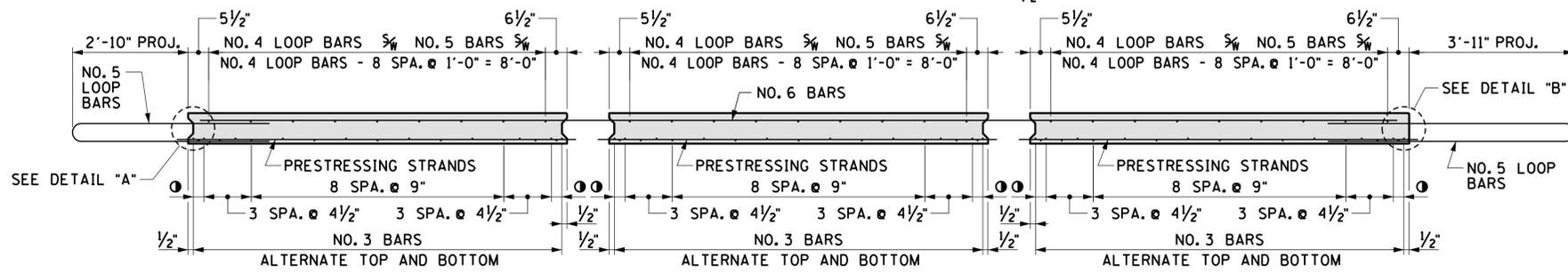




**PLAN VIEW OF THE PRECAST PANEL NO. 6 SHOWING TOP REINFORCEMENT**  
(2 PANELS REQUIRED)

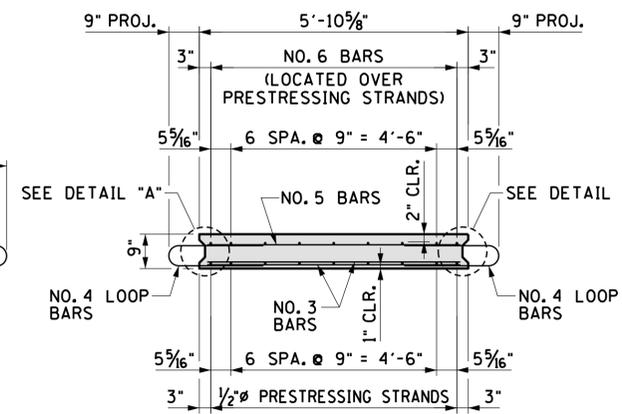


**PLAN VIEW OF THE PRECAST PANEL NO. 6 SHOWING BOTTOM REINFORCEMENT AND PRESTRESSING STRANDS**  
(2 PANELS REQUIRED)  
● DENOTES: 2 SPA. @ 1 1/2"



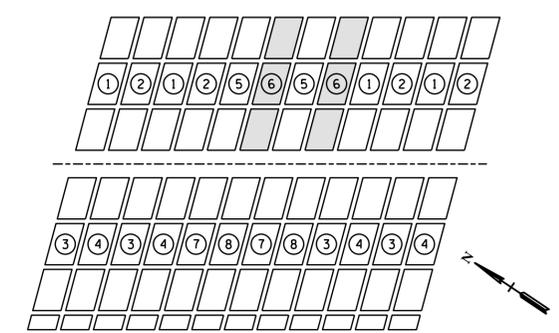
**SECTION "A-A"**  
(NOT TO SCALE)  
● DENOTES: 2 SPA. @ 1 1/2"

NOTE: ALL REINFORCING STEEL IS EPOXY COATED.

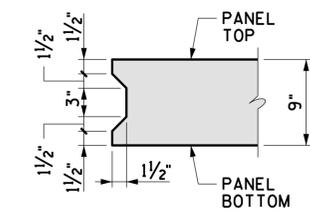


**SECTION "B-B"**  
(NOT TO SCALE)

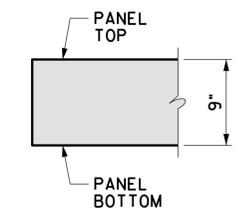
CONST. NO.			
PROJECT NO.	YEAR	SHEET NO.	
75002-4287-04	2016		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



**KEY MAP**



**DETAIL "A"**  
(REINFORCING STEEL NOT SHOWN FOR CLARITY)



**DETAIL "B"**  
(REINFORCING STEEL NOT SHOWN FOR CLARITY)

**UNOFFICIAL SET**  
NOT FOR BIDDING

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

SUPERSTRUCTURE DETAILS  
PRECAST DECK PANEL NO. 6

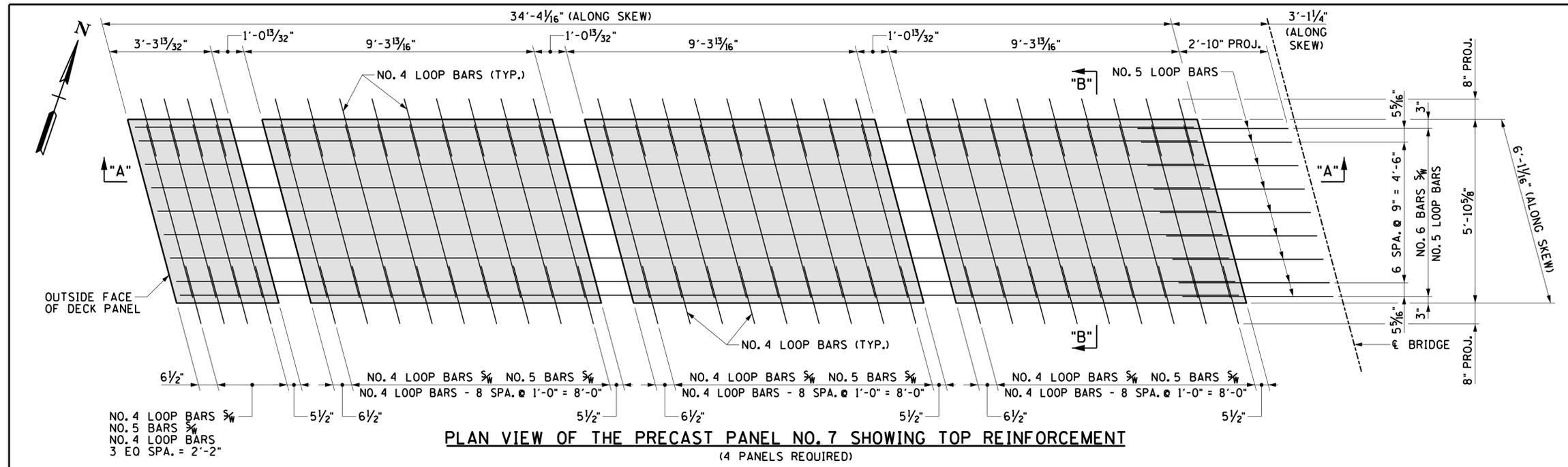
BRIDGE NO. 75-SR1-4.76  
STATE ROUTE 1  
OVER HARTS BRANCH  
RUTHERFORD COUNTY  
2016

DESIGNED BY D. THOMPSON/D. KEATON  
DRAWN BY ANGELA MOORE  
SUPERVISED BY DARRELL JAMES  
CHECKED BY JAMIE GILLESPIE

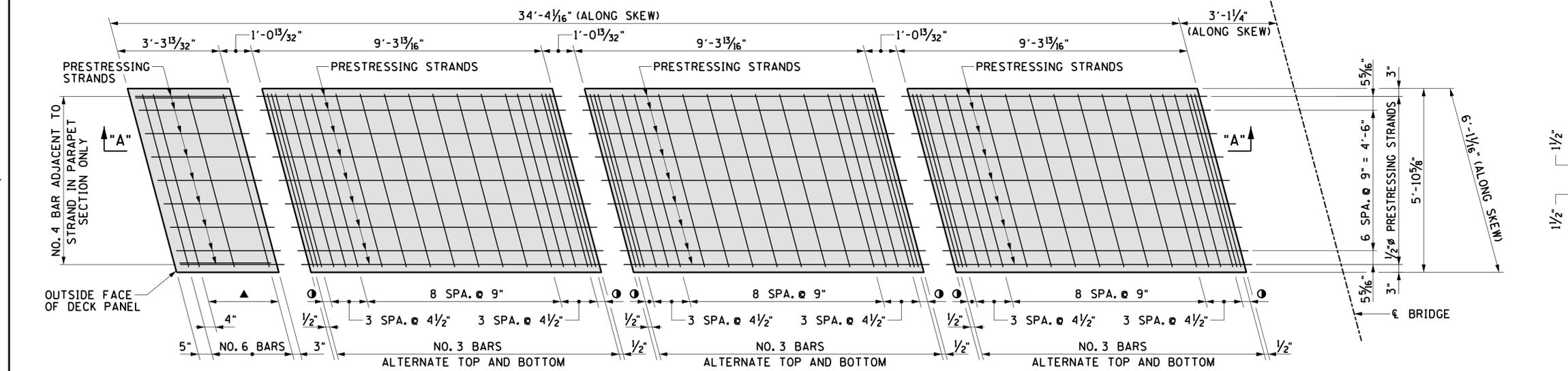
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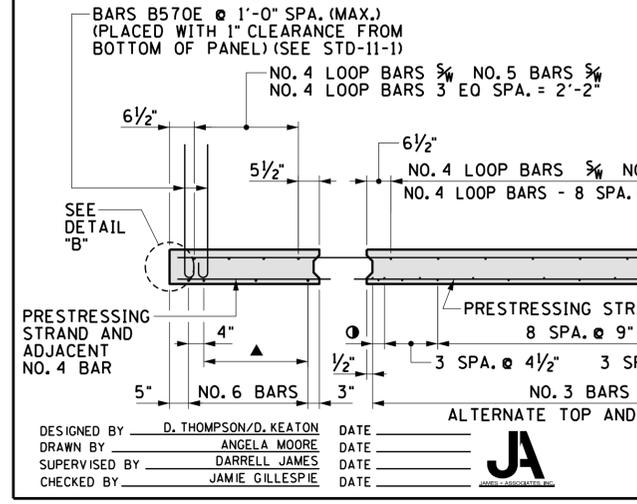
CONST. NO.			
PROJECT NO.	YEAR	SHEET NO.	
75002-4287-04	2016		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



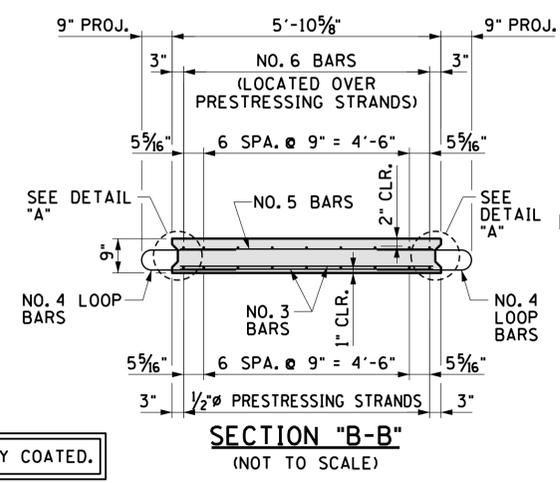
**PLAN VIEW OF THE PRECAST PANEL NO. 7 SHOWING TOP REINFORCEMENT**  
(4 PANELS REQUIRED)



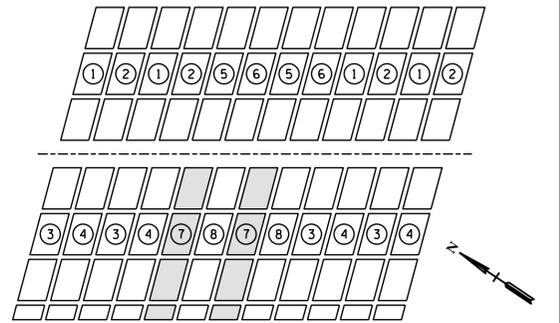
**PLAN VIEW OF THE PRECAST PANEL NO. 7 SHOWING BOTTOM REINFORCEMENT AND PRESTRESSING STRANDS**  
(2 PANELS REQUIRED)



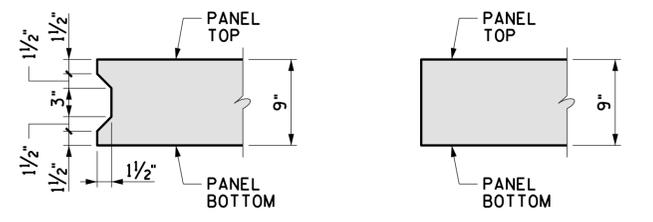
**SECTION "A-A"**  
(NOT TO SCALE)



**SECTION "B-B"**  
(NOT TO SCALE)



**KEY MAP**



**DETAIL "A"**  
(REINFORCING STEEL NOT SHOWN FOR CLARITY)

**DETAIL "B"**  
(REINFORCING STEEL NOT SHOWN FOR CLARITY)

**UNOFFICIAL SET**  
NOT FOR BIDDING

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

SUPERSTRUCTURE DETAILS  
PRECAST DECK PANEL NO. 7

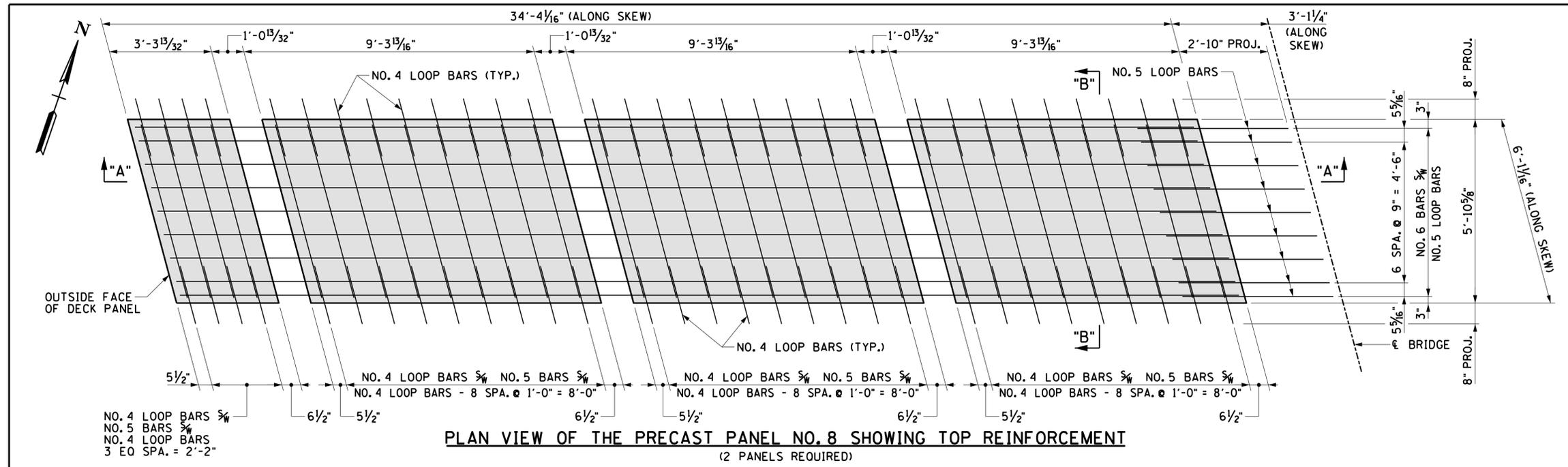
BRIDGE NO. 75-SR1-4.76  
STATE ROUTE 1  
OVER HARTS BRANCH  
RUTHERFORD COUNTY  
2016

DESIGNED BY D. THOMPSON/D. KEATON  
DRAWN BY ANGELA MOORE  
SUPERVISED BY DARRELL JAMES  
CHECKED BY JAMIE GILLESPIE

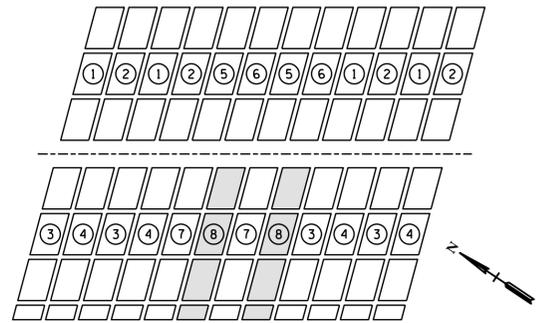


NOTE: ALL REINFORCING STEEL IS EPOXY COATED.

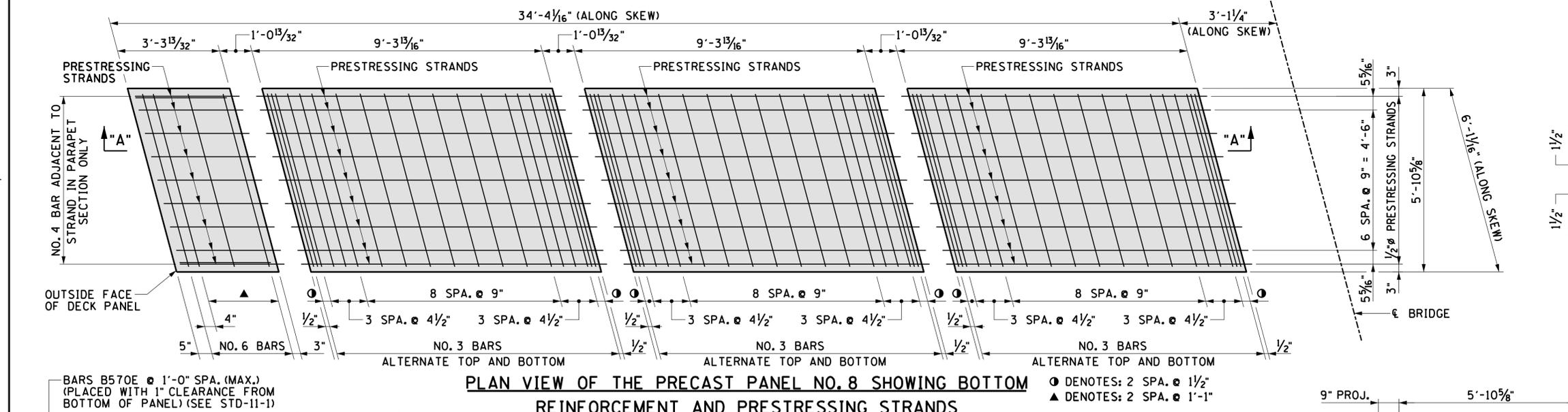
CONST. NO.			
PROJECT NO.	YEAR	SHEET NO.	
75002-4287-04	2016		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



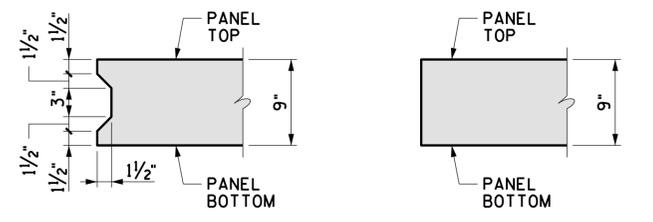
**PLAN VIEW OF THE PRECAST PANEL NO. 8 SHOWING TOP REINFORCEMENT**  
(2 PANELS REQUIRED)



**KEY MAP**

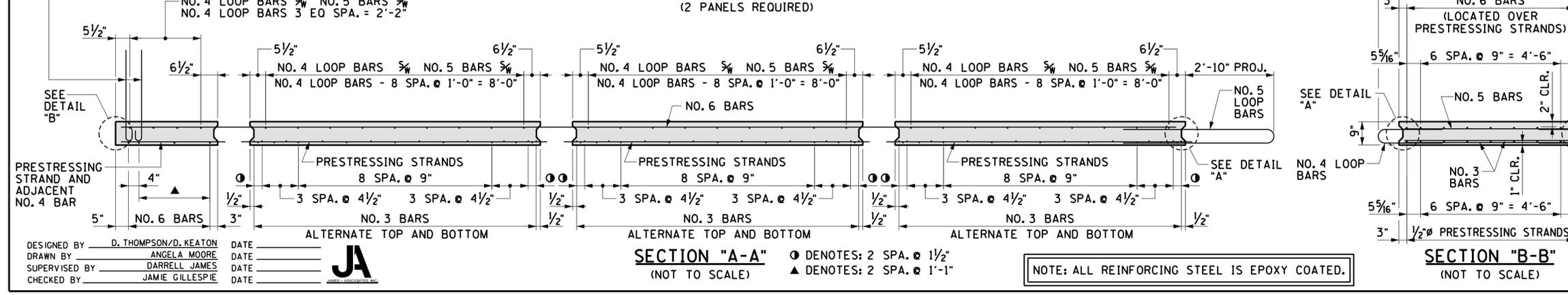


**PLAN VIEW OF THE PRECAST PANEL NO. 8 SHOWING BOTTOM REINFORCEMENT AND PRESTRESSING STRANDS**  
(2 PANELS REQUIRED)



**DETAIL "A"**  
(REINFORCING STEEL NOT SHOWN FOR CLARITY)

**DETAIL "B"**



**SECTION "A-A"**  
(NOT TO SCALE)

**SECTION "B-B"**  
(NOT TO SCALE)

NOTE: ALL REINFORCING STEEL IS EPOXY COATED.

**UNOFFICIAL SET**  
NOT FOR BIDDING

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

SUPERSTRUCTURE DETAILS  
PRECAST DECK PANEL NO. 8

BRIDGE NO. 75-SR1-4.76  
STATE ROUTE 1  
OVER HARTS BRANCH  
RUTHERFORD COUNTY  
2016

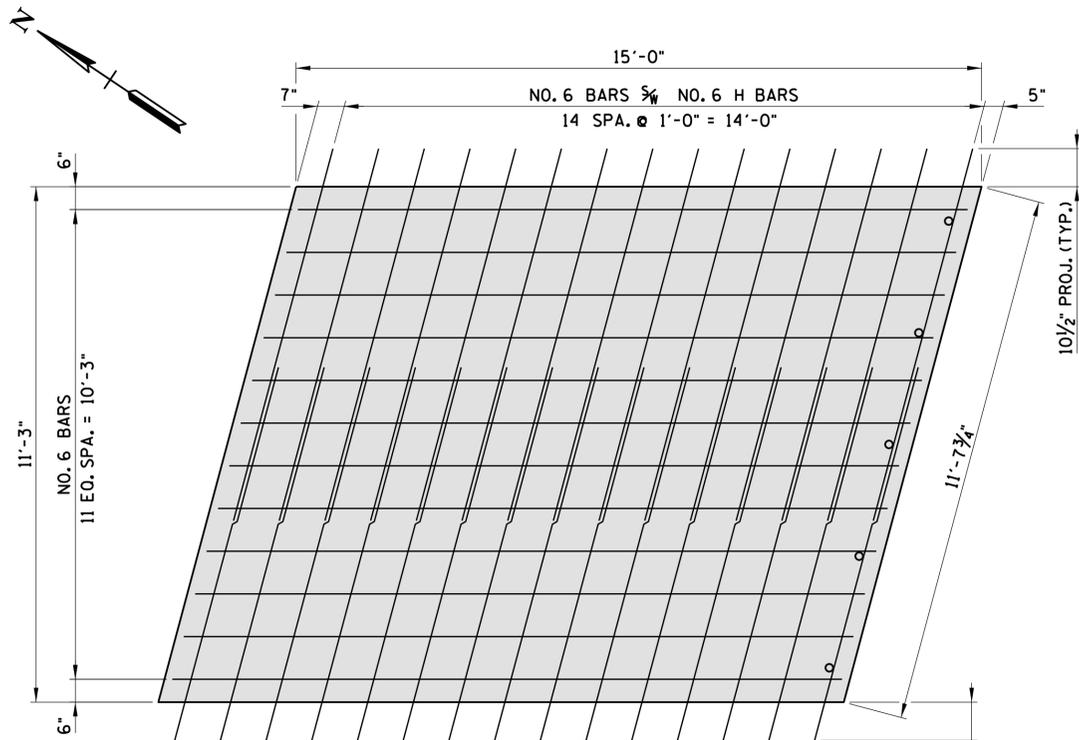
DESIGNED BY D. THOMPSON/D. KEATON  
DRAWN BY ANGELA MOORE  
SUPERVISED BY DARRELL JAMES  
CHECKED BY JAMIE GILLESPIE

DATE  
DATE  
DATE  
DATE



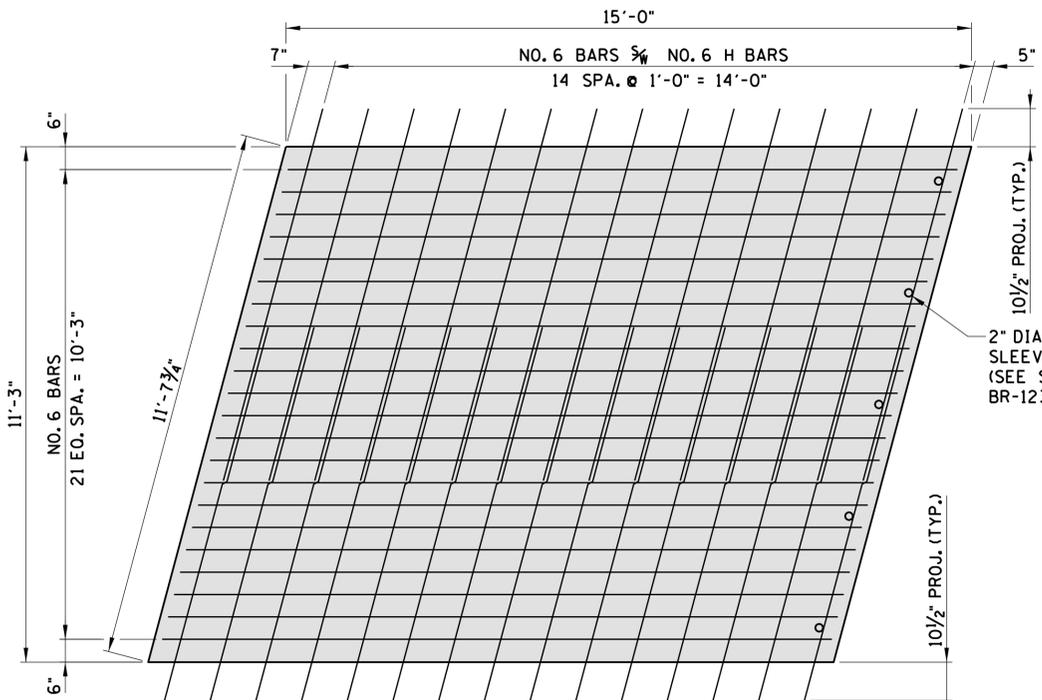






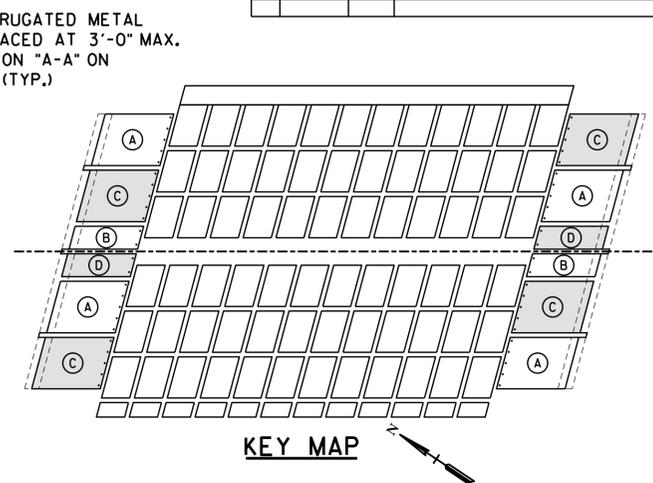
**PLAN VIEW OF PRECAST P.A.B.E. PANEL "C"**  
(SHOWING TOP REINFORCEMENT)  
(4 PANELS REQUIRED)

NOTE: ORIENTATION SHOWN FOR P.A.B.E. PANEL "C" AT ABUTMENT NO. 1 (BEGINNING OF BRIDGE). ROTATE 180° FOR P.A.B.E. PANEL "C" AT ABUTMENT NO. 2 (END OF BRIDGE).

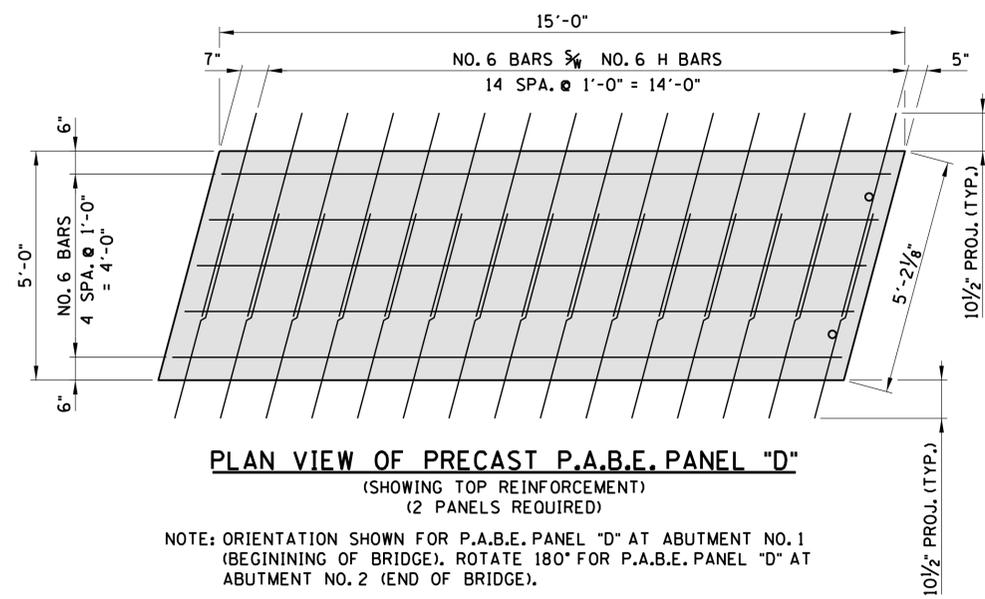


**PLAN VIEW OF PRECAST P.A.B.E. PANEL "C"**  
(SHOWING BOTTOM REINFORCEMENT)  
(4 PANELS REQUIRED)

NOTE: ORIENTATION SHOWN FOR P.A.B.E. PANEL "C" AT ABUTMENT NO. 1 (BEGINNING OF BRIDGE). ROTATE 180° FOR P.A.B.E. PANEL "C" AT ABUTMENT NO. 2 (END OF BRIDGE).

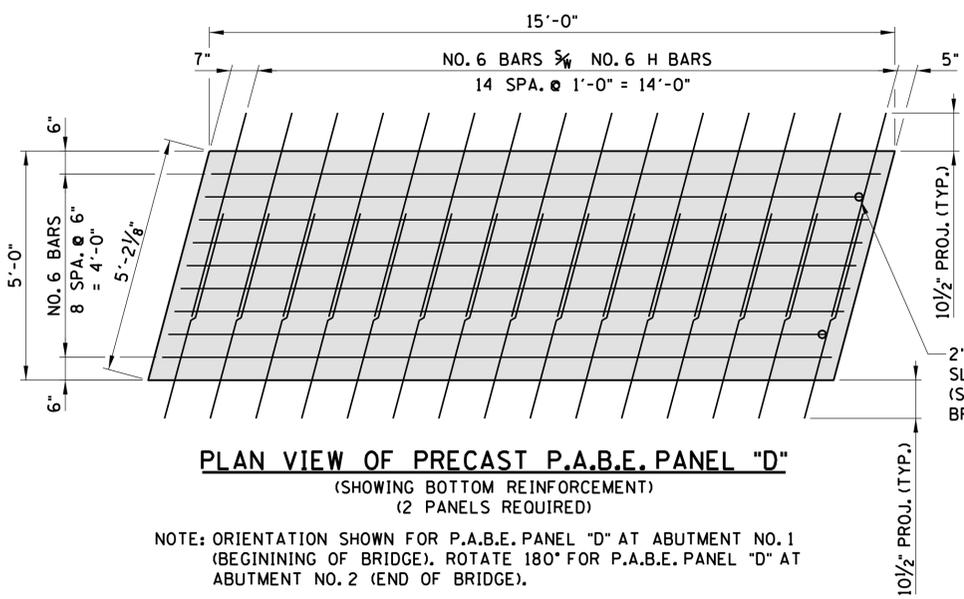


**KEY MAP**



**PLAN VIEW OF PRECAST P.A.B.E. PANEL "D"**  
(SHOWING TOP REINFORCEMENT)  
(2 PANELS REQUIRED)

NOTE: ORIENTATION SHOWN FOR P.A.B.E. PANEL "D" AT ABUTMENT NO. 1 (BEGINNING OF BRIDGE). ROTATE 180° FOR P.A.B.E. PANEL "D" AT ABUTMENT NO. 2 (END OF BRIDGE).



**PLAN VIEW OF PRECAST P.A.B.E. PANEL "D"**  
(SHOWING BOTTOM REINFORCEMENT)  
(2 PANELS REQUIRED)

NOTE: ORIENTATION SHOWN FOR P.A.B.E. PANEL "D" AT ABUTMENT NO. 1 (BEGINNING OF BRIDGE). ROTATE 180° FOR P.A.B.E. PANEL "D" AT ABUTMENT NO. 2 (END OF BRIDGE).

2" DIA. CORRUGATED METAL SLEEVE SPACED AT 3'-0" MAX. (SEE SECTION "A-A" ON BR-123-19) (TYP.)

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NO.	DATE	BY	BRIEF DESCRIPTION

**UNOFFICIAL SET**  
NOT FOR BIDDING



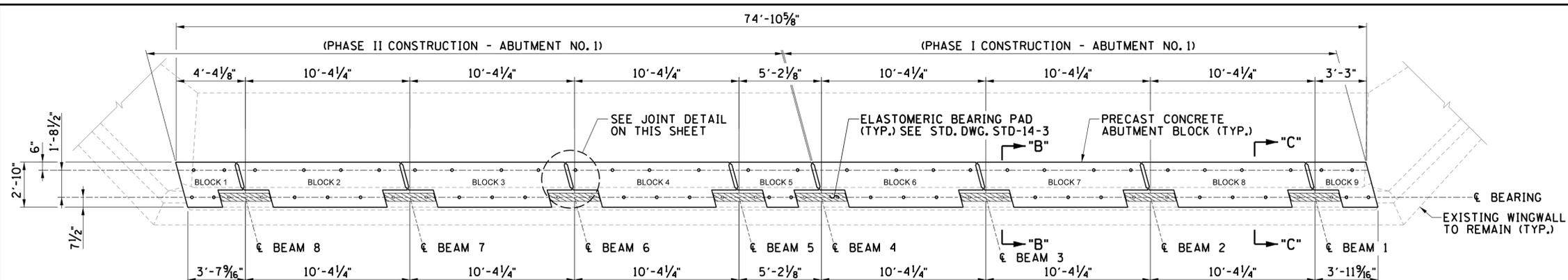
STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

PAVEMENT AT BRIDGE END  
PANELS "C" AND "D"

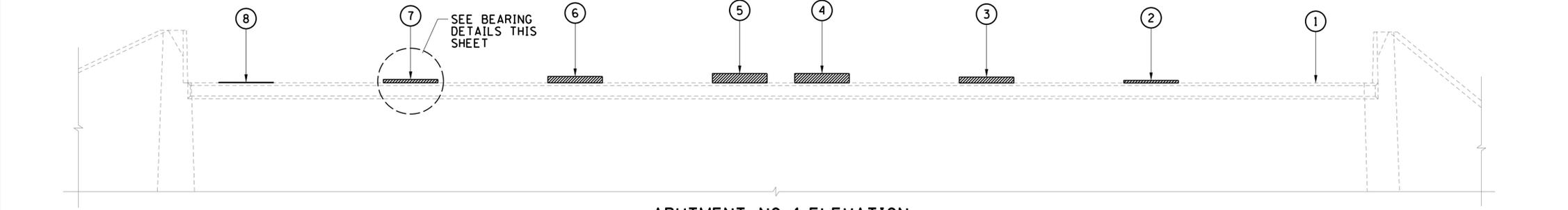
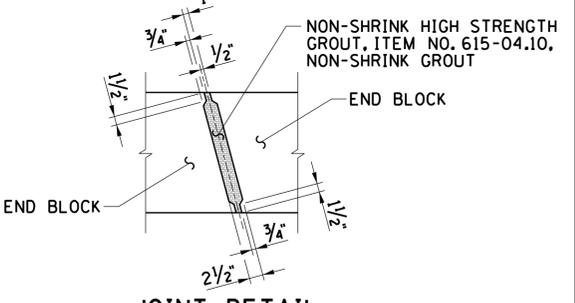
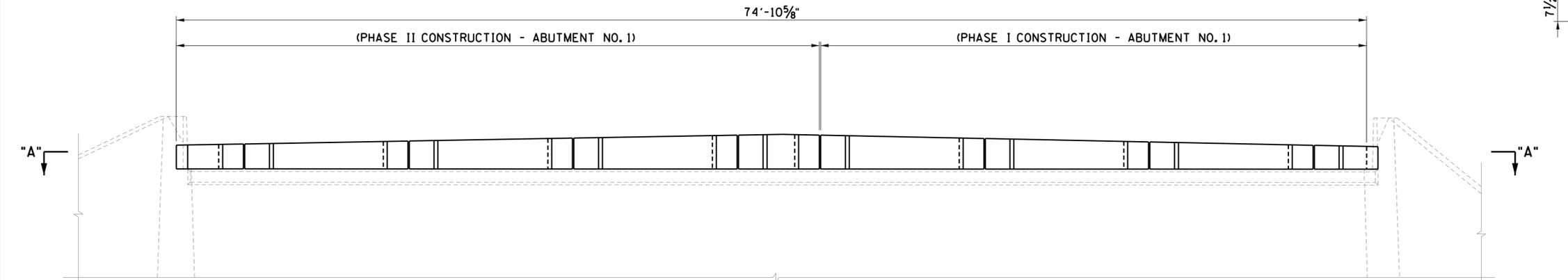
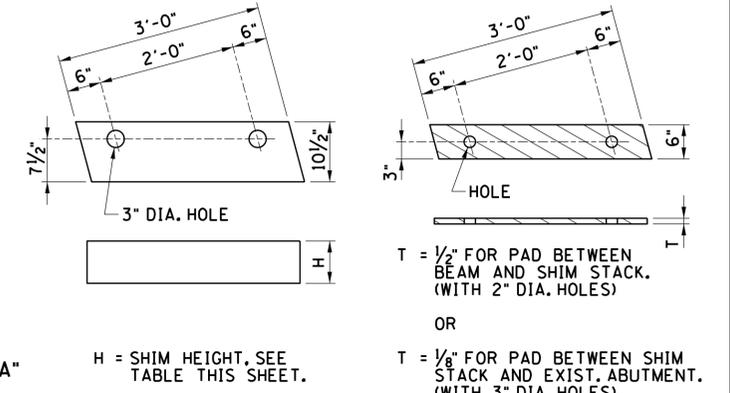
BRIDGE NO. 75-SR1-4.76  
STATE ROUTE 1  
OVER HARTS BRANCH  
RUTHERFORD COUNTY  
2016

DESIGNED BY D. THOMPSON/D. KEATON  
DRAWN BY ANGELA MOORE  
SUPERVISED BY DARRELL JAMES  
CHECKED BY JAMIE GILLESPIE





CONST. NO.			
PROJECT NO.	YEAR	SHEET NO.	
75002-4287-04	2016		
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NO.	DATE	BY	BRIEF DESCRIPTION



ABUTMENT NO. 1	BEAM 8	BEAM 7	BEAM 6	BEAM 5	BEAM 4	BEAM 3	BEAM 2	BEAM 1
BOTTOM OF BEAM EL.	523.44	523.62	523.81	523.99	523.99	523.77	523.56	523.35
SHIM HEIGHT (INCHES)	0 <sup>1</sup> / <sub>4</sub> "	2 <sup>3</sup> / <sub>8</sub> "	4 <sup>3</sup> / <sub>4</sub> "	6 <sup>7</sup> / <sub>8</sub> "	7"	4 <sup>3</sup> / <sub>8</sub> "	1 <sup>7</sup> / <sub>8</sub> "	N/A

UNOFFICIAL SET  
NOT FOR BIDDING

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

END BLOCK LAYOUT  
ABUTMENT NO. 1

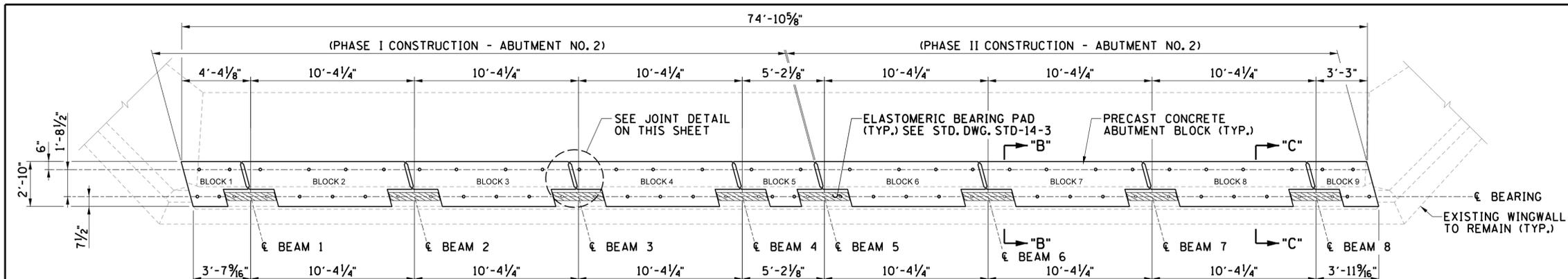
BRIDGE NO. 75-SR1-4.76  
STATE ROUTE 1  
OVER HARTS BRANCH  
RUTHERFORD COUNTY  
2016

DESIGNED BY D. THOMPSON/D. KEATON  
DRAWN BY ANGELA MOORE  
SUPERVISED BY DARRELL JAMES  
CHECKED BY JAMIE GILLESPIE

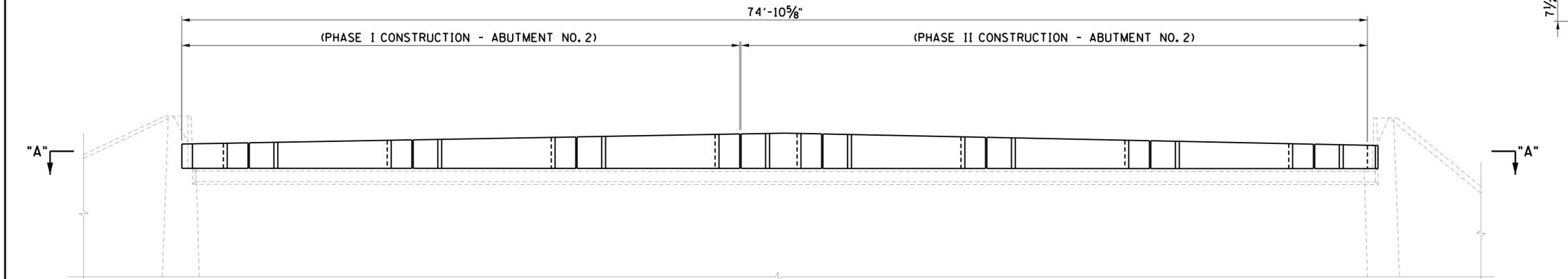
DATE \_\_\_\_\_  
DATE \_\_\_\_\_  
DATE \_\_\_\_\_  
DATE \_\_\_\_\_



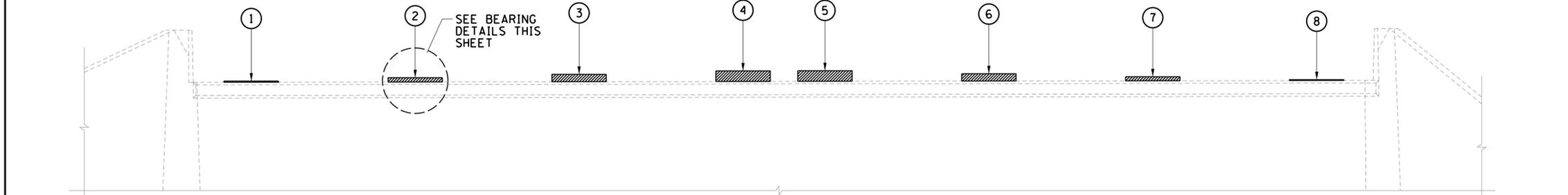
NOTE: 1/8" BEARING PAD REQUIRED BETWEEN BEAM AND TOP OF ABUTMENT WHERE NO SHIM PLATE IS REQUIRED.



**SECTION "A-A"**  
(SHOWING DIMENSIONS)  
(LOOKING FORWARD ON SURVEY - ABUTMENT NO. 2)  
(SEE BR-123-24 FOR SECTIONS "B-B" AND "C-C")



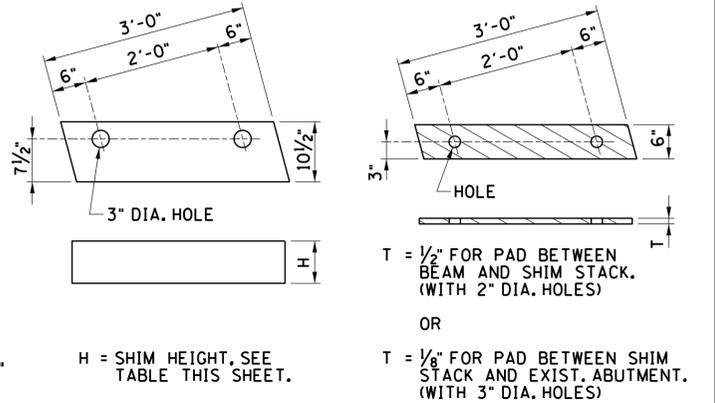
**ABUTMENT NO. 2 ELEVATION**  
(LOOKING FORWARD ON SURVEY - ABUTMENT NO. 2)  
(SEE BR-123-24 AND BR-123-25 FOR END BLOCK DETAILS)



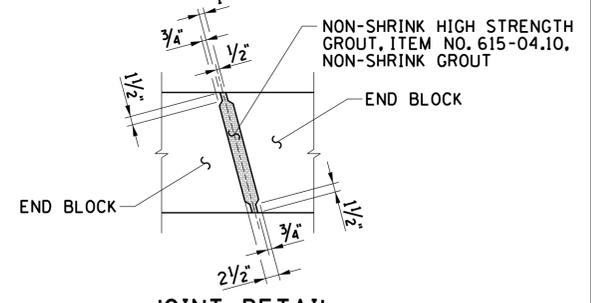
**ABUTMENT NO. 2 ELEVATION**  
(LOOKING FORWARD ON SURVEY - ABUTMENT NO. 2)

ABUTMENT NO. 2	BEAM 1	BEAM 2	BEAM 3	BEAM 4	BEAM 5	BEAM 6	BEAM 7	BEAM 8
BOTTOM OF BEAM EL.	523.44	523.62	523.81	523.99	523.99	523.77	523.56	523.35
SHIM HEIGHT (INCHES)	0 1/4"	2 5/8"	4 7/8"	7 3/8"	7 3/8"	4 7/8"	2 3/8"	N/A

CONST. NO.			
PROJECT NO.	YEAR	SHEET NO.	
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REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



**BEARING DETAILS**



**JOINT DETAIL**

NOTE: END BLOCK FINISHED PRIOR TO GROUT.

**UNOFFICIAL SET**

NOT FOR BIDDING

THOMPSON ENGINEERS

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

END BLOCK LAYOUT  
ABUTMENT NO. 2

BRIDGE NO. 75-SR1-4.76  
STATE ROUTE 1  
OVER HARTS BRANCH  
RUTHERFORD COUNTY  
2016

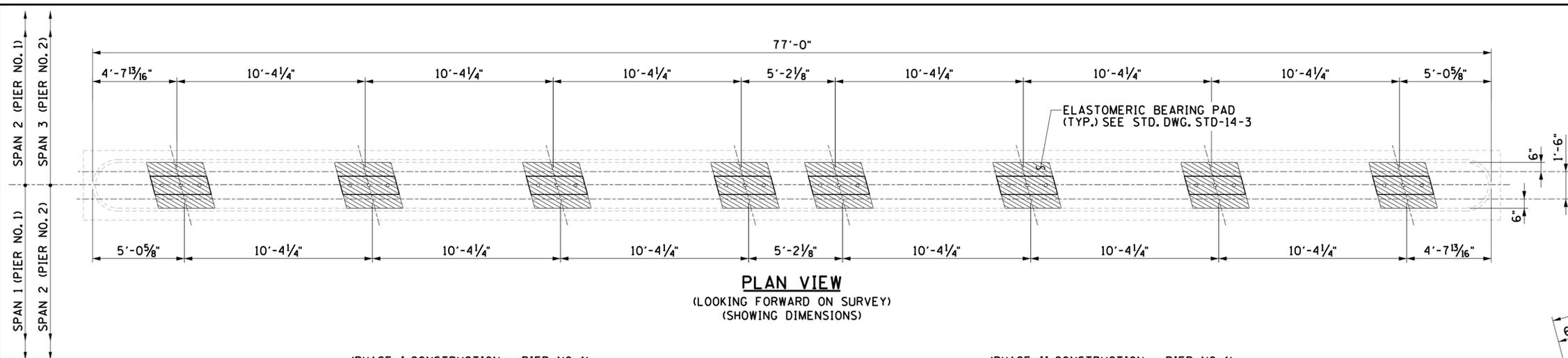
DESIGNED BY D. THOMPSON/D. KEATON DATE \_\_\_\_\_  
DRAWN BY ANGELA MOORE DATE \_\_\_\_\_  
SUPERVISED BY DARRELL JAMES DATE \_\_\_\_\_  
CHECKED BY JAMIE GILLESPIE DATE \_\_\_\_\_

**JA**  
JAMES ASSOCIATES, INC.

NOTE: 1/8" BEARING PAD REQUIRED BETWEEN BEAM AND TOP OF ABUTMENT WHERE NO SHIM PLATE IS REQUIRED.

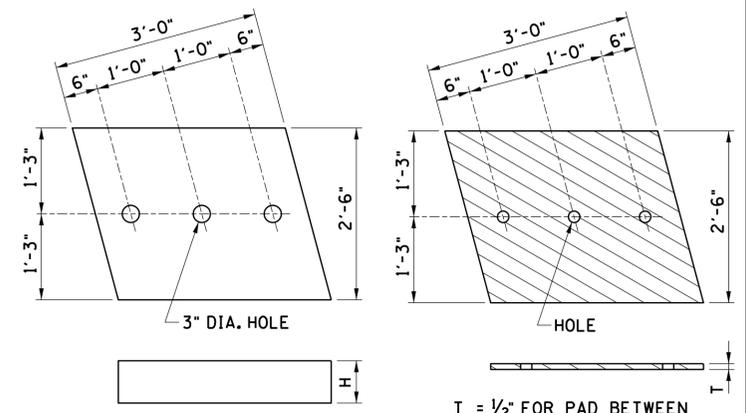






**PLAN VIEW**  
(LOOKING FORWARD ON SURVEY)  
(SHOWING DIMENSIONS)

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T = 1/2" FOR PAD BETWEEN BEAM AND SHIM STACK. (WITH 2" DIA. HOLES)  
OR  
T = 1/8" FOR PAD BETWEEN SHIM STACK AND EXIST. PIER. (WITH 3" DIA. HOLES)  
H = SHIM HEIGHT, SEE TABLE THIS SHEET.

**BEARING DETAILS**

**UNOFFICIAL SET**  
NOT FOR BIDDING

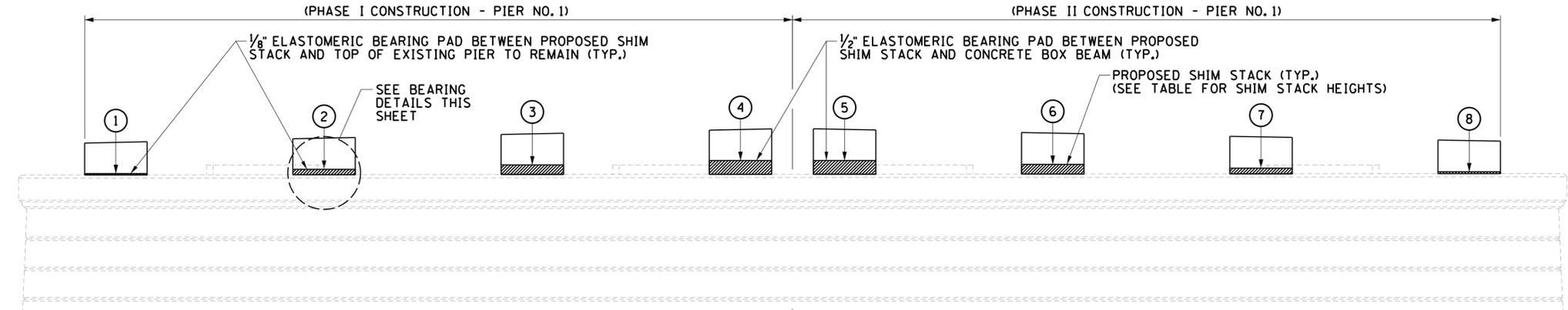


STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

PIER NO. 1 AND PIER NO. 2

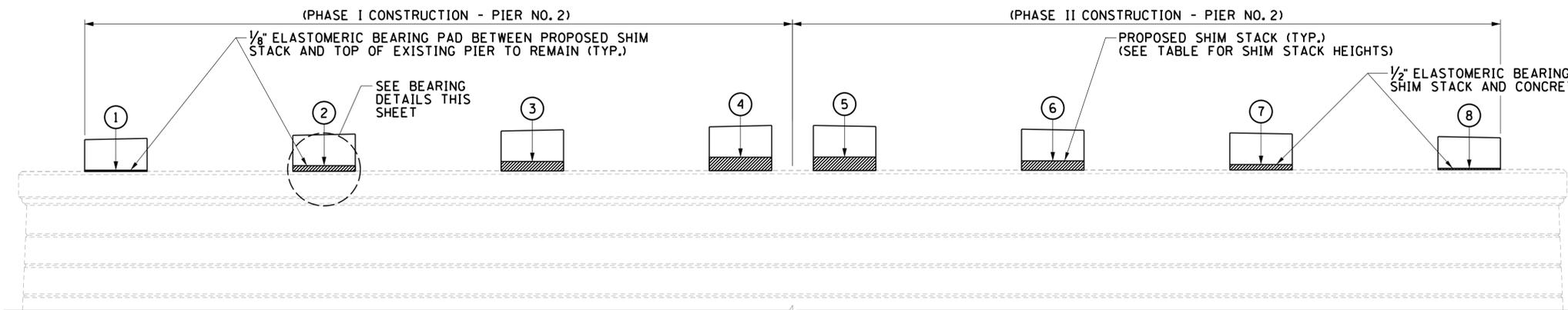
BRIDGE NO. 75-SR1-4.76  
STATE ROUTE 1  
OVER HARTS BRANCH  
RUTHERFORD COUNTY  
2016

BR-123-26



**PIER NO. 1 ELEVATION**  
(LOOKING FORWARD ON SURVEY - PIER NO. 1)  
(SEE SECTION "X-X" ON BR-123-10)

PIER NO. 1	BEAM 1	BEAM 2	BEAM 3	BEAM 4	BEAM 5	BEAM 6	BEAM 7	BEAM 8
BOTTOM OF BEAM EL.	523.21	523.42	523.64	523.85	523.86	523.67	523.48	523.30
SHIM HEIGHT (INCHES)	N/A	2 5/8"	5 1/8"	7 5/8"	7 5/8"	5 1/8"	2 3/4"	0 1/2"



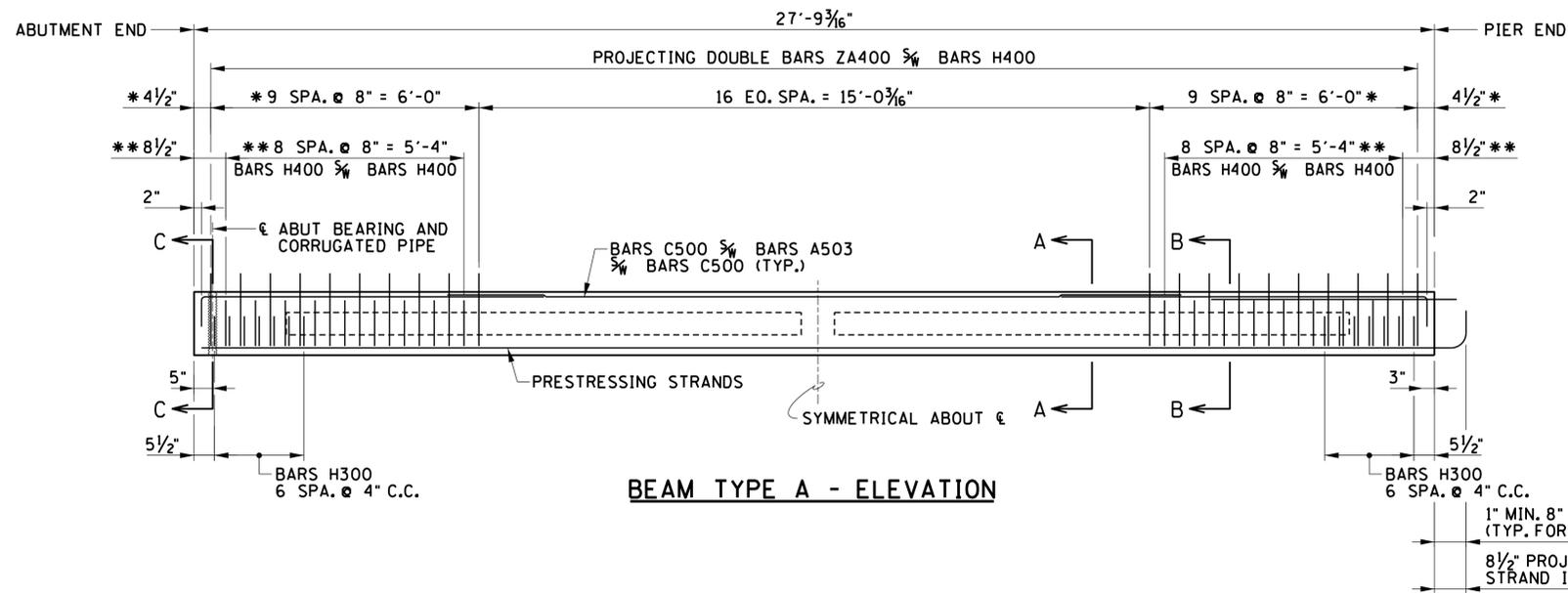
**PIER NO. 2 ELEVATION**  
(LOOKING FORWARD ON SURVEY - PIER NO. 2)  
(SEE SECTION "X-X" ON BR-123-10)

PIER NO. 2	BEAM 1	BEAM 2	BEAM 3	BEAM 4	BEAM 5	BEAM 6	BEAM 7	BEAM 8
BOTTOM OF BEAM EL.	523.06	523.28	523.49	523.70	523.71	523.52	523.34	523.15
SHIM HEIGHT (INCHES)	N/A	2 5/8"	5"	7 3/8"	7 3/8"	4 7/8"	2 5/8"	N/A

DESIGNED BY D. THOMPSON/D. KEATON  
DRAWN BY ANGELA MOORE  
SUPERVISED BY DARRELL JAMES  
CHECKED BY JAMIE GILLESPIE



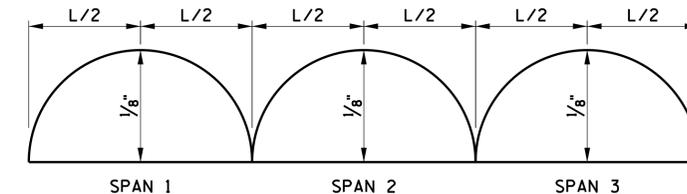
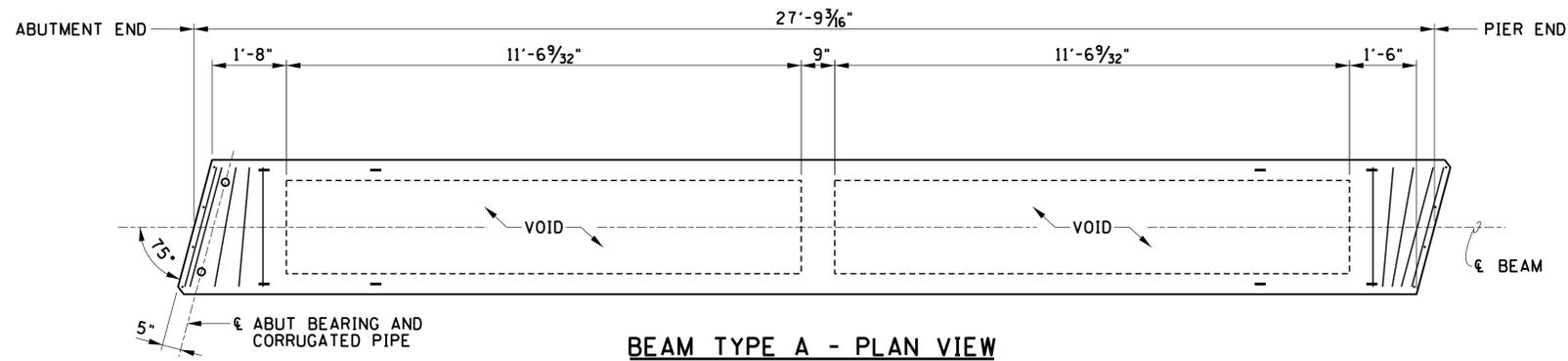
DATE \_\_\_\_\_  
DATE \_\_\_\_\_  
DATE \_\_\_\_\_  
DATE \_\_\_\_\_



**BEAM NOTES:**

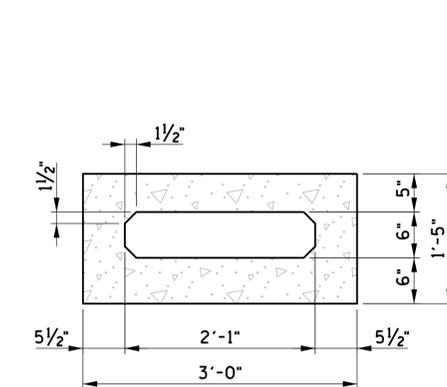
- \* DENOTES: PROJECTING BARS
- \*\* DENOTES: NON-PROJECTING BARS
- NOTE: 2'-0" MIN. SPLICE NO. 5 BARS (TYP.)
- NOTE: SEE STD-14-3 FOR BOX BEAM STANDARD DETAILS, NOTES AND REINFORCING. THE CONCRETE FOR THIS CONSTRUCTION SHALL BE OF SUCH PROPERTIES AS TO ATTAIN A COMPRESSIVE STRENGTH OF NOT LESS THAN 4,000 PSI AT THE AGE OF 28 DAYS AND STRESS TRANSFER SHALL NOT BE MADE TO THE BRIDGE MEMBER UNTIL THE TEST SPECIMENS INDICATE THAT THE CONCRETE HAS REACHED A COMPRESSIVE STRENGTH OF AT LEAST 6,000 PSI. SEE GENERAL NOTES (SHEET 2A) FOR CONCRETE FINISHING NOTE.

CONST. NO.			
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NO.	DATE	BY	BRIEF DESCRIPTION

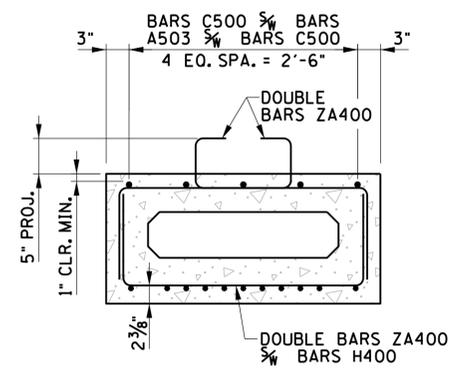


**DEAD LOAD CORRECTION CURVE**

NOTE: THIS CURVE IS FOR SLAB DEAD LOAD AND LOADS THAT ARE APPLIED AFTER THE SLAB IS IN PLACE.

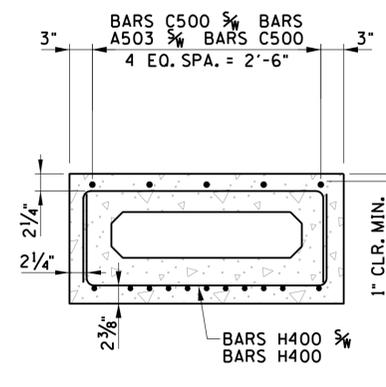


**SECTION SHOWING PROPERTIES**



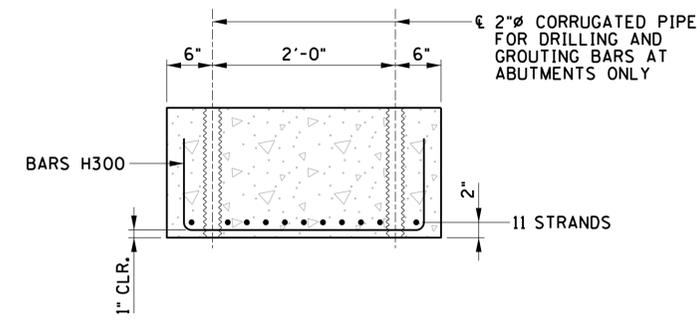
**SECTION "A-A"**

(SECTION SHOWING PROJECTING BARS)



**SECTION "B-B"**

(SECTION SHOWING NON-PROJECTING BARS)



**SECTION "C-C"**

(SHOWING PRESTRESSING STRANDS @ 11 TOTAL STRANDS)

**UNOFFICIAL SET**

NOT FOR BIDDING

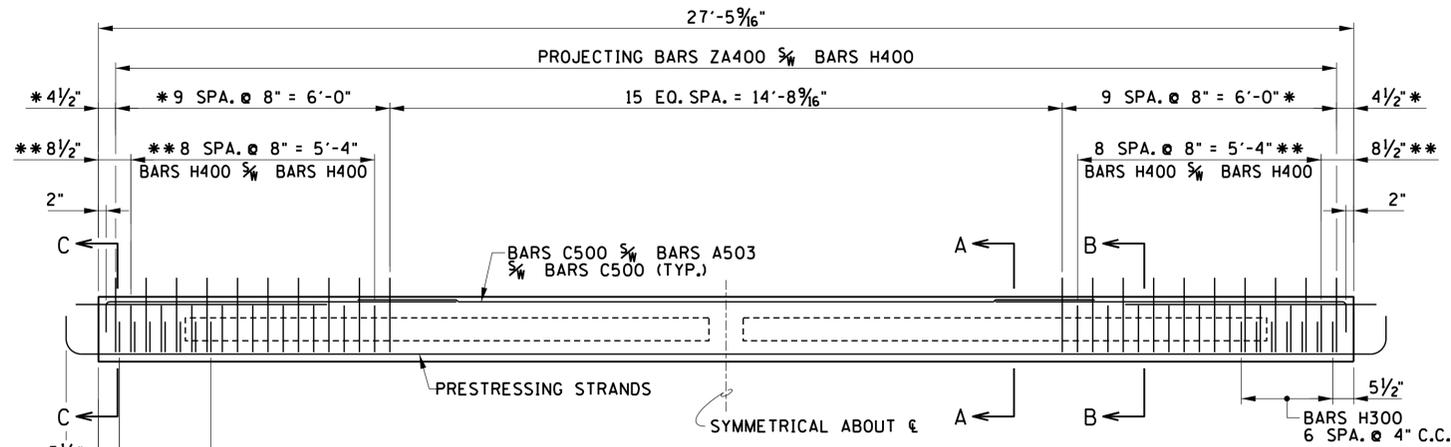
STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

PRESTRESSED CONCRETE  
BOX BEAM DETAILS  
(SPANS 1 AND 3)

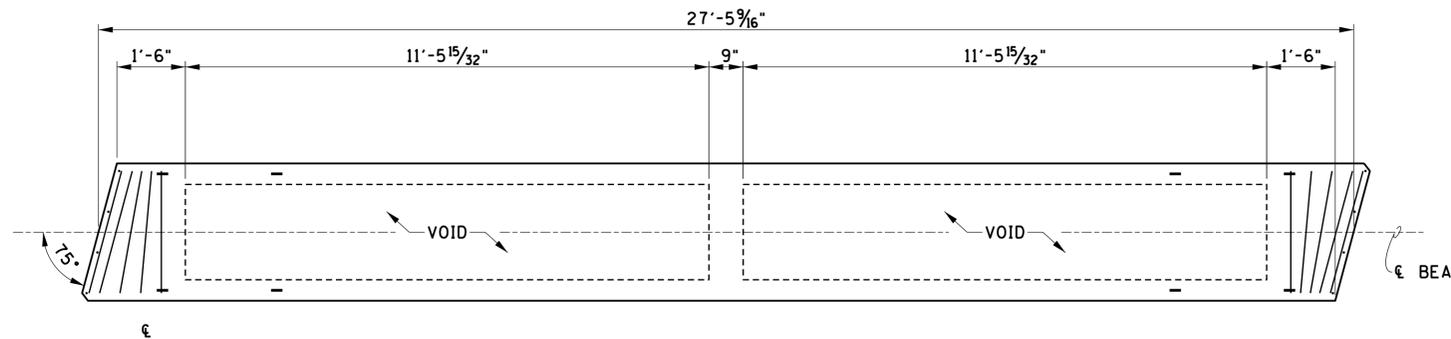
BRIDGE NO. 75-SR1-4.76  
STATE ROUTE 1  
OVER HARTS BRANCH  
RUTHERFORD COUNTY  
2016

DESIGNED BY D. THOMPSON/D. KEATON DATE \_\_\_\_\_  
 DRAWN BY ANGELA MOORE DATE \_\_\_\_\_  
 SUPERVISED BY DARRELL JAMES DATE \_\_\_\_\_  
 CHECKED BY JAMIE GILLESPIE DATE \_\_\_\_\_





**BEAM TYPE B - ELEVATION**

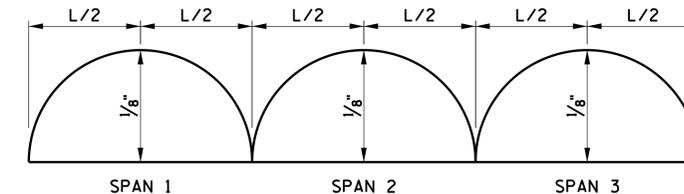


**BEAM TYPE B - PLAN VIEW**

**BEAM NOTES:**

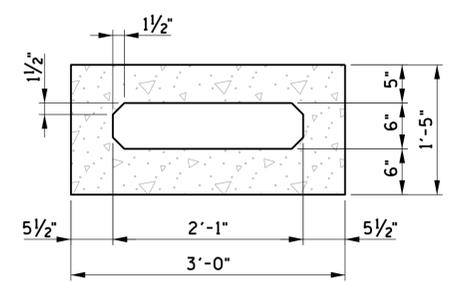
- \* DENOTES: PROJECTING BARS
- \*\* DENOTES: NON-PROJECTING BARS
- NOTE: 2'-0" MIN. SPLICE NO. 5 BARS (TYP.)
- NOTE: SEE STD-14-3 FOR BOX BEAM STANDARD DETAILS, NOTES AND REINFORCING. THE CONCRETE FOR THIS CONSTRUCTION SHALL BE OF SUCH PROPERTIES AS TO ATTAIN A COMPRESSIVE STRENGTH OF NOT LESS THAN 4,000 PSI AT THE AGE OF 28 DAYS AND STRESS TRANSFER SHALL NOT BE MADE TO THE BRIDGE MEMBER UNTIL THE TEST SPECIMENS INDICATE THAT THE CONCRETE HAS REACHED A COMPRESSIVE STRENGTH OF AT LEAST 6,000 PSI. SEE GENERAL NOTES (SHEET 2A) FOR CONCRETE FINISHING NOTE.

CONST. NO.			
PROJECT NO.	YEAR	SHEET NO.	
75002-4287-04	2016		
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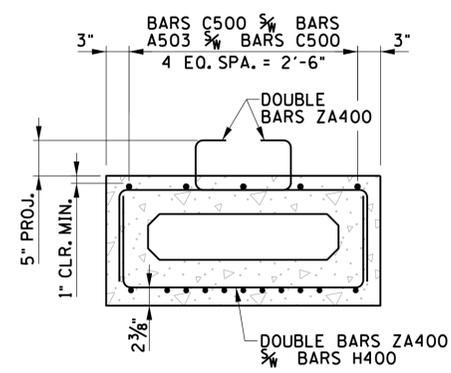


**DEAD LOAD CORRECTION CURVE**

NOTE: THIS CURVE IS FOR SLAB DEAD LOAD AND LOADS THAT ARE APPLIED AFTER THE SLAB IS IN PLACE.

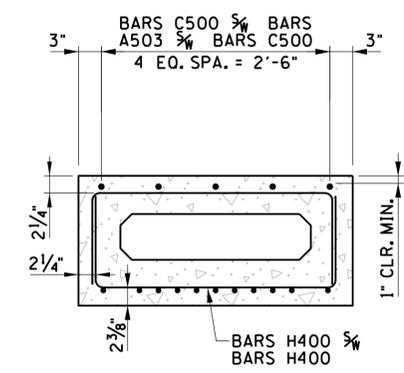


**SECTION SHOWING PROPERTIES**



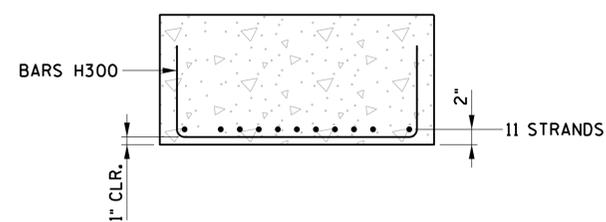
**SECTION "A-A"**

(SECTION SHOWING PROJECTING BARS)



**SECTION "B-B"**

(SECTION SHOWING NON-PROJECTING BARS)



**SECTION "C-C"**

(SHOWING PRESTRESSING STRANDS 11 TOTAL STRANDS)

**UNOFFICIAL SET**  
NOT FOR BIDDING

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

PRESTRESSED CONCRETE  
BOX BEAM DETAILS  
(SPAN 2)

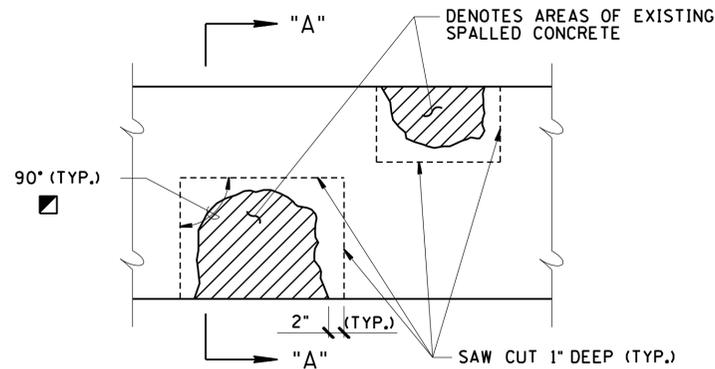
BRIDGE NO. 75-SR1-4.76  
STATE ROUTE 1  
OVER HARTS BRANCH  
RUTHERFORD COUNTY  
2016

DESIGNED BY D. THOMPSON/D. KEATON DATE \_\_\_\_\_  
DRAWN BY ANGELA MOORE DATE \_\_\_\_\_  
SUPERVISED BY DARRELL JAMES DATE \_\_\_\_\_  
CHECKED BY JAMIE GILLESPIE DATE \_\_\_\_\_







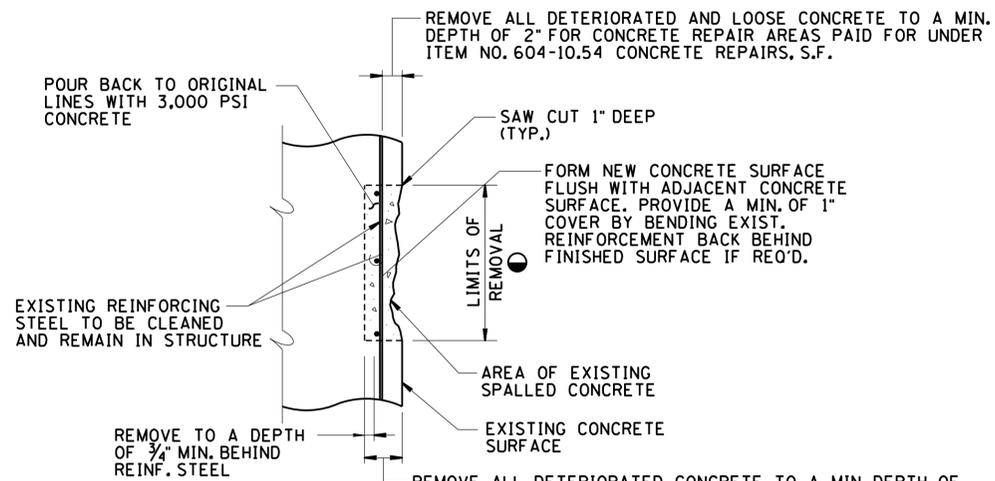


**DETAIL SHOWING AREAS OF EXISTING SPALLED CONCRETE SURFACES TO BE REMOVED AND REPAIRED**

- DENOTES: LIMITS AND LOCATION OF REPAIRS DESIGNATED ON THE CONCRETE REPAIR SHEETS.
- ▣ DENOTES: SAW CUT EXISTING CONCRETE SURFACES SO AS TO OBTAIN SQUARED CORNERS.

NOTE: THE COST OF REMOVING PORTIONS OF THE EXISTING CONCRETE WITHIN THE LIMITS SHOWN, SAW CUTTING, COMPLETELY CLEANING EXISTING REINFORCING STEEL, CONCRETE, FORMING, LABOR AND ALL MISCELLANEOUS MATERIAL, INCLUDING REINFORCING STEEL, NECESSARY TO COMPLETE THE REPAIRS AS SHOWN SHALL BE INCLUDED IN PRICE BID FOR ITEM NO. 604-10.05, CONCRETE, S.F. OR ITEM NO. 604-10.54, CONCRETE REPAIRS, S.F.

1. THE ENGINEER SHALL DESIGNATE ALL CONCRETE REPAIR AREAS IN THE FIELD. QUANTITIES GIVEN ARE APPROXIMATE. ITEM NO. 604-10.05 AND ITEM NO. 604-10.54 MAY BE INCREASED, DECREASED, OR ELIMINATED BY THE ENGINEER.
2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE STABILITY OF THE STRUCTURE DURING CONSTRUCTION. DESIGN CALCULATION AND DETAILS OF TEMPORARY SUPPORT SYSTEM OR FALSEWORK REQUIRED SHALL BE SUBMITTED TO THE ENGINEER FOR REVIEW AND SHALL MEET WITH THE FULL SATISFACTION OF THE ENGINEER BEFORE ANY DEMOLITION IS BEGUN. COST OF STABILIZING THE STRUCTURE SHALL BE INCLUDED IN ITEM NO. 602-10.05, BRACING REPAIRS, L.S..
3. THE ENGINEER SHALL HAVE THE OPTION OF DESIGNATING A SPALL AREA TO BE REPAIRED UNDER ITEM NO. 604-10.05 OR ITEM NO. 604-10.54. PATCHING MATERIAL FOR ITEM NO. 604-10.05 SHALL BE 3,000 PSI CONCRETE. PATCHING MATERIAL FOR ITEM NO. 604-10.54 SHALL BE A POLYMER MODIFIED CEMENTITIOUS STRUCTURAL PATCHING MATERIAL. SEE QUALIFIED PRODUCTS LIST 13, SECTION B.6.
4. EXTREME CARE SHALL BE TAKEN WHEN REMOVING THE EXISTING SPALLED CONCRETE SO AS NOT TO DAMAGE THE EXISTING REINFORCING STEEL. ALL EXPOSED REINFORCING STEEL SHALL RECEIVE A COMPLETE CLEANING TO REMOVE ALL RUST. ALL EXISTING REINFORCEMENT SHALL REMAIN IN PLACE. ALL WORK MUST MEET WITH THE FULL APPROVAL OF THE ENGINEER.
5. POWER DRIVEN HAND TOOLS USED FOR REMOVAL OF UNSOUND CONCRETE ARE SUBJECT TO THE FOLLOWING RESTRICTIONS:
  - A. PNEUMATIC HAMMERS HEAVIER THAN 35 LB. CLASS SHALL NOT BE USED.
  - B. CHIPPING HAMMERS OF THE 15 LB. CLASS SHALL BE USED TO REMOVE CONCRETE FROM BEHIND REINFORCING STEEL AND BEAM END REPAIRS.
6. EXTREME CARE SHALL BE TAKEN WHEN REMOVING EXISTING CONCRETE FROM OVER THE BENT CAPS SO AS NOT TO DAMAGE ANY REINFORCING STEEL TO REMAIN. ALL EXPOSED REINFORCING STEEL SHALL BE COMPLETELY CLEANED TO THE SATISFACTION OF THE ENGINEER.



**SECTION "A-A"**

BRIDGE NO. 75-SR1-4.76	
ESTIMATED QUANTITIES	
<b>ABUTMENT NO. 1</b>	
ITEM NO. 604-10.05 CONCRETE S.F.	ITEM NO. 604-10.54 CONCRETE REPAIRS S.F.
0	5
<b>PIER NO. 1</b>	
ITEM NO. 604-10.05 CONCRETE S.F.	ITEM NO. 604-10.54 CONCRETE REPAIRS S.F.
35	45
<b>PIER NO. 2</b>	
ITEM NO. 604-10.05 CONCRETE S.F.	ITEM NO. 604-10.54 CONCRETE REPAIRS S.F.
0	20
<b>ABUTMENT NO. 2</b>	
ITEM NO. 604-10.05 CONCRETE S.F.	ITEM NO. 604-10.54 CONCRETE REPAIRS S.F.
0	15
TOTAL FOR ITEM NO. 604-10.05 CONCRETE S.F.	TOTAL FOR ITEM NO. 604-10.54 CONCRETE REPAIRS S.F.
35	85

CONST. NO.			
PROJECT NO.	YEAR	SHEET NO.	
75002-4287-04	2016		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

**UNOFFICIAL SET**  
 NOT FOR BIDDING

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

CONCRETE REPAIR DETAILS

BRIDGE NO. 75-SR1-4.76  
STATE ROUTE 1  
OVER HARTS BRANCH  
RUTHERFORD COUNTY  
2016

DESIGNED BY D. THOMPSON/D. KEATON DATE \_\_\_\_\_  
 DRAWN BY ANGELA MOORE DATE \_\_\_\_\_  
 SUPERVISED BY DARRELL JAMES DATE \_\_\_\_\_  
 CHECKED BY JAMIE GILLESPIE DATE \_\_\_\_\_





