

Index Of Sheets
RESURFACING

SHEET NO.	DESCRIPTION
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2	ESTIMATED ROADWAY QUANTITIES
2A	TYPICAL SECTIONS AND PAVEMENT SCHEDULE
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2C	SPECIAL NOTES
2D	DETAILS

"NO PROJECT COMMITMENTS INCLUDED IN THIS SET OF PLANS"
"NO UTILITY SHEETS"

STANDARD ROADWAY DRAWINGS

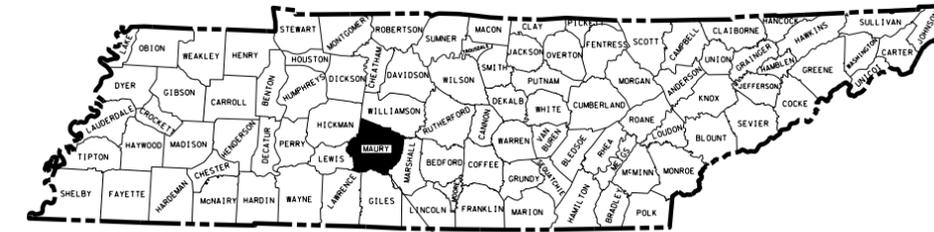
DWG. NO.	REV.	DESCRIPTION
ROADWAY DESIGN STANDARDS		
RD-A-1	12-18-99	STANDARD ABBREVIATIONS
RD-L-1	10-26-94	STANDARD LEGEND
DRAINAGE - CULVERTS AND ENDWALL		
D-PG-3	04-15-97	FERROUS AND ALUMINUM CORRUGATED METAL PIPE
SAFETY APPURTENANCES AND FENCE		
S-GR31-1		W-BEAM GUARDRAIL
S-GRT-2	06-30-14	TYPE 38 GUARDRAIL TERMINAL
S-GRT-2P		EARTH PAD FOR TYPE 38 TERMINAL
S-GRT-2R		EARTH PAD FOR TYPE 38 TERMINAL (RETROFIT)
TRAFFIC CONTROL APPURTENANCES		
T-M-1	07-24-14	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS AND MARKING ABBREVIATIONS
T-M-2	07-24-14	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS
T-M-3	07-24-14	MARKING STANDARDS FOR TRAFFIC ISLANDS, MEDIANS & PAVED SHOULDERS ON CONVENTIONAL ROADS
T-M-4	07-24-14	STANDARD INTERSECTION PAVEMENT MARKINGS
T-M-16	11-01-11	ASPHALT SHOULDER RUMBLE STRIPE INSTALLATION DETAILS FOR NON-ACCESS CONTROLLED ROUTES
T-WZ-10	04-02-12	ADVANCE ROAD WORK SIGNING ON HIGHWAYS AND FREEWAYS

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF ENGINEERING

MAURY COUNTY

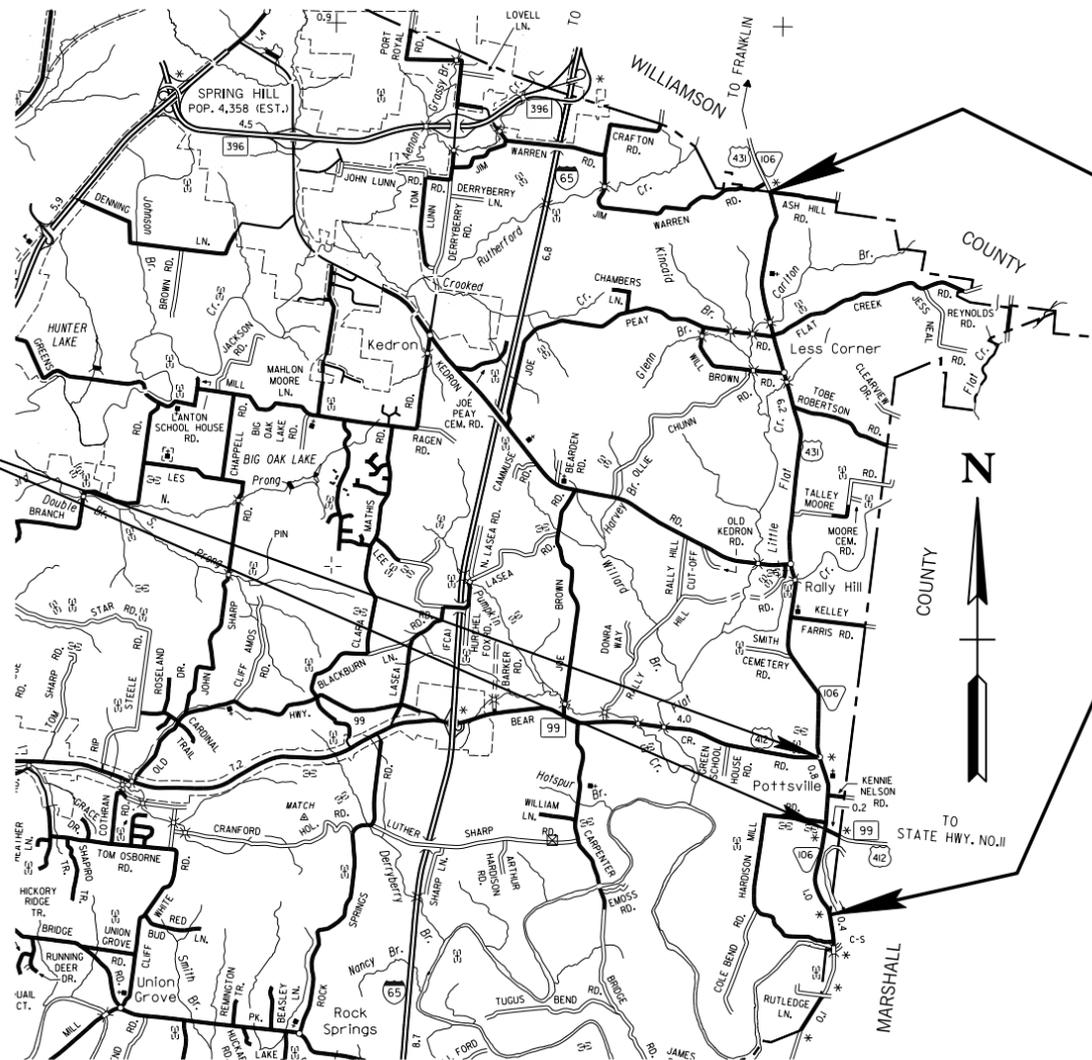
STATE ROUTE NO. 106
FROM MARSHALL COUNTY LINE (L.M. 0.00)
TO WILLIAMSON COUNTY LINE (L.M. 7.14)
RESURFACING

TENN.	YEAR	SHEET NO.
	2015	1
FED. AID PROJ. NO.	STP/HSIP-106(37)	
STATE PROJ. NO.	60014-8216-14	
STATE PROJ. NO.	60014-3216-94	



PROJECT LOCATION

STATE HIGHWAY NO. 106 F.A.H.S. NO. 431



RESURFACING PROJECT
STP/HSIP-99(49)
60011-8218-14, 60011-3218-94

THIS PROJECT IS TO BE LET WITH PROJECT
PIN NO. 120926.00

**UNOFFICIAL
SET
NOT FOR
BIDDING**

SPECIAL NOTES

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED MARCH 1, 2006 AND ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS AND IN THE PROPOSAL CONTRACT.

TDOT ROAD SP. SV. 2 BILLY BINION
DESIGNER SCOTT JOHNSON CHECKED BY JACOB BROOKS
P.E. NO. 98034-4299-04
PIN NO. 117717.00

PROJECT LENGTH **6.69 MILES**
TOTAL LANE MILES RESURFACED **13.4 MILES**

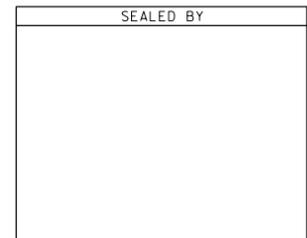
SCALE: 1" = 1 MILE

60014-8216-14
60014-3216-94
END PROJ. NO. STP/HSIP-106(37)
(L.M. 7.14)

60014-8216-14
60014-3216-94
BEGIN PROJ. NO. STP/HSIP-106(37)
(L.M. 0.00)

EXCLUSIONS	
L.M. TO L.M.	LENGTH (FT.)
0.530-0.960	2270.4
L.M. 2.94	96
-	-
-	-
-	-
TOTAL =	2366.4

TRAFFIC DATA	
ADT (2015)	4090
V	45-55 MPH



APPROVED: Paul D. Degges
PAUL D. DEGGES, CHIEF ENGINEER

DATE: _____

APPROVED: John Schroer
JOHN SCHROER, COMMISSIONER

U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION

APPROVED: _____
DIVISION ADMINISTRATOR DATE

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2015	STP/HSIP-106(37)	2

ESTIMATED ROADWAY QUANTITIES					
ITEM NO.	DESCRIPTION	UNIT	60014-8216-14 QUANTITY	60014-3216-94 QUANTITY	TOTAL QUANTITY
(1)(12)	303-01 MINERAL AGGREGATE, TYPE A BASE, GRADING D	TON	1400	50	1450
(2)	307-01.08 ASPHALT CONCRETE MIX (PG64-22) (BPMB-HM) GRADING B-M2	TON	750		750
	402-01 BITUMINOUS MATERIAL FOR PRIME COAT (PC)	TON	45		45
	403-01 BITUMINOUS MATERIAL FOR TACK COAT (TC)	TON	59		59
(3)(4)	411-02.10 ACS MIX(PG70-22) GRADING D	TON	8972		8972
	411-12.03 SCORING FOR RUMBLE STRIPE (NON-CONTINUOUS) (8IN WIDTH)	L.M.		10	10
	415-01.01 COLD PLANING BITUMINOUS PAVEMENT	TON	7824		7824
	607-39.02 18" PIPE CULVERT (SIDE DRAIN)	L.F.	20		20
	607-39.03 24" PIPE CULVERT (SIDE DRAIN)	L.F.	20		20
	705-02.02 SINGLE GUARDRAIL (TYPE 2)	L.F.		150	150
	705-04.07 TAN ENERGY ABSORBING TERM (NCHRP 350, TL3)	EACH		2	2
	706-01 GUARDRAIL REMOVED	L.F.		250	250
(5)	712-01 TRAFFIC CONTROL	LS	0.9		0.9
	712-04.01 FLEXIBLE DRUMS (CHANNELIZING)	EACH	25		25
	712-05.01 WARNING LIGHTS (TYPE A)	EACH	4		4
(6)	712-06 SIGNS (CONSTRUCTION)	S.F.	365		365
	716-01.21 SNWPLWBLE PVMT MRKRS (BI-DIR)(1 COLOR)	EACH		497	497
	716-01.22 SNWPLWBLE PVMT MRKRS (MONO-DIR)(1 COLOR)	EACH		10	10
(7)	716-02.04 PLASTIC PAVEMENT MARKING(CHANNELIZATION STRIPING)	S.Y.	66		66
(7)	716-02.06 PLASTIC PAVEMENT MARKING (TURN LANE ARROW)	EACH	6		6
(8)	716-05.01 PAINTED PAVEMENT MARKING (4" LINE)	L.M.	26		26
(9)	716-12.02 ENHANCED FLATLINE THERMO PVMT MRKNG (6IN LINE)	L.M.	12.5		12.5
	716-12.03 ENHANCED FLATLINE THERMO PVMT MRKNG (8IN BARRIER LINE)	L.F.	140		140
10(11)	716-13.02 SPRAY THERMO PVMT MRKNG (60 mil) (6IN LINE)	L.M.		13.5	13.5
	717-01 MOBILIZATION	LS	0.9		0.9

FOOTNOTES

- TO BE USED FOR GUARDRAIL ANCHORS.
- TO BE USED AS DIRECTED BY THE ENGINEER FOR SPOT LEVELING.
- FOR RIDEABILITY, SEE SPECIAL PROVISION 411C. FOR MATERIAL TRANSFER DEVICE, SEE SPECIAL PROVISION NO. 407G.
- 226 TONS TO BE USED FOR COUNTY ROADS, PRIVATE DRIVES, FIELD ENTRANCES AND BUISNESS ENTRANCES. 875 TONS TO BE USED AS DIRECTED BY THE ENGINEER.
- THE CONTRACTOR SHALL COMPLY WITH SECTION 712 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION REGARDING TEMPORARY TRAFFIC CONTROL AND THE CURRENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- IN THE EVENT THAT A CONSTRUCTION AND/OR REGULATORY SIGN IS TEMPOARILY DESIGNATED NOT IN USE DURING THE CONSTRUCTION PHASE OF A PROJECT, THE CONTRACTOR SHALL CHOOSE A SIGN COVERING METHOD FROM A DESIGNATED LIST OF METHODS ACCEPTED BY THE DEPARTMENT. SAID SIGN COVERING METHOD SHALL BE SELECTED FROM QUALIFIED PRODUCTS LIST 10, SECTION D OR APPROVED BY THE DIVISION OF MATERIALS AND TEST AND SHALL BE USED TO COVER THESE SIGNS. NO OTHER METHOD FOR SIGN COVERING WILL BE ALLOWED. TEMPORARY SIGN COVERINGS SHALL NOT BE PAID FOR DIRECTLY BUT SHALL BE INCLUDED IN THE PRICE BID FOR ITEM NO. 712-06 SIGNS (CONSTRUCTION).
- THE CONTRACTOR MAY ELECT TO SUBSITUTE PREFORMED PLASTIC FOR THERMOPLASTIC. PREFORMED PLASTIC SHALL BE PAID FOR AT THE SAME UNIT PRICE AS BID FOR THERMOPLASTIC.
- FOR TEMPORARY PAVEMENT MARKING ON COLD PLANED SURFACE.
- QUANTITY IS BASED ON 4% PASSING, 19% NO PASS ON ONE SIDE, AND 77% NO PASS BOTH SIDES FOR FINAL PAVEMENT MARKINGS FOR CENTERLINE.
- QUANTITY IS FOR FINAL PAVEMENT MARKINGS FOR EDGELINES.
- SEE SPECIAL PROVISIONS 716ST FOR FINAL PAVEMENT MARKING DETAILS.
- TO BE USED FOR SHOULDER REBUILD AS DIRECTED BY THE ENGINEER.

SIGNS (CONSTRUCTION) 712-06				
QUANTITY	M.U.T.C.D. NO.	DESCRIPTION	SIZE	712-06 S.F.
2	G20-1	ROAD WORK NEXT 8 MILES	64 x 24	22
2	G20-2	END ROAD WORK	48 x 24	16
2	W20-1	ROAD WORK 1 MILE	48 x 48	32
2	W20-1	ROAD WORK 1/2 MILE	48 x 48	32
2	W20-1	ROAD WORK 1000'	48 x 48	32
9	W20-1	ROAD WORK AHEAD	36 x 36	81
2	W20-4	ONE LANE ROAD 1000' - PORTABLE	36 x 36	18
2	W20-7a	FLAGGER SYMBOL - PORTABLE	36 x 36	18
2	W16-2	SUPPLEMENTAL PLATE	24 x 18	6
2	W21-2	FRESH OIL - PORTABLE	36 x 36	18
2	W21-5	SHOULDER WORK - PORTABLE	36 x 36	18
2	W8-9a	SHOULDER DROP-OFF - PORTABLE	36 x 36	18
6	W8-11	UNEVEN LANES - PORTABLE	36 x 36	54
TOTAL S.F.				365

OTHER SIGNS, AS DIRECTED BY THE ENGINEER MAY BE REQUIRED DURING VARIOUS PHASES OF CONSTRUCTION AND WILL BE MEASURED AND PAID FOR AT THE UNIT BID PRICE. SEE CURRENT M.U.T.C.D. FOR STANDARDS AND TYPICAL APPLICATIONS.

GUARDRAIL						
LOCATION (L.M. TO L.M.)	SIDE		GUARDRAIL AT BRIDGE 705-01.01 (L.F.)	GUARDRAIL SINGLE 705-02.02 (L.F.)	ANCHOR TYPE 38 705-04.07 (EACH)	GUARDRAIL REMOVED 706-01 (L.F.)
	LT	RT				
4.465 TO 4.513		X		150	2	250
			0	150	2	250

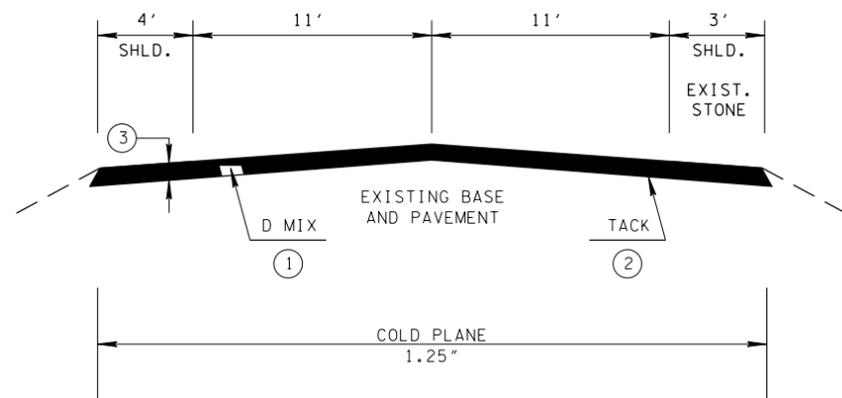
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DEPARTMENT OF TRANSPORTATION

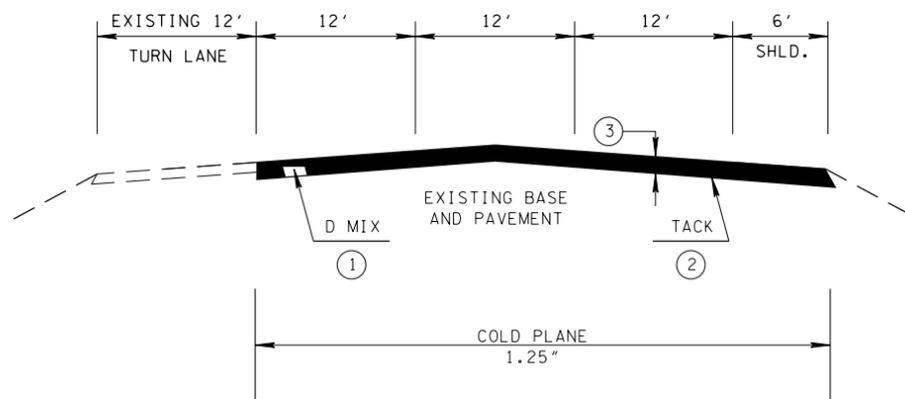
**ESTIMATED
ROADWAY
QUANTITIES**

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2015	STP/HSIP-106(37)	2A



SR-106 TYPICAL SECTION

L.M. 0.00 TO L.M. 0.53
L.M. 1.10 TO L.M. 7.14



SR-106 TYPICAL SECTION

L.M. 0.96 TO L.M. 1.10

* NOTE L.M. 0.53 TO L.M. 0.96 IS EXCLUDED

PROPOSED PAVEMENT SCHEDULE

- ① SURFACE @ 1.25" THICK (APPROX. 132.5 LBS/SY)
411-02.10 ACS MIX (PG70-22) GRADING D
- ② TACK COAT @ 0.1 GAL/SY
403-01 BITUMINOUS MATERIAL FOR TACK COAT (TC)
- ③ COLD PLANING @ 1.25"
415-01.01 COLD PLANING OF BITUMINOUS PAVEMENT

*** SPECIAL PAVING NOTE ***

ALL SHOULDERS WILL BE DEGRADED / CLIPPED AND CLEANED OF ALL VEGETATION, GRASS, AND DEBRIS. SEE SPECIAL NOTES. ALL COST TO BE INCLUDED IN OTHER ITEMS OF WORK.

Test Performed by TDOT Road Profiler									Average Roughness Pre-Construction HC-IRI Number Per Mile 80.1
Test Limits		Direction and Measured Roughness (HC-IRI Number Per Mile)							
From	To	NSEW	*Lane	Pre Ride Number	Payment Table	Max. # for Final Ride	Min. Required % Improvement	Percent Pay	
0.00	1.00	N	1	76	Table 1	55.0		100	
1.00	2.00	N	1	89	Table 2	62.3	30	100	
2.00	3.00	N	1	68	Table 1	55.0		100	
3.00	4.00	N	1	87	Table 2	60.9	30	100	
4.00	5.00	N	1	106	Table 2	74.2	30	100	
5.00	6.00	N	1	77	Table 1	55.0		100	
6.00	7.00	N	1	114	Table 2	79.8	30	100	
7.00	8.00	N	1	78	Table 1	55.0		100	
REMARKS									
0.00	1.00	S	1	72	Table 1	55.0		100	
1.00	2.00	S	1	78	Table 1	55.0		100	
2.00	3.00	S	1	70	Table 1	55.0		100	
3.00	4.00	S	1	69	Table 1	55.0		100	
4.00	5.00	S	1	71	Table 1	55.0		100	
5.00	6.00	S	1	68	Table 1	55.0		100	
6.00	7.00	S	1	83	Table 2	58.1	30	100	
7.00	8.00	S	1	75	Table 1	55.0		100	

*Lanes to be Left to Right each Direction - 1,2,3,4, etc. with 1 being the Inside Lane

BRIDGE NOTES

LOCATION	LENGTH (FT.)	RECOMMENDATION
L.M. 2.94	96	LEAVE AS IS
L.M. 5.06	34	PAVE WITH PLANS QUANTITY
L.M. 5.72	33	PAVE WITH PLANS QUANTITY

SPECIAL NOTE

THE CONTRACTOR SHOULD TAKE EXTREME CARE WHEN COLD PLANING THE EXISTING ASPHALT OFF BRIDGE DECK, SO AS NOT TO DAMAGE THE EXISTING DECK, SEALANT AND/OR EXPANSION JOINT MEMBERS (STEEL PLATES, BARS, AND/OR HEADERS). IF DAMAGE DOES OCCUR, THE CONTRACTOR SHALL BE RESPONSIBLE FOR NECESSARY REPAIRS ON ALL DAMAGED MEMBERS TO THE SATISFACTION OF OF THE PROJECT ENGINEER AT NO ADDITIONAL COST.

THERE ARE NO UTILITY ADJUSTMENTS ON THIS PROJECT

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DEPARTMENT OF TRANSPORTATION

TYPICAL
SECTIONS
AND
PAVEMENT
SCHEDULE

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2015	STP/HSIP-106(37)	2B

GENERAL NOTES

GUARDRAIL

- (1) THE PROPOSED GUARDRAIL, INCLUDING ANY ANCHOR SYSTEM, SHALL BE INSTALLED QUICKLY TO MINIMIZE TRAFFIC EXPOSURE TO ANY HAZARD. NO PAYMENT WILL BE MADE FOR A SECTION OF PROPOSED GUARDRAIL, INCLUDING ANCHORS, UNTIL IT IS COMPLETE IN PLACE.

DRAINAGE

- (2) EXCAVATION FOR SIDE DRAINS WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT WILL BE INCLUDED IN THE PRICE BID PER LINEAR FOOT OF PIPE.

MISCELLANEOUS

- (3) THE CONTRACTOR SHALL BE REQUIRED TO REMOVE AND RESET MAILBOXES WHERE AND AS DIRECTED BY THE ENGINEER.
- (4) NOTHING IN THE GENERAL NOTES OR SPECIAL PROVISIONS SHALL RELIEVE THE CONTRACTOR FROM HIS RESPONSIBILITIES TOWARD THE SAFETY AND CONVENIENCE OF THE GENERAL PUBLIC AND THE RESIDENTS ALONG THE PROPOSED CONSTRUCTION AREA

PAVEMENT MARKINGS

TEMPORARY PAVEMENT MARKING ON INTERMEDIATE LAYERS

- (5) TEMPORARY PAVEMENT LINE MARKINGS ON INTERMEDIATE LAYERS OF PAVEMENT SHALL BE REFLECTIVE TAPE OR REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAYS WORK. SHORT, UNMARKED SECTIONS SHALL NOT BE ALLOWED. THESE MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-05.01, PAINTED PAVEMENT MARKING (4" LINE), L.M.

FINAL PAVEMENT MARKING IF 6" ENHANCED FLATLINE THERMOPLASTIC IS USED

- (6) PERMANENT PAVEMENT LINE MARKINGS SHALL BE 6" ENHANCED FLATLINE THERMOPLASTIC INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-12.02, ENHANCED FLATLINE THERMO PVMT MRKNG (6IN LINE), L.M. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.

FINAL PAVEMENT MARKING IF 8" ENHANCED FLATLINE THERMOPLASTIC IS USED

- (7) PERMANENT PAVEMENT LINE MARKINGS SHALL BE 8" ENHANCED FLATLINE THERMOPLASTIC INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-12.03, ENHANCED FLATLINE THERMO PVMT MRKNG (8IN BARRIER LINE), L.F. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.

FINAL PAVEMENT MARKING IF 6" SPRAY THERMOPLASTIC (60 mil) IS USED

- (8) PERMANENT PAVEMENT LINE MARKINGS SHALL BE 6" SPRAY THERMOPLASTIC (60 mil) INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-13.02, SPRAY THERMO PVMT MRKNG (60 mil) (6IN LINE), L.M. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.

PAVEMENT

PAVING

- (9) THE CONTRACTOR SHALL BE REQUIRED TO COLD PLANE AND PAVE IN THE DIRECTION OF TRAFFIC.

RESURFACING

- (10) WHERE DIRECTED BY THE TDOT ENGINEER, THE CONTRACTOR SHALL BE REQUIRED TO SHAPE PUBLIC SIDE ROADS, BUSINESS ENTRANCES, AND PRIVATE DRIVES, AS WELL AS CLEANING OF EXISTING DRAINS BEFORE PLACING MATERIALS. ALL COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (11) ALL PUBLIC SIDE ROADS SHALL BE PAVED ONE PAVER WIDTH THROUGH THE INTERSECTION AS A MINIMUM. A SATISFACTORY TRANSITION FROM THE NEW PAVEMENT TO THE EXISTING GRADE OF THE INTERSECTING PUBLIC ROAD OR BUSINESS ENTRANCE SHALL BE PROVIDED. SHOULD THE PAVEMENT OF THE INTERSECTING PUBLIC ROAD BE DISTRESSED, THE RESURFACING WIDTH MAY BE INCREASED TO THE NORMAL RIGHT OF WAY LINE.
- (12) PRIVATE DRIVEWAYS, FIELD ENTRANCES, AND BUSINESS ENTRANCES WILL BE RESURFACED A PAVER WIDTH (LANE WIDTH) AS A MINIMUM. A PAVEMENT TAPER TO TRANSITION THE NEW PAVEMENT SHALL BE REQUIRED, IT SHALL BE BASED ON AN ADDITIONAL ONE FOOT OF WIDTH PER ONE INCH DEPTH OF PAVEMENT. IF THE SHOULDER IS NARROW ENOUGH THAT THE SUM OF THE SHOULDER AND THE TRANSITION ARE LESS THAN A PAVER WIDTH, THE TRANSITION SHALL OCCUR WITHIN THE PAVER WIDTH. IF THE SUM OF THE SHOULDER AND THE TRANSITION IS GREATER THAN A PAVER WIDTH (LANE WIDTH), THE TRANSITION SHALL OCCUR OUTSIDE OF THE PAVER WIDTH.
- (13) IN ALL CASES, THE LENGTH OF THE PAVEMENT TRANSITION, THE THICKNESS AND WIDTH OF THE RESURFACING AND ANY ADDITIONAL PAVEMENT MATERIALS SHALL BE AS DIRECTED BY THE TDOT ENGINEER.

CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

- (14) ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE WEEK BEFORE NEEDED, IF THE SIGN FACE IS FULLY COVERED.
- (15) IF THE CONTRACTOR MOVES OFF THE PROJECT, HE SHALL COVER OR REMOVE ALL UNNEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OF REMOVAL, COVERING, AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO 712-06, SIGNS (CONSTRUCTION) PER SQUARE FOOT.
- (16) A LONG TERM BUT SPORADIC USE WARNING SIGN, SUCH AS A FLAGGER SIGN, MAY REMAIN IN PLACE WHEN NOT REQUIRED PROVIDED THE SIGN FACE IS FULLY COVERED.
- (17) TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.
- (18) USE OF BARRICADES, PORTABLE BARRIER RAILS, VERTICAL PANELS, AND DRUMS SHALL BE LIMITED TO THE IMMEDIATE AREAS OF CONSTRUCTION WHERE A HAZARD IS PRESENT. THESE DEVICES SHALL NOT BE STORED ALONG THE ROADWAY WITHIN THIRTY (30) FEET OF THE EDGE OF THE TRAVELED WAY BEFORE OR AFTER USE UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL INCREASE TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. THESE DEVICES SHALL BE REMOVED FROM THE CONSTRUCTION WORK ZONE WHEN THE ENGINEER DETERMINES THEY ARE NO LONGER NEEDED. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.
- (19) THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY, WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHEN THE LANE IS OPEN TO TRAFFIC UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR

ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO PARK WITHIN THIRTY (30) FEET OF A OPEN TRAFFIC LANE AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE.. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.

- (20) ALL DETOUR AND CONSTRUCTION SIGNING SHALL BE IN STRICT ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

EROSION PREVENTION AND SEDIMENT CONTROL

LITTER, DEBRIS, WASTE, PETROLEUM

- (21) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT LITTER, CONSTRUCTION DEBRIS, AND CONSTRUCTION WASTES FROM ENTERING WATERS OF THE STATE/U.S. THESE MATERIALS WILL BE PICKED UP AND REMOVED FROM STORMWATER EXPOSURE PRIOR TO ANTICIPATED STORM EVENTS. AFTER USE, MATERIALS USED FOR EPSC WILL BE REMOVED FROM THE SITE.
- (22) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS TO ENSURE THAT PETROLEUM PRODUCTS OR OTHER CHEMICAL POLLUTANTS ARE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. ALL EQUIPMENT REFUELING, SERVICING, AND STAGING AREAS SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL LAWS, RULES, REGULATIONS, AND ORDINANCES, INCLUDING THOSE OF THE NATIONAL FIRE PROTECTION ASSOCIATION (NFPA). APPROPRIATE CONTAINMENT MEASURES FOR THESE AREAS SHALL BE USED. ALL SPILLS MUST BE REPORTED TO THE APPROPRIATE AGENCY, AND MEASURES SHALL BE TAKEN IMMEDIATELY TO PREVENT THE POLLUTION OF WATERS OF THE STATE/U.S., INCLUDING GROUNDWATER, SHOULD A SPILL OCCUR.

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DEPARTMENT OF TRANSPORTATION

**GENERAL
NOTES**

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2015	STP/HSIP-106(37)	2C

SPECIAL NOTES

MISCELLANEOUS

- (1) THE CONTRACTOR WILL BE REQUIRED TO PERFORM THE FOLLOWING WORK AND INCLUDE ALL COSTS THEREOF IN THE PRICES BID FOR OTHER ITEMS OF CONSTRUCTION: REPAIR OF SHOULDERS AND DITCHES. SHOULDERS SHALL BE DEGRASSED, MATERIAL SHALL BE PICKED UP AND REMOVED. CLEANING AND STRAIGHTENING ENDS OF EXISTING SIDEDRAINS AND CROSSEDRAINS WHERE DIRECTED BY THE ENGINEER.
- (2) THE CONTRACTOR SHALL TAKE EXTREME CARE WHEN WORKING AROUND EXISTING GUARDRAIL, HIGHWAY SIGNS, OR ANY DEPARTMENT OF TRANSPORTATION STRUCTURE TO AVOID DAMAGE DURING THE COURSE OF CONSTRUCTION. ANY DAMAGE WILL BE REPLACED OR REPAIRED AT HIS OWN EXPENSE.

PAVEMENT MARKINGS

- (3) THE CONTRACTOR WILL BE REQUIRED TO ESTABLISH AND LOCATE ALL NO-PASSING ZONES. THE ESTABLISHMENT OF NO-PASSING ZONES IS TO BE IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, 2009, PART III, MARKING. ALL NO-PASSING ZONES WILL BE APPROVED BY THE ENGINEER WITH ADVICE FROM THE REGIONAL TRAFFIC ENGINEER PRIOR TO THE MARKING APPLICATION.
- (4) THE CONTRACTOR IS TO PROVIDE THE LAYOUT OF ALL EXISTING PAVEMENT MARKINGS BEFORE BEGINNING THE RESURFACING PROJECT. EXISTING MARKINGS SHALL BE REFERENCED AS NEEDED TO ENSURE THAT ALL NEW MARKINGS ARE REPLACED IN THEIR CORRESPONDING LOCATIONS UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

PAVEMENT

PAVING

- (5) SURFACE TO BE CROWNED AS DIRECTED BY THE ENGINEER.
- (6) ALL STRUCTURES (DRAINAGE STRUCTURES, MANHOLES, UTILITY BOXES, ETC.) SHALL HAVE A FINISHED GRADE COMPATIBLE WITH THE FINISHED GRADE OF THE ROADWAY.

RESURFACING

- (7) DURING COLD PLANING OPERATION, A MINIMUM OF ONE LANE OF TRAFFIC IN EACH DIRECTION WILL REMAIN OPEN.
- (8) AFTER ANY COLD PLANING OPERATION, TEMPORARY STRIPING WILL BE REQUIRED BEFORE RETURNING TRAFFIC ONTO EXCAVATED ROADWAY.

CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

- (9) THE CONSTRUCTION SIGNS, FLAGMEN AND OTHER TRAFFIC CONTROL DEVICES WILL BE IN ACCORDANCE WITH THE CURRENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- (10) ONE ADVANCE ROAD CONSTRUCTION AHEAD SIGN IS TO BE PLACED ON ALL SIDE ROADS PRIOR TO THEIR INTERSECTION WITH THE PROPOSED PROJECT. LOCATION TO BE DETERMINED IN THE FIELD BY THE ENGINEER OR CONTRACTORS SUPERINTENDENT.

EROSION PREVENTION AND SEDIMENT CONTROL

FOR GUARDRAIL TERMINALS

- (11) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS BEFORE CONSTRUCTING EARTH PADS FOR THE TYPE 38 TERMINALS TO ENSURE THAT ENVIRONMENTAL FEATURES (STREAMS, WETLANDS, OR SPRINGS) ARE NOT IMPACTED DURING CONSTRUCTION. IF THE CONTRACTOR / TDOT INSPECTOR IS UNSURE WHETHER THE ENVIRONMENTAL FEATURES ARE STREAMS, SPRINGS OR WETLANDS, THE INSPECTOR SHALL CONTACT THE TDOT ENVIRONMENTAL DIVISION ECOLOGY OR PERMITS SECTION SO AN ACCURATE DETERMINATION CAN BE ASSESSED.
- (12) DURING CONSTRUCTION OF THE EARTH PADS FOR THE TYPE 38 GUARDRAIL TERMINALS, EXISTING VEGETATION WILL BE REQUIRED TO BE REMOVED FOR THE PROPOSED PAD AND SLOPES. IF DURING THE VEGETATION REMOVAL, ANY TREES GREATER THAN 5 INCHES IN DIAMETER ARE REQUIRED TO BE REMOVED, CONTACT THE TDOT ENVIRONMENTAL DIVISION ECOLOGY SECTION IMMEDIATELY. DUE TO THE RECENT MODIFICATION OF THE USFWS STANCE REGARDING THE INDIANA BAT NO TREES > 5" DBH SHOULD BE CUT WITHOUT CLEARANCE FROM THE TDOT ENVIRONMENTAL DIVISION ECOLOGY SECTION.

NPDES

- (13) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS, BEFORE CLEANING OUT OR REMOVAL/REPLACEMENT OF DITCHES, SIDE DRAINS, AND CROSS DRAINS, TO ENSURE THAT THE DRAINAGE FEATURES / WATERCOURSES ARE NOT STREAMS OR WETLANDS. IF THE CONTRACTOR / TDOT INSPECTOR IS UNSURE WHETHER THE DRAINAGE FEATURES ARE STREAMS OR WETLANDS, THE INSPECTOR SHALL CONTACT THE TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION TO OBTAIN THE APPROPRIATE PERMITS.
- (14) DUE TO THE TYPE OF WORK IT IS OUR UNDERSTANDING THAT THE PROJECT WILL NOT DISTURB MORE THAN ONE ACRE. IF THE DISTURBED AREA IS MORE THAN ONE ACRE, PLEASE CONTACT THE TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION BECAUSE A NPDES PERMIT WILL BE REQUIRED.

PAVEMENT EDGE DROP-OFF TRAFFIC CONTROL NOTES

- (15) DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES OR TRAFFIC LANE AND SHOULDER WHERE THE TRAFFIC LANE IS BEING USED BY TRAFFIC, CAUSED BY BASE, PAVING OR RESURFACING:
 1. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 0.75 INCH AND NOT EXCEEDING 2 INCHES:
 - a. WARNING SIGNS, UNEVEN PAVEMENT (W8-11) AND/OR SHOULDER DROP-OFF (W8-9A), SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.
 - b. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY ADDED PAVEMENT SHALL BE ELIMINATED WITHIN THREE WORKDAYS.
 - c. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY COLD PLANING SHALL BE ELIMINATED WITHIN THREE WORKDAYS.
 - d. WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE TRAFFIC LANE BEING UTILIZED BY TRAFFIC AND SHOULDER THE DIFFERENCE IN ELEVATION SHALL BE ELIMINATED WITHIN SEVEN WORKDAYS AFTER THE CONDITION IS CREATED.

WORK RESTRICTIONS

- (16) NO WORK SHALL BE ALLOWED DURING THE FOLLOWING HOLIDAY PERIODS:
 - A) MULE DAY WEEKEND
 - B) THANKSGIVING
 - C) MEMORIAL DAY
 - D) JULY 4 th
 - E) LABOR DAY
 - F) VETERANS DAY

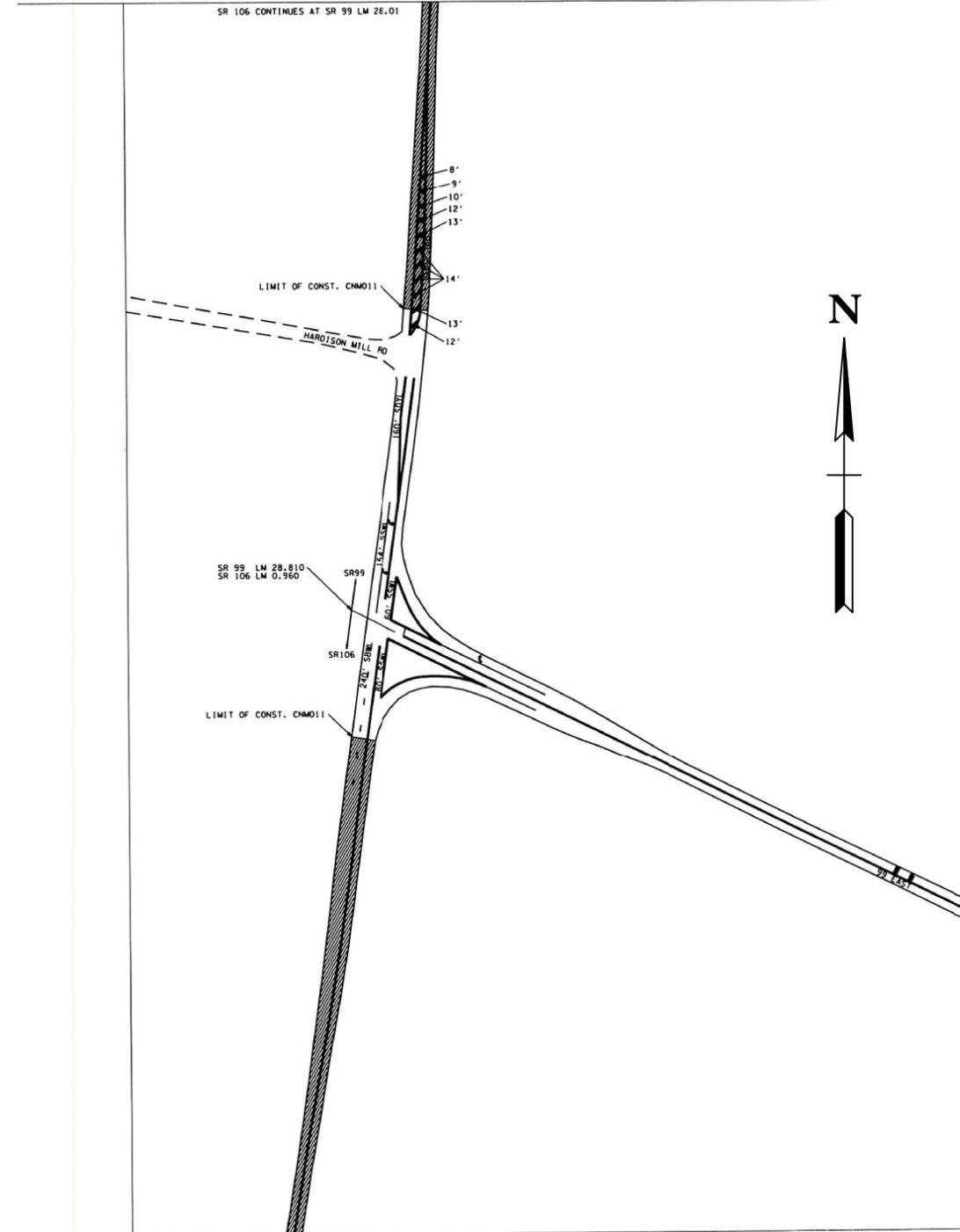
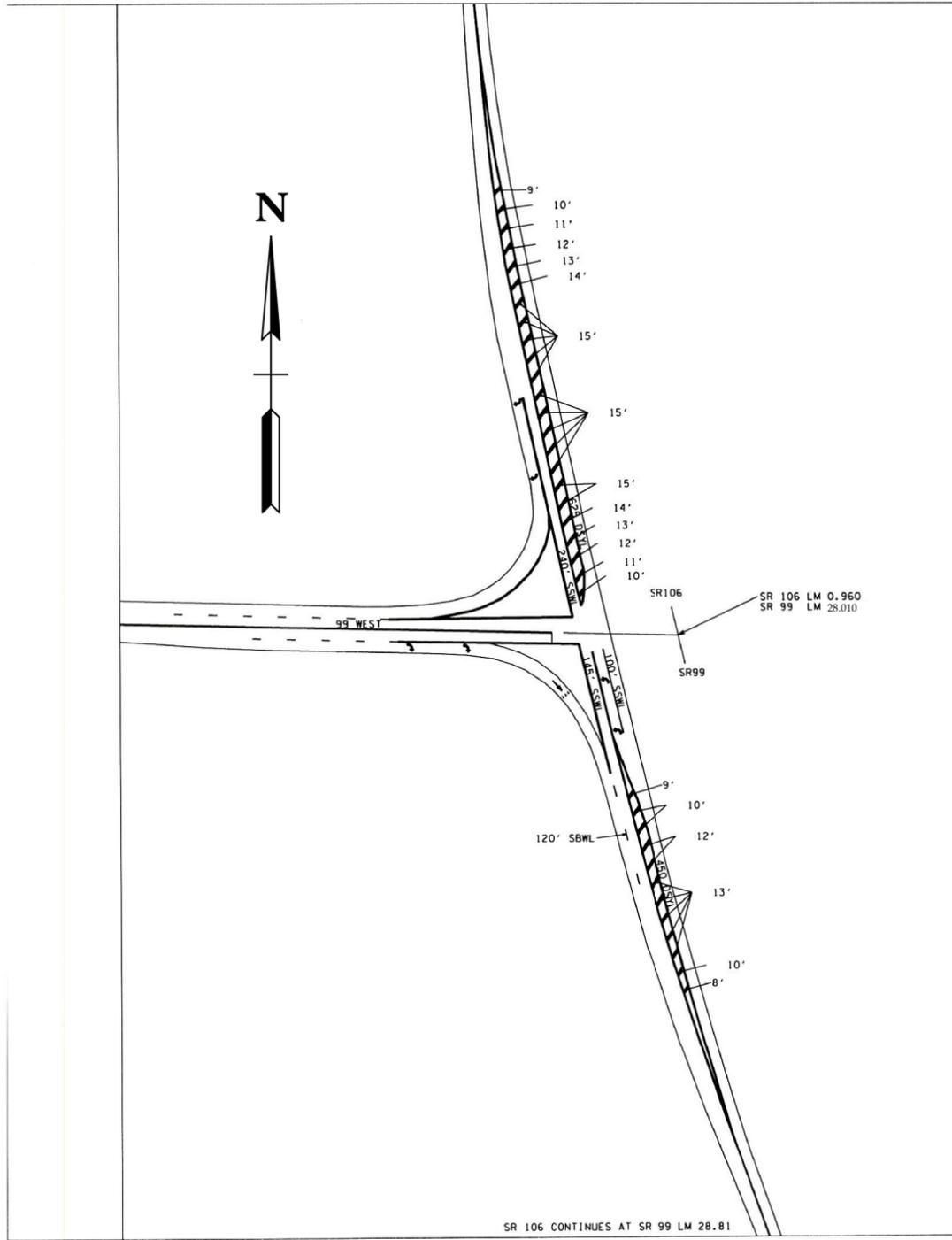
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**SPECIAL
NOTES**

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2015	STP/HSIP-106(37)	20



NOTE : NOT TO SCALE

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DEPARTMENT OF TRANSPORTATION

DETAILS

Index Of Sheets
RESURFACING

SHEET NO.	DESCRIPTION
1	TITLE SHEET
2	ESTIMATED ROADWAY QUANTITIES
2A	TYPICAL SECTIONS AND PAVEMENT SCHEDULE
2B	GENERAL NOTES
2C	SPECIAL NOTES
"NO PROJECT COMMITMENTS INCLUDED IN THIS SET OF PLANS"	
"NO UTILITY SHEETS"	

STANDARD ROADWAY DRAWINGS

DWG. NO.	REV.	DESCRIPTION
ROADWAY DESIGN STANDARDS		
RD-A-1	12-18-99	STANDARD ABBREVIATIONS
RD-L-1	10-26-94	STANDARD LEGEND
TRAFFIC CONTROL APPURTENANCES		
T-M-1	07-24-14	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS AND MARKING ABBREVIATIONS
T-M-2	07-24-14	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS
T-M-3	07-24-14	MARKING STANDARDS FOR TRAFFIC ISLANDS, MEDIANS & PAVED SHOULDERS ON CONVENTIONAL ROADS
T-M-4	07-24-14	STANDARD INTERSECTION PAVEMENT MARKINGS
T-M-16	11-01-11	ASPHALT SHOULDER RUMBLE STRIPE INSTALLATION DETAILS FOR NON-ACCESS CONTROLLED ROUTES
T-WZ-10	04-02-12	ADVANCE ROAD WORK SIGNING ON HIGHWAYS AND FREEWAYS

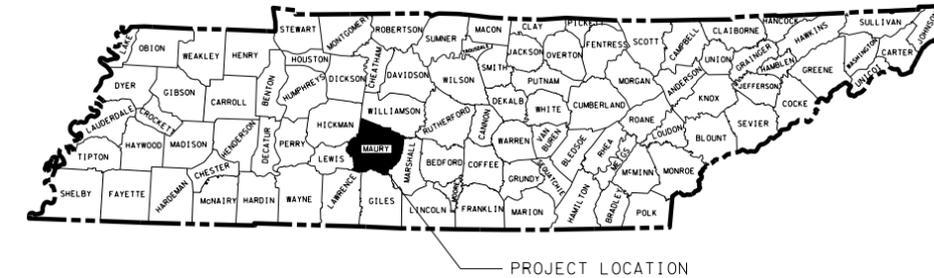
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF ENGINEERING

MAURY COUNTY

STATE ROUTE NO. 99
FROM RAMP FROM STATE ROUTE 106 (L.M. 28.01)
TO NORTH OF KENNIE NELSON RD. (L.M. 28.41)
RESURFACING

STATE HIGHWAY NO. 99 F.A.H.S. NO. 431

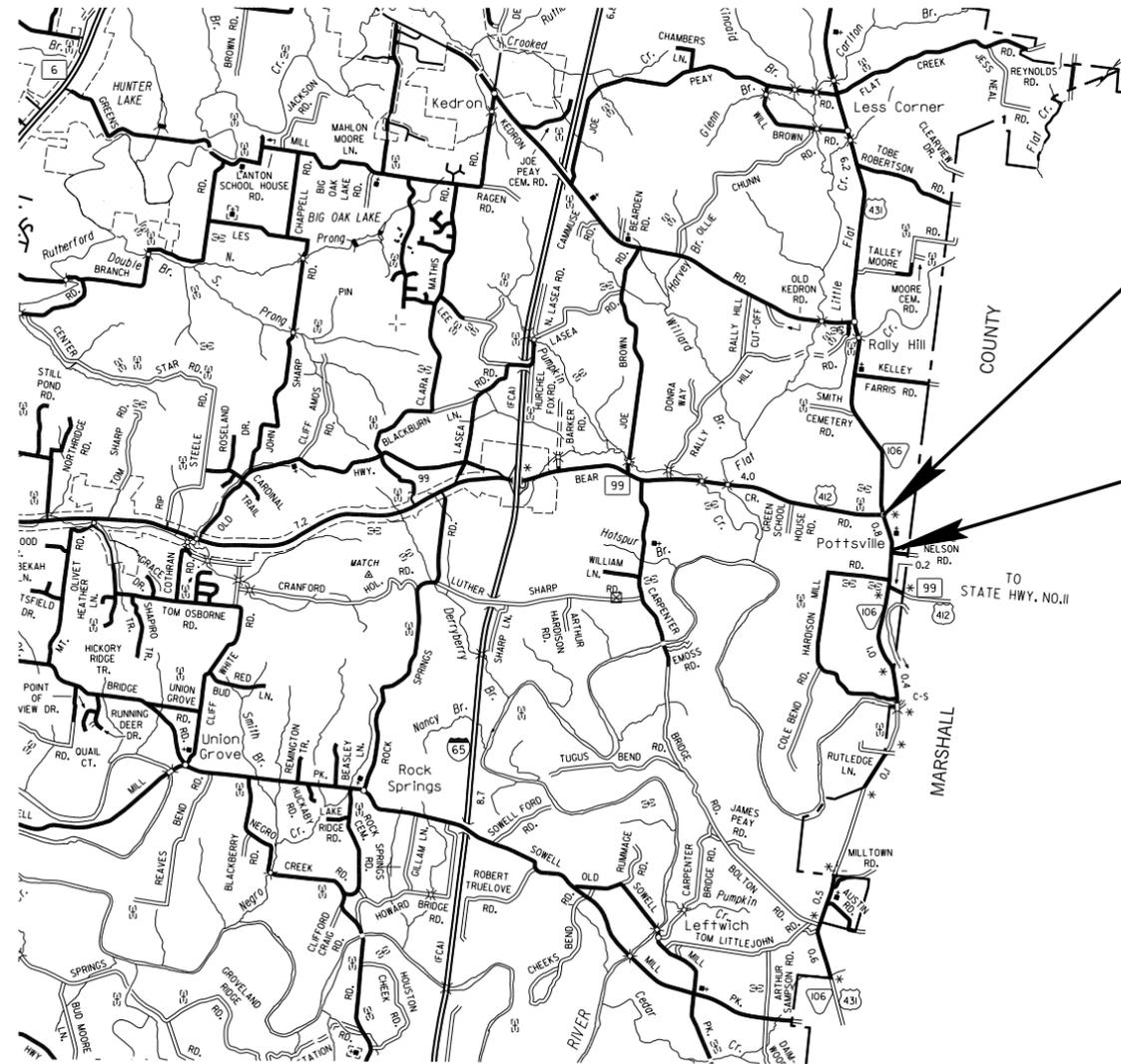
TENN.	YEAR	SHEET NO.
	2015	1
FED. AID PROJ. NO.	STP/HSIP-99(49)	
STATE PROJ. NO.	60011-8218-14	
STATE PROJ. NO.	60011-3218-94	



PROJECT LOCATION

THIS PROJECT IS TO BE LET WITH PROJECT
PIN NO. 117717.00

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SCALE: 1" = 1 MILE

60011-8218-14
60011-3218-94
BEGIN PROJ. NO. STP/HSIP-99(49)
(L.M. 28.01)

60011-8218-14
60011-3218-94
END PROJ. NO. STP/HSIP-99(49)
(L.M. 28.41)

NO EXCLUSIONS



TRAFFIC DATA	
ADT (2015)	7030
V	45 MPH

APPROVED: *Paul D. Degges*
PAUL D. DEGGES, CHIEF ENGINEER

DATE: _____

APPROVED: *John Schroer*
JOHN SCHROER, COMMISSIONER

SPECIAL NOTES

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED MARCH 1, 2006 AND ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS AND IN THE PROPOSAL CONTRACT.

PROJECT LENGTH 0.40 MILES
TOTAL LANE MILES RESURFACED 0.8 MILES

TDOT ROAD SP. SV. 2 BILLY BINION

DESIGNER SCOTT JOHNSON CHECKED BY ALVIN WHITE

P.E. NO. 98034-4299-04

PIN NO. 120926.00

U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION

APPROVED: _____
DIVISION ADMINISTRATOR DATE

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2015	STP/HSIP-99(49)	2

ESTIMATED ROADWAY QUANTITIES

ITEM NO.	DESCRIPTION	UNIT	60011-8218-14 QUANTITY	60011-3218-94 QUANTITY	TOTAL QUANTITY
(11)	303-01 MINERAL AGGREGATE, TYPE A BASE, GRADING D	TON	215		215
(1)	307-01.08 ASPHALT CONCRETE MIX (PG64-22) (BPMB-HM) GRADING B-M2	TON	300		300
(11)	402-01 BITUMINOUS MATERIAL FOR PRIME COAT (PC)	TON	3		3
	403-01 BITUMINOUS MATERIAL FOR TACK COAT (TC)	TON	5		5
(2)(3)	411-02.10 ACS MIX(PG70-22) GRADING D	TON	760		760
	411-12.03 SCORING FOR RUMBLE STRIPE (NON-CONTINUOUS) (8IN WIDTH)	L.M.		1	1
	415-01.01 COLD PLANING BITUMINOUS PAVEMENT	TON	700		730
(4)	712-01 TRAFFIC CONTROL	LS	0.1		0.1
	712-04.01 FLEXIBLE DRUMS (CHANNELIZING)	EACH	25		25
	712-05.01 WARNING LIGHTS (TYPE A)	EACH	4		4
(5)	712-06 SIGNS (CONSTRUCTION)	S.F.	316		316
	716-01.21 SNWPLWBLE PVMT MRKRS (BI-DIR)(1 COLOR)	EACH		75	75
	716-01.22 SNWPLWBLE PVMT MRKRS (MONO-DIR)(1 COLOR)	EACH		15	15
(6)	716-02.04 PLASTIC PAVEMENT MARKING(CHANNELIZATION STRIPING)	S.Y.	26		26
(6)	716-02.06 PLASTIC PAVEMENT MARKING (TURN LANE ARROW)	EACH	3		3
(7)	716-05.01 PAINTED PAVEMENT MARKING (4" LINE)	L.M.	2.5		2.5
(8)	716-12.02 ENHANCED FLATLINE THERMO PVMT MRKNG (6IN LINE)	L.M.	1.5		1.5
	716-12.03 ENHANCED FLATLINE THERMO PVMT MRKNG (8IN BARRIER LINE)	L.F.	285		285
(9)(10)	716-13.02 SPRAY THERMO PVMT MRKNG (60 mil) (6IN LINE)	L.M.		1	1
	717-01 MOBILIZATION	LS	0.1		0.1

FOOTNOTES

1. TO BE USED AS DIRECTED BY THE ENGINEER FOR SPOT LEVELING.
2. FOR RIDEABILITY, SEE SPECIAL PROVISION 411C. FOR MATERIAL TRANSFER DEVICE, SEE SPECIAL PROVISION NO. 407G.
3. 64 TONS TO BE USED FOR COUNTY ROADS, PRIVATE DRIVES, FIELD ENTRANCES AND BUSINESS ENTRANCES.
4. THE CONTRACTOR SHALL COMPLY WITH SECTION 712 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION REGARDING TEMPORARY TRAFFIC CONTROL AND THE CURRENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
5. IN THE EVENT THAT A CONSTRUCTION AND/OR REGULATORY SIGN IS TEMPORARILY DESIGNATED NOT IN USE DURING THE CONSTRUCTION PHASE OF A PROJECT, THE CONTRACTOR SHALL CHOOSE A SIGN COVERING METHOD FROM A DESIGNATED LIST OF METHODS ACCEPTED BY THE DEPARTMENT. SAID SIGN COVERING METHOD SHALL BE SELECTED FROM QUALIFIED PRODUCTS LIST 10, SECTION D OR APPROVED BY THE DIVISION OF MATERIALS AND TEST AND SHALL BE USED TO COVER THESE SIGNS. NO OTHER METHOD FOR SIGN COVERING WILL BE ALLOWED. TEMPORARY SIGN COVERINGS SHALL NOT BE PAID FOR DIRECTLY BUT SHALL BE INCLUDED IN THE PRICE BID FOR ITEM NO. 712-06 SIGNS (CONSTRUCTION).
6. THE CONTRACTOR MAY ELECT TO SUBSTITUTE PREFORMED PLASTIC FOR THERMOPLASTIC. PREFORMED PLASTIC SHALL BE PAID FOR AT THE SAME UNIT PRICE AS BID FOR THERMOPLASTIC.
7. FOR TEMPORARY PAVEMENT MARKING ON INTERMEDIATE LAYERS AND COLD PLANED SURFACE.
8. QUANTITY IS BASED ON 100% NO PASS BOTH SIDES FOR FINAL PAVEMENT MARKINGS FOR CENTERLINE.
9. QUANTITY IS FOR FINAL PAVEMENT MARKINGS FOR EDGELINES.
10. SEE SPECIAL PROVISIONS 716ST FOR FINAL PAVEMENT MARKINGS DETAIL.
11. TO BE USED FOR SHOULDER REBUILD AS DIRECTED BY THE ENGINEER.

THERE ARE NO UTILITY ADJUSTMENTS ON THIS PROJECT

THERE ARE NO BRIDGES IN PROJECT LIMITS

SIGNS (CONSTRUCTION) 712-06

QUANTITY	M.U.T.C.D. NO.	DESCRIPTION	SIZE	712-06 S.F.
2	G20-2	END ROAD WORK	48 x 24	16
2	W20-1	ROAD WORK 1 MILE	48 x 48	32
2	W20-1	ROAD WORK 1/2 MILE	48 x 48	32
2	W20-1	ROAD WORK 1000'	48 x 48	32
6	W20-1	ROAD WORK AHEAD	36 x 36	54
2	W20-4	ONE LANE ROAD 1000' - PORTABLE	36 x 36	18
2	W20-7a	FLAGGER SYMBOL - PORTABLE	36 x 36	18
2	W16-2	SUPPLEMENTAL PLATE	24 x 18	6
2	W21-2	FRESH OIL - PORTABLE	36 x 36	18
2	W21-5	SHOULDER WORK - PORTABLE	36 x 36	18
2	W8-9a	SHOULDER DROP-OFF - PORTABLE	36 x 36	18
6	W8-11	UNEVEN LANES - PORTABLE	36 x 36	54
TOTAL S.F.				316

OTHER SIGNS, AS DIRECTED BY THE ENGINEER MAY BE REQUIRED DURING VARIOUS PHASES OF CONSTRUCTION AND WILL BE MEASURED AND PAID FOR AT THE UNIT BID PRICE. SEE CURRENT M.U.T.C.D. FOR STANDARDS AND TYPICAL APPLICATIONS.

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DEPARTMENT OF TRANSPORTATION

ESTIMATED
ROADWAY
QUANTITIES

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2015	STP/HSIP-99(49)	2B

GENERAL NOTES

MISCELLANEOUS

- (1) THE CONTRACTOR SHALL BE REQUIRED TO REMOVE AND RESET MAILBOXES WHERE AND AS DIRECTED BY THE ENGINEER.
- (2) NOTHING IN THE GENERAL NOTES OR SPECIAL PROVISIONS SHALL RELIEVE THE CONTRACTOR FROM HIS RESPONSIBILITIES TOWARD THE SAFETY AND CONVENIENCE OF THE GENERAL PUBLIC AND THE RESIDENTS ALONG THE PROPOSED CONSTRUCTION AREA

PAVEMENT MARKINGS

TEMPORARY PAVEMENT MARKING ON INTERMEDIATE LAYERS

- (3) TEMPORARY PAVEMENT LINE MARKINGS ON INTERMEDIATE LAYERS OF PAVEMENT SHALL BE REFLECTIVE TAPE OR REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAYS WORK. SHORT, UNMARKED SECTIONS SHALL NOT BE ALLOWED. THESE MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-05.01, PAINTED PAVEMENT MARKING (4" LINE), L.M.

FINAL PAVEMENT MARKING IF 6" ENHANCED FLATLINE THERMOPLASTIC IS USED

- (4) PERMANENT PAVEMENT LINE MARKINGS SHALL BE 6" ENHANCED FLATLINE THERMOPLASTIC INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-12.02, ENHANCED FLATLINE THERMO PVMT MRKNG (6IN LINE), L.M. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.

FINAL PAVEMENT MARKING IF 8" ENHANCED FLATLINE THERMOPLASTIC IS USED

- (5) PERMANENT PAVEMENT LINE MARKINGS SHALL BE 8" ENHANCED FLATLINE THERMOPLASTIC INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-12.03, ENHANCED FLATLINE THERMO PVMT MRKNG (8IN BARRIER LINE), L.F. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.

FINAL PAVEMENT MARKING IF 6" SPRAY THERMOPLASTIC (60 mil) IS USED

- (6) PERMANENT PAVEMENT LINE MARKINGS SHALL BE 6" SPRAY THERMOPLASTIC (60 mil) INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-13.02, SPRAY THERMO PVMT MRKNG (60 mil) (6IN LINE), L.M. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.

PAVEMENT

PAVING

- (7) THE CONTRACTOR SHALL BE REQUIRED TO COLD PLANE AND PAVE IN THE DIRECTION OF TRAFFIC.

RESURFACING

- (8) WHERE DIRECTED BY THE TDOT ENGINEER, THE CONTRACTOR SHALL BE REQUIRED TO SHAPE PUBLIC SIDE ROADS, BUSINESS ENTRANCES, AND PRIVATE DRIVES, AS WELL AS CLEANING OF EXISTING DRAINS BEFORE

PLACING MATERIALS. ALL COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.

- (9) ALL PUBLIC SIDE ROADS SHALL BE PAVED ONE PAVER WIDTH THROUGH THE INTERSECTION AS A MINIMUM. A SATISFACTORY TRANSITION FROM THE NEW PAVEMENT TO THE EXISTING GRADE OF THE INTERSECTING PUBLIC ROAD OR BUSINESS ENTRANCE SHALL BE PROVIDED. SHOULD THE PAVEMENT OF THE INTERSECTING PUBLIC ROAD BE DISTRESSED, THE RESURFACING WIDTH MAY BE INCREASED TO THE NORMAL RIGHT OF WAY LINE.
- (10) PRIVATE DRIVEWAYS, FIELD ENTRANCES, AND BUSINESS ENTRANCES WILL BE RESURFACED A PAVER WIDTH (LANE WIDTH) AS A MINIMUM. A PAVEMENT TAPER TO TRANSITION THE NEW PAVEMENT SHALL BE REQUIRED, IT SHALL BE BASED ON AN ADDITIONAL ONE FOOT OF WIDTH PER ONE INCH DEPTH OF PAVEMENT. IF THE SHOULDER IS NARROW ENOUGH THAT THE SUM OF THE SHOULDER AND THE TRANSITION ARE LESS THAN A PAVER WIDTH, THE TRANSITION SHALL OCCUR WITHIN THE PAVER WIDTH. IF THE SUM OF THE SHOULDER AND THE TRANSITION IS GREATER THAN A PAVER WIDTH (LANE WIDTH), THE TRANSITION SHALL OCCUR OUTSIDE OF THE PAVER WIDTH.
- (11) IN ALL CASES, THE LENGTH OF THE PAVEMENT TRANSITION, THE THICKNESS AND WIDTH OF THE RESURFACING AND ANY ADDITIONAL PAVEMENT MATERIALS SHALL BE AS DIRECTED BY THE TDOT ENGINEER.

CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

- (12) ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE WEEK BEFORE NEEDED, IF THE SIGN FACE IS FULLY COVERED.
- (13) IF THE CONTRACTOR MOVES OFF THE PROJECT, HE SHALL COVER OR REMOVE ALL UNNEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OF REMOVAL, COVERING, AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO 712-06, SIGNS (CONSTRUCTION) PER SQUARE FOOT.
- (14) A LONG TERM BUT SPORADIC USE WARNING SIGN, SUCH AS A FLAGGER SIGN, MAY REMAIN IN PLACE WHEN NOT REQUIRED PROVIDED THE SIGN FACE IS FULLY COVERED.
- (15) TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.
- (16) USE OF BARRICADES, PORTABLE BARRIER RAILS, VERTICAL PANELS, AND DRUMS SHALL BE LIMITED TO THE IMMEDIATE AREAS OF CONSTRUCTION WHERE A HAZARD IS PRESENT. THESE DEVICES SHALL NOT BE STORED ALONG THE ROADWAY WITHIN THIRTY (30) FEET OF THE EDGE OF THE TRAVELED WAY BEFORE OR AFTER USE UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL INCREASE TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. THESE DEVICES SHALL BE REMOVED FROM THE CONSTRUCTION WORK ZONE WHEN THE ENGINEER DETERMINES THEY ARE NO LONGER NEEDED. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.
- (17) THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY, WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHEN THE LANE IS OPEN TO TRAFFIC UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO PARK WITHIN THIRTY (30) FEET OF A OPEN TRAFFIC LANE AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED

SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.

- (18) ALL DETOUR AND CONSTRUCTION SIGNING SHALL BE IN STRICT ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

EROSION PREVENTION AND SEDIMENT CONTROL

LITTER, DEBRIS, WASTE, PETROLEUM

- (19) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT LITTER, CONSTRUCTION DEBRIS, AND CONSTRUCTION WASTES FROM ENTERING WATERS OF THE STATE/U.S. THESE MATERIALS WILL BE PICKED UP AND REMOVED FROM STORMWATER EXPOSURE PRIOR TO ANTICIPATED STORM EVENTS. AFTER USE, MATERIALS USED FOR EPSC WILL BE REMOVED FROM THE SITE.
- (20) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS TO ENSURE THAT PETROLEUM PRODUCTS OR OTHER CHEMICAL POLLUTANTS ARE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. ALL EQUIPMENT REFUELING, SERVICING, AND STAGING AREAS SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL LAWS, RULES, REGULATIONS, AND ORDINANCES, INCLUDING THOSE OF THE NATIONAL FIRE PROTECTION ASSOCIATION (NFPA). APPROPRIATE CONTAINMENT MEASURES FOR THESE AREAS SHALL BE USED. ALL SPILLS MUST BE REPORTED TO THE APPROPRIATE AGENCY, AND MEASURES SHALL BE TAKEN IMMEDIATELY TO PREVENT THE POLLUTION OF WATERS OF THE STATE/U.S., INCLUDING GROUNDWATER, SHOULD A SPILL OCCUR.

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**GENERAL
NOTES**

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2015	STP/HSIP-99(49)	2C

SPECIAL NOTES

MISCELLANEOUS

- (1) THE CONTRACTOR WILL BE REQUIRED TO PERFORM THE FOLLOWING WORK AND INCLUDE ALL COSTS THEREOF IN THE PRICES BID FOR OTHER ITEMS OF CONSTRUCTION: REPAIR OF SHOULDERS AND DITCHES. SHOULDERS SHALL BE DEGRASSED, MATERIAL SHALL BE PICKED UP AND REMOVED. CLEANING AND STRAIGHTENING ENDS OF EXISTING SIDEDRAINS AND CROSSDRAINS WHERE DIRECTED BY THE ENGINEER.

PAVEMENT MARKINGS

- (2) THE CONTRACTOR WILL BE REQUIRED TO ESTABLISH AND LOCATE ALL NO-PASSING ZONES. THE ESTABLISHMENT OF NO-PASSING ZONES IS TO BE IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, 2009, PART III, MARKING. ALL NO-PASSING ZONES WILL BE APPROVED BY THE ENGINEER WITH ADVICE FROM THE REGIONAL TRAFFIC ENGINEER PRIOR TO THE MARKING APPLICATION.
- (3) THE CONTRACTOR IS TO PROVIDE THE LAYOUT OF ALL EXISTING PAVEMENT MARKINGS BEFORE BEGINNING THE RESURFACING PROJECT. EXISTING MARKINGS SHALL BE REFERENCED AS NEEDED TO ENSURE THAT ALL NEW MARKINGS ARE REPLACED IN THEIR CORRESPONDING LOCATIONS UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

PAVEMENT

PAVING

- (4) SURFACE TO BE CROWNED AS DIRECTED BY THE ENGINEER.

RESURFACING

- (5) DURING COLD PLANING OPERATION, A MINIMUM OF ONE LANE OF TRAFFIC IN EACH DIRECTION WILL REMAIN OPEN.
- (6) AFTER ANY COLD PLANING OPERATION, TEMPORARY STRIPING WILL BE REQUIRED BEFORE RETURNING TRAFFIC ONTO EXCAVATED ROADWAY.

CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

- (7) THE CONSTRUCTION SIGNS, FLAGMEN AND OTHER TRAFFIC CONTROL DEVICES WILL BE IN ACCORDANCE WITH THE CURRENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- (8) ONE ADVANCE ROAD CONSTRUCTION AHEAD SIGN IS TO BE PLACED ON ALL SIDE ROADS PRIOR TO THEIR INTERSECTION WITH THE PROPOSED PROJECT. LOCATION TO BE DETERMINED IN THE FIELD BY THE ENGINEER OR CONTRACTORS SUPERINTENDENT.

EROSION PREVENTION AND SEDIMENT CONTROL

NPDES

- (9) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS, BEFORE CLEANING OUT OR REMOVAL/REPLACEMENT OF DITCHES, SIDE DRAINS, AND CROSS DRAINS, TO ENSURE THAT THE DRAINAGE FEATURES / WATERCOURSES ARE NOT STREAMS OR WETLANDS. IF THE CONTRACTOR / TDOT INSPECTOR IS UNSURE WHETHER THE DRAINAGE FEATURES ARE STREAMS OR WETLANDS, THE INSPECTOR SHALL CONTACT THE TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION TO OBTAIN THE APPROPRIATE PERMITS.
- (10) DUE TO THE TYPE OF WORK IT IS OUR UNDERSTANDING THAT THE PROJECT WILL NOT DISTURB MORE THAN ONE ACRE. IF THE DISTURBED AREA IS MORE THAN ONE ACRE, PLEASE CONTACT THE TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION BECAUSE A NPDES PERMIT WILL BE REQUIRED.

PAVEMENT EDGE DROP-OFF TRAFFIC CONTROL NOTES

- (11) DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES OR TRAFFIC LANE AND SHOULDER WHERE THE TRAFFIC LANE IS BEING USED BY TRAFFIC, CAUSED BY BASE, PAVING OR RESURFACING:
 - 1. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 0.75 INCH AND NOT EXCEEDING 2 INCHES:
 - a. WARNING SIGNS, UNEVEN PAVEMENT (W8-11) AND/OR SHOULDER DROP-OFF (W8-9A), SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER

EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

- b. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY ADDED PAVEMENT SHALL BE ELIMINATED WITHIN THREE WORKDAYS.
- c. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY COLD PLANING SHALL BE ELIMINATED WITHIN THREE WORKDAYS.
- d. WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE TRAFFIC LANE BEING UTILIZED BY TRAFFIC AND SHOULDER THE DIFFERENCE IN ELEVATION SHALL BE ELIMINATED WITHIN SEVEN WORKDAYS AFTER THE CONDITION IS CREATED.

WORK RESTRICTIONS

- (12) NO WORK SHALL BE ALLOWED DURING THE FOLLOWING HOLIDAY PERIODS:

- A) MULE DAY WEEKEND
- B) THANKSGIVING
- C) MEMORIAL DAY
- D) JULY 4th
- E) LABOR DAY
- F) VETERANS DAY

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