

Index Of Sheets
SEE SHEET NO. IA

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF ENGINEERING

TENN.	YEAR	SHEET NO.
	2015	1
FED. AID PROJ. NO.	BR-STP-128(23)	
STATE PROJ. NO.	68006-3215-94	

PERRY COUNTY

S.R. 128

BRIDGE OVER CEDAR CREEK (L.M. 6.78)

CONST.

STATE HIGHWAY NO. 128 F.A.H.S. NO. 128



68006-3215-94
END PROJECT BR-STP-128(23) CONST.

STA. 35+50
N 439693.3371
E 1395850.3852



68006-3215-94
BEGIN PROJECT BR-STP-128(23) CONST.

STA. 21+39.18
N 438428.8384
E 1395469.8747

SPECIAL NOTES

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED JANUARY 1, 2015 AND ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS AND IN THE PROPOSAL CONTRACT.

TDOT PROJ. SPEC. SPV. 2 _____ ROBERT BRAUN P.E.
DESIGNER WAYNE HALL CHECKED BY FRANK RAINEAR
P.E. NO. 68006-1215-94 (DESIGN)
PIN NO. 115686.00

ROADWAY LENGTH 0.231 MILES
BRIDGE LENGTH 0.036 MILES
BOX BRIDGE LENGTH 0.000 MILES
PROJECT LENGTH 0.267 MILES

SCALE: 1" = 5280'

**NO EXCLUSIONS
NO EQUATIONS**

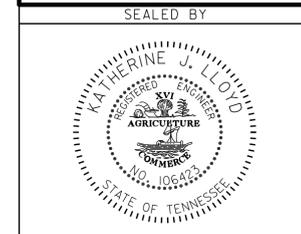
TRAFFIC DATA	
ADT (2015)	620
ADT (2035)	700
DHV (2035)	91
D	65 - 35
T (ADT)	2 %
T (DHV)	1 %
V	50 MPH

SURVEY REC: 10/10/2012
UPDATE REC: 12/4/2012

DESIGN EXCEPTION: APPROVED 5/27/2014

- 1: 30 MPH HORIZONTAL CURVE RADIUS @ PI STA. 24+19.41 INSTEAD OF 50 MPH
- 2: 40 MPH SUPERELEVATION FOR CURVE @ PI STA. 30+25.44 INSTEAD OF 50 MPH
- 3: 40 MPH SUPERELEVATION FOR CURVE @ PI STA. 33+22.58 INSTEAD OF 50 MPH
- 4: 30 MPH VERTICAL CURVE @ VPI STA. 24+87.60 INSTEAD OF 50 MPH

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APPROVED: Paul D. Degges
PAUL D. DEGGES, CHIEF ENGINEER

DATE: _____

APPROVED: [Signature]
JOHN SCHROER, COMMISSIONER

U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION

APPROVED: _____
DIVISION ADMINISTRATOR DATE

TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	2015	BR-STP-128(23)	1 A

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"NO PROJECT COMMITMENT SHEET INCLUDED IN THIS SET OF PLANS"

STANDARD BRIDGE DRAWINGS

DWG. NO.	REV.	DESCRIPTION
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BRIDGE APPURTENANCES ENGLISH (NEW STRUCTURES)

STD-1-1	5-01-14	BRIDGE RAILING CONCRETE PARAPET
STD-1-2	3-28-08	SLIDER PLATES AND DECK DRAINS
STD-1-5	3-26-14	PAVEMENT AT BRIDGE ENDS
STD-1-6	4-28-97	BRIDGE END DRAIN W/ PABE
STD-1-7	8-24-11	BRIDGE END DRAIN W/ PABE
STD-1-8	5-01-95	BRIDGE END DRAIN 2'x8'-7" W/ PABE

STANDARD ROADWAY DRAWINGS

DWG. NO	REV.	DESCRIPTION
ROADWAY DESIGN STANDARDS		
RD-A-1	12-18-99	STANDARD ABBREVIATIONS
RD-L-1	10-26-94	STANDARD LEGEND
RD-L-2	09-05-01	STANDARD LEGEND FOR UTILITY INSTALLATIONS
RD-L-5	05-01-08	STANDARD LEGEND FOR EROSION PREVENTION AND SEDIMENT CONTROL
RD-L-6	03-30-10	STANDARD LEGEND FOR EROSION PREVENTION AND SEDIMENT CONTROL
RD-L-7	05-24-12	STANDARD LEGEND FOR EROSION PREVENTION AND SEDIMENT CONTROL
RD01-S-11	04-04-03	DESIGN AND CONSTRUCTION DETAILS FOR ROADSIDE SLOPE DEVELOPMENT
RD01-S-11A	10-15-02	ROADSIDE DITCH DETAILS FOR DESIGN AND CONSTRUCTION
RD01-SE-3	10-15-02	RURAL SUPERELEVATION DETAILS
RD01-TS-2	10-15-02	DESIGN STANDARDS FOR COLLECTOR ROADS AND STREETS
ROADWAY AND PAVEMENT APPURTENANCES		
RP-R-1	05-27-01	STANDARD RAMPS TO SIDE ROADS
SAFETY APPURTENANCES AND FENCE		
S-F-1	05-24-12	HIGH VISIBILITY FENCE
S-PL-3		SAFETY PLAN MINIMUM INSTALLATION AT BRIDGE ENDS
S-GR31-1	12-01-14	W-BEAM GUARDRAIL
S-GRC-1		GUARDRAIL CONNECTED TO BRIDGE ENDS OR BARRIER WALL
S-GRT-2	11-03-14	TYPE 38 GUARDRAIL TERMINAL
S-GRT-2P		EARTH PAD FOR TYPE 38 TERMINAL
S-RP-2	01-19-99	STANDARD CONCRETE RIGHT-OF-WAY MARKERS
TRAFFIC CONTROL APPURTENANCES		
T-M-1	07-24-14	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS AND MARKING ABBREVIATIONS
T-M-2	07-24-14	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS
T-PBR-1	06-30-09	INTERCONNECTED PORTABLE BARRIER RAIL
T-PBR-2	11-01-11	DETAIL FOR VERTICAL PANELS AND FLEXIBLE DELINEATORS
T-S-16	06-05-14	GROUND MOUNTED ROADSIDE SIGN AND DETAILS
T-S-17	07-19-13	STANDARD GROUND MOUNTED SIGN USING PERFORATED/KNOCKOUT SQUARE TUBE
T-S-19	07-19-13	STANDARD STEEL SIGN SUPPORTS
T-S-20	11-01-11	SIGN DETAILS
T-WZ-32	10-29-13	TRAFFIC CONTROL PLAN SIGNAL LAYOUT FOR TRAFFIC SIGNAL AT TWO LANE BRIDGE RECONSTRUCTION SITE
T-WZ-33	05-27-98	TRAFFIC CONTROL PLAN FOR CLOSE INTERSECTION CONDITIONS USING TRAFFIC SIGNAL AT TWO LANE BRIDGE RECONSTRUCTION SITE
T-WZ-34	09-01-05	TRAFFIC CONTROL PLAN GENERAL NOTES FOR TRAFFIC SIGNAL AT TWO LANE BRIDGE RECONSTRUCTION SITE

DWG. NO	REV.	DESCRIPTION
T-WZ-35	04-02-12	TRAFFIC CONTROL PLAN PAY ITEM AND SIGN DETAILS FOR TRAFFIC SIGNAL AT TWO LANE BRIDGE RECONSTRUCTION SITE
EROSION PREVENTION AND SEDIMENT CONTROL		
EC-STR-2	08-01-12	SEDIMENT FILTER BAG
EC-STR-3C	08-01-12	SILT FENCE WITH WIRE BACKING
EC-STR-3E	04-01-08	SILT FENCE FABRIC JOINING DETAILS
EC-STR-6	08-01-12	ROCK CHECK DAM
EC-STR-6A	08-01-12	ENHANCED ROCK CHECK DAM
EC-STR-25	08-01-12	TEMPORARY CULVERT CROSSING, CONSTRUCTION EXIT, CONSTRUCTION FORD
EC-STR-30		INSTREAM DIVERSION (WITHOUT TRAFFIC)
EC-STR-30A		INSTREAM DIVERSION (WITH TRAFFIC)
EC-STR-34	08-01-12	EROSION CONTROL BLANKET FOR SLOPE INSTALLATION
EC-STR-37	06-10-14	SEDIMENT TUBE

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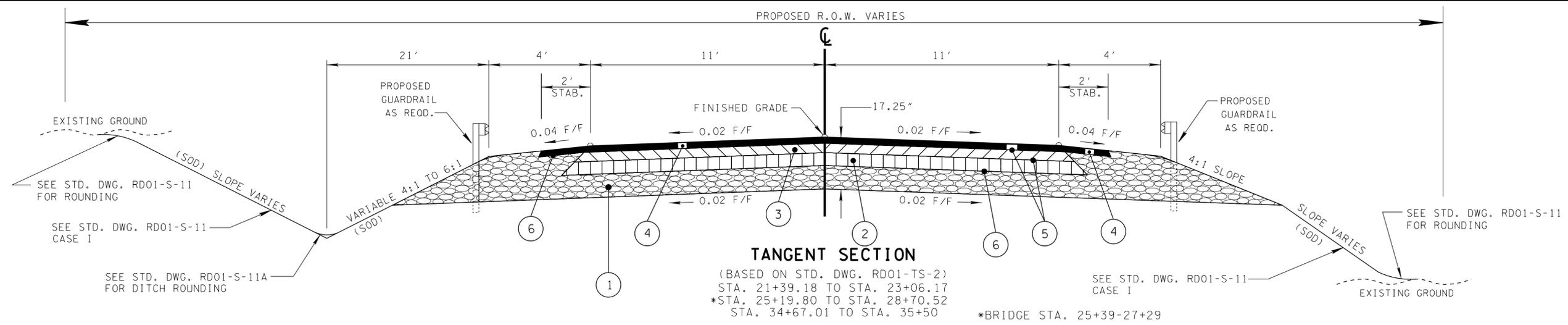
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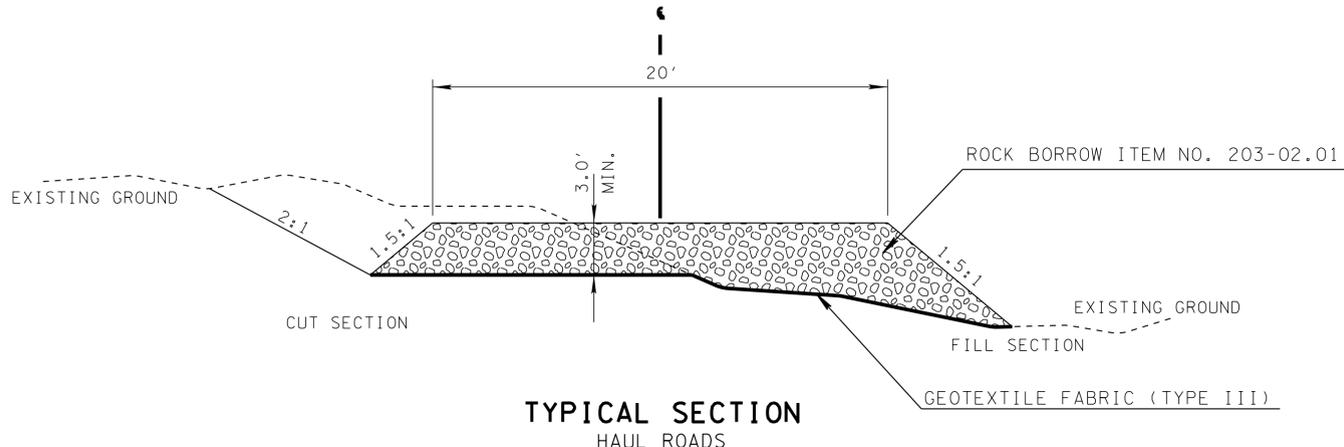
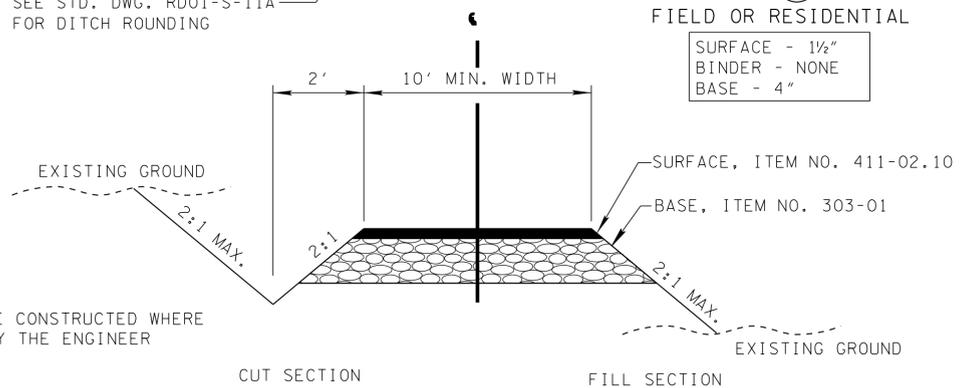
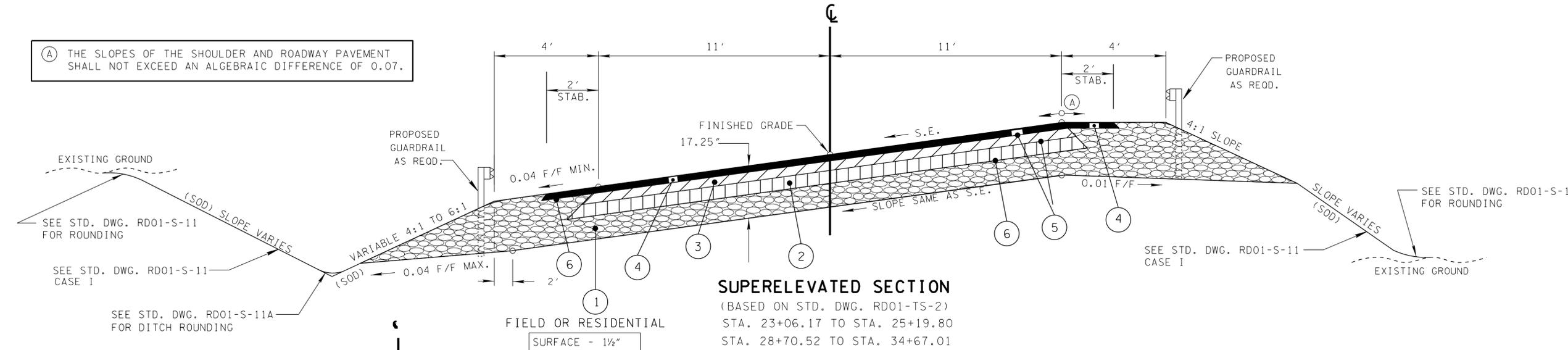
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

INDEX
AND
STANDARD
DRAWINGS

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2013	BR-STP-128(23)	2
CONST	2015	BR-STP-128(23)	2B



(A) THE SLOPES OF THE SHOULDER AND ROADWAY PAVEMENT SHALL NOT EXCEED AN ALGEBRAIC DIFFERENCE OF 0.07.



PROPOSED PAVEMENT SCHEDULE

<p>① MINERAL AGGREGATE @ 8" THICK</p> <p>303-01 MINERAL AGGREGATE, TYPE "A" BASE, GRADING "D"</p>	<p>④ ASPHALTIC SURFACE (HOT MIX) PG70-22</p> <p>GRADING "D" @ 1.25" THICK (MAINLINE) (APPROX. 132.5 LB./S.Y.)</p> <p>@ 1.50" THICK (SHOULDERS) (APPROX. 159.0 LB./S.Y.)</p> <p>411-02.10 ACS MIX (PG70-22) GRADING D</p>
<p>② BITUMINOUS PLANT MIX BASE (HOT MIX) PG70-22</p> <p>GRADING "A" @ 6.00" THICK (APPROX. 690 LB./S.Y.)</p> <p>307-02.01 ASPHALT CONCRETE MIX (PG70-22) (BPMB-HM) GRADING "A"</p>	<p>⑤ TACK COAT</p> <p>403-01 BITUMINOUS MATERIAL FOR TACK COAT (TC) 0.07 GAL/SY</p>
<p>③ BITUMINOUS PLANT MIX BASE (HOT MIX) PG70-22</p> <p>GRADING "BM-2" @ 2.00" THICK (APPROX. 226 LB./S.Y.)</p> <p>307-02.08 ASPHALT CONCRETE MIX (PG70-22) (BPMB-HM) GRADING "B-M2"</p>	<p>⑥ PRIME COAT</p> <p>402-01 BITUMINOUS MATERIAL FOR PRIME COAT (PC) 0.30-0.35 GAL/SY</p> <p>402-02 AGGREGATE FOR COVER MATERIAL (PC) 8-12 LBS/SY</p>

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STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS AND PAVEMENT SCHEDULE

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TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	2015	BR-STP-128(23)	2C

GENERAL NOTES

GRADING

- ANY AREA THAT IS DISTURBED OUTSIDE LIMITS OF CONSTRUCTION DURING THE LIFE OF THIS PROJECT SHALL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE.
- CERTIFICATION FOR ALL BORROW PITS MUST BE OBTAINED IN ACCORDANCE WITH SUBSECTION 107.06 OF THE STANDARD SPECIFICATIONS.
- THE CONTRACTOR SHALL NOT DISPOSE OF ANY MATERIAL EITHER ON OR OFF STATE-OWNED R.O.W. IN A REGULATORY FLOOD WAY AS DEFINED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY WITHOUT APPROVAL BY SAME. ALL MATERIAL SHALL BE DISPOSED OF IN UPLAND (NON-WETLAND) AREAS AND ABOVE ORDINARY HIGH WATER OF ANY ADJACENT WATERCOURSE. THIS DOES NOT ELIMINATE THE NEED TO OBTAIN ANY OTHER LICENSES OR PERMITS THAT MAY BE REQUIRED BY ANY OTHER FEDERAL, STATE OR LOCAL AGENCY.

SEEDING AND SODDING

- SOD SHALL BE PLACED AT LOCATIONS SHOWN ON THE PLANS TO PREVENT DAMAGE TO ADJACENT FACILITIES AND PROPERTY DUE TO EROSION ON ALL NEWLY GRADED CUT AND FILL SLOPES AS WORK PROGRESSES.

GUARDRAIL

- THE CONTRACTOR SHALL NOT REMOVE ANY SECTIONS OF EXISTING GUARDRAIL TO REWORK SHOULDERS OR FLATTEN SLOPES UNTIL THE ENGINEER CONCURS IN THE NECESSITY OF REMOVAL DUE TO CONSTRUCTION REQUIREMENTS AND THE APPROPRIATE WARNING DEVICES ARE INSTALLED. THE PROPOSED GUARDRAIL, INCLUDING ANY ANCHOR SYSTEM, SHALL BE INSTALLED QUICKLY TO MINIMIZE TRAFFIC EXPOSURE TO ANY HAZARD. NO PAYMENT WILL BE MADE FOR A SECTION OF PROPOSED GUARDRAIL, INCLUDING ANCHORS, UNTIL IT IS COMPLETE IN PLACE.

DRAINAGE

- THE CONTRACTOR SHALL SHAPE DITCHES TO THE SPECIFIED DESIGN. THIS WORK WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COST WILL BE INCLUDED IN THE COST OF OTHER ITEMS.
- THE CUTTING OF INLET AND OUTLET DITCHES WHERE SHOWN ON PLANS OR AS DIRECTED BY THE ENGINEER WILL BE MEASURED AND PAID FOR AS ITEM NO. 203-01 ROAD AND DRAINAGE EXCAVATION (UNCLASSIFIED).

UTILITIES (SEE SHEET NO. 3)

MISCELLANEOUS

- THE CONTRACTOR SHALL BE REQUIRED TO REMOVE AND RESET MAILBOXES WHERE AND AS DIRECTED BY THE ENGINEER.
- NOTHING IN THE GENERAL NOTES OR SPECIAL PROVISIONS SHALL RELIEVE THE CONTRACTOR FROM HIS RESPONSIBILITIES TOWARD THE SAFETY AND CONVENIENCE OF THE GENERAL PUBLIC AND THE RESIDENTS ALONG THE PROPOSED CONSTRUCTION AREA

ROW-OF WAY (SEE SHEET NO. 3)

FINAL PAVEMENT MARKING IF 4" SPRAY THERMOPLASTIC (40 mil) IS USED

- PERMANENT PAVEMENT LINE MARKINGS SHALL BE 4" SPRAY THERMOPLASTIC (40 mil) INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-13.06, SPRAY THERMO PAVEMENT MARKING (40 mil) (4IN LINE), L.M. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.

CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

- ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE WEEK BEFORE NEEDED, IF THE SIGN FACE IS FULLY COVERED.
- IF THE CONTRACTOR MOVES OFF THE PROJECT, HE SHALL COVER OR REMOVE ALL UNNEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OF REMOVAL, COVERING, AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO 712-06, SIGNS (CONSTRUCTION) PER SQUARE FOOT.
- A LONG TERM BUT SPORADIC USE WARNING SIGN, SUCH AS A FLAGGER SIGN, MAY REMAIN IN PLACE WHEN NOT REQUIRED PROVIDED THE SIGN FACE IS FULLY COVERED.
- TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.
- USE OF BARRICADES, PORTABLE BARRIER RAILS, VERTICAL PANELS, AND DRUMS SHALL BE LIMITED TO THE IMMEDIATE AREAS OF CONSTRUCTION WHERE A HAZARD IS PRESENT. THESE DEVICES SHALL NOT BE STORED ALONG THE ROADWAY WITHIN THIRTY (30) FEET OF THE EDGE OF THE TRAVELED WAY BEFORE OR AFTER USE UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL INCREASE TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. THESE DEVICES SHALL BE REMOVED FROM THE CONSTRUCTION WORK ZONE WHEN THE ENGINEER DETERMINES THEY ARE NO LONGER NEEDED. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.
- THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY, WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHEN THE LANE IS OPEN TO TRAFFIC UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO PARK WITHIN THIRTY (30) FEET OF A OPEN TRAFFIC LANE AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.
- ALL DETOUR AND CONSTRUCTION SIGNING SHALL BE IN STRICT ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- ALL DETOURS SHALL BE PAVED, STRIPED, SIGNED AND THE VERTICAL PANELS ARE TO BE IN PLACE BEFORE IT IS OPENED TO TRAFFIC.

EROSION PREVENTION AND SEDIMENT CONTROL

DISTURBED AREA

- AREAS TO BE UNDISTURBED SHALL BE CLEARLY MARKED IN THE FIELD BEFORE CONSTRUCTION ACTIVITIES BEGIN.
- PRE-CONSTRUCTION VEGETATIVE GROUND COVER SHALL NOT BE DESTROYED, REMOVED OR DISTURBED (I.E. CLEARING AND GRUBBING INITIATED) MORE THAN 15 CALENDAR DAYS PRIOR TO GRADING OR EARTH MOVING ACTIVITIES UNLESS THE AREA IS MULCHED, SEEDED WITH MULCH, OR OTHER TEMPORARY COVER IS INSTALLED.
- CLEARING, GRUBBING, AND OTHER DISTURBANCE TO RIPARIAN VEGETATION SHALL BE LIMITED TO THE MINIMUM NECESSARY FOR SLOPE CONSTRUCTION AND EQUIPMENT OPERATIONS. EXISTING VEGETATION SHOULD BE PRESERVED TO THE MAXIMUM EXTENT POSSIBLE. UNNECESSARY VEGETATION REMOVAL IS PROHIBITED.

- ALL DISTURBED AREAS SHALL BE PROPERLY STABILIZED AS SOON AS PRACTICABLE. PRIORITY SHALL BE GIVEN TO FINISHING OPERATIONS AND PERMANENT EPSC MEASURES OVER TEMPORARY EPSC MEASURES ON ALL PROJECTS.
- CONSTRUCTION SHALL BE SEQUENCED AND STAGED TO MINIMIZE THE EXPOSURE TIME OF GRADED OR DENUDED SOIL AREAS, PRESERVE TOPSOIL, AND MINIMIZE SOIL COMPACTION.
- NO MORE THAN 50 ACRES OF ACTIVE SOIL DISTURBANCE IS ALLOWED AT ANY TIME DURING THE CONSTRUCTION OF THE PROJECT. OFF-SITE BORROW OR WASTE AREAS ARE TO BE INCLUDED IN THE TOTAL DISTURBED AREA IF THE BORROW OR WASTE AREA IS EXCLUSIVE TO THE PROJECT PER TDOT'S WASTE AND BORROW MANUAL.

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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

**GENERAL
NOTES**

TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	2015	BR-STP-128(23)	20

GENERAL NOTES (CONTINUED)

SEDIMENT CONTROL

- (7) EPSC MEASURES SHALL BE INSTALLED AND FUNCTIONAL PRIOR TO ANY EARTH MOVING OPERATIONS, AND SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD.
- (8) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT THE OFF-SITE MIGRATION OR DEPOSIT OF SEDIMENT ON ROADWAYS USED BY THE GENERAL PUBLIC. IF SEDIMENT ESCAPES THE CONSTRUCTION SITE, OFF-SITE ACCUMULATIONS OF SEDIMENT THAT HAVE NOT REACHED A STREAM MUST BE REMOVED AT A FREQUENCY SUFFICIENT TO MINIMIZE OFF-SITE IMPACTS (E.G., FUGITIVE SEDIMENT THAT HAS ESCAPED THE CONSTRUCTION SITE AND HAS COLLECTED IN A STREET MUST BE REMOVED SO THAT IT IS NOT SUBSEQUENTLY WASHED INTO STORM SEWERS AND STREAMS BY THE NEXT RAIN AND/OR SO THAT IT DOES NOT POSE A SAFETY HAZARD TO USERS OF PUBLIC STREETS). ARRANGEMENTS CONCERNING REMOVAL OF SEDIMENT ON ADJOINING PROPERTY MUST BE SETTLED WITH THE ADJOINING PROPERTY OWNER BEFORE REMOVAL OF SEDIMENT.
- (9) WATER PUMPED FROM WORK AREAS AND EXCAVATION MUST BE HELD IN SETTLING BASINS OR TREATED BY FILTRATION OR CHEMICAL TREATMENT PRIOR TO ITS DISCHARGE INTO SURFACE WATERS. ALL PHYSICAL AND/OR CHEMICAL TREATMENT WILL BE APPLIED IN ACCORDANCE WITH MANUFACTURER'S GUIDELINES AND FULLY DESCRIBED IN THE EPSC PLANS. WATER MUST BE HELD IN SETTLING BASINS UNTIL AT LEAST AS CLEAR AS THE RECEIVING WATERS. SETTLING BASINS SHALL NOT BE LOCATED CLOSER THAN 20 FEET FROM THE TOP BANK OF A STREAM. SETTLING BASINS AND SEDIMENT TRAPS SHALL BE PROPERLY DESIGNED ACCORDING TO THE SIZE OF THE DRAINAGE AREAS OR VOLUME OF WATER TO BE TREATED. TREATED WATER MUST BE DISCHARGED THROUGH A PIPE OR WELL- VEGETATED OR LINED CHANNEL, SO THAT THE DISCHARGE DOES NOT CAUSE EROSION OR SEDIMENT TRANSPORT. DISCHARGES FROM BASINS AND IMPOUNDMENTS SHALL UTILIZE OUTLET STRUCTURES THAT ONLY WITHDRAW WATER FROM NEAR THE SURFACE OF THE BASIN OR IMPOUNDMENT. DISCHARGES MUST NOT CAUSE AN OBJECTIONABLE COLOR CONTRAST WITH THE RECEIVING STREAM.
- (10) CHECK DAMS SHALL BE USED WHERE RUNOFF IS CONCENTRATED. CLEAN ROCK, BRUSH, GABION, OR SANDBAG CHECK DAMS SHALL BE PROPERLY CONSTRUCTED TO REDUCE VELOCITY AND CONTROL EROSION.
- (11) FOR AN OUTFALL IN A DRAINAGE AREA OF 10 ACRES OR MORE, A TEMPORARY (OR PERMANENT) SEDIMENT BASIN OR EQUIVALENT CONTROL MEASURES THAT PROVIDES STORAGE FOR A CALCULATED VOLUME OF RUNOFF FROM A MINIMUM 2-YEAR/ 24-HOUR STORM EVENT, SHALL BE PROVIDED UNTIL FINAL STABILIZATION OF THE SITE. THE ENVIRONMENTAL AND ROADWAY DESIGN DIVISIONS MAY BE CONTACTED TO REVIEW AND CONCUR WITH ANY REVISION OF THE SWPPP BEFORE DISTURBANCE OF THE OUTFALL PROCEEDS.
- (12) IF PERMANENT OR TEMPORARY VEGETATION IS TO BE USED AS AN EPSC MEASURE, THEN THE TIMING OF PLANTING OF VEGETATION SHALL BE SHOWN IN THE SWPPP. DELAYING PLANTING OF COVER VEGETATION UNTIL WINTER MONTHS OR DRY MONTHS SHOULD BE AVOIDED, IF POSSIBLE.
- (13) OFFSITE VEHICLE TRACKING OF SEDIMENTS AND THE GENERATION OF DUST SHALL BE MINIMIZED. A STABILIZED CONSTRUCTION ACCESS (A POINT OF ENTRANCE/EXIT TO THE CONSTRUCTION PROJECT) SHALL BE PROVIDED, AS NEEDED, TO REDUCE THE TRACKING OF MUD AND DIRT ONTO PUBLIC ROADS BY CONSTRUCTION VEHICLES.
- (14) TEMPORARY EPSC MEASURES MAY BE REMOVED AT THE BEGINNING OF THE WORKDAY, BUT MUST BE REPLACED AT THE END OF THE WORKDAY.

STREAM/WETLAND

- (15) SOIL MATERIALS MUST BE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. EPSC MEASURES TO PROTECT WATER QUALITY MUST BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD. APPROPRIATE EPSC MEASURES MUST BE INSTALLED ALONG THE BASE OF ALL FILLS AND CUTS, ON THE DOWNHILL SIDE OF STOCKPILED SOIL, AND ALONG STREAM BANKS IN CLEARED AREAS TO PREVENT SEDIMENT MIGRATION INTO STREAMS IN ACCORDANCE WITH TDOT STANDARDS. THEY MUST BE INSTALLED ON THE CONTOUR, ENTRENCHED AND STAKED, AND EXTEND THE WIDTH OF THE AREA TO BE CLEARED.
- (16) INSTREAM EPSC DEVICES REQUIRE THE ENVIRONMENTAL DIVISION'S PERMITS SECTION REVIEW AND MUST BE PROCESSED BY THE PERMITS SECTION TO OBTAIN TDEC, USACE, AND TVA PERMITS.
- (17) THE OPERATION OF EQUIPMENT IN WATERS OF THE STATE/U.S., INCLUDING WETLANDS, SHALL BE ONLY AS SHOWN ON THE PROJECT PLANS AND/OR

AS SO SPECIFIED IN THE ARAP/401, SECTION 404 PERMIT(S) AND/OR TVA26(A), IF APPLICABLE. ANY ADDITIONAL PERMITS REQUIRED BY THE CONTRACTOR'S METHOD OF OPERATION SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO OBTAIN, AFTER RECEIVING THE APPROVAL OF TDOT ENVIRONMENTAL DIVISION.

INSPECTION, MAINTENANCE, REPAIR

- (18) EPSC CONTROLS WILL BE MAINTAINED IN ACCORDANCE WITH TDOT STANDARD DRAWINGS AND GOOD ENGINEERING PRACTICES.
- (19) INSPECTION, REPAIR, AND MAINTENANCE OF EPSC MEASURES/STRUCTURES IS TO BE PERFORMED ON A REGULAR BASIS. SEDIMENT SHALL BE REMOVED FROM SEDIMENT CONTROL STRUCTURES WHEN THE DESIGN CAPACITY HAS BEEN REDUCED BY FIFTY PERCENT (50%). DURING SEDIMENT REMOVAL, THE CONTRACTOR SHALL TAKE CARE TO ENSURE THAT STRUCTURAL COMPONENTS OF EPSC MEASURES ARE NOT DAMAGED AND THUS MADE INEFFECTIVE. IF DAMAGE DOES OCCUR, THE CONTRACTOR SHALL REPAIR THE STRUCTURES AT THE CONTRACTOR'S OWN EXPENSE.
- (20) SEDIMENT REMOVED FROM SEDIMENT CONTROL STRUCTURES SHALL BE PLACED AND BE TREATED IN A MANNER SO THAT THE SEDIMENT IS CONTAINED WITHIN THE PROJECT LIMITS AND DOES NOT MIGRATE INTO WATERS OF THE STATE/U.S. COST FOR THIS TREATMENT IS TO BE INCLUDED IN PRICE BID FOR ITEM NO. 209-05 SEDIMENT REMOVAL, C.Y.
- (21) INSPECTION OF EPSC MEASURES SHALL BE DONE AT LEAST TWICE PER CALENDAR WEEK AT LEAST 72 HOURS APART. A CALENDAR WEEK IS DEFINED AS SUNDAY THROUGH SATURDAY. QUALITY ASSURANCE/QUALITY CONTROL SITE ASSESSMENT OF EPSC SHALL BE PERFORMED PER THE TDOT ENVIRONMENTAL DIVISION'S COMPREHENSIVE INSPECTION OFFICE GUIDELINES.
- (22) OUTFALL POINTS SHALL BE INSPECTED TO ASCERTAIN WHETHER EPSC MEASURES ARE EFFECTIVE IN PREVENTING SIGNIFICANT IMPACTS TO SURROUNDING WATERS. WHERE DISCHARGE LOCATIONS ARE INACCESSIBLE, NEARBY DOWNSTREAM LOCATIONS SHALL BE INSPECTED. LOCATIONS WHERE VEHICLES ENTER AND EXIT THE SITE SHALL BE INSPECTED FOR EVIDENCE OF OFF-SITE ROADWAY SEDIMENT TRACKING.
- (23) UPON CONCLUSION OF THE INSPECTIONS, EPSC MEASURES FOUND TO BE INEFFECTIVE SHALL BE REPAIRED, REPLACED, OR MODIFIED BEFORE THE NEXT RAIN EVENT, IF POSSIBLE, BUT IN NO CASE MORE THAN 24 HOURS AFTER THE INSPECTION OR WHEN THE CONDITION IS IDENTIFIED. IF THE REPAIR, REPLACEMENT OR MODIFICATION IS NOT PRACTICAL WITHIN THE TIMEFRAME, WRITTEN DOCUMENTATION MUST BE PROVIDED IN THE FIELD BOOK AND AN ESTIMATED REPAIR, REPLACEMENT OR MODIFICATION SCHEDULE SHALL BE DOCUMENTED WITHIN 24 HOURS AFTER IDENTIFICATION.
- (24) THE TDOT PROJECT SUPERVISOR (OR THEIR DESIGNEE) AND THE CONTRACTOR'S SITE SUPERINTENDENT ARE RESPONSIBLE FOR INSPECTIONS. MAINTENANCE AND REPAIR ACTIVITIES ARE THE RESPONSIBILITY OF THE CONTRACTOR. THE TDOT PROJECT SUPERVISOR OR THEIR DESIGNEE WILL COMPLETE THE INSPECTION REPORTS AND DISTRIBUTE COPIES PER THE CONTRACT.

MATERIALS

- (25) WASTE AND BORROW AREAS SHALL BE LOCATED IN NON-WETLAND AREAS AND ABOVE THE 100-YEAR, FEDERAL EMERGENCY MANAGEMENT AGENCY FLOODPLAIN. BORROW AND WASTE DISPOSAL AREAS SHALL NOT AFFECT ANY WATERS OF THE STATE/U.S. UNLESS THESE AREAS ARE SPECIFICALLY COVERED BY AN ARAP, 404, OR NPDES PERMIT, OBTAINED SOLELY BY THE CONTRACTOR.

SWPPP, PERMITS, PLANS, RECORDS

- (26) THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR AND OBTAIN ANY NECESSARY ENVIRONMENTAL PERMITS OR APPROVALS, INCLUDING BUT NOT LIMITED TO TDEC ARAP/401, USACE SECTION 404, TVA SECTION 26A, AND TDEC NPDES PERMITS, FROM FEDERAL, STATE AND/OR LOCAL AGENCIES REGARDING THE OPERATION OF ANY PROJECT-DEDICATED ASPHALT AND/OR CONCRETE PLANTS.
- (27) ANY DISAGREEMENT BETWEEN THE PROJECT PLANS, THE PROJECT AS CONSTRUCTED, AND THE PERMIT(S) ISSUED FOR THE PROJECT, SHALL BE BROUGHT TO THE ATTENTION OF THE TDOT PROJECT ENGINEER. THE ENVIRONMENTAL DIVISION, ROADWAY DESIGN DIVISION, AND HEADQUARTERS CONSTRUCTION OFFICE SHALL BE CONTACTED IN THESE INSTANCES AND DECIDE WHICH HAS PRECEDENCE AND WHETHER PERMIT OR PLANS REVISIONS ARE NEEDED. IN GENERAL, PERMIT CONDITIONS WILL PREVAIL.

- (28) THE FOLLOWING INFORMATION SHALL BE MAINTAINED ON OR NEAR THE SITE: DATES THAT MAJOR GRADING ACTIVITIES OCCUR, DATES WHERE CONSTRUCTION ACTIVITIES TEMPORARILY OR PERMANENTLY CEASE ON A PORTION OF THE SITE, DATES WHEN STABILIZATION MEASURES ARE INITIATED, EPSC INSPECTION RECORDS, QUALITY ASSURANCE SITE ASSESSMENT RECORDS, PRECIPITATION RECORDS, SWPPP, PROJECT ENVIRONMENTAL PERMITS, AND A COPY OF THE PROJECT EPSC INSPECTOR'S TDEC LEVEL 1 CERTIFICATION.
- (29) ALL WATER QUALITY AND STORM WATER PERMITS, INCLUDING A COPY OF THE NOC WITH NPDES PERMIT TRACKING NUMBER AND THE LOCATION OF THE SWPPP, SHALL BE POSTED NEAR THE MAIN ENTRANCE OF THE CONSTRUCTION SITE ACCESSIBLE TO THE PUBLIC. THE NAME, COMPANY NAME, EMAIL ADDRESS, TELEPHONE NUMBER AND ADDRESS OF THE PROJECT SITE OWNER, OPERATOR, OR A LOCAL CONTACT PERSON WITH A BREIF DESCRIPTION OF THE PROJECT SHALL ALSO BE POSTED. IF POSTING THIS INFORMATION NEAR A MAIN ENTRANCE IS INFEASIBLE, THE INFORMATION SHALL BE PLACED IN A PUBLICLY ACCESSIBLE LOCATION NEAR WHERE THE CONSTRUCTION IS ACTIVELY UNDERWAY AND MOVED AS NECESSARY. THIS LOCATION SHALL BE POSTED AT THE CONSTRUCTION SITE. ALL POSTINGS SHALL BE MAINTAINED IN LEGIBLE CONDITION.
- (30) IF A CHANGE IN PROJECT SCOPE OCCURS DURING CONSTRUCTION, INCLUDING VALUE ENGINEERING, THE ENVIRONMENTAL DIVISION SHALL BE CONTACTED TO DETERMINE WHETHER PERMIT REVISIONS OR MODIFICATIONS OF THE SWPPP ARE NEEDED. THE ROADWAY DESIGN DIVISION SHALL BE CONTACTED TO DETERMINE IF ANY PLAN REVISIONS ARE NEEDED.
- (31) THE SWPPP SHALL BE UPDATED BY CONSTRUCTION WHENEVER EPSC INSPECTIONS INDICATE, OR WHERE STATE OR FEDERAL OFFICIALS DETERMINE EPSC MEASURES ARE PROVING INEFFECTIVE IN ELIMINATING OR SIGNIFICANTLY MINIMIZING POLLUTANT SOURCES OR ARE OTHERWISE NOT ACHIEVING THE GENERAL OBJECTIVES OF CONTROLLING POLLUTANTS IN STORM WATER DISCHARGES ASSOCIATED WITH THE CONSTRUCTION ACTIVITY. THE ENVIRONMENTAL DIVISION SHALL BE CONTACTED WHEN MAJOR DESIGN REVISIONS ARE REQUESTED BY CONSTRUCTION. THE ENVIRONMENTAL DIVISION MAY BE CONTACTED FOR GUIDANCE ON SPECIFIC SWPPP NEEDS. A COPY OF ANY CORRESPONDENCE REGARDING THE EFFECTIVENESS OF THE SWPPP OR EPSC CONTROLS SHALL BE RETAINED IN THE SWPPP.
- (32) THE SWPPP SHALL BE UPDATED BY CONSTRUCTION WHENEVER A CHANGE IN CHEMICAL TREATMENT METHODS IS MADE INCLUDING USE OF A DIFFERENT CHEMICAL, DIFFERENT DOSAGE OR APPLICATION RATE, OR A DIFFERENT AREA OF APPLICATION.
- (33) IF A TMDL IS DEVELOPED FOR THE RECEIVING WATERS FOR A POLLUTANT OF CONCERN (SILTATION AND/OR HABITAT ALTERATION) THE SWPPP SHALL BE MODIFIED OR UPDATED.
- (34) PROJECT INSPECTORS AND SUPERVISORS (INCLUDING TDOT STAFF, CONSULTANTS AND CONTRACTOR STAFF) RESPONSIBLE FOR THE IMPLEMENTATION AND MAINTENANCE OF EPSC PLANS SHALL SUCCESSFULLY COMPLETE THE TDEC "LEVEL 1 - FUNDAMENTALS OF EROSION PREVENTION AND SEDIMENT CONTROL FOR CONSTRUCTION SITES" COURSE AND ANY REFRESHER COURSES AS REQUIRED TO MAINTAIN CERTIFICATION. A COPY OF CERTIFICATION RECORDS FOR THE COURSES SHALL BE KEPT ON SITE AND AVAILABLE UPON REQUEST.

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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

**GENERAL
NOTES**

TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	2015	BR-STP-128(23)	2E

GENERAL NOTES (CONTINUED)

LITTER, DEBRIS, WASTE, PETROLEUM

- (35) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT LITTER, CONSTRUCTION DEBRIS, AND CONSTRUCTION WASTES FROM ENTERING WATERS OF THE STATE/U.S. THESE MATERIALS WILL BE PICKED UP AND REMOVED FROM STORMWATER EXPOSURE PRIOR TO ANTICIPATED STORM EVENTS. AFTER USE, MATERIALS USED FOR EPSC WILL BE REMOVED FROM THE SITE.
- (36) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS TO ENSURE THAT PETROLEUM PRODUCTS OR OTHER CHEMICAL POLLUTANTS ARE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. ALL EQUIPMENT REFUELING, SERVICING, AND STAGING AREAS SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL LAWS, RULES, REGULATIONS, AND ORDINANCES, INCLUDING THOSE OF THE NATIONAL FIRE PROTECTION ASSOCIATION (NFPA). APPROPRIATE CONTAINMENT MEASURES FOR THESE AREAS SHALL BE USED. ALL SPILLS MUST BE REPORTED TO THE APPROPRIATE AGENCY, AND MEASURES SHALL BE TAKEN IMMEDIATELY TO PREVENT THE POLLUTION OF WATERS OF THE STATE/U.S., INCLUDING GROUNDWATER, SHOULD A SPILL OCCUR.

SPECIAL NOTES

GRADING

- (1) THE GRADING TABULATIONS AND RESULTING EARTHWORK ASSOCIATED BID QUANTITIES WERE PREPARED UTILIZING AVAILABLE GEOTECHNICAL INFORMATION AND/OR REPORTS PREPARED FOR THIS PROJECT. THIS INFORMATION IS PROVIDED FOR GENERAL INFORMATION AND ESTIMATION GUIDANCE ONLY.
- (2) BORING DEPICTIONS SHOWN ON THE FOUNDATION DATA SHEETS, SOILS SHEETS, PLANS, AND CROSS-SECTIONS INDICATE SOIL AND ROCK CONDITIONS AT THE SPECIFIC BORING LOCATIONS. ANY SOIL PROFILE AND/OR ROCK LINE IS INTERPRETIVE BASED ON THE JUDGMENT OF THE GEOTECHNICAL ENGINEER/GEOLOGIST. THE TRANSITION BETWEEN BORINGS AND LAYERS MAY VARY SIGNIFICANTLY DEPENDING ON THE GEOLOGIC FORMATIONS ENCOUNTERED.
- (3) TO ASSIST IN BID PREPARATION FOR EARTHWORK AND FOUNDATION CONSTRUCTION, DETAIL ROCK AND SOIL DESCRIPTION AND ON SOME PROJECTS, ROCK CORE SAMPLES ARE AVAILABLE FOR INSPECTION AT THE MATERIALS AND TESTS HEADQUARTERS AT 6601 CENTENNIAL BOULEVARD, NASHVILLE, TN OR AT THE TDOT REGION 1 BUILDING IN KNOXVILLE, TN.
- (4) THE CONTRACTOR SHALL UTILIZE ALL INFORMATION PROVIDED IN THE PLANS, CROSS-SECTIONS AND CONTRACT DOCUMENTS INCLUDING ANY SPECIAL PROVISIONS AS WELL AS UTILIZING HIS PAST EXPERIENCE WITH PROJECTS OF SIMILAR NATURE, SCOPE AND LOCATION IN PREPARATION OF HIS BID FOR EARTHWORK ITEMS. IT IS THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE AND PROVIDE EQUIPMENT AND MEANS NECESSARY TO CONDUCT THE EXCAVATION ACTIVITIES IN ACCORDANCE WITH PLANS AND SPECIFICATIONS.
- (5) EARTHWORK IS PAID FOR UNDER ITEM 203-01, ROAD AND DRAINAGE EXCAVATION (UNCLASSIFIED). NO ADDITIONAL PAYMENT WILL BE MADE FOR EARTHWORK QUANTITIES BASED SOLELY ON A CLAIM THAT THE QUANTITIES SHOWN IN THE GRADING TABULATION OR ELSEWHERE IN THE PLANS ARE INACCURATE WITH RESPECT TO THE TYPE OF MATERIALS ENCOUNTERED DURING CONSTRUCTION EXCEPT AS PROVIDED FOR BY SECTION 104.02 IN THE CURRENT EDITION OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION OR AS AMENDED IN SUPPLEMENTAL SPECIFICATIONS.

EROSION PREVENTION AND SEDIMENT CONTROL NPDES

- (1) REFER TO THE EROSION PREVENTION AND SEDIMENT CONTROL PLAN, SHEET _8_, FOR NOTES REGARDING SEASONAL WORK LIMITATION OR LIMITATION ON THE TOTAL AREA OF EXPOSED SOIL.

SOILS

- (1) THE PROJECT IS LOCATED IN THE SOUTHWESTERN PART OF PERRY COUNTY, JUST EAST OF THE TENNESSEE RIVER. SOILS ON SITE CONSIST MOSTLY OF ALLUVIAL CLAY, SAND AND GRAVEL. ROCK IN THE AREA IS COMPRISED OF SHALEY-LIMESTONE FROM THE FORT PAYNE FORMATION.
- (2) STATIONS 21+50 TO 25+37.84 THE ROADWAY DESCENDS A HILL ALONG AN APPROXIMATE 6% GRADE. THERE ARE DITCH FILLS OF LESS THAN 4 FEET WITH CUTS UP TO 23 FEET LEFT OF CENTER LINE AND 14 FEET RIGHT OF CENTER LINE. MATERIAL IN THESE CUTS IS ANTICIPATED TO BE TRANSITIONAL MATERIAL INCLUDING ROCK BOULDERS WITH SHALEY LIMESTONE PINNACLES POSSIBLE AS DEFINED IN SECTION 4-230.02 OF THE TDOT ROADWAY DESIGN GUIDELINES. SEE STATION 24+00 FOR TYPICAL CROSS-SECTION. SLOPES SHOULD BE CONSTRUCTED NO STEEPER THAN 2:1.
- (3) STATIONS 27+20.14 TO STATION 35+81.10 THE ROADWAY CROSSES A LOW FLAT PLAIN. THE ROADWAY IN THIS PORTION OF THE JOB IS BEING SHIFTED TO THE LEFT OF ITS PRESENT LOCATION AND CONSTRUCTION WILL CONSIST OF FILLS OF UP TO 8 FEET FROM THE CENTERLINE LEFT. SLOPES SHOULD BE CONSTRUCTED NO STEEPER THAN 2:1.

DRIVEWAY

- (1) THE PROPOSED DRIVEWAY FOR TRACT 8 (STATION 23+05.67) WILL BE COMPLETE AND OPEN BEFORE THE EXISTING ACCESS FOR TRACT NUMBER 8 (RIGHT OF STATION 25+20) IS CLOSED.

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DEPARTMENT OF TRANSPORTATION

**GENERAL NOTES
AND
SPECIAL NOTES**

TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	2015	BR-STP-128(23)	2F

ESTIMATED GRADING QUANTITIES							
STATION TO STATION	ROAD & DRAINAGE EXC. (UNCL.)		BORROW EXCAVATION		CHANNEL EXC. C.Y.	EXCESS EXC. WASTE C.Y.	EMB. C.Y.
	COMMON - C.Y.	S. ROCK - C.Y.	UNCL. - C.Y.	S. ROCK - C.Y.			
21+50 - 25+38.5	635	3464				4325	207
27+29.5 - 35+00	567	0		2130		-3783	4256
TOTALS	1202	3464	0	2130	0	542	4463

R.O.W. MARKERS				
SHEET NO.	QUANTITIES			TOTALS
	"A"	"B"	"C"	
4A	12	1	3	16
5A	6	1	1	8
				0
				0
				0
TOTALS	18	2	4	24

SIDE DRAIN TABULATION																	
STATION	LOCATION		DESCRIPTION	SURFACE WIDTH (L.F.)	RCP CLASS III OR CMP 16 GA. OR HDPE OR PVC OR SRTRP (L.F.) FILL HEIGHT < OR = 10 FT.					RCP CLASS III OR CMP 14 GA. OR HDPE OR PVC OR SRTRP (L.F.) FILL HEIGHT > 10 FT. AND < OR = 16 FT.					END TREATMENT		REMARKS
	LT	RT			18"	24"	30"	36"	48"	18"	24"	30"	36"	48"	TYPE	DRAWING NO.	
23+05.67		X	PRIVATE DRIVE	22'	27												
29+00		X	FIELD ENTRANCE	16'	20												
TOTALS					47	0	0	0	0	0	0	0	0	0	0		

BRIDGE DRAINS					
STATION	LOCATION	SPILLWAY WIDTH	RUBBLE-STONE RIP-RAP 709-01.01 (CU. YD.)	18" PIPE DRAIN (BRIDGE DRAIN) 610-07.03 (L.F.)	REMARKS
27+33.7	LEFT	4'-8"	2.6	24	
27+33.7	RIGHT	4'-8"	2.6	28	
TOTALS			5.2	52	

REMOVAL OF BUILDINGS AND OBSTRUCTIONS DESCRIPTION BLOCK		
PAY ITEM	TRACT NO.	DESCRIPTION
202-06.01	6	1 STORY BRICK RESIDENCE

NO ADDITIONAL COMPENSATION WILL BE MADE FOR THESE REMOVALS.

PROPOSED GUARDRAIL															
SHEET NO.	LOCATION	SIDE		STATION		GUARDRAIL				TERMINAL ANCHORS					REMARKS
		LT	RT	FROM	TO	BRIDGE ENDS	BRIDGE PIERS	SINGLE	MEDIAN	TYPE 12	TYPE 13	TYPE 21	TYPE IN-LINE	TYPE 38	
						705-01.01 (L.F.)	705-01.02 (L.F.)	705-02.02 (L.F.)	705-03.03 (L.F.)						
4B	S.R.128	X		24+65	25+39	26.9								1	
4B	S.R.128		X	24+65	25+39	26.9								1	
4B	S.R. 128	X		27+29	28+03	26.9								1	
4B	S.R. 128		X	27+29	28+03	26.9								1	
TOTALS						107.6	0	0	0	0	0	0	0	4	

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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TABULATED QUANTITIES

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2013	BR-STP-128(23)	3
CONST	2015	BR-STP-128(23)	3

REV. 11-27-2013: CHANGED PROPERTY OWNERS NAME, DEED BOOK AND PAGE NUMBER ON TRACT NUMBER 8.

REV. 2-14-2014: CHANGED TRACT NUMBER 4 TO 4B. ADDED TRACT NUMBERS 4A AND 4C. CHANGED TOTAL AREA AND AREA TO BE ACQ. ON TRACT NUMBERS 3, 4B AND 7. REMOVED SECOND SET OF STATION VALUES, ALSO REMOVED P.C., P.T., AND P.O.T. VALUES.

REV. 9-09-2014: ADDED CONSTRUCTION EASEMENT TO TRACT NUMBER 6 AND ADDED FOOTNOTE NUMBER 2.

REV. 3-11-2015: DELETED CONST. ESMT. ON RIGHT OF C.L. AT STATION 23+50. EXTENDED DRIVEWAY AT STA. 23+05.67. ADDED NOTE ABOUT DRIVEWAY. ADDED TRACT NUMBERS 8003 AND 8006. CORRECTED TOTAL AREA AND AREA REMAINING ON TRACT NUMBERS 3 AND 6. REMOVED CONST. ESMT. FROM ACQ. TABLE FOR TRACT NUMBER 6

UTILITY OWNERS
SR-128 BRIDGE OVER CEDAR CREEK
PERRY COUNTY

TELEPHONE:

TDS TELECOM
264 E. MAIN ST.
P.O. Box 610
PARSONS, TN 38363
CONTACT: RONALD JONES
TONY BUGHER
(731)874-7577

ELECTRIC:

MERIWETHER LEWIS ELECTRIC
1625 Hwy. 100
P.O. Box 240
CENTERVILLE, TN 37033
STEVE SKELTON
STEVE SKELTON MLEC.COM
(931)729-3558

WATER:

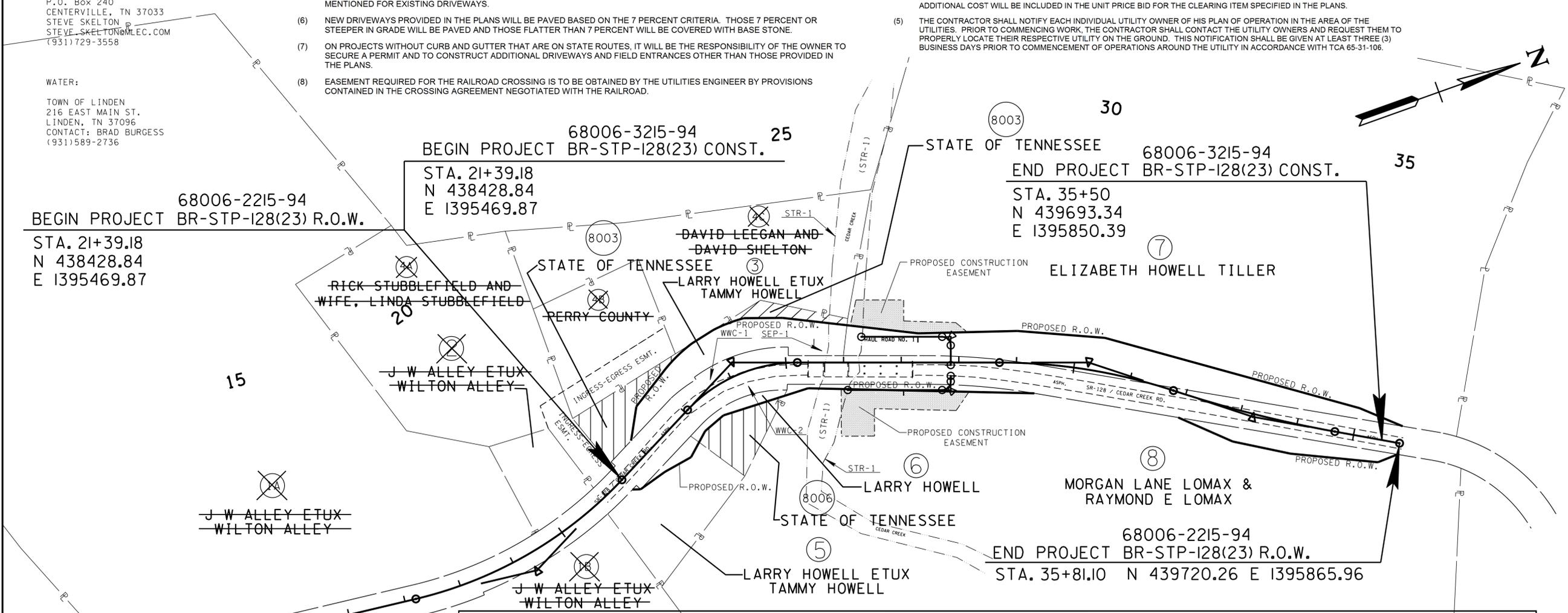
TOWN OF LINDEN
216 EAST MAIN ST.
LINDEN, TN 37096
CONTACT: BRAD BURGESS
(931)589-2736

RIGHT - OF - WAY NOTES

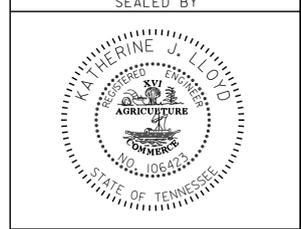
- IT IS INTENDED THAT ALL BUILDINGS AND/OR PORTIONS OF BUILDINGS THAT ARE WITHIN THE PROPOSED RIGHT-OF-WAY AND/OR EASEMENT LINES FOR THE PROJECT BE REMOVED THERE FROM IN THE PROCESS OF RIGHT-OF-WAY ACQUISITION. IF ANY SUCH BUILDINGS OR IMPROVEMENTS ARE NOT REMOVED IN THE COURSE OF RIGHT-OF-WAY ACQUISITION, THE CIVIL ENGINEERING MANAGER 2, DESIGN DIVISION IS TO BE NOTIFIED IN SUFFICIENT TIME TO PERMIT HAVING SUCH REMOVALS DESIGNATED AS A PART OF THE CONSTRUCTION CONTRACT.
- ALL RAMPS MUST CONFORM TO THE DEPARTMENT'S "POLICY ON FINANCING CONSTRUCTION OF PUBLIC ROAD INTERSECTIONS AND DRIVEWAYS ON HIGHWAY RESURFACING, RECONSTRUCTION AND CONSTRUCTION PROJECTS ON NEW LOCATIONS", THE MANUAL ON RULES AND REGULATIONS FOR CONSTRUCTING DRIVEWAYS ON STATE HIGHWAY RIGHT-OF-WAY, STANDARD DRAWING RP-R-1, AND OTHER ACCEPTED DESIGN AND SAFETY STANDARDS.
- WHERE THE EXISTING DRIVEWAY IS UNPAVED AND THE PROPOSED DRIVEWAY EXCEEDS 7 PERCENT IN GRADE, EACH DRIVEWAY WILL BE PAVED TO A TOUCHDOWN POINT OR UNTIL THE GRADE IS LESS THAN 7 PERCENT.
- WHERE THE EXISTING DRIVEWAY IS UNPAVED AND THE PROPOSED DRIVEWAY IS LESS THAN 7 PERCENT IN GRADE, EACH DRIVEWAY WILL BE PAVED A SHOULDER WIDTH FROM THE EDGE OF PAVEMENT AND THE REMAINDER OF THAT DRIVEWAY REPLACED IN KIND TO A TOUCHDOWN POINT.
- TRACT REMAINDERS NOT HAVING AN EXISTING DRIVEWAY WILL BE PROVIDED ONE 50-FOOT OPENING IN THE ACCESS CONTROL FENCE AND A DRIVEWAY WILL BE CONSTRUCTED UNLESS ACCESS IS PROVIDED FROM AN INTERSECTING ROAD OR BASED ON PHYSICAL CONDITIONS AND/OR CONFLICTS WITH OTHER DESIGN CONSIDERATIONS WHICH PREVENT AN ACCESS OPENING. PAVING OF THESE NEW DRIVEWAYS WILL BE IN ACCORDANCE TO THE 7 PERCENT CRITERIA PREVIOUSLY MENTIONED FOR EXISTING DRIVEWAYS.
- NEW DRIVEWAYS PROVIDED IN THE PLANS WILL BE PAVED BASED ON THE 7 PERCENT CRITERIA. THOSE 7 PERCENT OR STEEPER IN GRADE WILL BE PAVED AND THOSE FLATTER THAN 7 PERCENT WILL BE COVERED WITH BASE STONE.
- ON PROJECTS WITHOUT CURB AND GUTTER THAT ARE ON STATE ROUTES, IT WILL BE THE RESPONSIBILITY OF THE OWNER TO SECURE A PERMIT AND TO CONSTRUCT ADDITIONAL DRIVEWAYS AND FIELD ENTRANCES OTHER THAN THOSE PROVIDED IN THE PLANS.
- EASEMENT REQUIRED FOR THE RAILROAD CROSSING IS TO BE OBTAINED BY THE UTILITIES ENGINEER BY PROVISIONS CONTAINED IN THE CROSSING AGREEMENT NEGOTIATED WITH THE RAILROAD.

UTILITIES

- THE LOCATIONS OF UTILITIES SHOWN WITHIN THESE PLANS ARE APPROXIMATE ONLY. EXACT LOCATIONS SHALL BE DETERMINED IN THE FIELD BY CONTACTING THE UTILITY COMPANIES INVOLVED. NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC., AT 1-800-351-1111 AS REQUIRED BY TCA 65-31-106 WILL BE REQUIRED.
- UNLESS OTHERWISE NOTED, ALL UTILITY ADJUSTMENTS WILL BE PERFORMED BY THE UTILITY OR ITS REPRESENTATIVE. THE CONTRACTOR AND UTILITY OWNERS WILL BE REQUIRED TO COOPERATE WITH EACH OTHER IN ORDER TO EXPEDITE THE WORK REQUIRED BY THIS CONTRACT. ON CONTRACTS WHERE CONSTRUCTION STAKES, LINES, AND GRADES ARE CONTRACT ITEMS, THE CONTRACTOR WILL BE REQUIRED TO PROVIDE RIGHT-OF-WAY OR SLOPE STAKES, DITCH OR STREAM BED GRADES, OR OTHER ESSENTIAL SURVEY STAKING TO PREVENT CONFLICTS WITH THE HIGHWAY CONSTRUCTION. FREQUENTLY, THIS WILL BE REQUIRED AS THE FIRST ITEM OF WORK AND AT ANY LOCATION ON THE PROJECT DIRECTED BY THE ENGINEER.
- THE CONTRACTOR WILL PROVIDE ALL NECESSARY PROTECTIVE MEASURES TO SAFEGUARD EXISTING UTILITIES FROM DAMAGE DURING CONSTRUCTION OF THIS PROJECT. IN THE EVENT THAT SPECIAL EQUIPMENT IS REQUIRED TO WORK OVER AND AROUND THE UTILITIES, THE CONTRACTOR WILL BE REQUIRED TO FURNISH SUCH EQUIPMENT. THE COST OF PROTECTING UTILITIES FROM DAMAGE AND FURNISHING SPECIAL EQUIPMENT WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- PRIOR TO SUBMITTING HIS BID, THE CONTRACTOR WILL BE SOLELY RESPONSIBLE FOR CONTACTING OWNERS OF ALL AFFECTED UTILITIES IN ORDER TO DETERMINE THE EXTENT TO WHICH UTILITY RELOCATIONS AND/OR ADJUSTMENTS WILL HAVE UPON THE SCHEDULE OF WORK FOR THE PROJECT. WHILE SOME WORK MAY BE REQUIRED 'AROUND' UTILITY FACILITIES THAT WILL REMAIN IN PLACE, OTHER UTILITY FACILITIES MAY NEED TO BE ADJUSTED CONCURRENTLY WITH THE CONTRACTOR'S OPERATIONS. ADVANCE CLEAR CUTTING MAY BE REQUIRED BY THE ENGINEER AT ANY LOCATION WHERE CLEARING IS CALLED FOR IN THE SPECIFICATIONS AND CLEAR CUTTING IS NECESSARY FOR A UTILITY RELOCATION. ANY ADDITIONAL COST WILL BE INCLUDED IN THE UNIT PRICE BID FOR THE CLEARING ITEM SPECIFIED IN THE PLANS.
- THE CONTRACTOR SHALL NOTIFY EACH INDIVIDUAL UTILITY OWNER OF HIS PLAN OF OPERATION IN THE AREA OF THE UTILITIES. PRIOR TO COMMENCING WORK, THE CONTRACTOR SHALL CONTACT THE UTILITY OWNERS AND REQUEST THEM TO PROPERLY LOCATE THEIR RESPECTIVE UTILITY ON THE GROUND. THIS NOTIFICATION SHALL BE GIVEN AT LEAST THREE (3) BUSINESS DAYS PRIOR TO COMMENCEMENT OF OPERATIONS AROUND THE UTILITY IN ACCORDANCE WITH TCA 65-31-106.



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COORDINATES ARE NAD/83(1995), ARE DATUM ADJUSTED BY THE FACTOR OF 1.00005 AND TIED TO THE TGRN. ALL ELEVATIONS ARE REFERENCED TO THE NAVD 1988.

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PROPERTY MAP

STA. 10+00 TO STA. 35+81.10
SCALE: 1"=100'

TRACT NO.	PROPERTY OWNERS	COUNTY RECORDS		TOTAL AREA ACRES			AREA TO BE ACQUIRED ACRES			AREA REMAINING ACRES		EASEMENT (SQUARE FEET)					
		TAX MAP NO.	PARCEL NO.	DEED DOCUMENT REFERENCE		LEFT	RIGHT	TOTAL	LEFT	RIGHT	TOTAL	LEFT	RIGHT	CONST.			
				BK.	PAGE										PERM. DRAINAGE	SLOPE	
1A	J.W. ALLEY ETUX WILTON ALLEY	107	27-00	Z20	126	20.774		20.774			20.774						
1B	J.W. ALLEY ETUX WILTON ALLEY	107	27-00	Z20	126		28.024	28.024				28.024					
2	J.W. ALLEY ETUX WILTON ALLEY	107	26-00	Z20	123	0.406		0.406			0.406						
3	LARRY HOWELL ETUX TAMMY HOWELL	107	25-01	H11	104	0.495		0.495	0.495		0.495						
4B	PERRY COUNTY	107	25-00	H11	224	0.894		0.894			0.894						
5	LARRY HOWELL ETUX TAMMY HOWELL	107	28-00	H11	104		0.670	0.670				0.630					
6	LARRY HOWELL	107	28-01	G9	815		2916 S.F.	2916 S.F.									
7	ELIZABETH HOWELL TILLER	107	5-01	P19	79	16.226	923 S.F.	16.247	0.950	923 S.F.	0.971	15.276		0.145 AC			
8	MORGAN LANE LOMAX & RAYMOND E LOMAX	107	5-02	D15	483		15.305	15.305						0.224 AC			
4A	RICK STUBBLEFIELD AND WIFE, LINDA STUBBLEFIELD	107	25-00	D16	1069	2.824		2.824			2.824						
4C	DAVID LEEGAN AND DAVID SHELTON	107	25-03	D16	1065	2.202		2.202			2.202						
8003	STATE OF TENNESSEE			D18	1069	0.334		0.334									
8006	STATE OF TENNESSEE			D18	1073		0.218	0.218									
ACQUISITION TOTALS (ACRES)												1.445	0.4064	1.8514			0.5150

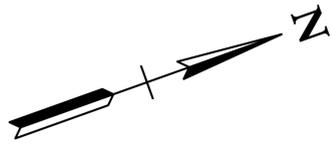
DISTURBED AREA	
IN BETWEEN SLOPE LINES	2.235 (AC)
15 FOOT WIDE STRIP (OUTSIDE SLOPE LINES)	0.836 (AC)
TOTAL DISTURBED AREA	3.071 (AC)

① FOR EROSION CONTROL AND HAUL ROADS

12-MAR-2015 14:40 C:\Projects\Perry Co\3.R.128\Working Files\03.sht

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2013	BR-STP-128(23)	4
CONST.	2015	BR-STP-128(23)	4

REV. 11-27-2013: CHANGED PROPERTY OWNERS NAME ON TRACT NUMBER 8.
 REV. 2-14-2014: CHANGED TRACT NUMBER 4 TO 4B. ADDED TRACT NUMBERS 4A AND 4C. CHANGED SHAPE OF TRACT NUMBER 3. LOCATED SEPTIC SYSTEMS ON TRACTS 5 & 6. LOCATED STORM SHELTER ON TRACT 6.
 REV. 9-09-2014: ADDED CONSTRUCTION EASEMENT TO TRACT NUMBER 6.
 REV. 12-10-2014: DELETED FIELD ENTRANCE AT STATION 29+50 ON LEFT OF CENTERLINE.
 REV. 3-11-2015: DELETED CONST. ESMT. ON RIGHT OF C.L. AT STATION 23+50. EXTENDED DRIVEWAY AT STA. 23+05.67, AND ADDED NOTE. ADDED TRACT NUMBERS 8003 & 8006.



68006-3215-94
 BEGIN PROJECT BR-STP-128(23) CONST.

STA. 21+39.18
 N 438428.8384
 E 1395469.8747

68006-2215-94
 BEGIN PROJECT BR-STP-128(23) R.O.W.

STA. 21+39.18
 N 438428.8384
 E 1395469.8747

~~RICK STUBBLEFIELD AND WIFE, LINDA STUBBLEFIELD~~

~~DAVID LEEGAN AND DAVID SHELTON~~

~~LARRY HOWELL ETUX TAMMY HOWELL~~

~~PERRY COUNTY~~

~~ELIZABETH HOWELL TILLER~~

25 STATE OF TENNESSEE

STATE OF TENNESSEE

LARRY HOWELL ETUX TAMMY HOWELL

MORGAN LANE LOMAX & RAYMOND E LOMAX

~~J W ALLEY ETUX WILTON ALLEY~~

~~J W ALLEY ETUX WILTON ALLEY~~

NOTE:
 THE PROPOSED DRIVEWAY FOR TRACT 8 (STATION 23+05.67) WILL BE COMPLETE AND OPEN BEFORE THE EXISTING ACCESS FOR TRACT NUMBER 8 (RIGHT OF STATION 25+20) IS CLOSED.

S.R. 128
 CURVE DC102
 PI 30+25.44
 N 439,260.4153
 E 1,395,550.8917
 Δ 18° 25' 45" (RT)
 D 6' 00' 00"
 R 954.93
 L 307.15
 T 154.91
 SE 0.062 FT/FT
 DESIGN SPEED 40+ MPH
 TRANS. LENGTH 175

HAUL ROAD NO. 1
 CURVE DC110
 PI 0+49.38
 N 439,050.2990
 E 1,395,427.5795
 Δ 90° 00' 00" (LT)
 D 381' 58' 19"
 R 15.00
 L 23.56
 T 15.00

HAUL ROAD NO. 2
 CURVE DC113
 PI 0+24.65
 N 439,018.8521
 E 1,395,515.1015
 Δ 90° 00' 00" (RT)
 D 381' 58' 19"
 R 15.00
 L 23.56
 T 15.00

S.R. 128
 CURVE DC101
 PI 24+19.41
 N 438,677.9941
 E 1,395,341.6256
 Δ 47° 00' 00" (RT)
 D 22' 00' 00"
 R 260.44
 L 213.64
 T 113.24
 SE 0.080 FT/FT
 DESIGN SPEED 30+ MPH
 TRANS. LENGTH 180'

POINT	NORTHING	EASTING	ELEVATION	DESCRIPTION	STATION	OFFSET	PHYSICAL DESCRIPTION
CP-S1	439056.4594	1395478.6607	400.8190	68-128-01	28+09.63	0.9892	STAMPED ALUM. DISK ON 3/4" REBAR
CP-S2	439780.7730	1395886.1235	402.3320	68-128-02	OffChain	OffChain	STAMPED ALUM. DISK ON 3/4" REBAR
CP-S3	439932.9449	1396160.2194	408.4780	68-128-03	OffChain	OffChain	STAMPED ALUM. DISK ON 3/4" REBAR

UNOFFICIAL SET
NOT FOR BIDDING



COORDINATES ARE NAD/83(1995), ARE DATUM ADJUSTED BY THE FACTOR OF 1.00005 AND TIED TO THE TGRN. ALL ELEVATIONS ARE REFERENCED TO THE NAVD 1988.

STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

PRESENT LAYOUT

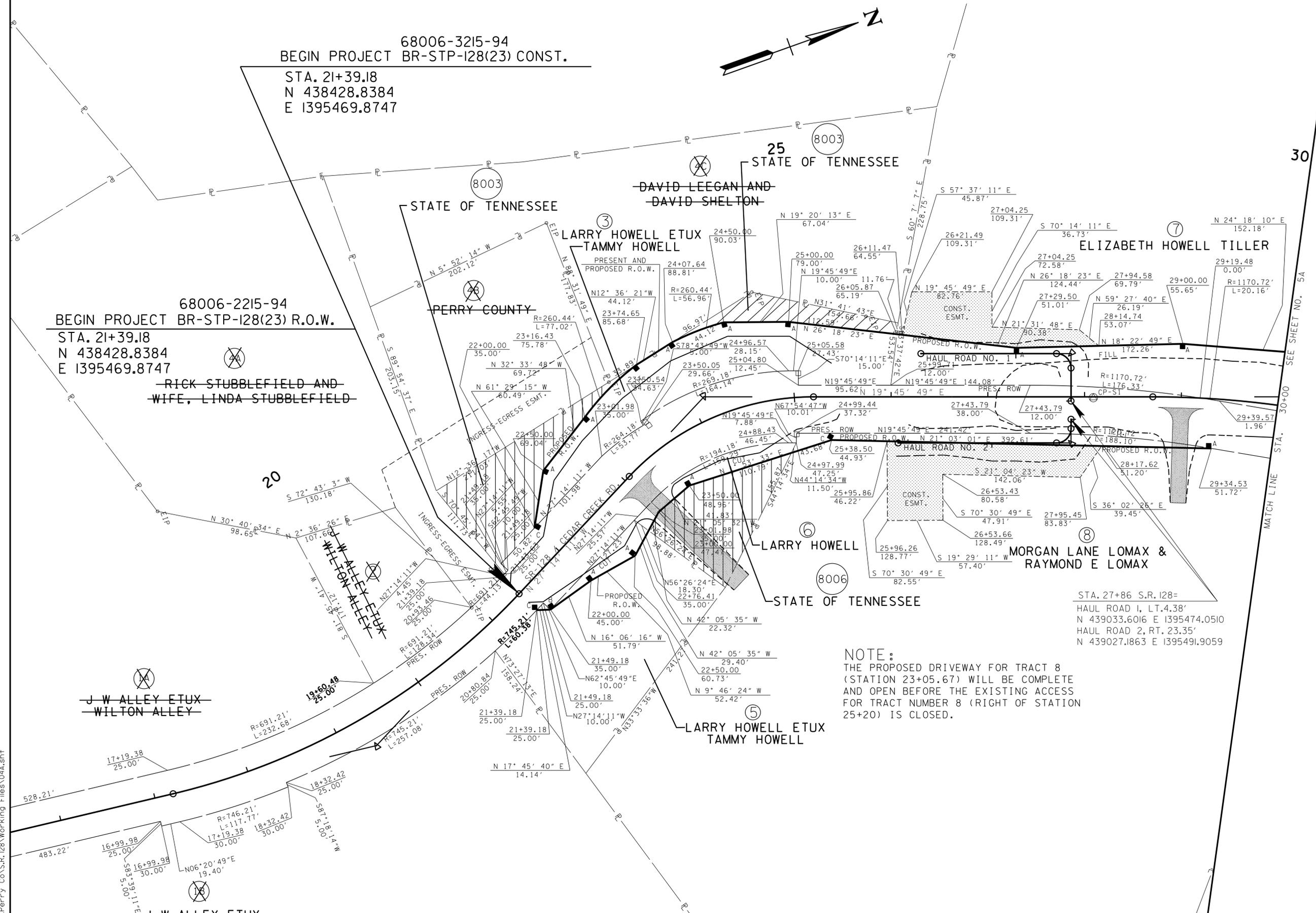
STA. 20+00 TO STA. 30+00

SCALE: 1" = 50'

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TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2013	BR-STP-128(23)	4A
CONST.	2015	BR-STP-128(23)	4A

REV. 11-27-2013: CHANGED PROPERTY OWNERS NAME ON TRACT NUMBER 8.
 REV. 2-14-2014: CHANGED TRACT NUMBER 4 TO 4B. ADDED TRACT NUMBERS 4A AND 4C. CHANGED SHAPE OF TRACT NUMBER 3.
 REV. 9-09-2014: ADDED CONSTRUCTION EASEMENT TO TRACT NUMBER 6.
 REV. 12-10-2014: DELETED FIELD ENTRANCE AT STATION 29+50 ON LEFT OF CENTERLINE.
 REV. 3-11-2015: DELETED CONST. ESMT. ON RIGHT OF C.L. AT STATION 23+50. EXTENDED DRIVEWAY AT STA. 23+05.67, AND ADDED NOTE. ADDED TRACT NUMBERS 8003 & 8006.



68006-3215-94
 BEGIN PROJECT BR-STP-128(23) CONST.
 STA. 21+39.18
 N 438428.8384
 E 1395469.8747

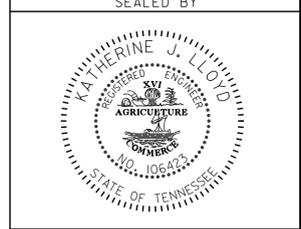
68006-2215-94
 BEGIN PROJECT BR-STP-128(23) R.O.W.
 STA. 21+39.18
 N 438428.8384
 E 1395469.8747

STA. 27+86 S.R. 128=
 HAUL ROAD 1, LT. 4.38'
 N 439033.6016 E 1395474.0510
 HAUL ROAD 2, RT. 23.35'
 N 439027.1863 E 1395491.9059

NOTE:
 THE PROPOSED DRIVEWAY FOR TRACT 8 (STATION 23+05.67) WILL BE COMPLETE AND OPEN BEFORE THE EXISTING ACCESS FOR TRACT NUMBER 8 (RIGHT OF STATION 25+20) IS CLOSED.

POINT	NORTHING	EASTING	ELEVATION	DESCRIPTION	STATION	OFFSET	PHYSICAL DESCRIPTION
CP-S1	439056.4594	1395478.6607	400.8190	68-128-01	28+09.63	0.9892	STAMPED ALUM. DISK ON 3/4" REBAR
CP-S2	439780.7730	1395886.1235	402.3320	68-128-02	OffChain	OffChain	STAMPED ALUM. DISK ON 3/4" REBAR
CP-S3	439932.9449	1396160.2194	408.4780	68-128-03	OffChain	OffChain	STAMPED ALUM. DISK ON 3/4" REBAR

UNOFFICIAL SET
NOT FOR BIDDING



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STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

R.O.W. DETAILS

STA. 20+00 TO STA. 30+00

SCALE: 1" = 50'

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TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2013	BR-STP-128(23)	4B
CONST.	2015	BR-STP-128(23)	4B

REV. 12-10-2014: DELETED FIELD ENTRANCE AT STATION 29+50 ON LEFT OF CENTERLINE.
 REV. 3-11-2015: DELETED CONST. ESMT. ON RIGHT OF C.L. AT STATION 23+50.
 REV. 3-11-2015: EXTENDED DRIVEWAY AT STATION 23+05.67 AND ADDED NOTE FOR DRIVEWAY.

68006-3215-94
 BEGIN PROJECT BR-STP-128(23) CONST.

STA. 21+39.18
 N 438428.8384
 E 1395469.8747

ESQUIRE FRED ASHLEY HOWELL
 AND
 TILDA HILDREN CULP HOWELL
 MEMORIAL BRIDGE

STA. 27+86 S.R. I28=
 HAUL ROAD 1, LT. 4.38' STA. 0+00
 N 439033.6016 E 1395474.0510
 HAUL ROAD 2, RT. 23.35' STA. 0+00
 N 439027.1863 E 1395491.9059

20

30



PROPOSED CLASS "A-1" RIP-RAP

PROPOSED ROCK PAD
 SEE NOTE AT BOTTOM OF SHEET

NOTE
 SEE SHEET 2A NOTE NUMBER 18 FOR SIGN DESCRIPTION

TYPE 38

TYPE 38

TYPE 38

TYPE 38

NOTE
 SEE SHEET 2A NOTE NUMBER 18 FOR SIGN DESCRIPTION

PROPOSED CLASS "A-1" RIP-RAP

PROPOSED PAVEMENT AT BRIDGE ENDS
 SEE STANDARD DWGS. STD-1-5 & STD-1-6 FOR BRIDGE DRAINS

SPECIAL "V" BOTTOM DITCH RIGHT

PROPOSED ROCK PAD
 SEE NOTE AT BOTTOM OF SHEET

LIMIT OF CONST. 1+10.00

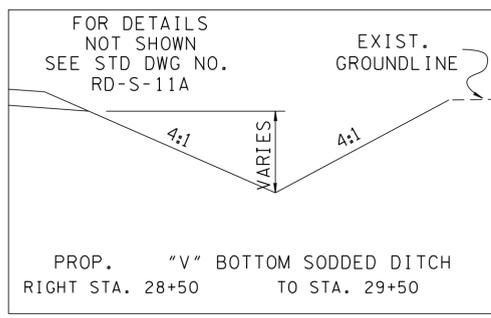
STA. 29+00.00
 16' FIELD ENT.
 20' OF 18' S.D. REOD.

MATCH LINE STA. 30+00 SEE SHEET NO. 5B

NOTE:
 THE PROPOSED DRIVEWAY FOR TRACT 8 (STATION 23+05.67) WILL BE COMPLETE AND OPEN BEFORE THE EXISTING ACCESS FOR TRACT NUMBER 8 (RIGHT OF STATION 25+20) IS CLOSED.

68006-2215-94
 BEGIN PROJECT BR-STP-128(23) R.O.W.

STA. 21+39.18
 N 438428.8384
 E 1395469.8747



NOTES

1. THE ROCK PAD SHALL NOT EXCEED 1/3 OF THE WIDTH OF THE STREAM. THE ROCK PAD SHALL CONSIST OF CLEAN ROCK FREE OF FINES. IF A LARGER ROCK PAD IS REQUIRED, CONTACT THE ENVIRONMENTAL DIVISION. ADDITIONAL PERMITS MAY BE REQUIRED.
2. THE ROCK PADS ARE TO BE REMOVED AND THE BANKS STABILIZED. ANY TREES REMOVED FOR THE CONSTRUCTION OF THE HAUL ROADS AND ROCK PAD SHOULD BE REPLACED.

UNOFFICIAL SET
NOT FOR BIDDING



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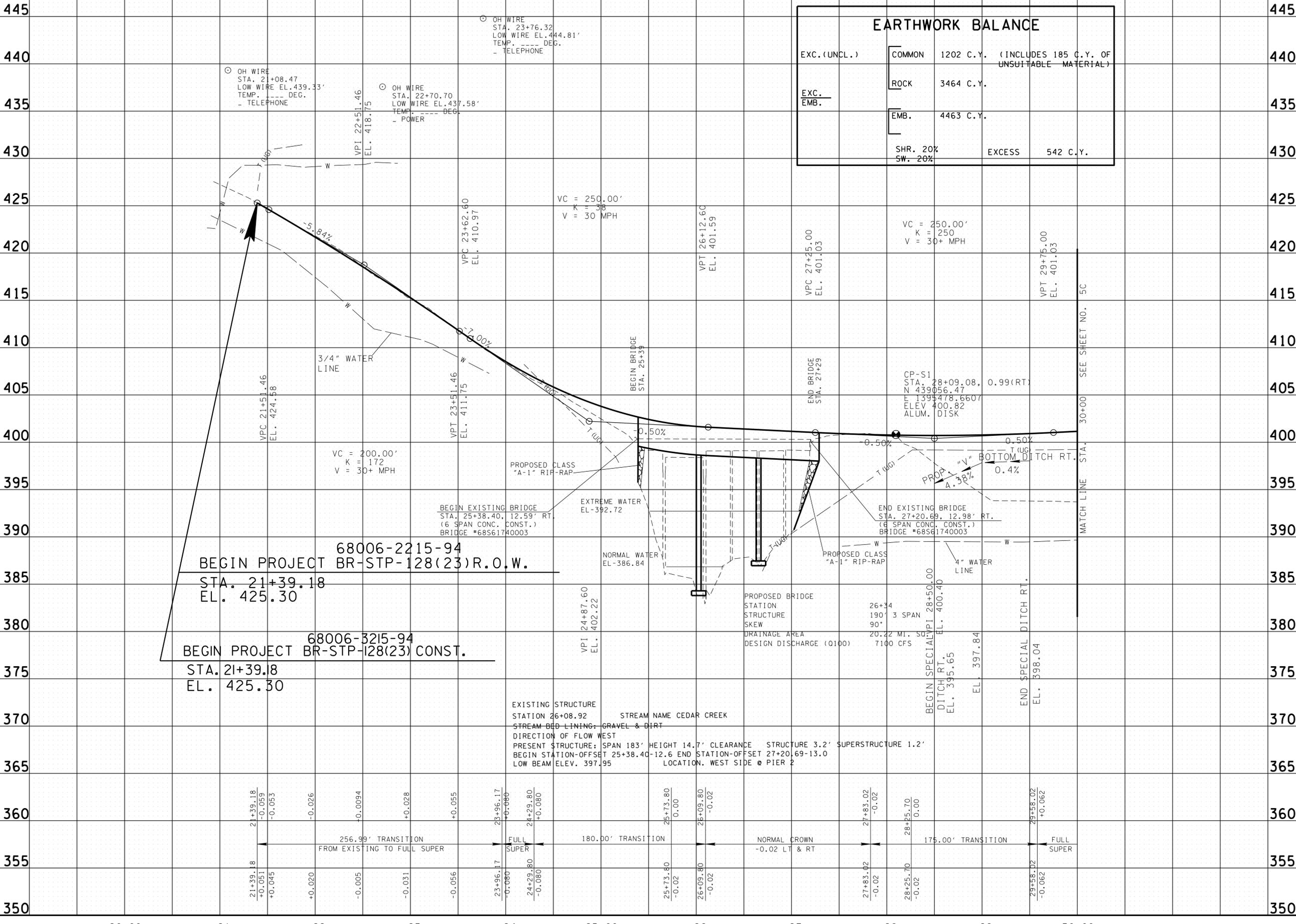
STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

PROPOSED LAYOUT

STA. 20+00 TO STA. 30+00

SCALE: 1" = 50'

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2013	BR-STP-128(23)	4C
CONST.	2015	BR-STP-128(23)	4C



EARTHWORK BALANCE			
EXC. (UNCL.)	COMMON	1202 C.Y.	(INCLUDES 185 C.Y. OF UNSUITABLE MATERIAL)
	ROCK	3464 C.Y.	
EXC. EMB.	EMB.	4463 C.Y.	
	SHR. 20%		EXCESS 542 C.Y.
	SW. 20%		

UNOFFICIAL SET
NOT FOR BIDDING



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

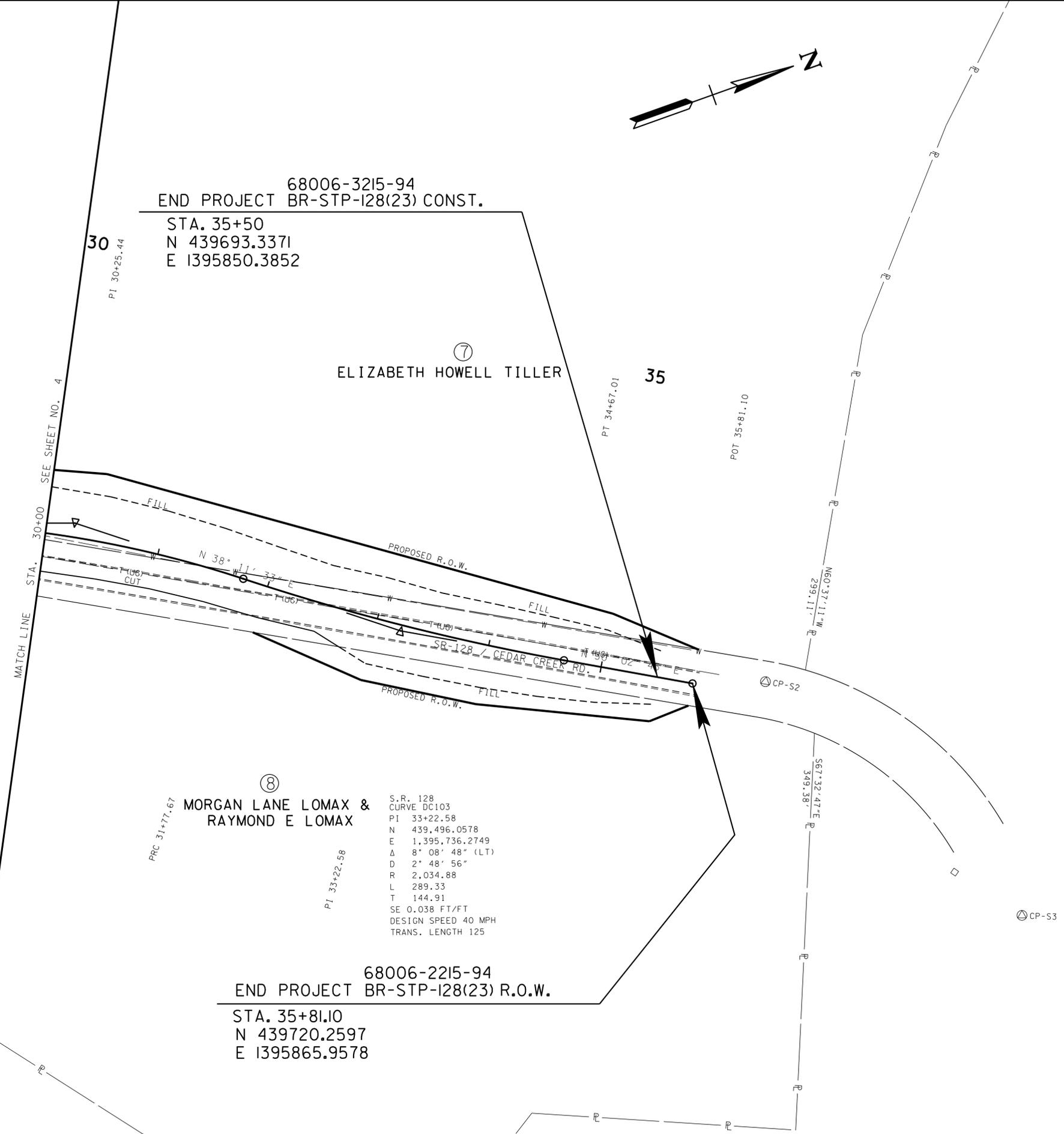
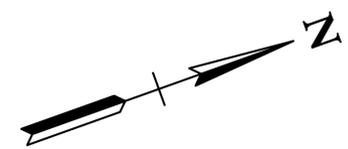
PROFILE

STA. 20+00 TO STA. 30+00
SCALE: 1" = 50' HORIZ.
1" = 5' VERT.

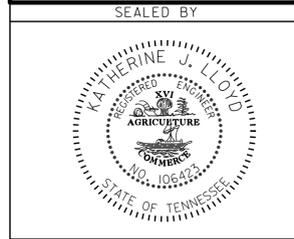
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TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2013	BR-STP-128(23)	5
CONST.	2015	BR-STP-128(23)	5

REV. 2-14-2014: CHANGED PROPERTY OWNERS NAME ON TRACT NUMBER 8.



UNOFFICIAL SET
NOT FOR BIDDING



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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PRESENT LAYOUT

STA. 30+00 TO STA. 35+81

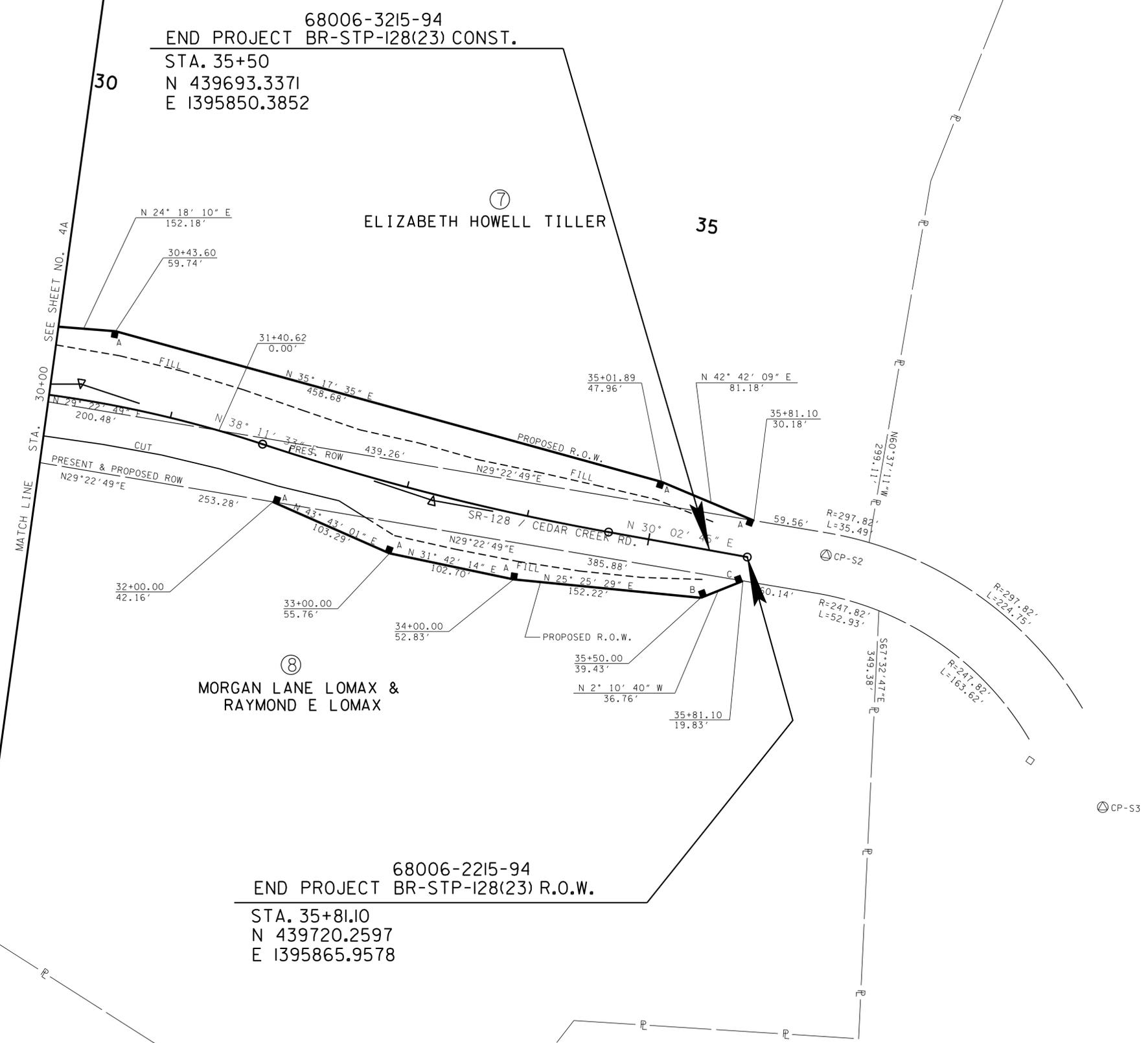
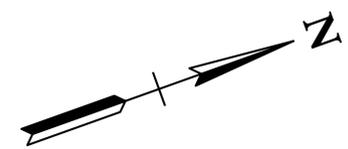
SCALE: 1" = 50'

POINT	NORTHING	EASTING	ELEVATION	DESCRIPTION	STATION	OFFSET	PHYSICAL DESCRIPTION
CP-S1	439056.4594	1395478.6607	400.8190	68-128-01	28+09.63	0.9892	STAMPED ALUM. DISK ON 3/4" REBAR
CP-S2	439780.7730	1395886.1235	402.3320	68-128-02	OffChain	OffChain	STAMPED ALUM. DISK ON 3/4" REBAR
CP-S3	439932.9449	1396160.2194	408.4780	68-128-03	OffChain	OffChain	STAMPED ALUM. DISK ON 3/4" REBAR

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TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2013	BR-STP-128(23)	5A
CONST.	2015	BR-STP-128(23)	5A

REV. 2-14-2014: CHANGED PROPERTY OWNERS NAME ON TRACT NUMBER 8.

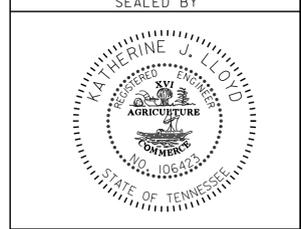


68006-3215-94
 END PROJECT BR-STP-128(23) CONST.
 STA. 35+50
 N 439693.3371
 E 1395850.3852

68006-2215-94
 END PROJECT BR-STP-128(23) R.O.W.
 STA. 35+81.10
 N 439720.2597
 E 1395865.9578

POINT	NORTHING	EASTING	ELEVATION	DESCRIPTION	STATION	OFFSET	PHYSICAL DESCRIPTION
CP-S1	439056.4594	1395478.6607	400.8190	68-128-01	28+09.63	0.9892	STAMPED ALUM. DISK ON 3/4" REBAR
CP-S2	439780.7730	1395886.1235	402.3320	68-128-02	OffChain	OffChain	STAMPED ALUM. DISK ON 3/4" REBAR
CP-S3	439932.9449	1396160.2194	408.4780	68-128-03	OffChain	OffChain	STAMPED ALUM. DISK ON 3/4" REBAR

UNOFFICIAL SET
NOT FOR BIDDING



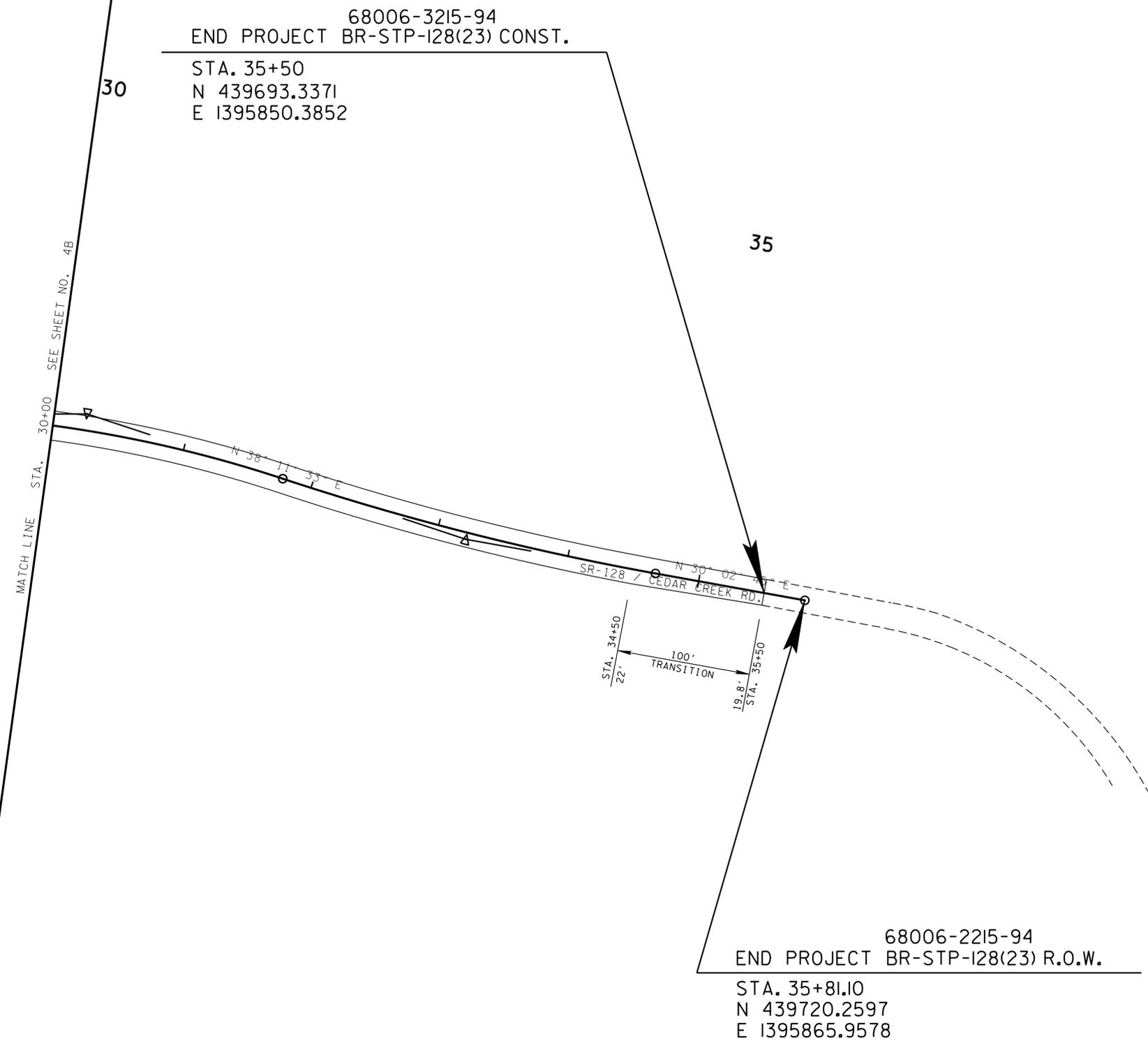
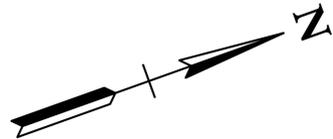
COORDINATES ARE NAD/83(1995), ARE DATUM ADJUSTED BY THE FACTOR OF 1.00005 AND TIED TO THE TGRN. ALL ELEVATIONS ARE REFERENCED TO THE NAVD 1988.

STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

R.O.W. DETAILS
 STA. 30+00 TO STA. 35+81

SCALE: 1" = 50'

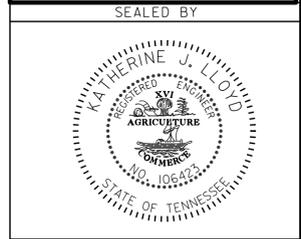
TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2013	BR-STP-128(23)	5B
CONST.	2015	BR-STP-128(23)	5B



68006-3215-94
 END PROJECT BR-STP-128(23) CONST.
 STA. 35+50
 N 439693.3371
 E 1395850.3852

68006-2215-94
 END PROJECT BR-STP-128(23) R.O.W.
 STA. 35+81.10
 N 439720.2597
 E 1395865.9578

**UNOFFICIAL
 SET
 NOT FOR
 BIDDING**



COORDINATES ARE NAD/83(1995),
 ARE DATUM ADJUSTED BY THE
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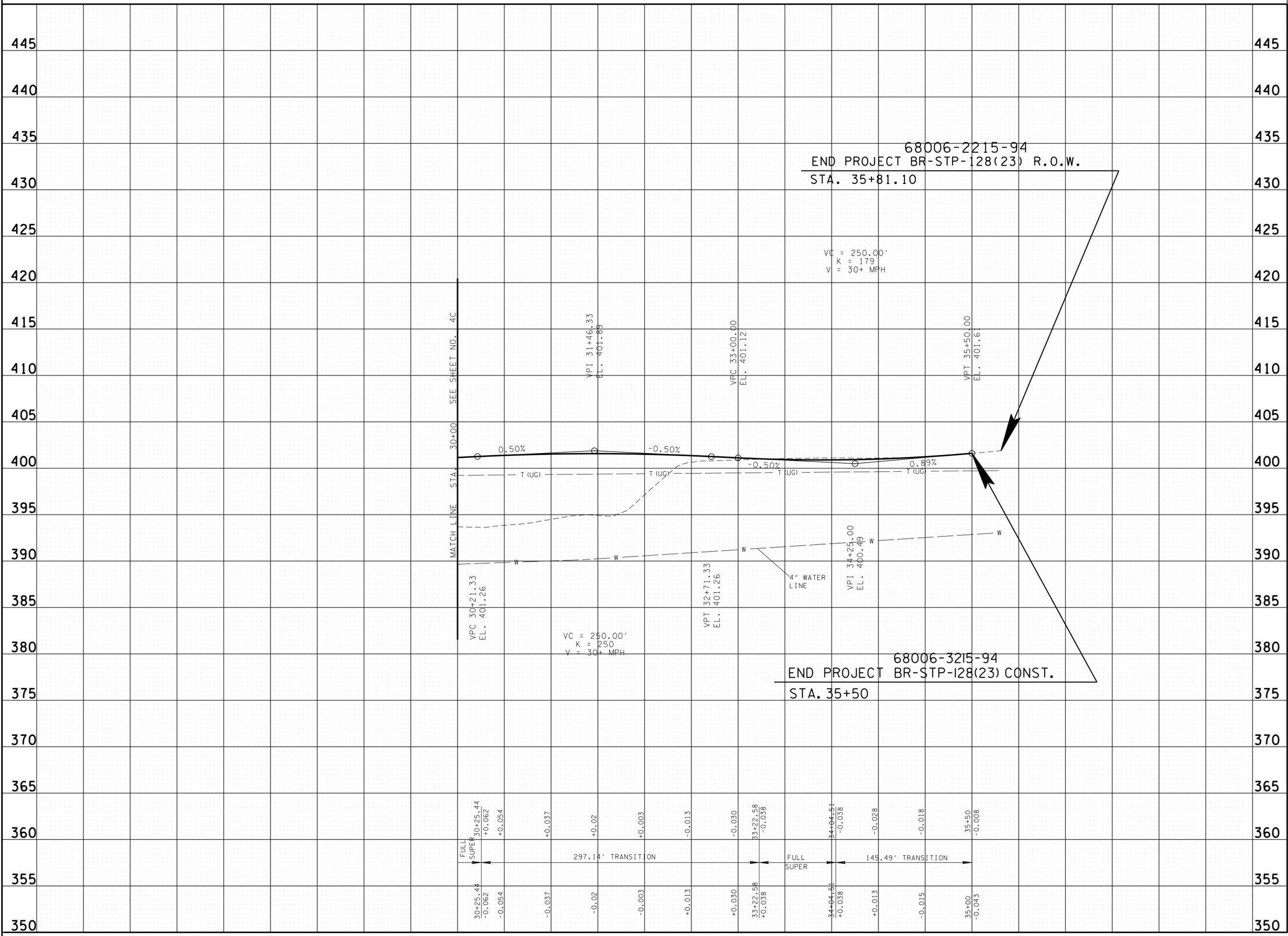
STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

**PROPOSED
 LAYOUT**

STA. 30+00 TO STA. 35+81.10

SCALE: 1" = 50'

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2013	BR-STP-128(23)	5C
CONST.	2015	BR-STP-128(23)	5C



68006-2215-94
 END PROJECT BR-STP-128(23) R.O.W.
 STA. 35+81.10

VC = 250.00'
 K = 179
 V = 30+ MPH

VC = 250.00'
 K = 250
 V = 30+ MPH

68006-3215-94
 END PROJECT BR-STP-128(23) CONST.
 STA. 35+50

**UNOFFICIAL
 SET
 NOT FOR
 BIDDING**



STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

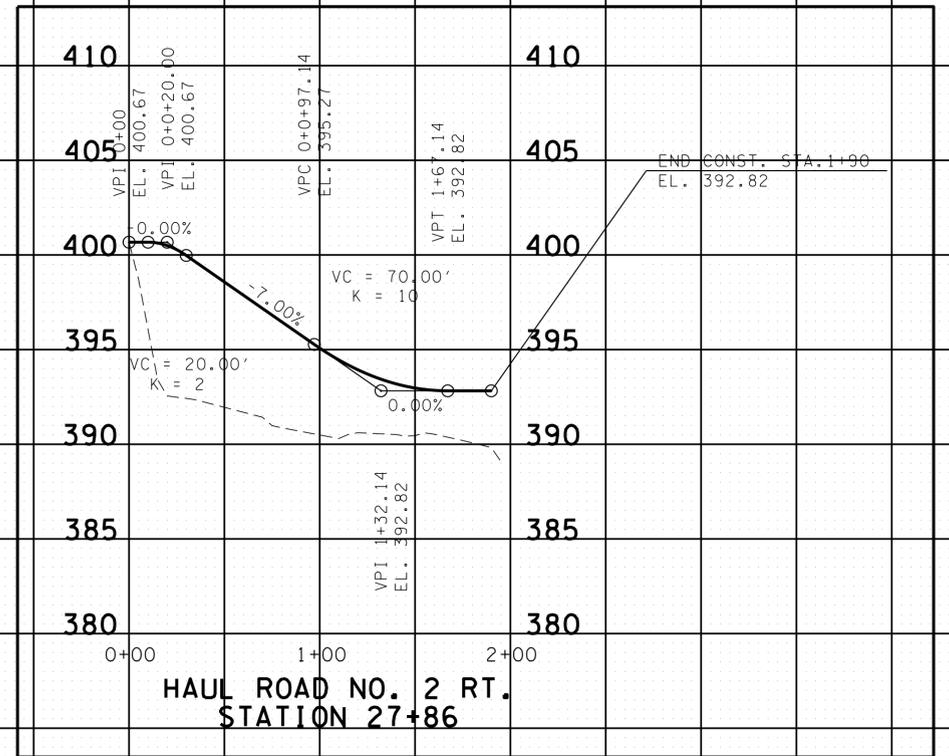
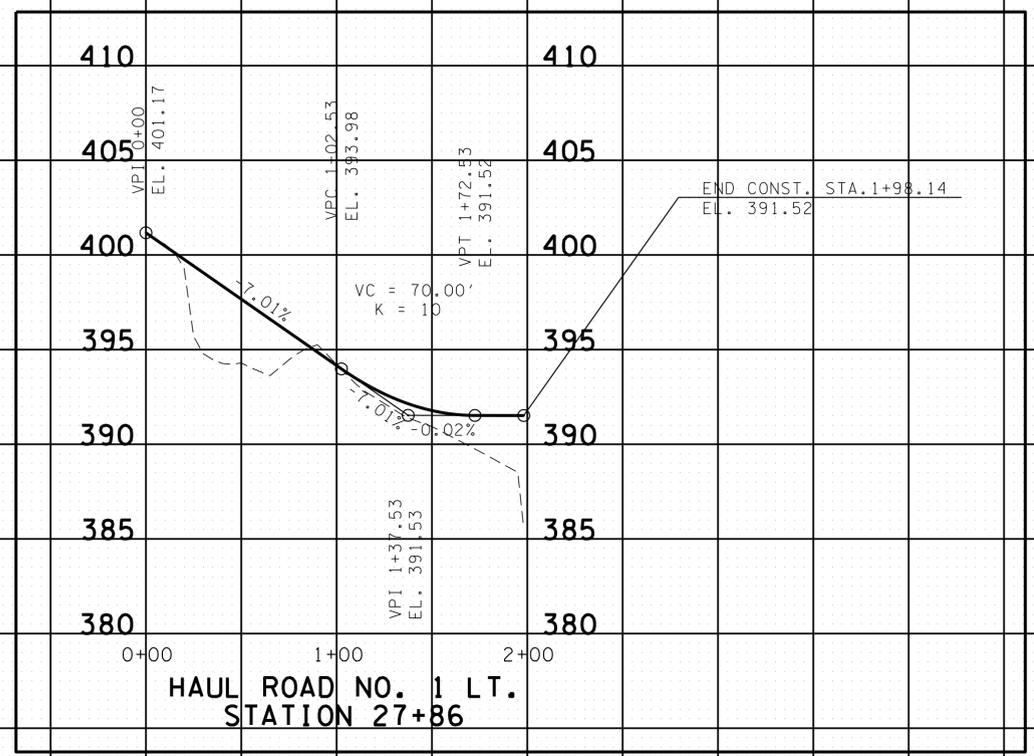
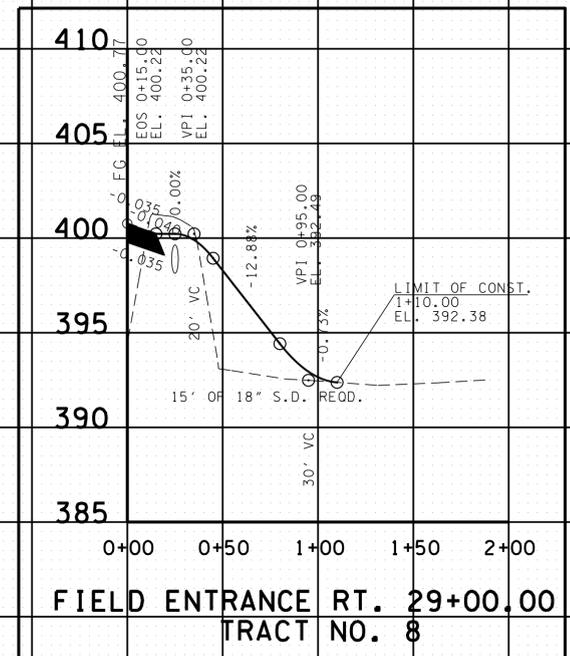
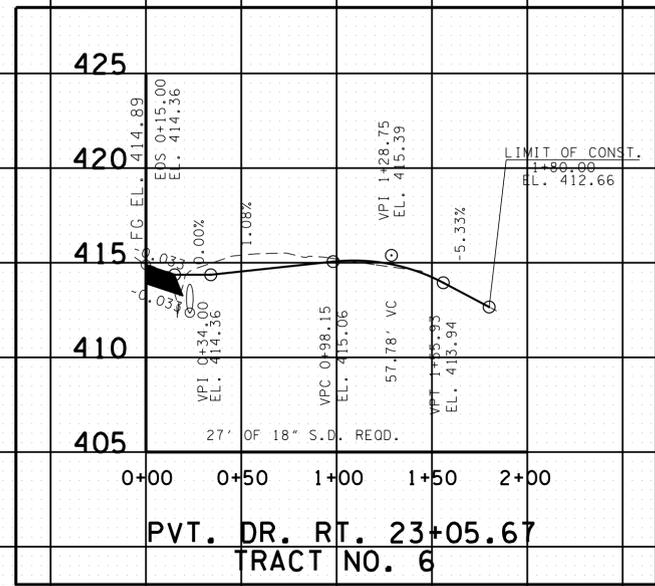
PROFILE

STA. 30+00 TO STA. 35+81
 SCALE: 1" = 50' HORIZ.
 1" = 5' VERT.

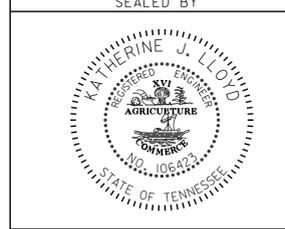
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TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2013	BR-STP-128(23)	6
CONST.	2015	BR-STP-128(23)	6

REV. 12-10-2014: DELETED PROFILE OF FIELD ENTRANCE AT STATION 29+50.
 REV. 3-11-2015: EXTENDED PROFILE OF FIELD ENTRANCE AT STATION 23+05.67 RT.



**UNOFFICIAL
 SET
 NOT FOR
 BIDDING**

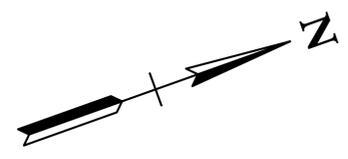
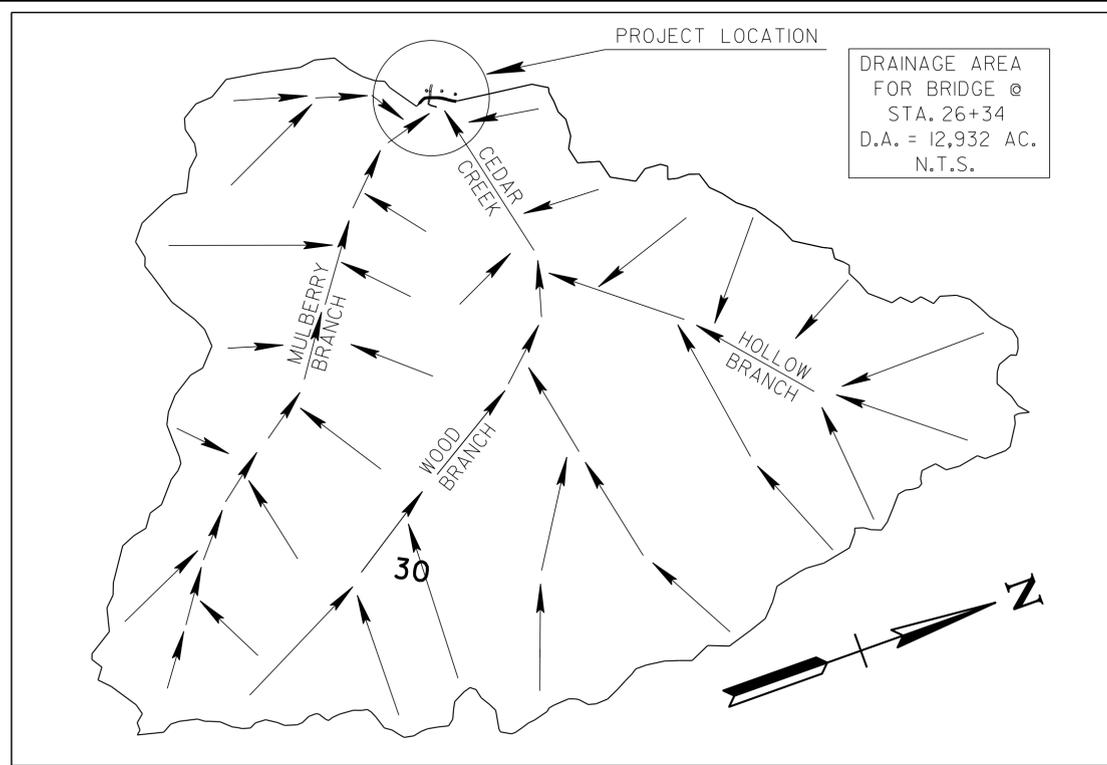


STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

**PROFILE OF
 PVT. DRIVES
 & HAUL ROADS**
 SCALE: 1" = 50' HORIZ.
 1" = 5' VERT.

12-MAR-2015 14:40 C:\Projects\Perry Co\3.R.128\Working Files\06.sht

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2013	BR-STP-128(23)	7
CONST.	2015	BR-STP-128(23)	7



68006-3215-94
BEGIN PROJECT BR-STP-128(23) CONST.

STA. 21+39.18
N 438428.8384
E 1395469.8747

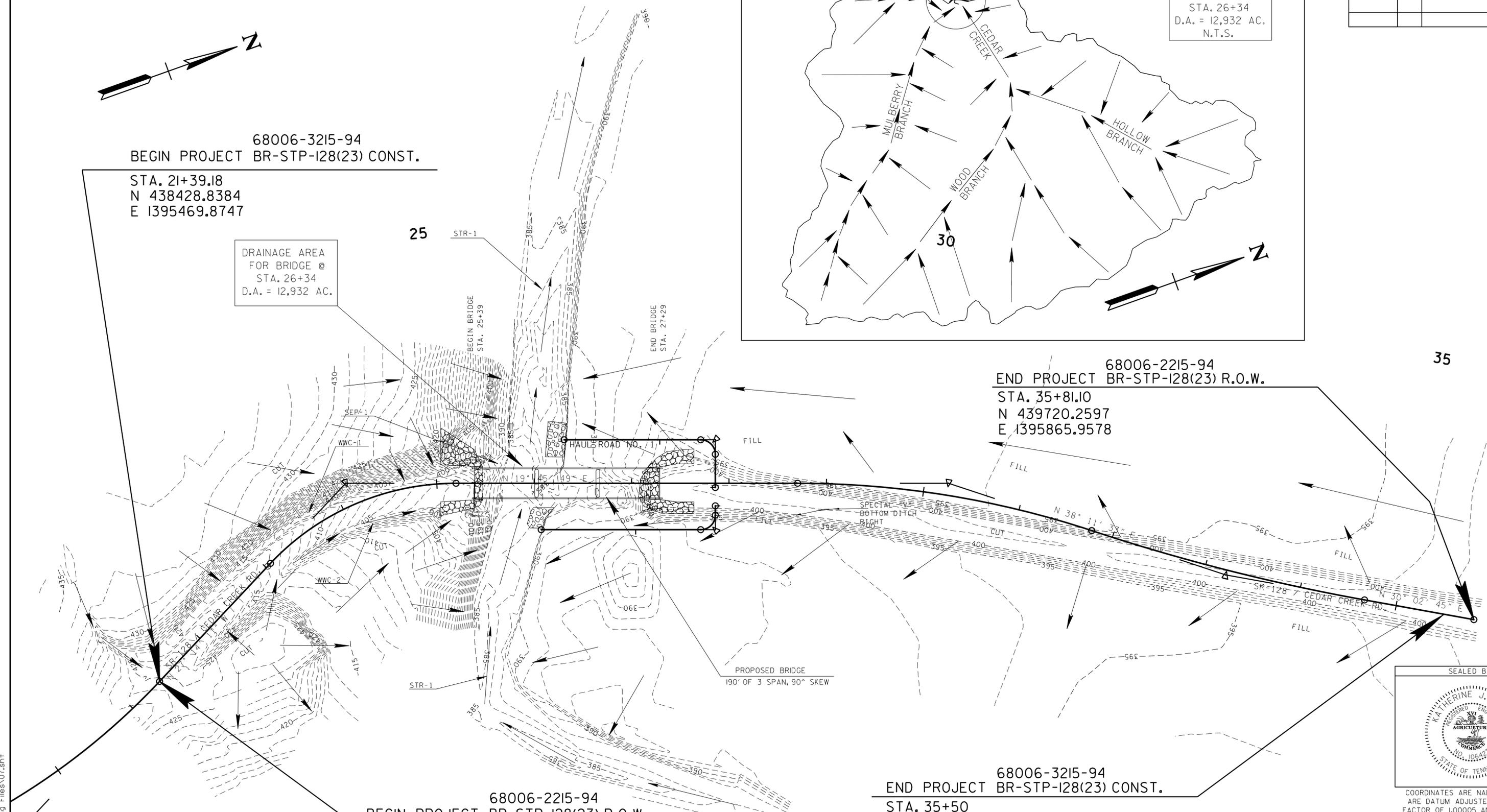
DRAINAGE AREA
FOR BRIDGE @
STA. 26+34
D.A. = 12,932 AC.

25

68006-2215-94
END PROJECT BR-STP-128(23) R.O.W.

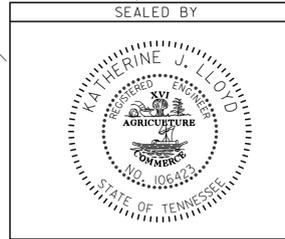
STA. 35+81.10
N 439720.2597
E 1395865.9578

35



68006-2215-94
BEGIN PROJECT BR-STP-128(23) R.O.W.
STA. 21+39.18
N 438428.8384
E 1395469.8747

68006-3215-94
END PROJECT BR-STP-128(23) CONST.
STA. 35+50
N 439693.3371
E 1395850.3852



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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

**UNOFFICIAL
SET
NOT FOR
BIDDING**

**DRAINAGE
MAP**
STA. 20+00 TO STA. 35+81.1
SCALE: 1" = 50'

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2013	BR-STP-128(23)	8
CONST.	2015	BR-STP-128(23)	8

EROSION PREVENTION AND SEDIMENT CONTROL NOTES

STREAM/WETLAND

- (1) ANY WORK WITHIN THE STREAM CHANNEL AREA (E.G., FOR PIER FOOTING, RIP-RAP PLACEMENT, MULTI-BARREL CULVERT/BRIDGE CONSTRUCTION, ETC.) SHALL BE SEPARATED FROM FLOWING WATER OR EXPECTED FLOW PATH AND PERFORMED DURING LOW FLOW CONDITIONS. ALL ITEMS USED WITHIN THE STREAM CHANNEL AREA FOR DIVERSION OF FLOW (OR EXPECTED FLOW), UNLESS SPECIFIED IN THE PLANS, SHALL NOT BE PAID FOR DIRECTLY BUT SHALL BE INCLUDED IN THE COST OF OTHER ITEMS. THIS NOTE EXCLUDES ANY ITEMS SPECIFIED IN THE PLANS FOR THE TEMPORARY DIVERSION CHANNELS, EC-STR-31 AND TEMPORARY DIVERSION CULVERTS, EC-STR-32 FOR SINGLE BARREL CULVERT CONSTRUCTION.
- (2) A 30 FOOT NATURAL RIPARIAN BUFFER ZONE ADJACENT TO AND ON BOTH SIDES OF THE RECEIVING STREAM SHALL BE PRESERVED, TO THE MAXIMUM EXTENT PRACTICABLE, DURING CONSTRUCTION ACTIVITIES AT THE SITE. BUFFER ZONES ARE NOT SEDIMENT CONTROL MEASURES AND SHOULD NOT BE RELIED UPON AS PRIMARY SEDIMENT CONTROL MEASURES. THE RIPARIAN BUFFER ZONE SHALL BE ESTABLISHED BETWEEN THE TOP OF THE STREAM BANK AND THE DISTURBED CONSTRUCTION AREA. THE 30 FOOT CRITERION FOR THE WIDTH OF THE BUFFER ZONE CAN BE ESTABLISHED ON AN AVERAGE WIDTH BASIS AT A PROJECT, AS LONG AS THE MINIMUM WIDTH OF THE BUFFER ZONE IS MORE THAN 15 FEET AT ANY MEASURED LOCATION. EVERY ATTEMPT SHALL BE MADE FOR CONSTRUCTION ACTIVITIES NOT TO TAKE PLACE WITHIN THE BUFFER ZONES. BEST MANAGEMENT PRACTICES (BMPS) PROVIDING EQUIVALENT PROTECTION AS THE NATURAL RIPARIAN ZONE MAY BE USED. A JUSTIFICATION FOR USE AND DESIGN EQUIVALENCY SHALL BE DOCUMENTED WITHIN THE SWPPP. THE ENVIRONMENTAL AND DESIGN DIVISIONS SHALL REVIEW AND APPROVE THIS REVISION OF THE SWPPP BEFORE DISTURBANCE OF THE SITE PROCEEDS, UNLESS PREVIOUSLY EXEMPT IN THE NPDES CONSTRUCTION GENERAL PERMIT. WHERE ISSUED, ARAP/401 REQUIREMENTS WILL PREVAIL IF IN CONFLICT WITH THESE BUFFER ZONE REQUIREMENTS.

NPDES

- (3) NO WORK SHALL BE STARTED UNTIL THE CONTRACTOR'S PLAN FOR THE STAGING OF THEIR OPERATIONS, INCLUDING THE PLAN FOR STAGING OF TEMPORARY AND PERMANENT EPSC MEASURES, HAS BEEN ACCEPTED BY THE ENGINEER. THE CONTRACTOR'S EPSC PLAN SHALL INCORPORATE AND SUPPLEMENT, AS ACCEPTABLE, THE BASIC EPSC DEVICES ON THE EPSC PLAN CONTAINED IN THE APPROVED SWPPP.
- (4) THE EPSC MEASURES AND/OR PLAN SHALL BE MODIFIED AS NECESSARY SO THAT THEY ARE EFFECTIVE AT ALL TIMES THROUGHOUT THE COURSE OF THE PROJECT.
- (5) THE ACCEPTED EPSC PLAN SHALL REQUIRE THAT EPSC MEASURES BE IN PLACE BEFORE CLEARING, GRUBBING, EXCAVATION, GRADING, CUTTING OR FILLING OCCURS, EXCEPT AS SUCH WORK MAY BE NECESSARY TO INSTALL EPSC MEASURES, INCLUDING WITHOUT LIMITATION AS FOLLOWS:
 - A. INITIAL CLEARING AND GRUBBING SHALL BE LIMITED TO THAT NECESSARY FOR THE INSTALLATION OF APPLICABLE EPSC MEASURES IN ACCORDANCE WITH THE ACCEPTED EPSC PLAN INCORPORATED INTO THE SWPPP.
 - B. NO OTHER CLEARING AND GRUBBING OPERATIONS SHALL BE STARTED BEFORE APPLICABLE EPSC MEASURES ARE IN PLACE IN ACCORDANCE WITH THE ACCEPTED EPSC PLAN INCORPORATED INTO THE SWPPP.
 - C. NO CULVERT OR BRIDGE CONSTRUCTION SHALL BE STARTED BEFORE APPLICABLE EPSC MEASURES ARE IN PLACE IN ACCORDANCE WITH THE ACCEPTED EPSC PLAN INCORPORATED INTO THE SWPPP.
 - D. NO GRADING, EXCAVATION, CUTTING, FILLING, OR OTHER EARTHWORK SHALL BE STARTED BEFORE EPSC MEASURES ARE IN PLACE IN ACCORDANCE WITH THE ACCEPTED EPSC PLAN INCORPORATED INTO THE SWPPP.
- (6) PERMANENT EPSC MEASURES SHALL BE INITIATED WITHIN 14 CALENDAR DAYS AFTER FINAL GRADING OF ANY SEQUENCE OR PHASE. TEMPORARY OR PERMANENT STABILIZATION SHALL BE INITIATED WITHIN 14 CALENDAR DAYS AFTER FINAL GRADING OR WHEN CONSTRUCTION ACTIVITIES ON A PORTION OF THE SITE ARE TEMPORARILY CEASED AND EARTH DISTURBING ACTIVITIES WILL NOT RESUME UNTIL AFTER 14 CALENDAR DAYS. PERMANENT STABILIZATION WITH PERENNIAL VEGETATION OR

OTHER PERMANENTLY STABLE NON-ERODING SURFACE SHALL REPLACE ANY TEMPORARY MEASURES AS SOON AS PRACTICABLE. UNPACKED GRAVEL CONTAINING FINES (SILT AND CLAY SIZED PARTICLES) OR CRUSHER-RUN WILL NOT BE CONSIDERED A NON-ERODIBLE SURFACE.

- (7) STEEP SLOPES (A NATURAL OR CREATED SLOPE OF 35% GRADE (2.8H:1V) OR GREATER REGARDLESS OF HEIGHT) SHALL BE TEMPORARILY STABILIZED NO LATER THAN 7 CALENDAR DAYS AFTER CONSTRUCTION ACTIVITY ON THE SLOPE HAS TEMPORARILY OR PERMANENTLY CEASED.
- (8) FOR STORMWATER DISCHARGES ASSOCIATED WITH CONSTRUCTION SUPPORT ACTIVITIES; TDOT PROJECTS ARE COVERED UNDER THE "WASTE AND BORROW" MANUAL PER THE SSWMP.
- (9) EXCEPT AS OTHERWISE SPECIFIED, THERE ARE NO KNOWN SPECIAL ENVIRONMENTAL FACTORS PRESENT ON THIS PROJECT THAT INDICATE A NEED FOR SEASONAL LIMITATIONS ON THE CLEARING, GRUBBING, EXCAVATION, GRADING, CUTTING OR FILLING OPERATIONS OR ON THE TOTAL AREA OF EXPOSED SOIL.

EPSC QUANTITIES			
ITEM NO.	DESCRIPTION	UNIT	QUANTITY
(1)	209-05	SEDIMENT REMOVAL	C.Y. 450
(1)	209-08.02	SILT FENCE WITH WIRE BACKING	L.F. 4223
(1)	209-08.07	ROCK CHECK DAM	EACH 15
(1)	209-08.08	ENHANCED ROCK CHECK DAM	EACH 4
(1)(2)	209-65.04	TEMPORARY IN STREAM DIVERSION	L.F. 100
(1)	209-09.03	SEDIMENT FILTER BAG (15' X 15')	EACH 2
(1)(4)	303-10.01	MINERAL AGGREGATE (SIZE 57)	TON 29
(1)(3)	709-05.05	MACHINED RIP-RAP (CLASS A-3)	TON 135
(1)(5)	740-10.03	GEOTEXTILE (TYPE III)(EROSION CONTROL)	S.Y. 1922
(1)(9)	740-11.02	TEMPORARY SEDIMENT TUBE (12 INCH)	L.F. 1060
(1)(6)	801-01.07	TEMPORARY SEEDING (WITH MULCH)	UNITS 47
(7)	803-01	SODDING (NEW SOD)	S.Y. 5239
(1)(8)	805-12.03	EROSION CONTROL BLANKET (TYPE III)	S.Y. 5239

NOTES:

- (1) SEE SUBSECTION 209.07 OF THE STANDARD SPECIFICATION FOR MAINTENANCE REPLACEMENT.
- (2) USED TO DIVERT WATER WITHIN STREAM
- (3) RIP-RAP USED FOR THREE (3) TEMPORARY CONSTRUCTION EXITS
- (4) FOR SEDIMENT FILTER BAGS
- (5) 1509 S.Y. FOR HAUL ROADS, 269 S.Y. FOR CONST. EXITS AND 144 S.Y. FOR FILTER BAGS
- (6) USED TO PROTECT SLOPES DURING CONSTRUCTION OF PROJECT
- (7) SOD TO BE USED ON ALL SLOPES
- (8) TO BE USED ON ALL SLOPES
- (9) TO BE USED AS NEEDED BY THE PROJECT ENGINEER.

EROSION PREVENTION AND SEDIMENT CONTROL LEGEND		
SYMBOL	ITEM	STD. DWG.
	ENHANCED ROCK CHECK DAM (V-DITCH)	EC-STR-6A
	ROCK CHECK DAM (V-DITCH)	EC-STR-6
** TUBE ** TUBE **	SEDIMENT TUBE	EC-STR-37
* SFB * SFB * SFB *	SILT FENCE WITH WIRE BACKING	EC-STR-3C
	SEDIMENT FILTER BAG	EC-STR-2
* *	TEMPORARY CONSTRUCTION EXIT	EC-STR-25
	EROSION CONTROL BLANKET	EC-STR-34
— IN — DIV —	INSTREAM DIVERSION	EC-STR-30 EC-STR-30A
* HVF * HVF	HIGH VISIBILITY FENCE	S-F-1

* TO BE FIELD LOCATED BY THE PROJECT ENGINEER

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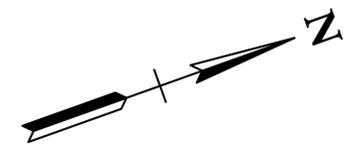


STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

**EROSION
PREVENTION
AND SEDIMENT
CONTROL NOTES
AND TABULATION**

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2013	BR-STP-128(23)	9
CONST.	2015	BR-STP-128(23)	9

REV. 2-14-2014: CHANGED PROPOSED ROW ON LEFT SIDE BEFORE BRIDGE.
 REV. 9-09-2014: ADDED CONSTRUCTION EASEMENT TO RIGHT OF STATION 23+50.
 REV. 12-10-2014: DELETED FIELD ENTRANCE AT STATION 29+50 ON LEFT OF CENTERLINE.
 REV. 3-11-2015: DELETED CONST. ESMT. ON RIGHT OF C.L. AT STATION 23+50.



68006-3215-94
 BEGIN PROJECT BR-STP-128(23) CONST.
 STA. 21+39.18
 N 438428.8384
 E 1395469.8747

68006-2215-94
 END PROJECT BR-STP-128(23) R.O.W.
 STA. 35+81.10
 N 439720.2597
 E 1395865.9578

1 OUTFALL 25
 D.A. = 0.811 AC.
 SLOPE = 8.74%

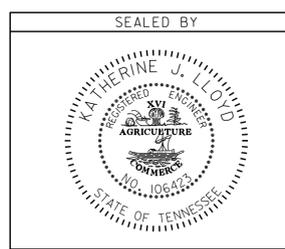
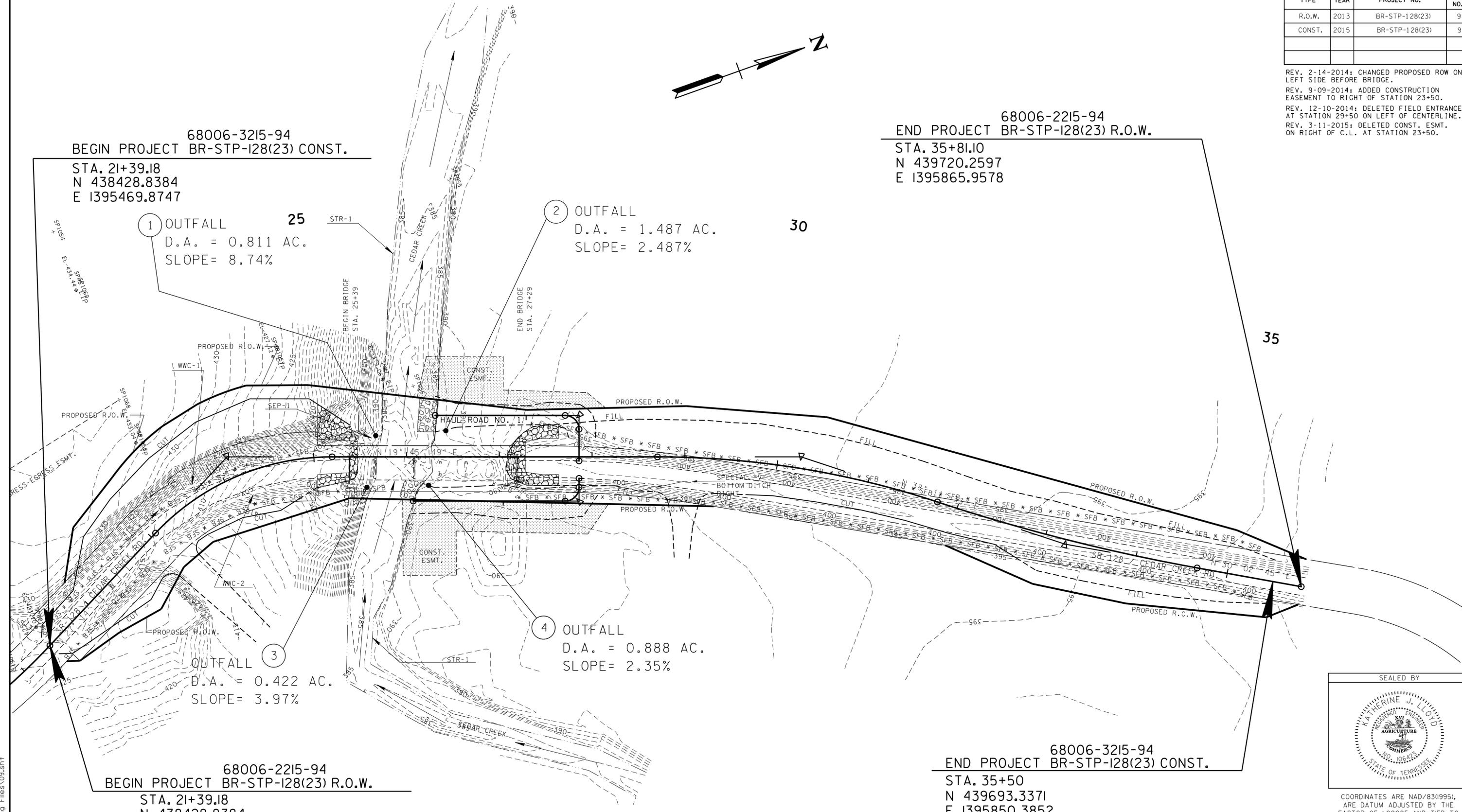
2 OUTFALL 30
 D.A. = 1.487 AC.
 SLOPE = 2.487%

3 OUTFALL
 D.A. = 0.422 AC.
 SLOPE = 3.97%

4 OUTFALL
 D.A. = 0.888 AC.
 SLOPE = 2.35%

68006-2215-94
 BEGIN PROJECT BR-STP-128(23) R.O.W.
 STA. 21+39.18
 N 438428.8384
 E 1395469.8747

68006-3215-94
 END PROJECT BR-STP-128(23) CONST.
 STA. 35+50
 N 439693.3371
 E 1395850.3852



COORDINATES ARE NAD/83(1995), ARE DATUM ADJUSTED BY THE FACTOR OF 1.00005 AND TIED TO THE TGRN. ALL ELEVATIONS ARE REFERENCED TO THE NAVD 1988.

STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

EROSION PREVENTION AND SEDIMENT CONTROL PLAN
 CLEARING & GRUBBING
 SCALE: 1" = 50'

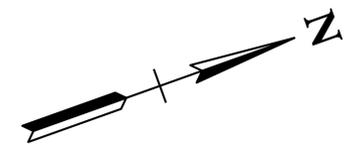
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 NOT FOR BIDDING

NOTE: EXISTING CONTOURS SHOWN

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TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2013	BR-STP-128(23)	10
CONST.	2015	BR-STP-128(23)	10

REV. 2-14-2014: CHANGED PROPOSED ROW ON LEFT SIDE BEFORE BRIDGE.
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 REV. 12-10-2014: DELETED FIELD ENTRANCE AT STATION 29+50 ON LEFT OF CENTERLINE.
 REV. 3-11-2015: DELETED CONST. ESMT. ON RIGHT OF C.L. AT STATION 23+50.



68006-3215-94
 BEGIN PROJECT BR-STP-128(23) CONST.

STA. 21+39.18
 N 438428.8384
 E 1395469.8747

68006-2215-94
 END PROJECT BR-STP-128(23) R.O.W.

STA. 35+81.10
 N 439720.2597
 E 1395865.9578

1 OUTFALL
 D.A. = 0.811 AC.
 SLOPE = 8.74%

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 D.A. = 1.487 AC.
 SLOPE = 2.487%

3 OUTFALL
 D.A. = 0.422 AC.
 SLOPE = 3.97%

4 OUTFALL
 D.A. = 0.888 AC.
 SLOPE = 2.35%

68006-2215-94
 BEGIN PROJECT BR-STP-128(23) R.O.W.

STA. 21+39.18
 N 438428.8384
 E 1395469.8747

68006-3215-94
 END PROJECT BR-STP-128(23) CONST.

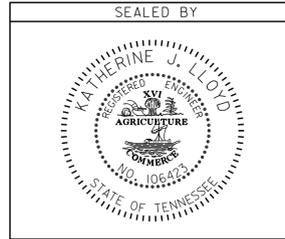
STA. 35+50
 N 439693.3371
 E 1395850.3852

NOTES

1. THE ROCK PAD SHALL NOT EXCEED 1/3 OF THE WIDTH OF THE STREAM. THE ROCK PAD SHALL CONSIST OF CLEAN ROCK FREE OF FINES. IF A LARGER ROCK PAD IS REQUIRED, CONTACT THE ENVIRONMENTAL DIVISION. ADDITIONAL PERMITS MAY BE REQUIRED.
2. THE ROCK PADS ARE TO BE REMOVED AND THE BANKS STABILIZED. ANY TREES REMOVED FOR THE CONSTRUCTION OF THE HAUL ROADS AND ROCK PAD SHOULD BE REPLACED.

NOTE: PROPOSED CONTOURS SHOWN

**UNOFFICIAL
 SET
 NOT FOR
 BIDDING**



COORDINATES ARE NAD/83(1995), ARE DATUM ADJUSTED BY THE FACTOR OF 1.00005 AND TIED TO THE TGRN. ALL ELEVATIONS ARE REFERENCED TO THE NAVD 1988.

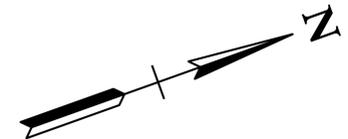
STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

**EROSION
 PREVENTION
 AND SEDIMENT
 CONTROL PLAN
 PHASE I**
 SCALE: 1" = 50'

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TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2013	BR-STP-128(23)	11
CONST.	2015	BR-STP-128(23)	11

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68006-3215-94
 BEGIN PROJECT BR-STP-I28(23) CONST.

STA. 21+39.18
 N 438428.8384
 E 1395469.8747

68006-2215-94
 END PROJECT BR-STP-I28(23) R.O.W.

STA. 35+81.10
 N 439720.2597
 E 1395865.9578

1 OUTFALL 25
 D.A. = 0.811 AC.
 SLOPE = 8.74%

2 OUTFALL 30
 D.A. = 1.487 AC.
 SLOPE = 2.487%

4 OUTFALL
 D.A. = 0.888 AC.
 SLOPE = 2.35%

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 D.A. = 0.422 AC.
 SLOPE = 3.97%

68006-2215-94
 BEGIN PROJECT BR-STP-I28(23) R.O.W.

STA. 21+39.18
 N 438428.8384
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68006-3215-94
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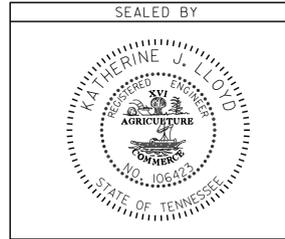
STA. 35+50
 N 439693.3371
 E 1395850.3852

NOTES

1. THE ROCK PAD SHALL NOT EXCEED 1/3 OF THE WIDTH OF THE STREAM. THE ROCK PAD SHALL CONSIST OF CLEAN ROCK FREE OF FINES. IF A LARGER ROCK PAD IS REQUIRED, CONTACT THE ENVIRONMENTAL DIVISION. ADDITIONAL PERMITS MAY BE REQUIRED.
2. THE ROCK PADS ARE TO BE REMOVED AND THE BANKS STABILIZED. ANY TREES REMOVED FOR THE CONSTRUCTION OF THE HAUL ROADS AND ROCK PAD SHOULD BE REPLACED.

NOTE: PROPOSED CONTOURS SHOWN

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COORDINATES ARE NAD/83(1995), ARE DATUM ADJUSTED BY THE FACTOR OF 1.00005 AND TIED TO THE TGRN. ALL ELEVATIONS ARE REFERENCED TO THE NAVD 1988.

STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

EROSION PREVENTION AND SEDIMENT CONTROL PLAN
 PHASE II
 SCALE: 1" = 50'

TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	2015	BR-STP-128(23)	12

PAVEMENT EDGE DROP-OFF TRAFFIC CONTROL NOTES

A. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES OR TRAFFIC LANE AND SHOULDER WHERE THE TRAFFIC LANE IS BEING USED BY TRAFFIC, CAUSED BY BASE, PAVING OR RESURFACING:

1. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 0.75 INCH AND NOT EXCEEDING 2 INCHES:
 - a. WARNING SIGNS, UNEVEN LANES (W8-11) AND/OR SHOULDER DROP-OFF WITH PLAQUE (W8-17 AND W8-17P), SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.
 - b. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY ADDED PAVEMENT SHALL BE ELIMINATED WITHIN THREE WORKDAYS.
 - c. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY COLD PLANING SHALL BE ELIMINATED WITHIN THREE WORKDAYS.
 - d. WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE TRAFFIC LANE BEING UTILIZED BY TRAFFIC AND SHOULDER THE DIFFERENCE IN ELEVATION SHALL BE ELIMINATED WITHIN SEVEN WORKDAYS AFTER THE CONDITION IS CREATED.
2. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 2 INCHES AND NOT EXCEEDING 6 INCHES. TRAFFIC IS NOT TO BE ALLOWED TO TRAVERSE THIS DIFFERENCE IN ELEVATION.
 - a. SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:
 - (1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.
 - (2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.
 - b. IF THE DIFFERENCE IN ELEVATION IS ELIMINATED OR DECREASED TO 2 INCHES OR LESS BY THE END OF EACH WORKDAY, CONES MAY BE USED DURING DAYLIGHT HOURS IN LIEU OF DRUMS, BARRICADES OR OTHER APPROVED PROTECTIVE DEVICES MENTIONED IN PARAGRAPH a, PROVIDED WARNING SIGNS ARE ERECTED. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.
 - c. WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE THROUGH TRAFFIC LANE AND THE SHOULDER AND THE ELEVATION DIFFERENCE IS LESS THAN 3.5 INCHES, THE CONTRACTOR MAY USE WARNING SIGNS AND/OR PROTECTIVE DEVICES AS APPLICABLE AND APPROVED BY THE ENGINEER. SEE PARAGRAPH a REGARDING USE OF DRUMS, BARRICADES OR OTHER APPROVED PROTECTIVE DEVICES. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) WILL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

IN THESE SITUATIONS, THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 2 MILES IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

3. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 6 INCHES BUT NOT EXCEEDING 18 INCHES, THE CONTRACTOR, WITH THE ENGINEER'S APPROVAL, MAY UTILIZE ONE OF THE FOLLOWING:

- a. THE CONTRACTOR SHALL ACCOMPLISH SEPARATION BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:
 - (1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.
 - (2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

IN ORDER TO USE THIS METHOD, THE CONTRACTOR MUST REDUCE THE DIFFERENCE IN ELEVATION TO 6 INCHES OR LESS BY THE END OF THE WORKDAY THAT THE CONDITION IS CREATED.

- b. THE CONTRACTOR SHALL PROVIDE DRUMS, BARRICADES OR OTHER APPROVED SEPARATION DEVICES AS SPECIFIED IN PARAGRAPH a, AND CONSTRUCT A STONE WEDGE WITH A 4:1 SLOPE, OR FLATTER, TO ELIMINATE THE VERTICAL OFFSET IF THE LOWER ELEVATION IS AT OR BELOW SUBGRADE AT THE END OF EACH DAY.
- c. THE CONTRACTOR SHALL PROVIDE DRUMS, BARRICADES OR OTHER APPROVED SEPARATION DEVICES AS SPECIFIED IN PARAGRAPH a AND IF THE LOWER ELEVATION IS BASE STONE OR ASPHALT PAVEMENT, PLACEMENT OF SUBSEQUENT LAYERS OF PAVEMENT MUST BEGIN THE NEXT WORK DAY AND PROGRESS CONTINUOUSLY UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED OR REDUCED TO SIX INCHES OR LESS.
- d. THE CONTRACTOR SHALL PROVIDE SEPARATION BY PORTABLE BARRIER RAIL.

FOR PRECEDING CONDITIONS a, b, AND c, THE CONTRACTOR SHALL USE THE SHOULDER DROP-OFF WARNING SIGN WITH PLAQUE (W8-17 AND W8-17P). IT SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN THE SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. IN THESE SITUATIONS, THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 1 MILE IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

4. FOR DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 18 INCHES. SEPARATION WILL BE PROVIDED BY USE OF PORTABLE BARRIER RAIL.

IN THIS SITUATION THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 1 MILE IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

B. IF THE DIFFERENCE IN ELEVATION IS WITHIN 30 FEET OF THE NEAREST TRAFFIC LANE BEING USED BY TRAFFIC CAUSED BY GRADING, EXCAVATION FOR UTILITIES, DRAINAGE STRUCTURES, UNDERCUTTING, ETC.:

1. IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 3/4 INCH AND NOT EXCEEDING 2 INCHES.

WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

2. IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 2 INCHES AND NOT EXCEEDING 6 INCHES:

- a. SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:
 - (1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.
 - (2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

3. IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 6 INCHES:

- a. SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:
 - (1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.
 - (2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.
- b. ELIMINATE VERTICAL OFFSET BY CONSTRUCTING A STONE WEDGE OR GRADING TO A 4:1 SLOPE, OR FLATTER, OR USE PORTABLE BARRIER RAIL.

THE CONTRACTOR SHALL SCHEDULE THE WORK SO AS TO MINIMIZE THE TIME TRAFFIC IS EXPOSED TO AN ELEVATION DIFFERENCE. ONCE THE CONTRACTOR BEGINS AN ACTIVITY THAT CREATES AN ELEVATION DIFFERENCE WITHIN 8 FEET OF A TRAFFIC LANE, THE ACTIVITY SHALL BE PURSUED AS A CONTINUOUS OPERATION UNTIL THE ELEVATION DIFFERENCE IS ELIMINATED.

C. IF THE DIFFERENCE IN ELEVATION IS FARTHER THAN 8 FEET FROM THE NEAREST TRAFFIC LANE BUT NOT MORE THAN 30 FEET FROM THE NEAREST TRAFFIC LANE:

SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

1. WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.
2. WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

THE CONTRACTOR SHALL SCHEDULE THE WORK SO AS TO MINIMIZE THE TIME TRAFFIC IS EXPOSED TO AN ELEVATION DIFFERENCE. ONCE THE CONTRACTOR BEGINS AN ACTIVITY THAT CREATES AN ELEVATION DIFFERENCE, THE ACTIVITY SHALL BE PURSUED AS A CONTINUOUS OPERATION UNTIL THE ELEVATION DIFFERENCE IS ELIMINATED.

**UNOFFICIAL
SET
NOT FOR
BIDDING**

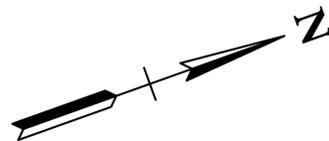
SEALED BY



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

**PAVEMENT EDGE
DROP OFF
NOTES**

TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	2015	BR-STP-128(23)	13A



68006-3215-94
 BEGIN PROJECT BR-STP-128(23) CONST.

STA. 21+39.18
 N 438428.8384
 E 1395469.8747

25

30

35

CEDAR CREEK

CONST. ESMT.

PROPOSED R.O.W.

PROPOSED R.O.W.

PROPOSED R.O.W.

HAUL ROAD NO. 1

PROPOSED R.O.W.

PROPOSED R.O.W.

PRESENT & PROPOSED ROW

PROPOSED R.O.W.

PROPOSED R.O.W.

CONST. ESMT.

PROPOSED R.O.W.

TRAFFIC CONTROL LEGEND

SYMBOL	ITEM
	WORK ZONE
	FLEXIBLE DRUMS (CHANNELIZING)
	PORTABLE BARRIER RAIL

STA. 27+86 S.R. 128=
 HAUL ROAD 1, LT. 4.38'
 N 439033.6016 E 1395474.0510
 HAUL ROAD 2, RT. 23.35'
 N 439027.1863 E 1395491.9059

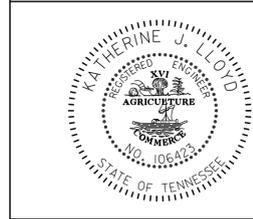
68006-3215-94
 END PROJECT BR-STP-128(23) CONST.

STA. 35+50
 N 439693.3371
 E 1395850.3852

SEQUENCE OF CONSTRUCTION (PHASE II)

1. SHIFT TRAFFIC ONTO NEW BRIDGE AND ROADWAY.
2. CONSTRUCT RIGHT LANE OF PROPOSED BRIDGE AND ROADWAY.
3. INSTALL GUARDRAIL
4. INSTALL FINAL PAVING AND PAVEMENT MARKINGS

SEALED BY



COORDINATES ARE NAD/83(1995),
 ARE DATUM ADJUSTED BY THE
 FACTOR OF 1.00005 AND TIED TO
 THE TGRN. ALL ELEVATIONS ARE
 REFERENCED TO THE NAVD 1988.

STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

**TRAFFIC CONTROL PLAN
 PHASE II**

SCALE: 1"=50'

**UNOFFICIAL
 SET
 NOT FOR
 BIDDING**

NOTE: FOR ADDITIONAL SIGNS SEE STANDARD DWG. T-WZ-32
 ONE LANE TO REMAIN OPEN WITH USE OF TRAFFIC SIGNAL

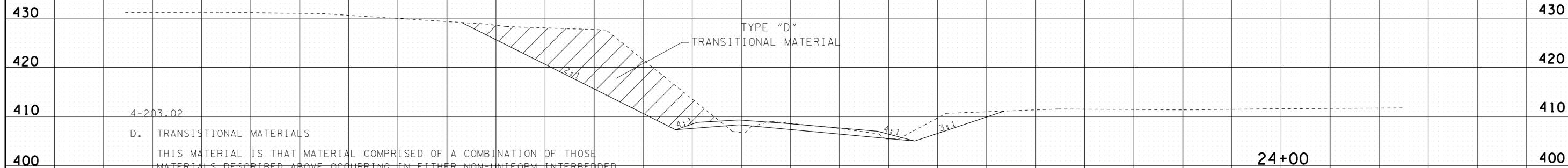
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TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	2015	BR-STP-128(23)	14

TENNESSEE D.O.T.
DESIGN DIVISION

FILE NO.

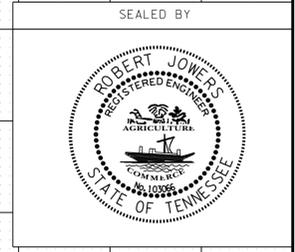
TYPICAL FOR STATION 21+50 TO STATION 25+37.84



4-203.02

D. TRANSITIONAL MATERIALS

THIS MATERIAL IS THAT MATERIAL COMPRISED OF A COMBINATION OF THOSE MATERIALS DESCRIBED ABOVE OCCURRING IN EITHER NON-UNIFORM INTERBEDDED LAYERS OF THE ABOVE MATERIALS (I.E., SHALE MATERIAL WITH RELATIVELY THIN LAYERS OF SOLID ROCK SUCH AS HARD LIMESTONE) OR ERRATIC LOCALIZED CHANGES OF MATERIAL TYPES BOTH Laterally AND WITH DEPTH (SUCH AS A GEOLOGIC FORMATION RESULTING IN PINNACLED ROCK COLUMNS, FLOATING BOULDERS OR LENSES INTERCALATED WITH CLAY SOIL, A COMMON OCCURRENCE IN CERTAIN REGIONS OF TENNESSEE). FOR CONSTRUCTION PURPOSES, THIS MATERIAL MAY HAVE TO BE EXCAVATED USING A COMBINATION OF EXCAVATION METHODS SUCH AS BLASTING OF ROCK PINNACLES, LAYERS OR BOULDERS ALONG WITH RIPPING OF WEATHERED ROCK AND EXCAVATING OF SOIL WITH TRACK HOES OR LOADERS ALL WITHIN A LOCALIZED AREA. THIS MATERIAL WOULD NOT BE SUITABLE FOR THE USE OF EXCAVATING PAN TYPE EQUIPMENT.

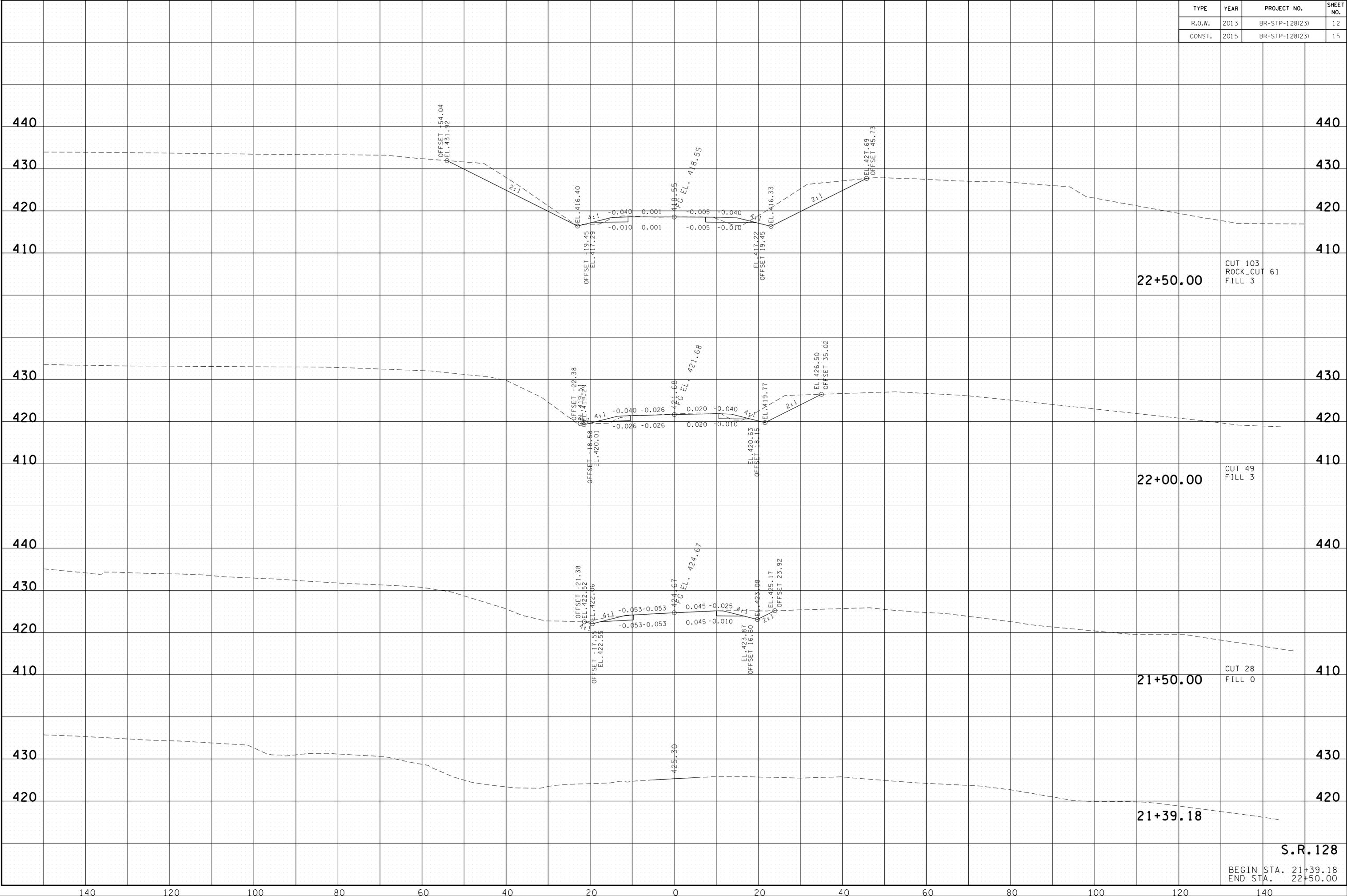


SOILS
STATE ROUTE 128
PERRY COUNTY

1/30/2015 2:53:30 PM N:\GeoTech\Files\Region 3\68-Perry\FY202\681752\cadd files\681752.dgn

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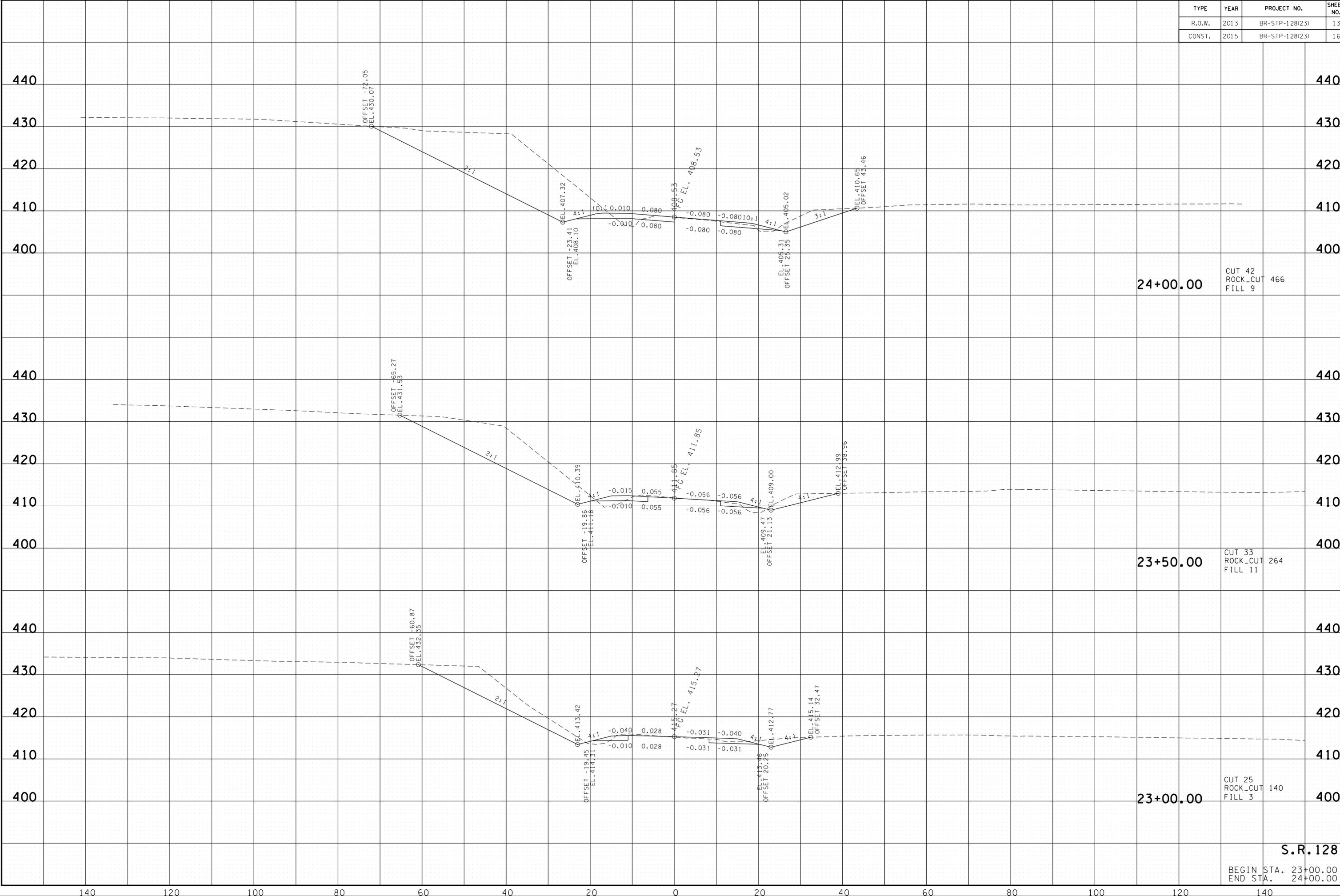
TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2013	BR-STP-128(23)	12
CONST.	2015	BR-STP-128(23)	15



30-JAN-2015 10:18
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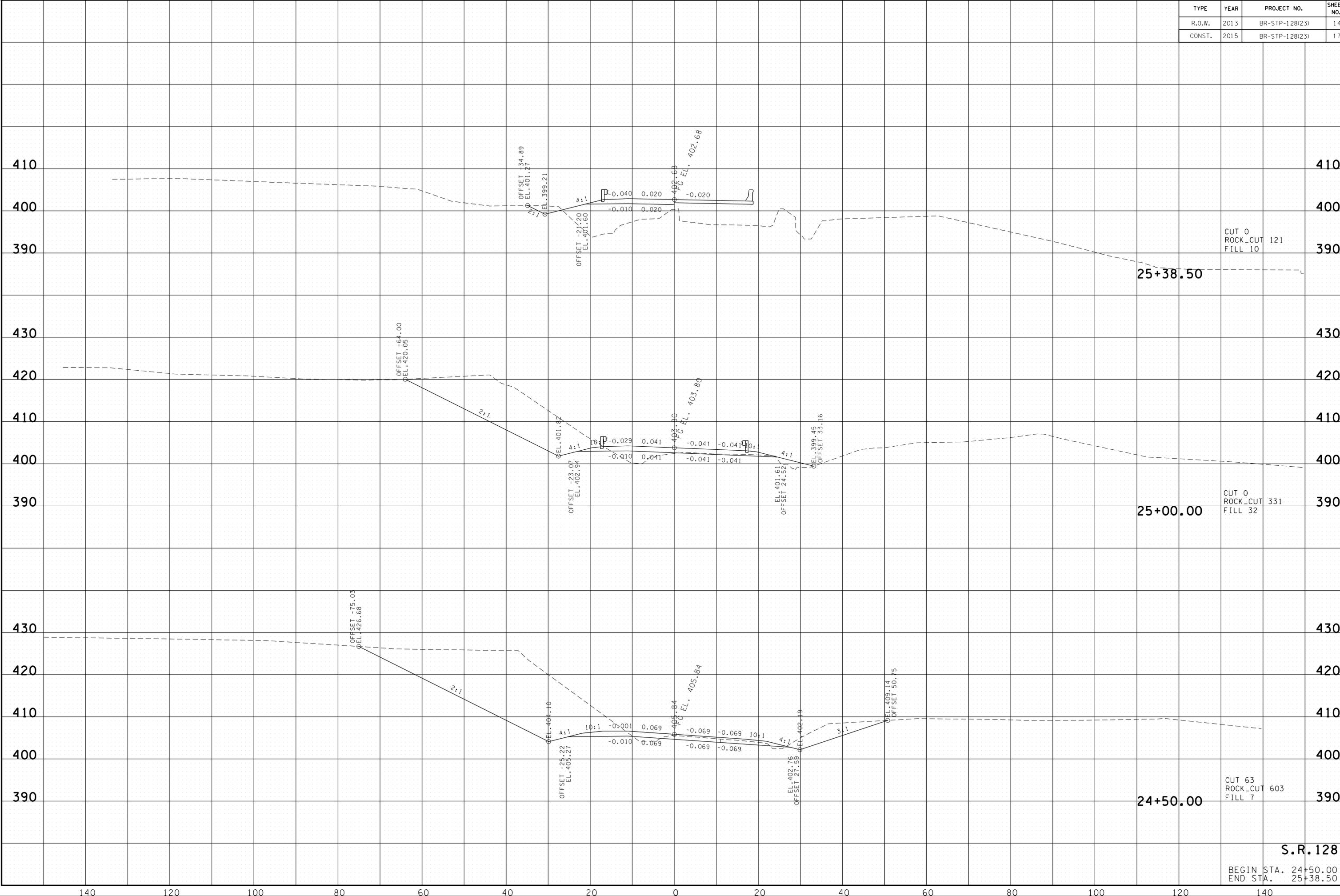
S.R. 128
 BEGIN STA. 21+39.18
 END STA. 22+50.00

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2013	BR-STP-128(23)	13
CONST.	2015	BR-STP-128(23)	16



S.R. 128
 BEGIN STA. 23+00.00
 END STA. 24+00.00

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2013	BR-STP-128(23)	14
CONST.	2015	BR-STP-128(23)	17

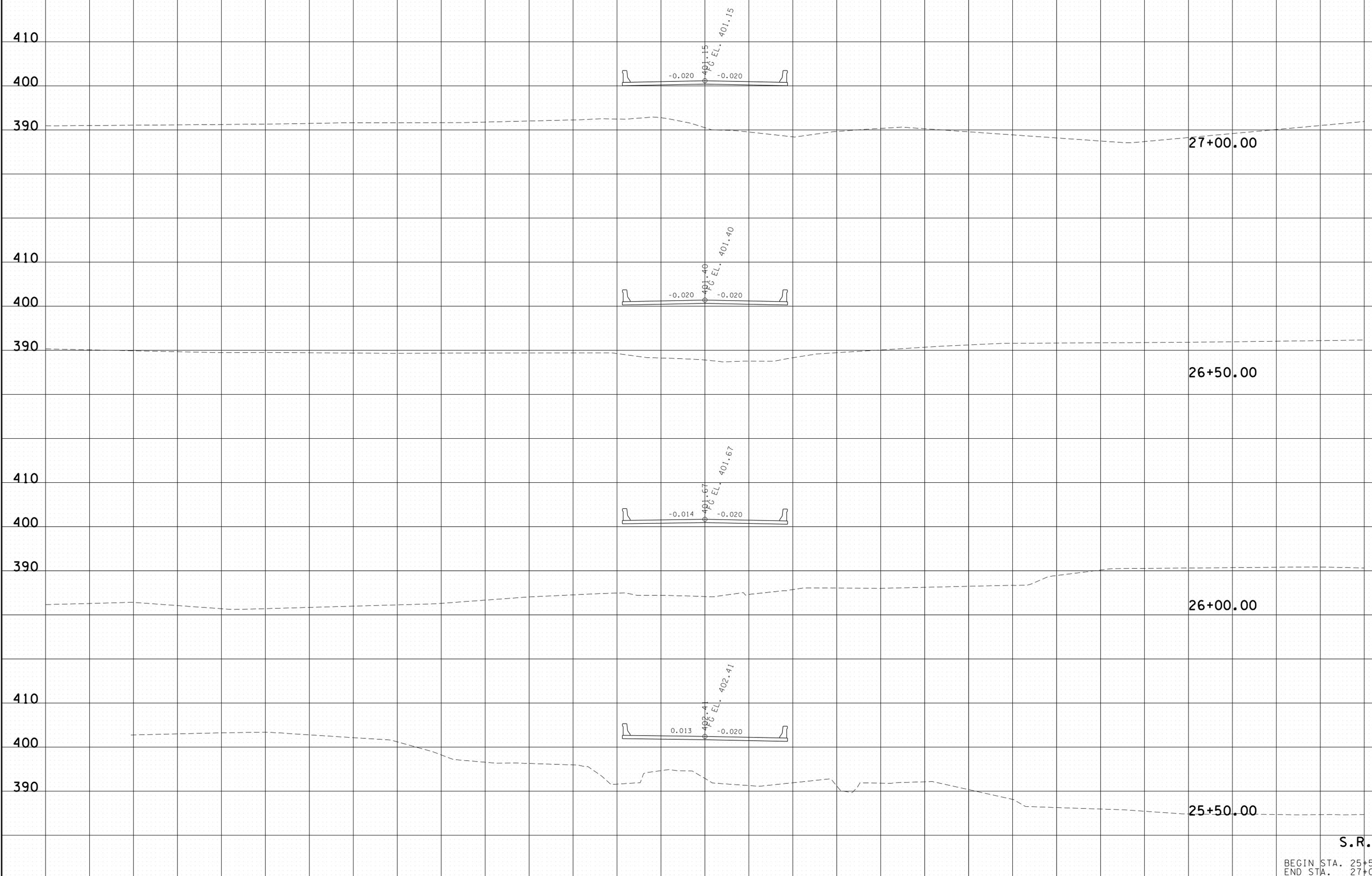


S.R. 128

BEGIN STA. 24+50.00
END STA. 25+38.50

30-JAN-2015 10:18 C:\Projects\Perry Co.\S.R. 128 Working Files\PESR28Mainline\XSheets.sht

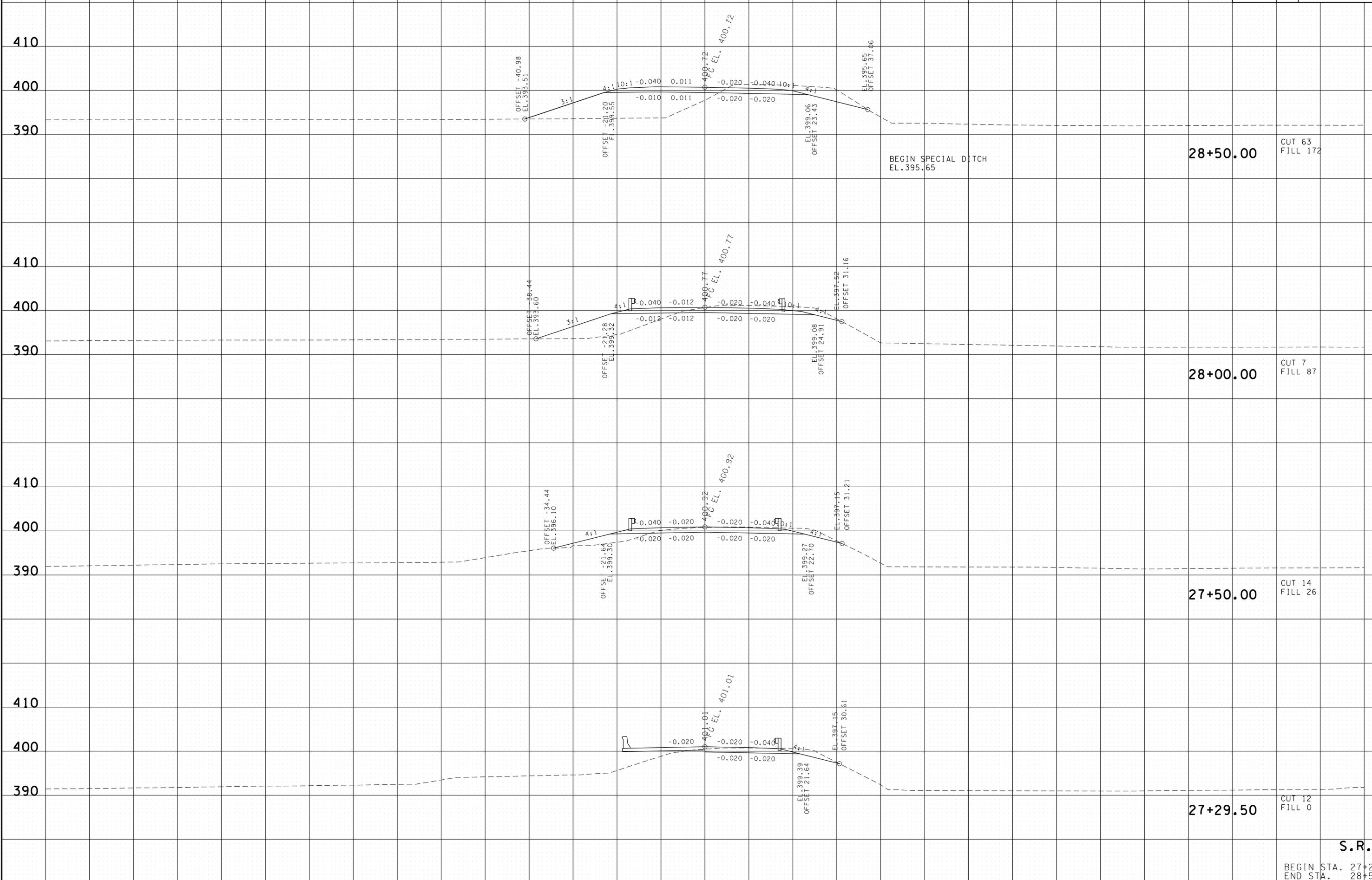
TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2013	BR-STP-128(23)	15
CONST.	2015	BR-STP-128(23)	18



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 30-JAN-2015 10:18

S.R. 128
 BEGIN STA. 25+50.00
 END STA. 27+00.00

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2013	BR-STP-128(23)	16
CONST.	2015	BR-STP-128(23)	19

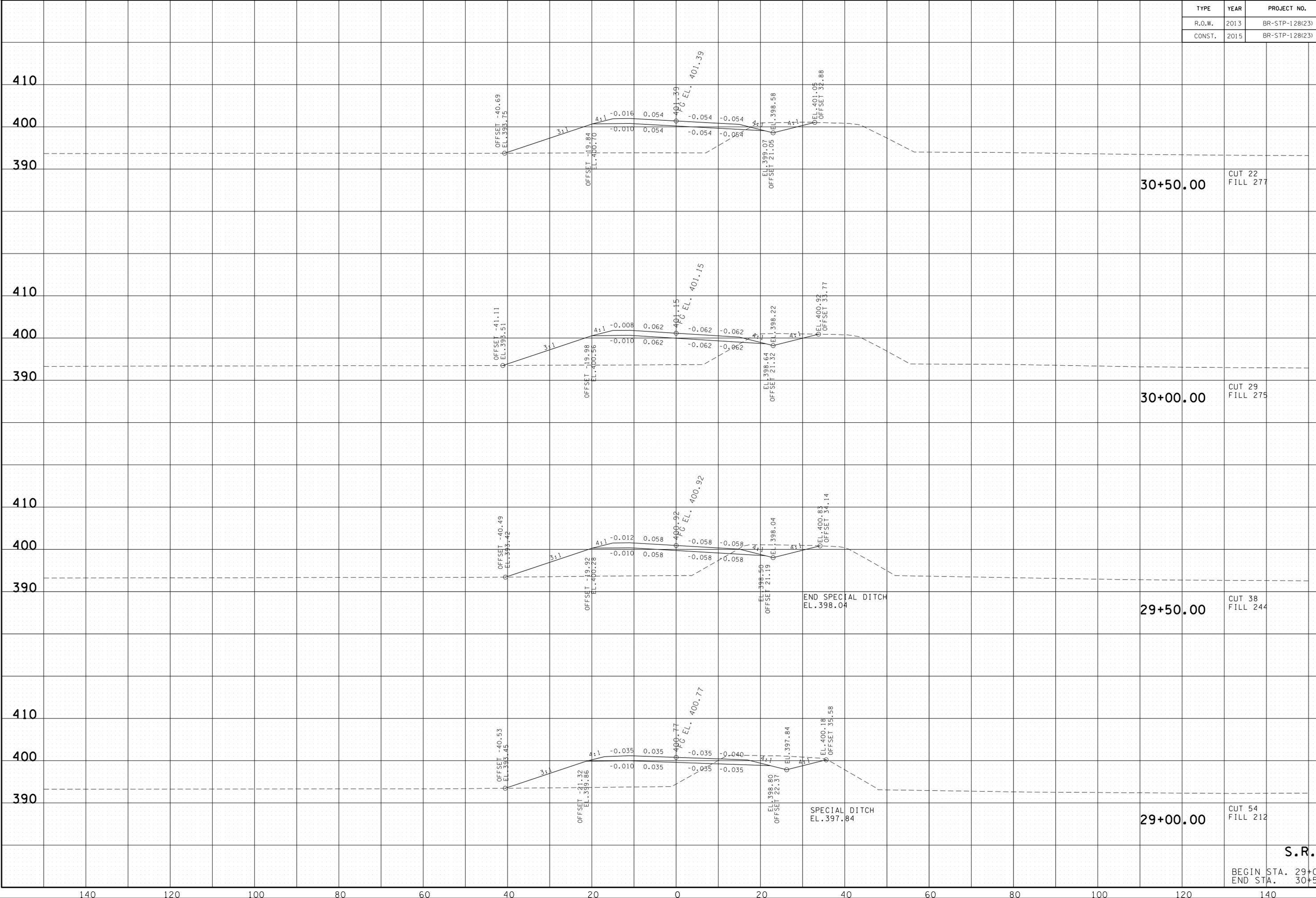


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S.R. 128
 BEGIN STA. 27+29.50
 END STA. 28+50.00

140 120 100 80 60 40 20 0 20 40 60 80 100 120 140

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2013	BR-STP-128(23)	17
CONST.	2015	BR-STP-128(23)	20



30+50.00

CUT 22
FILL 277

30+00.00

CUT 29
FILL 275

29+50.00

CUT 38
FILL 244

29+00.00

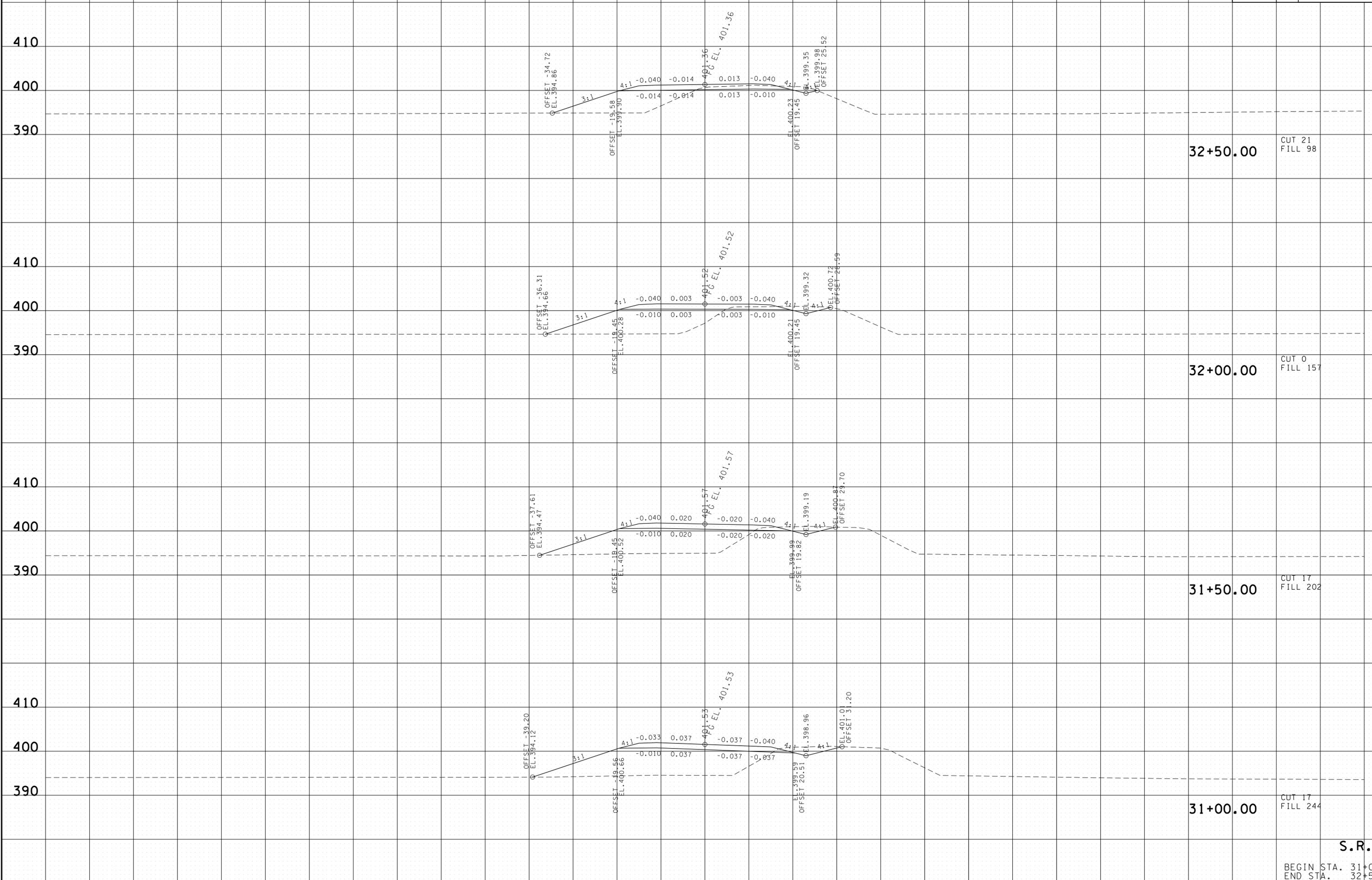
CUT 54
FILL 212

S.R. 128

BEGIN STA. 29+00.00
END STA. 30+50.00

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TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2013	BR-STP-128(23)	18
CONST.	2015	BR-STP-128(23)	21

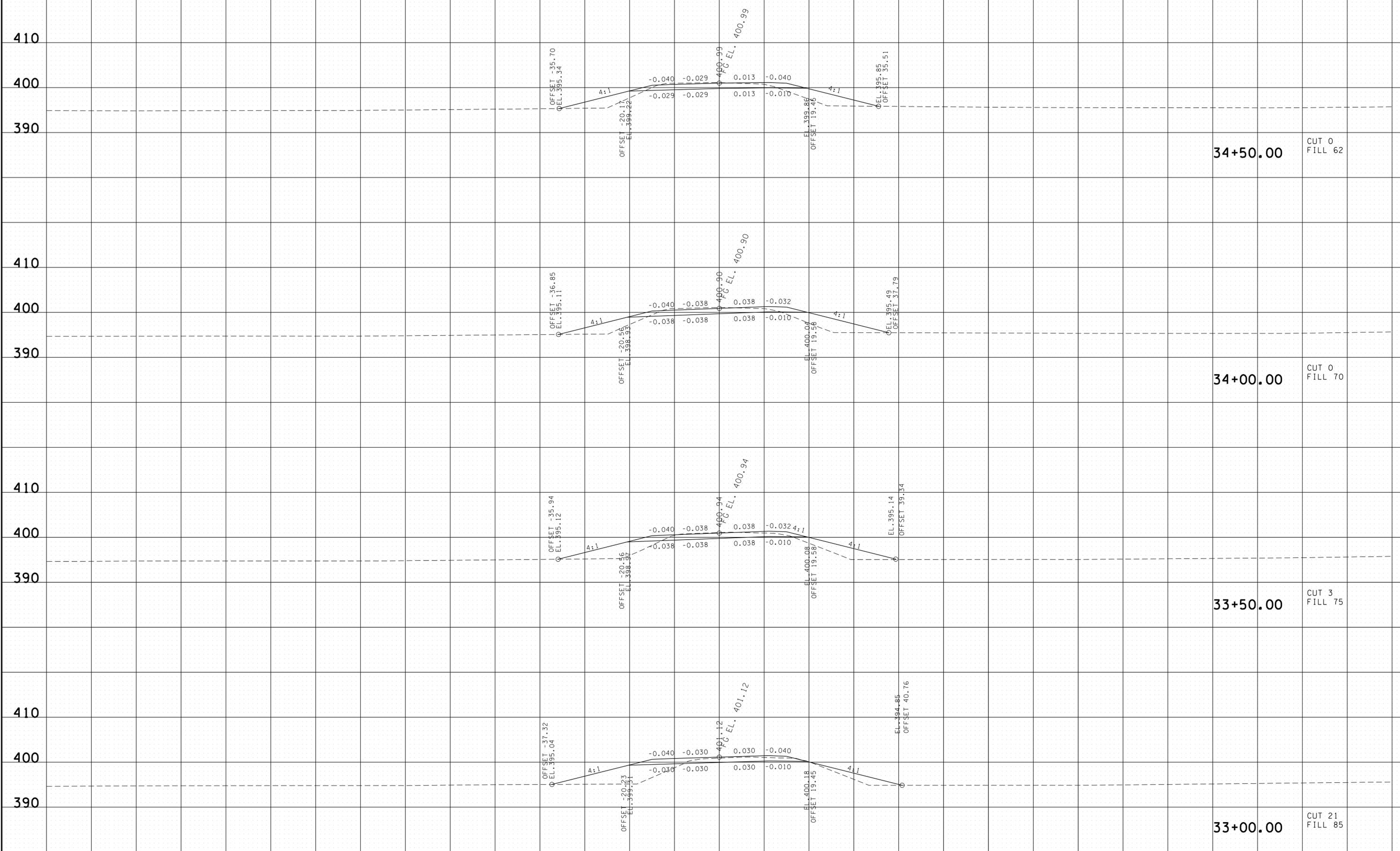


S.R. 128
 BEGIN STA. 31+00.00
 END STA. 32+50.00

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TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2013	BR-STP-128(23)	19
CONST.	2015	BR-STP-128(23)	22



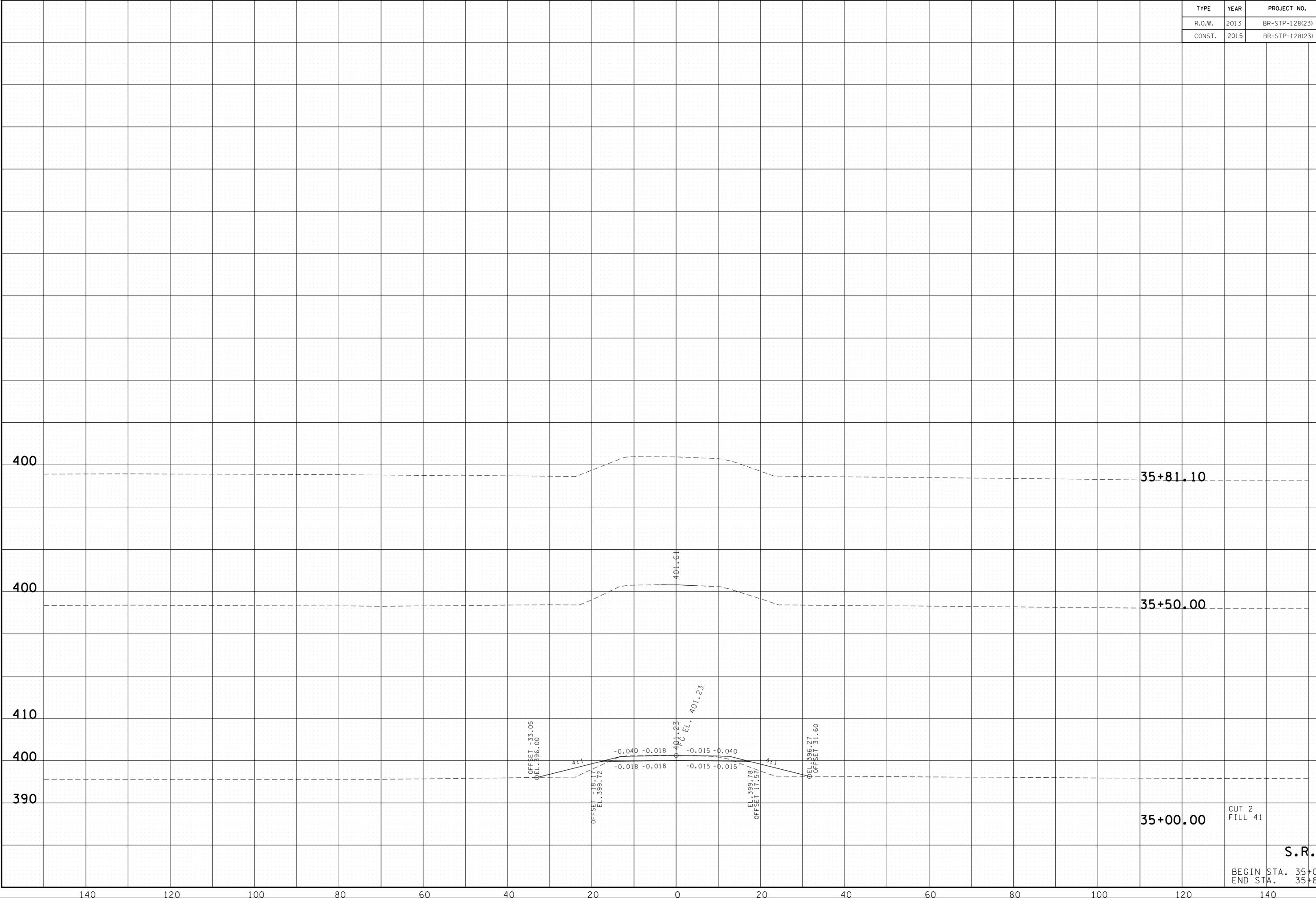
S.R. 128

BEGIN STA. 33+00.00
END STA. 34+50.00

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140 120 100 80 60 40 20 0 20 40 60 80 100 120 140

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2013	BR-STP-128(23)	20
CONST.	2015	BR-STP-128(23)	23



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S.R. 128
 BEGIN STA. 35+00.00
 END STA. 35+81.10

CUT 2
 FILL 41