

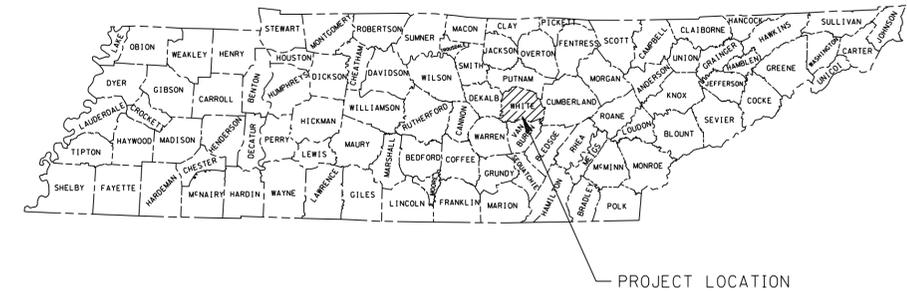
# Index Of Sheets

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2B	TRAFFIC CONTROL PLAN
2C	EROSION CONTROL PLAN

# STATE OF TENNESSEE DEPARTMENT OF TRANSPORTATION BUREAU OF ENGINEERING

## WHITE COUNTY STATE ROUTE 84 OVER CALFKILLER RIVER BRIDGE REPAIR

TENN.	YEAR 2015	SHEET NO. 1
FED. AID PROJ. NO.		
STATE PROJ. NO.		93004-4222-04



DRAWING NO.	CURRENT REVISION DATE	DESCRIPTION
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STATE ROUTE 84 OVER CALFKILLER RIVER  
BRIDGE NO. 93-SR084-01.58  
STATE HIGHWAY NO. F.A.H.S. NO. NA

### STANDARD ROADWAY DRAWINGS

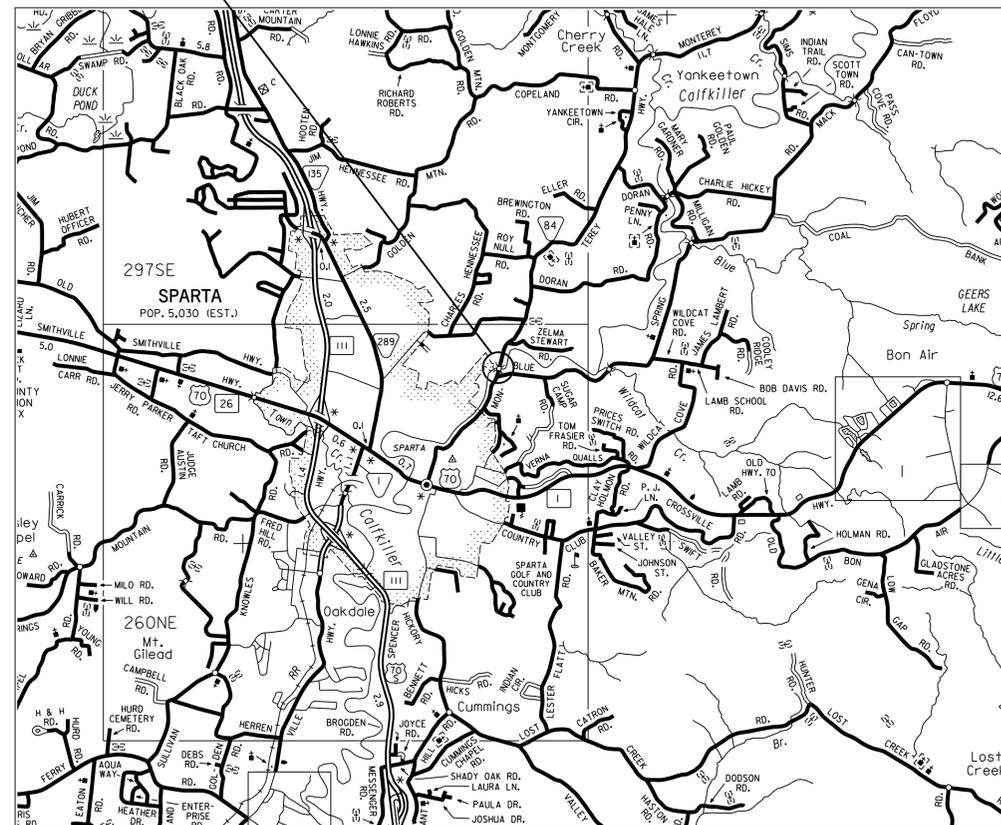
RD-A-1	12-18-99	STANDARD ABBREVIATIONS
RD-L-1	10-26-94	STANDARD LEGEND

### TRAFFIC CONTROL APPURTENANCES

T-M-1	07-24-14	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS AND MARKING ABBREVIATIONS
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T-PBR-2	11-01-11	WOODEN POLE DETAILS FOR SPAN MOUNTED SIGNALS
T-SG-1	11-01-11	LOOP LEAD-INS CONDUIT AND PULL BOXES
T-SG-2	7-29-04	STANDARD NOTES AND DETAILS OF INDUCTIVE LOOPS
T-SG-3	11-11-04	ALTERNATE DETECTION DETAILS
T-SG-3A		SPAN WIRE AND MESSENGER CABLE DETAILS
T-SG-4		CONTROLLER CABINET DETAILS
T-SG-5	12-04-13	SIGNAL HEAD ASSEMBLIES AND PEDESTRIAN PUSH BUTTON DETAILS
T-SG-7A	11-01-11	TYPICAL SIGN HEAD PLACEMENT
T-WZ-10	4-02-12	ADVANCE ROAD WORK SIGNING ON HIGHWAYS AND FREEWAYS
T-WZ-32	10-29-13	TRAFFIC CONTROL PLAN SIGNAL LAYOUT FOR TRAFFIC SIGNAL AT TWO LANE BRIDGE RECONSTRUCTION SITE
T-WZ-34	9-01-05	TRAFFIC CONTROL PLAN GENERAL NOTES FOR TRAFFIC SIGNAL AT TWO LANE BRIDGE RECONSTRUCTION SITE
T-WZ-35	4-02-12	TRAFFIC CONTROL PLAN PAY ITEM AND SIGN DETAILS FOR TRAFFIC SIGNAL AT TWO LANE BRIDGE RECONSTRUCTION SITE

### EROSION PREVENTION & SEDIMENT CONTROL

EC-STR-2	8-01-12	SEDIMENT FILTER BAGS
EC-STR-3C	8-01-12	SILT FENCE WITH WIRE BACKING
EC-STR-3E	4-01-08	SILT FENCE FABRIC JOINING DETAILS



NOT TO SCALE

ROUTE	ADT	YEAR	POSTED SPEED LIMIT
STATE ROUTE 84	1390	2015	55 MPH

### LIST OF BRIDGE DRAWINGS

DWG. NO.	REV.	DESCRIPTION
BR-119-89		LAYOUT OF BRIDGE TO BE REPAIRED
BR-119-90		BRIDGE ESTIMATED QUANTITIES AND GENERAL NOTES
BR-119-91		PHASED CONSTRUCTION
BR-119-92		GIRDER REPAIR
BR-119-93		REPAIR DETAILS
BR-119-94		EMBANKMENT REPAIRS

### LIST OF REFERENCE DRAWINGS

(TO BE PRINTED WITH PLANS)

DWG. NO.	DESCRIPTION
M-37-127	LAYOUT OF BRIDGE (ALT. P)
M-37-130	SUPERSTRUCTURE (ALT. P)
M-37-132	STRUCTURE DETAILS (ALT. P)
M-37-134	ABUTMENTS NO. 1 & 2 (ALT. P)
M-37-135	PIERS DETAILS (ALT. P)

### SPECIAL NOTES

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED JANUARY 1, 2015 AND ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS AND IN THE PROPOSAL CONTRACT.

TDOT TRANSPORTATION PROJECT SPECIALIST VALERIE NEWBERRY  
 DESIGNER TRCWW, INC. CHECKED BY W. PETRONE  
 P.E. NO. 93004-4222-04  
 PIN NO. 121364.00

APPROVED: Paul D. Degges  
PAUL D. DEGGES, CHIEF ENGINEER

DATE \_\_\_\_\_

APPROVED: John Schroer  
JOHN SCHROER, COMMISSIONER

**UNOFFICIAL SET**  
NOT FOR BIDDING

U.S. DEPARTMENT OF TRANSPORTATION  
FEDERAL HIGHWAY ADMINISTRATION

APPROVED: \_\_\_\_\_  
DIVISION ADMINISTRATOR DATE \_\_\_\_\_

### PROJECT COMMITMENTS

COMMITMENT ID	SOURCE DIVISION	DESCRIPTION	STA./LOCATION
EDHZ001	ENVIRONMENTAL DIVISION, HAZARDOUS MATERIALS	BRIDGE NO. 93SR0840001, SR-84 OVER CALFKILLER RIVER, LM 1.58 HAS ACM IN THE CREAM AND RED COATINGS. ALL MATERIAL OF THIS NATURE SHOULD BE TREATED AS ASBESTOS-CONTAINING. ABATEMENT OF THIS MATERIAL SHOULD BE ACCOMPLISHED PER SP202ACM SPECIAL PROVISION REGARDING REMOVAL OF ASBESTOS-CONTAINING MATERIALS. ACM ABATEMENT SHOULD BE COMPLETED PRIOR TO ANY REPAIR ACTIVITIES THAT WILL DISTURB THESE COATINGS.  STATE OF TENNESSEE ASBESTOS ACCREDITATION REQUIREMENTS (TCA 1200-01-20) MANDATE THAT ACM ABATEMENT WORK BE PERFORMED BY AN ACCREDITED FIRM (CONTRACTOR) USING ACCREDITED ABATEMENT WORKERS AND SUPERVISORS.  THE CONTRACTOR SHALL BE RESPONSIBLE FOR SUBMITTING A NOTICE TO THE TDEC DIVISION OF AIR POLLUTION CONTROL (10) DAYS IN ADVANCE OF ANY ACM ABATEMENT OR DEMOLITION.	

**UNOFFICIAL  
SET**

NOT FOR  
BIDDING

*17-15*



ESTIMATED ROADWAY QUANTITIES

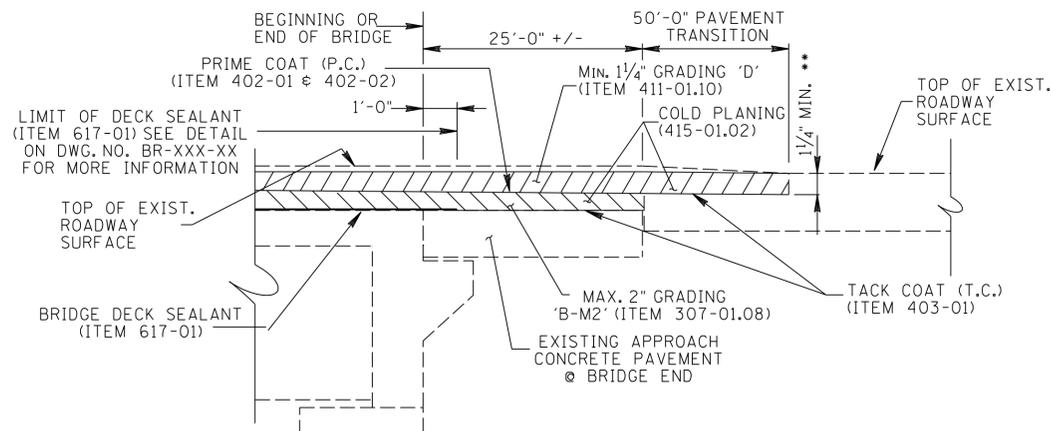
TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	2015	93004-4222-04	2A

TENNESSEE D.O.T.  
DESIGN DIVISION  
FILE NO.

ITEM NO.	DESCRIPTION	UNIT	QUANTITY
②	209-05 SEDIMENT REMOVAL	CY	7
①	209-08.02 TEMPORARY SILT FENCE (WITH BACKING)	LF	190
	209-09.04 SEDIMENT FILTER BAG (15'X10')	EA.	1
③②	303-10.01 MINERAL AGGREGATE (SIZE 57)	TON	14
	307-01.08 ASPHALT CONCRETE MIX (PG64-22) (BPMB-HM) GRADING B-M2	TON	138
	402-01 BITUMINOUS MATERIAL FOR PRIME COAT (PC)	TON	3
	402-02 AGGREGATE FOR COVER MATERIAL (PC)	TON	9
	403-01 BITUMINOUS MATERIAL FOR TACK COAT (TC)	TON	1
	411-01.10 ASPHALT CONCRETE MIX (PG64-22) GRADING D	TON	111
	415-01.02 COLD PLANING OF BITUMINOUS PAVEMENT	SY	667
	705-04.50 PORTABLE BARRIER RAIL DELINEATOR	EA.	16
④	705-08.51 PORTABLE IMPACT ATTENUATOR NCHRP350 TL-3	EA.	2
⑤	712-01 TRAFFIC CONTROL	LS	1
	712-02.02 INTERCONNECTED PORTABLE BARRIER RAIL	LF	464
	712-04.01 FLEXIBLE DRUMS (CHANNELIZING)	EA.	13
	712-05.01 WARNING LIGHTS (TYPE A)	EA.	13
	712-06 SIGNS (CONSTRUCTION)	SF	459
	712-07.03 TEMPORARY BARRICADES (TYPE III)	LF	12
	712-09.01 REMOVABLE PAVEMENT MARKING LINE	LF	3728
	712-09.04 REMOVABLE PAVEMENT MARKING LINE (STOP LINE)	LF	48
	716-05.01 PAINTED PAVEMENT MARKING (4" LINE)	LM	1.00
⑥	716-08.20 REMOVAL OF PAVEMENT MARKINGS (LINE)	LM	1.41
⑦	716-13.06 SPRAY THERMO PAVEMENT MARKING (40 MIL) (4 IN LINE)	LM	.3
	717-01 MOBILIZATION	LS	1
	730-40 TEMPORARY TRAFFIC SIGNAL SYSTEM	EA.	1
③②	740-10.03 GEOTEXTILE (TYPE III) (EROSION CONTROL)	SY	40

FOOTNOTES

- ① INCLUDES THE COST OF ALL LABOR AND MATERAILS FOR FURNISHING AND INSTALLING THE TEMPORARY FILTER BARRIER WHERE LOCATED BY THE ENGINEER AND REMOVAL UPON PROJECT COMPLETION. SEE STD. DWG. EC-STR-3C. THE ENGINEER MAY INCREASE OR DECREASE OR ELIMINATE THE QUANTITY FOR THIS ITEM.
- ② ITEM MAY BE INCREASED OR DECREASED AS DIRECTED BY THE ENGINEER.
- ③ ITEM USED IN INSTALLATION OF SEDIMENT FILTER BAG, STD. DRAWING EC-STR-2
- ④ THIS ITEM SHALL BE A PORTABLE ENERGY ABSORBING TERMINAL MEETING THE REQUIREMENTS OF NCHRP 350 FOR TEST LEVEL 3. EXAMPLES WOULD BE A QUAD-GUARD, A REACT 350 OR A TRACC. THE PAY ITEM WILL INCLUDE FURNISHING AND INSTALLING ALL COMPONENTS AS SHOWN ON THE MANUFACTURER'S DRAWING.
- ⑤ THIS PAY ITEM WILL INCLUDE THE COST OF RELOCATION FOR THE TRAFFIC CONTROL PHASES.
- ⑥ THIS ITEM SHALL BE USED FOR PHASE I AND II TRAFFIC CONTROL TEMPORARY PAINTED LINES (4 IN.). SEE TRAFFIC CONTROL PLAN ON SHEET NO. 2C FOR LOCATION AND DESCRIPTION.
- ⑦ THIS ITEM INCLUDES THE REMOVAL OF PHASE I AND II TRAFFIC CONTROL TEMPORARY PAINTED LINES AND PERTINENT EXISTING PAINTED LINES.
- ⑧ EXISTING REMOVED MATERIALS OF THIS ITEM SHALL BECOME PROPERTY OF CONTRACTOR AND DISPOSED OF PROPERLY.



PAVEMENT TRANSITION DETAIL

NOTES:

PAVEMENT AT EACH END OF BRIDGE TO BE REPAIRED AFTER COMPLETION OF BRIDGE REPAIRS TO PROVIDE A SMOOTH RIDING SURFACE FROM EXISTING ASPHALT TO NEW CONCRETE DECK.

\*\* DENOTES: 1" VERTICAL SAWCUT TO ENSURE SMOOTH EDGE AT END OF ASPHALT TRANSITION. SAWCUT TO BE INCLUDED IN BID ITEMS.

SEE SECTION 617 AND 906.04 OF THE TENNESSEE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.

REMOVAL OF 50' EXISTING ASPHALT TRANSITION AND 25' APPROACHES FOR FULL WIDTH OF PAVEMENT TO BE PAID FOR UNDER ITEM NO. 415-01.02.

ASPHALT TO BE SLOPED OR FEATHERED AS NECESSARY TO OBTAIN DESIRED SURFACE SLOPE.

PAVEMENT EDGE DROP-OFF TRAFFIC CONTROL NOTES

A. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES OR TRAFFIC LANE AND SHOULDER WHERE THE TRAFFIC LANE IS BEING USED BY TRAFFIC, CAUSED BY BASE, PAVING OR RESURFACING:

1. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 0.75 INCH AND NOT EXCEEDING 2 INCHES:

a. WARNING SIGNS, UNEVEN PAVEMENT (W8-11) AND/OR SHOULDER DROP-OFF (W8-9A), SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

b. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY ADDED PAVEMENT SHALL BE ELIMINATED WITHIN THREE WORKDAYS.

c. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY COLD PLANING SHALL BE ELIMINATED WITHIN THREE WORKDAYS.

d. WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE TRAFFIC LANE BEING UTILIZED BY TRAFFIC AND SHOULDER THE DIFFERENCE IN ELEVATION SHALL BE ELIMINATED WITHIN SEVEN WORKDAYS AFTER THE CONDITION IS CREATED.

2. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 2 INCHES AND NOT EXCEEDING 6 INCHES. TRAFFIC IS NOT TO BE ALLOWED TO TRAVERSE THIS DIFFERENCE IN ELEVATION.

a. SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

(1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

(2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

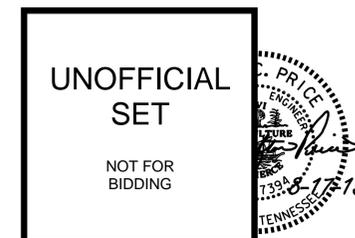
b. IF THE DIFFERENCE IN ELEVATION IS ELIMINATED OR DECREASED TO 2 INCHES OR LESS BY THE END OF EACH WORKDAY, CONES MAY BE USED DURING DAYLIGHT HOURS IN LIEU OF DRUMS, BARRICADES OR OTHER APPROVED PROTECTIVE DEVICES MENTIONED IN PARAGRAPH a, PROVIDED WARNING SIGNS ARE ERECTED. WARNING SIGNS (UNEVEN PAVEMENT AND/OR LOW SHOULDER) SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

c. WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE THROUGH TRAFFIC LANE AND THE SHOULDER AND THE ELEVATION DIFFERENCE IS LESS THAN 3.5 INCHES, THE CONTRACTOR MAY USE WARNING SIGNS AND/OR PROTECTIVE DEVICES AS APPLICABLE AND APPROVED BY THE ENGINEER. SEE PARAGRAPH a REGARDING USE OF DRUMS, BARRICADES OR OTHER APPROVED PROTECTIVE DEVICES. WARNING SIGNS (UNEVEN PAVEMENT AND/OR LOW SHOULDER) WILL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY. IN THESE SITUATIONS, THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 2 MILES IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

B. THE DIFFERENCE IN ELEVATION IS FARTHER THAN 8 FEET FROM THE NEAREST TRAFFIC LANE BUT NOT MORE THAN 30 FEET FROM THE NEAREST TRAFFIC LANE; SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

1. WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

2. WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER. THE CONTRACTOR SHALL SCHEDULE THE WORK SO AS TO MINIMIZE THE TIME TRAFFIC IS EXPOSED TO AN ELEVATION DIFFERENCE. ONCE THE CONTRACTOR BEGINS AN ACTIVITY THAT CREATES AN ELEVATION DIFFERENCE, THE ACTIVITY SHALL BE PURSUED AS A CONTINUOUS OPERATION UNTIL THE ELEVATION DIFFERENCE IS ELIMINATED.

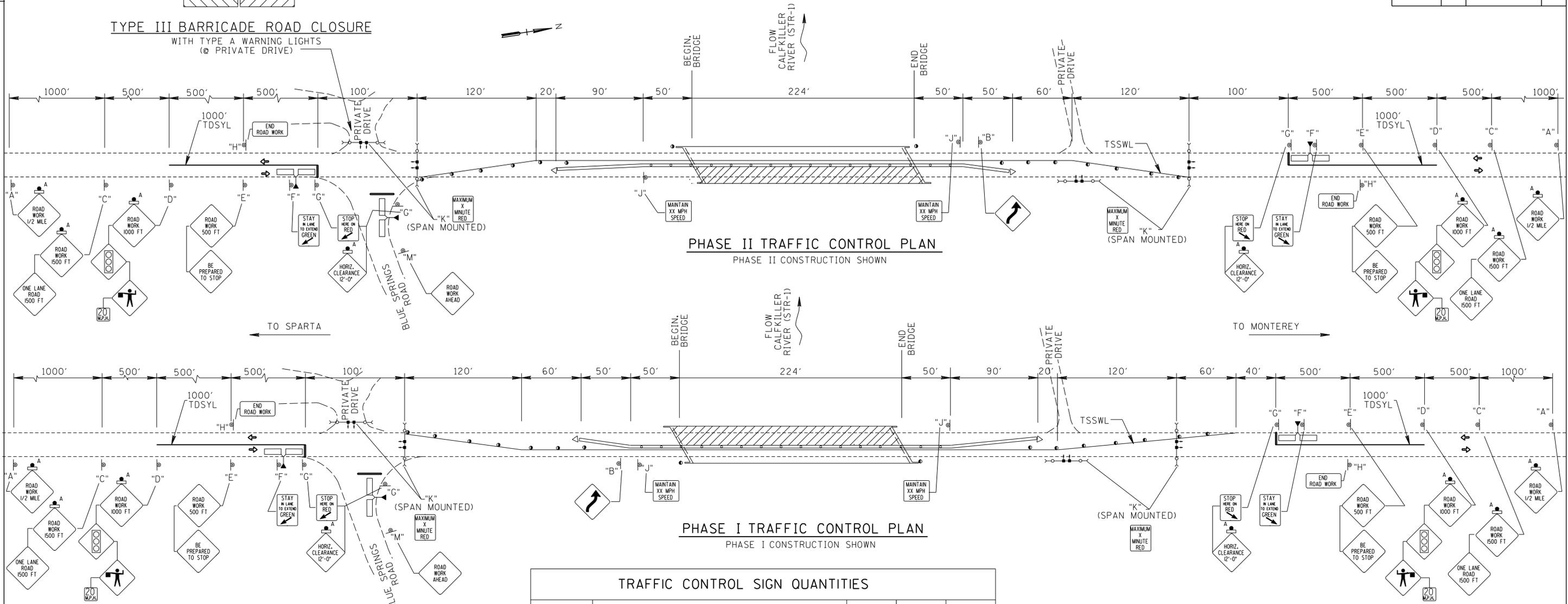
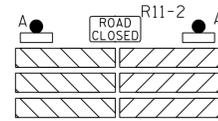


STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

ESTIMATED ROADWAY  
QUANTITIES  
AND  
GENERAL NOTES

BR. NO. 93-SR84-01.58

**TYPE III BARRICADE ROAD CLOSURE**  
WITH TYPE A WARNING LIGHTS  
(@ PRIVATE DRIVE)



**PHASE II TRAFFIC CONTROL PLAN**  
PHASE II CONSTRUCTION SHOWN

**PHASE I TRAFFIC CONTROL PLAN**  
PHASE I CONSTRUCTION SHOWN

**TRAFFIC CONTROL NOTES**

ADVANCE WARNING SIGNS TO BE PLACED PRIOR TO CONSTRUCTION AND REMAIN IN PLACE UNTIL THE COMPLETION OF THIS PROJECT.

THE TRAFFIC CONTROL PLAN DOES NOT RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY OF INSTALLING TRAFFIC CONTROL DEVICES IN ACCORDANCE WITH THE CURRENT EDITION OF THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES".

TRAFFIC SIGNALS SHALL BE PLACED IN FULL VIEW OF ALL PRIVATE DRIVES.

TYPE "A" WARNING LIGHTS SHALL BE PLACED ON DESIGNATED ADVANCED WARNING SIGNS.

IF ADDITIONAL SIGNS ARE DEEMED NECESSARY BY THE ENGINEER, THEY SHALL BE FURNISHED AND INSTALLED AT THE UNIT PRICE BID, ITEM NO. 712-06, SIGNS (CONSTRUCTION), S.F.

FOR ADDITIONAL DETAILS AND NOTES SEE STD. DWGS. T-WZ-32, T-WZ-34, AND T-WZ-35.

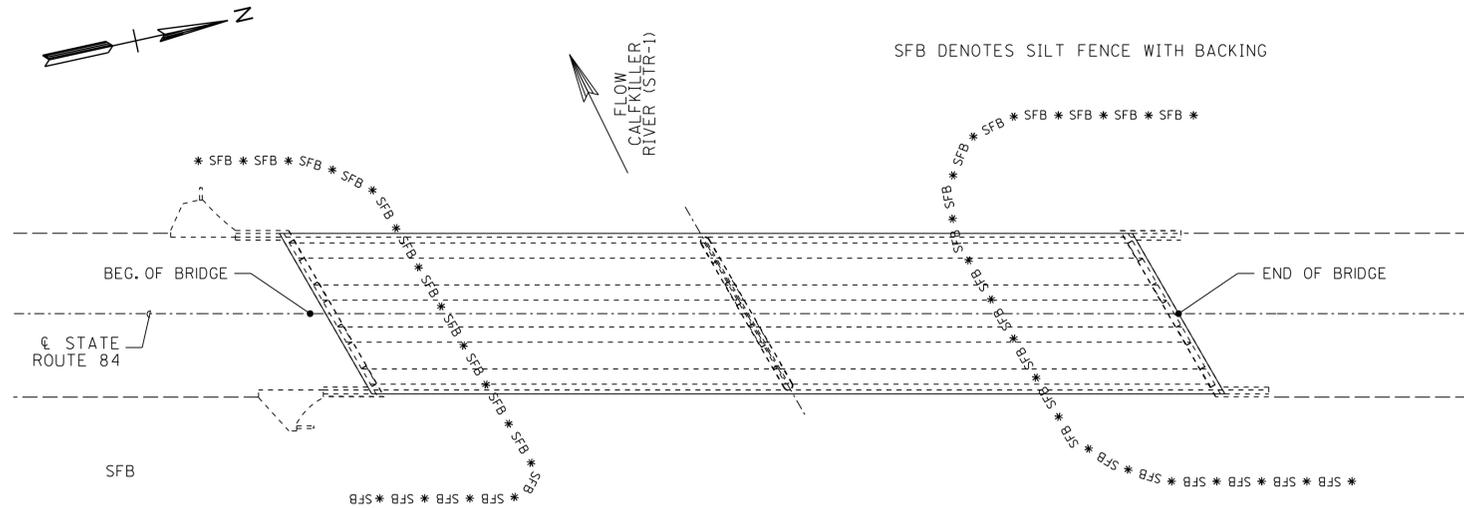
TRAFFIC CONTROL SIGN QUANTITIES				
TYPE	DESCRIPTION	SIZE	EACH	S.F.
① W20-1A	ROAD WORK 1/2 MILE (LOCATION "A")	48"X48"	2	32
① W20-1B	ROAD WORK 1500 FT. (LOCATION "C")	48"X48"	2	32
① W20-1C	ROAD WORK 1000 FT. (LOCATION "D")	48"X48"	2	32
W20-1D	ROAD WORK 500 FT. (LOCATION "E")	48"X48"	2	32
W20-1E	ROAD WORK AHEAD (LOCATION "M")	48"X48"	1	16
W20-4B	ONE LANE ROAD 1500 FT. (LOCATION "C")	48"X48"	2	32
W3-3	SIGNAL HEAD (SYMBOL) (LOCATION "D")	36"X36"	2	18
W20-7A	FLAGGER AHEAD (SYMBOL) (LOCATION "D")	36"X36"	2	18
W20-7B	BE PREPARED TO STOP (LOCATION "E")	48"X48"	2	32
W13-2	SUPPLEMENTAL PLATE (20MPH) (LOCATION "D")	24"X18"	2	6
R10-6	STOP HERE ON RED (LOCATION "G")	24"X36"	3	18
R10-6(MOD)	STAY IN LANE TO EXTEND GREEN (LOCATION "F")	30"X42"	2	17.5
R11-2	ROAD CLOSED (LOCATION "M")	12"X24"	1	2
G20-2A	END ROAD WORK (LOCATION "H")	36"X18"	2	9
SPECIAL	MAINTAIN 20 M.P.H. SPEED (LOCATION "J")	42"X36"	2	21
SPECIAL	MAXIMUM 3 MINUTE RED (LOCATION "K")	42"X48"	4	56
SPECIAL	ONE LANE BRIDGE 2 MILES HORIZ. CL. 12'-0"	48"X78"	2	52
SPECIAL	HORIZ. CLEARANCE 12'-0" (LOCATION "G")	36"X36"	3	27
W1-4aR	LANE SHIFT (SYMBOL) (LOCATION "B")	30"X30"	1	6.25
TOTAL	712-06 SIGNS (CONSTRUCTION)			459

- ① DENOTES SIGN WITH TYPE "A" WARNING LIGHTS"
- ② SIGN LOCATED ON STATE ROUTE 84 NORTH OF INTERSECTION WITH ELLER RD.
- ③ SIGN LOCATED ON STATE ROUTE 84 NORTH OF INTERSECTION WITH U.S. 70

TRAFFIC CONTROL LEGEND	
	FLEXIBLE DRUMS (CHANNELIZING) SPACED @ 30' +/- IN TAPERS AND 60' +/- IN TANGENTS
	PORTABLE BARRIER RAIL WITH RAIL DELINEATORS @ 20 +/-
	TRAFFIC SIGNAL SYSTEM
	TDSYL TEMPORARY DOUBLE SOLID YELLOW LINE
	TSSWL TEMPORARY SINGLE SOLID WHITE LINE
	REMOVABLE STOP LINE
	DETECTOR LOOP
	ATTENUATOR
	DENOTES : WORK ZONE
	DENOTES : DIRECTION OF TRAVEL
	DENOTES : TYPE "A" WARNING LIGHTS REQUIRED

**UNOFFICIAL SET**  
NOT FOR BIDDING

TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	2015	93004-4222-04	2C



PLAN  
SHOWING SILT FENCE LOCATION

**SPECIAL ENVIRONMENTAL NOTE**

DUE TO CONCERNS FOR THE ENDANGERED BLUEMASK DARTER (ETHEOSTOMA AKATULO) AND CUMBERLAND PIGTOE (PLEUROBEMA GIBBERUM) RIP RAP PLACEMENT WILL NOT OCCUR ABOVE NORMAL FLOW ELVATION; RIP RAP WILL BE DONE DURING A LOW FLOW PERIOD; PROPER BMP'S ARE IMPLEMENTED DURING THE BRIDGE REPAIR TO ENSURE WASTE MATERIALS ARE PROPERLY CONTAINED; AND PROJECT PLANS DO NOT CHANGE TO INCLUDED ANY INSTREAM WORK.

**CONSTRUCTION WORK ZONE & TRAFFIC CONTROL NOTES**

- ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE WEEK BEFORE NEEDED, IF THE SIGN FACE IS FULLY COVERED.
- IF THE CONTRACTOR MOVES OFF THE PROJECT, HE SHALL COVER OR REMOVE ALL UNNEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OF REMOVAL, COVERING, AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION) PER SQUARE FOOT.
- A LONG TERM BUT SPORADIC USE WARNING SIGN, SUCH AS FLAGGER SIGN, MAY REMAIN IN PLACE WHEN NOT REQUIRED PROVIDED THE SIGN FACE IS FULLY COVERED.
- TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.
- USE OF BARRICADES, PORTABLE BARRIER RAILS, VERTICAL PANELS, AND DRUMS SHALL BE LIMITED TO THE IMMEDIATE AREAS OF CONSTRUCTION WHERE A HAZARD IS PRESENT. THESE DEVICES SHALL NOT BE STORED ALONG THE ROADWAY WITHIN THIRTY (30) FEET OF THE EDGE OF THE TRAVELED WAY BEFORE OR AFTER USE UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL INCREASE TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. THESE DEVICES SHALL BE REMOVED FROM THE CONSTRUCTION WORK ZONE WHEN THE ENGINEER DETERMINES THEY ARE NO LONGER NEEDED. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.
- THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY, WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHEN THE LANE IS OPEN TO TRAFFIC UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO PARK WITHIN THIRTY (30) FEET OF A OPEN TRAFFIC LANE AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.
- ALL DETOUR AND CONSTRUCTION SIGNING SHALL BE IN STRICT ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

**MISCELLANEOUS NOTES**

- NOTHING IN THE GENERAL NOTES OR SPECIAL PROVISIONS SHALL RELIEVE THE CONTRACTOR FROM HIS RESPONSIBILITIES TOWARD THE SAFETY AND CONVENIENCE OF THE GENERAL PUBLIC AND THE RESIDENTS ALONG THE PROPOSED CONSTRUCTION AREA.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE OR LOSS OF EXISTING SIGNS DURING CONSTRUCTION. EXISTING SIGNS SHALL BE REMOVED AND REPLACED AS NECESSARY.

**GRADING NOTE**

- ANY AREA THAT IS DISTURBED OUTSIDE LIMITS OF CONSTRUCTION DURING THE LIFE OF THIS PROJECT SHALL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE.

**SPECIAL NOTES**

- THE CONTRACTOR SHALL NOT REMOVE, REWORK SHOULDERS OR FLATTEN SLOPES UNTIL THE APPROPRIATE WARNING DEVICES ARE INSTALLED.

**PAVEMENT MARKING NOTES**

- PERMANENT PAVEMENT LINE MARKING SHALL BE 4" SPRAY THERMOPLASTIC (40 MIL) INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-13.06, SPRAY THERMO PVMT MRKNG (40 MIL) (4 IN LINE), L.M.

**PAVING RESURFACING NOTES**

- THE CONTRACTOR SHALL BE REQUIRED TO COLD PLANE AND PAVE IN THE DIRECTION OF TRAFFIC.
- IN ALL CASES FOR THE LENGTH OF THE PAVEMENT TRANSITION, THE THICKNESS AND WIDTH OF THE RESURFACING AND ANY ADDITIONAL PAVEMENT MATERIALS SHALL BE AS DIRECTED BY THE TDOT ENGINEER.

**EROSION PREVENTION AND SEDIMENT CONTROL NOTES**

- CONTRACTOR SHALL USE EXTREME CARE AND TAKE ANY MEASURE NECESSARY TO INSURE THAT NO DEBRIS IS DROPPED INTO THE STREAM. THIS SHALL BE ACCOMPLISHED BY THE USE OF BASKETS, NETTING, WRAPPING, WORK PLATFORM, OR OTHER SIMILARLY EFFECTIVE MEANS. ANY DEBRIS WHICH IS ALLOWED TO DROP ON THE BANKS BELOW THE BRIDGE SHALL NOT BE ALLOWED TO ENTER THE STREAM AND SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR. COST OF REMOVING AND DISPOSING OF DEBRIS SHALL BE INCLUDED IN ITEMS BID ON.
- ANY WORK WITHIN THE STREAM CHANNEL AREA (E.G. FOR PIER FOOTING, RIP-RAP PLACEMENT, MULTI-BARREL CULVERT/BRIDGE CONSTRUCTION, ETC.) SHALL BE SEPARATED FROM FLOWING WATER OR EXPECTED FLOW PATH AND PERFORMED DURING LOW FLOW CONDITIONS. ALL ITEMS USED WITHIN THE STREAM CHANNEL AREA FOR DIVERSION OF FLOW (OR EXPECTED FLOW), UNLESS SPECIFIED IN PLANS, SHALL NOT BE PAID FOR DIRECTLY BUT SHALL BE INCLUDED IN THE COST OF OTHER ITEMS. THIS NOTE EXCLUDES ANY ITEMS SPECIFIED IN THE PLANS FOR THE TEMPORARY DIVERSION CHANNELS, EC-STR-31 AND TEMPORARY DIVERSION CULVERTS, EC-STR-32 FOR SINGLE BARREL CULVERT CONSTRUCTION.
- INSPECTION, REPAIR, AND MAINTENANCE OF EPSC MEASURES/STRUCTURES IS TO BE PERFORMED ON A REGULAR BASIS. SEDIMENT SHALL BE REMOVED FROM SEDIMENT CONTROL STRUCTURES WHEN THE DESIGN CAPACITY HAS BEEN REDUCED BY FIFTY PERCENT (50%). DURING SEDIMENT REMOVAL, THE CONTRACTOR SHALL TAKE CARE TO ENSURE THAT STRUCTURAL COMPONENTS OF EPSC MEASURES ARE NOT DAMAGED AND THUS MADE INEFFECTIVE. IF DAMAGE DOES OCCUR, THE CONTRACTOR SHALL REPAIR THE STRUCTURES AT THE CONTRACTOR'S OWN EXPENSE.
- SEDIMENT REMOVED FROM SEDIMENT CONTROL STRUCTURES SHALL BE PLACED AND BE TREATED IN A MANNER SO THAT THE SEDIMENT IS CONTAINED WITHIN THE PROJECT LIMITS AND DOES NOT MIGRATE INTO WATERS OF THE STATE/U.S. COST FOR THIS TREATMENT IS TO BE INCLUDED IN PRICE BID FOR ITEM NO. 209-05 SEDIMENT REMOVAL, C.Y.
- THE CONTRATOR SHALL INSTALL A RAIN GAUGE EVERY LINEAR MILE AT ALL SITES WHERE CLEARING, GRUBBING, EXCAVATION, GRADING, CUTTING OR FILLING IS BEING ACTIVELY PERFORMED, OR EXPOSED SOIL HAS NOT YET BEEN PERMANENTLY STABILIZED. IF THE PROJECT LENGTH IS LESS THAN ONE LINEAR MILE, ONE RAIN GAUGE SHALL BE INSTALLED AT THE CENTER OF THE PROJECT OR AS INDICATED BY THE TDOT EPSC INSPECTOR. THE CONTRACTOR SHALL ENSURE THAT EACH GAUGE IS MAINTAINED IN GOOD WORKING CONDITION. TDOT AND/OR THE CONTRACTOR SHALL RECORD DAILY PRECIPITATION AND FORECASTED PERCENTAGE OF PRECIPITATION IN DETAILED RECORDS OF RAINFALL EVENTS INCLUDING DATES, AMOUNTS OR RAINFALL PER GAUGE, THE ESTIMATED DURATION (OR STARTING AND ENDING TIMES), AND FORECASTED PERCENTAGE OF PRECIPITATION FOR THE PROJECT. THIS INFORMATION SHALL BE PROVIDED TO THE ENGINEER ON A MONTHLY BASIS. THE COST FOR THE RAIN GAUGES IS TO BE INCLUDED IN THE UNIT BID PRICES FOR OTHER ITEMS. RAIN GAUGES SHALL BE AS SPECIFIED IN THE APPROVED TDOT RAINFALL MONITORING PLAN.
- INSPECTION OF EPSC MEASURES SHALL BE DONE AT LEAST TWICE PER CALENDAR WEEK AT LEAST 72 HOURS APART. A CALENDAR WEEK IS DEFINED AS SUNDAY THROUGH SATURDAY.
- OUTFALL POINTS SHALL BE INSPECTED TO ASCERTAIN WHETHER EPSC MEASURES ARE EFFECTIVE IN PREVENTING SIGNIFICANT IMPACTS TO SURROUNDING WATERS. WHERE DISCHARGE LOCATIONS ARE INACCESSIBLE, NEARBY DOWNSTREAM LOCATIONS SHALL BE INSPECTED. LOCATIONS WHERE VEHICLES ENTER AND EXIT THE SITE SHALL BE INSPECTED FOR EVIDENCE OF OFF-SITE ROADWAY SEDIMENT TRACKING.
- UPON CONCLUSION OF THE INSPECTIONS, EPSC MEASURES FOUND TO BE INEFFECTIVE SHALL BE REPAIRED, REPLACED OR MODIFIED BEFORE THE NEXT RAIN EVENT, IF POSSIBLE, BUT IN NO CASE MORE THAN 24 HOURS AFTER THE INSPECTION OR WHEN THE CONDITION IS IDENTIFIED. IF THE REPAIR, REPLACEMENT OR MODIFICATION IS NOT PRACTICAL WITHIN THE TIMEFRAME, WRITTEN DOCUMENTATION MUST BE PROVIDED IN THE FIELD BOOK AND AN ESTIMATED REPAIR, REPLACEMENT OR MODIFICATION SCHEDULE SHALL BE DOCUMENTED WITHIN 24 HOURS AFTER IDENTIFICATION.
- THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT LITTER, CONSTRUCTION DEBRIS, AND CONSTRUCTION WASTES FROM ENTERING WATERS OF THE STATE/U.S.
- THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS TO ENSURE THAT PETROLEUM PRODUCTS OR OTHER CHEMICAL POLLUTANTS ARE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. ALL EQUIPMENT REFUELING, SERVICING, AND STAGING AREAS SHALL COMPLY WITH AL LOCAL, STATE, AND FEDERAL LAWS, RULES, REGULATIONS, AND ORDINANCES, INCLUDING THOSE OF THE NATIONAL FIRE PROTECTION ASSOCIATION (NFPA). APPROPRIATE CONTAINMENT MEASURES FOR THESE AREAS SHALL BE USED. ALL SPILLS MUST BE REPORTED TO THE APPROPRIATE AGENCY, AND MEASURES SHALL BE TAKEN IMMEDIATELY TO PREVENT THE POLLUTION OF WATERS OF THE STATE/U.S., INCLUDING GROUNDWATER, SHOULD A SPILL OCCUR.
- THE CONTRACTOR SHALL USE ANY MEASURES NECESSARY TO ENSURE THAT CONSTRUCTION AND CONSTRUCTION EQUIPMENT WILL NOT ENTER ANY PORTION OF THE CREEK UNLESS SPECIFICALLY ADDRESSED IN THE PLANS.

UNOFFICIAL SET  
NOT FOR BIDDING



STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
  
EROSION CONTROL PLAN  
  
BR. NO. 93-SR84-01.58

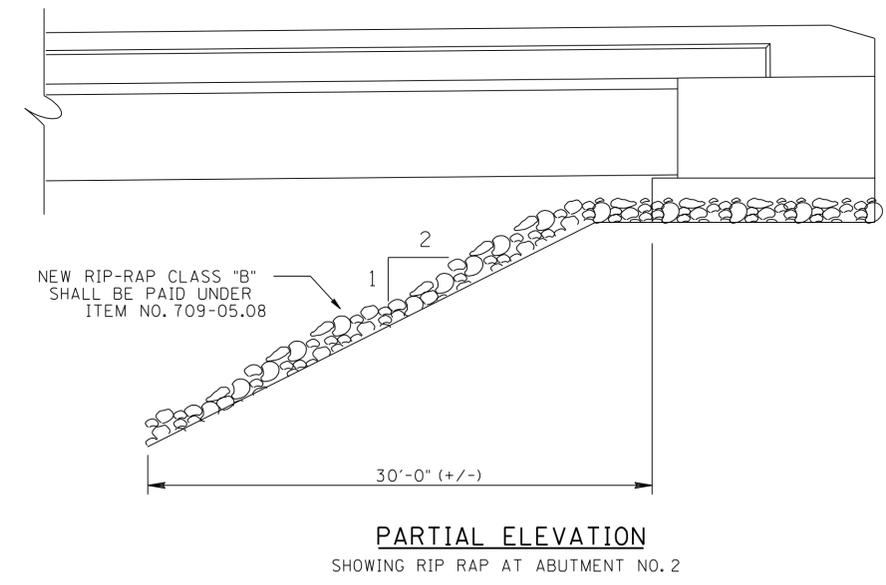
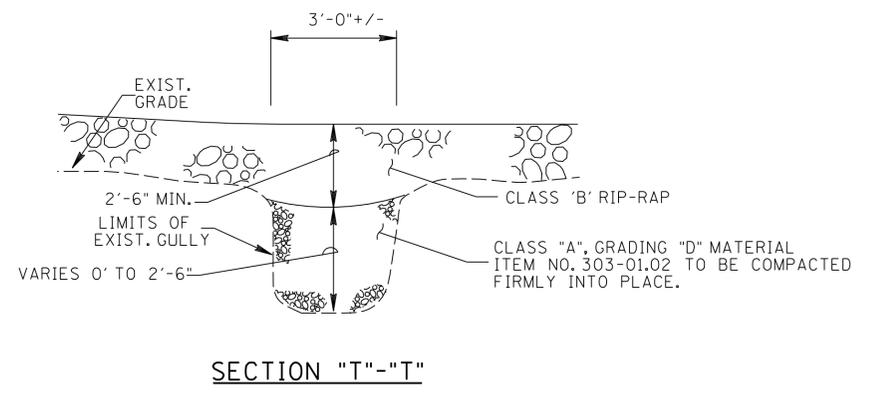
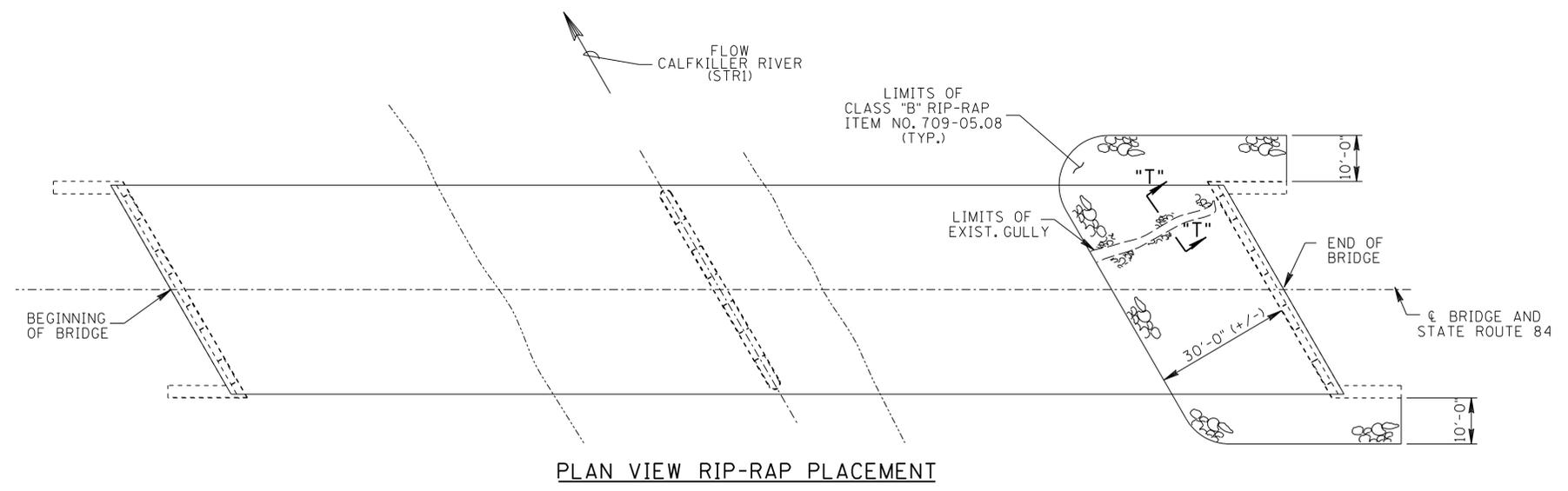








PROJECT NO.	YEAR	SHEET NO.	
93004-4222-04	2015		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



**UNOFFICIAL SET**  
 NOT FOR BIDDING

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
BUREAU OF HIGHWAYS

EMBANKMENT REPAIRS  
 STATE ROUTE 84  
 OVER  
 CALKILLER RIVER  
 BRIDGE NO. 93-SR084-01.58  
 WHITE COUNTY  
 2015

DESIGNED BY JCP DATE MARCH 2015  
 DRAWN BY JCP DATE MARCH 2015  
 SUPERVISED BY WAP DATE MARCH 2015  
 CHECKED BY JCP DATE MARCH 2015