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SEE SHEET IA

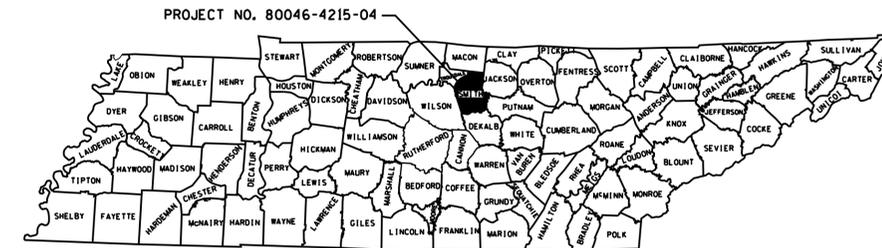
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF ENGINEERING

SMITH COUNTY

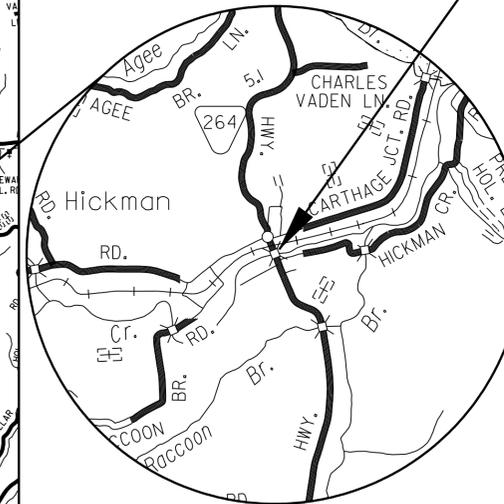
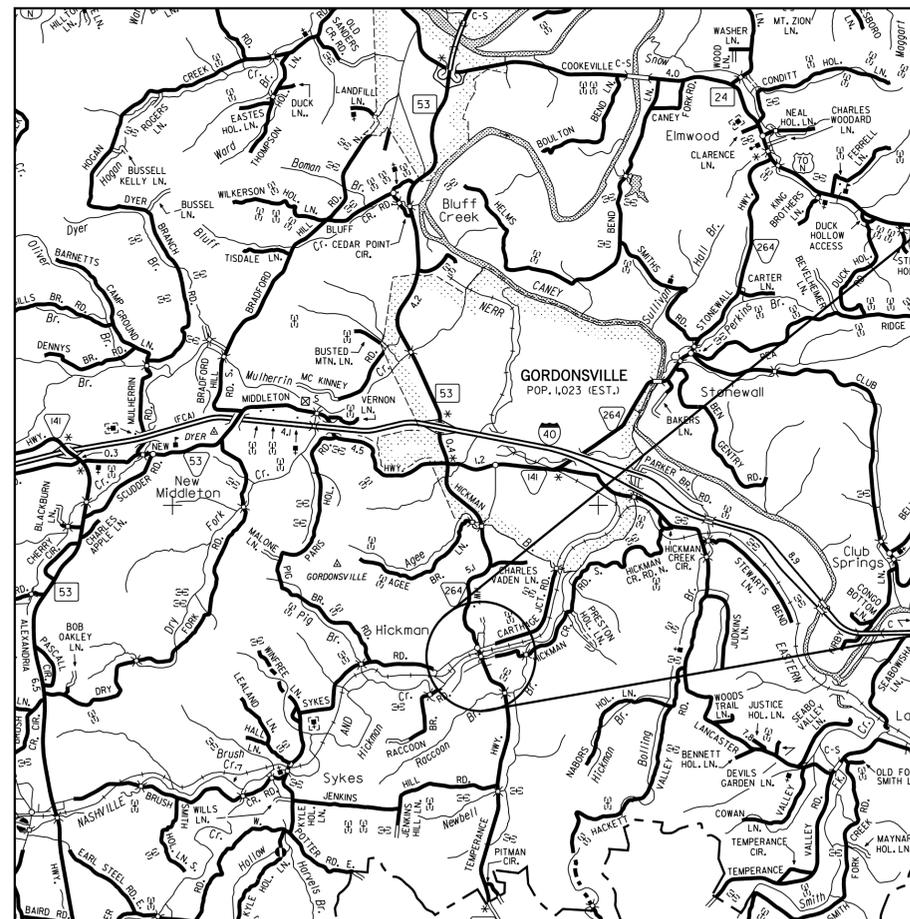
STATE ROUTE 264 OVER HICKMAN CREEK
BRIDGE NO. 80-SR264-02.88

TENN.	YEAR 2014	SHEET NO. 1
FED. AID PROJ. NO.		
STATE PROJ. NO.	80046-4215-04	

REV:



BRIDGE REPAIR



BRIDGE NO. 80-SR264-02.88
OVER
HICKMAN CREEK

UNOFFICIAL SET
NOT FOR BIDDING



APPROVED: Paul D. Degges
PAUL D. DEGGES, CHIEF ENGINEER

DATE: _____

APPROVED: John Schroer
JOHN SCHROER, COMMISSIONER

SPECIAL NOTES

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED MARCH 1, 2006 AND ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS AND IN THE PROPOSAL CONTRACT.

TDOT C.E. MANAGER 1 OR
TDOT DESIGN MANAGER 1 BRIAN EGLI

TDOT PROJECT MANAGER TERRY MACKIE

DESIGNED BY JAMES + ASSOCIATES, INC.

DESIGNER DAVID THOMPSON, P.E. CHECKED BY JAMIE GILLESPIE, P.E.

P.E. NO. 80046-4215-04

PIN NO. 120024.00

SCALE: 1" = 5,280'

TRAFFIC DATA	
ADT (2014)	1,320
ADT (2034)	1,450
DHV (2034)	145
D	65 - 35
T (ADT)	2 %
T (DHV)	1 %
V	30 MPH

ROADWAY LENGTH 0.0379 MILES
BRIDGE LENGTH 0.0243 MILES
PROJECT LENGTH 0.0622 MILES

U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION

APPROVED: _____
DIVISION ADMINISTRATOR DATE

*****SYTIME*****
*****DGN5PEC*****

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ROADWAY DESIGN STANDARDS

DWG. NO.	REVISION DATE	DESCRIPTION
RD-A-1	12-18-99	STANDARD ABBREVIATIONS
RD-L-1	10-26-94	STANDARD LEGEND
RD-L-2	09-05-01	STANDARD LEGEND FOR UTILITY INSTALLATIONS
RD-L-5	05-01-08	STANDARD LEGEND FOR EROSION PREVENTION AND SEDIMENT CONTROL
RD-L-6	03-30-10	STANDARD LEGEND FOR EROSION PREVENTION AND SEDIMENT CONTROL
RD-L-7	05-24-12	STANDARD LEGEND FOR EROSION PREVENTION AND SEDIMENT CONTROL

SAFETY APPURTENANCES AND FENCE

S-PL-2	SAFETY PLAN AT SIDE ROADS OR PRIVATE DRIVES
S-PL-3	SAFETY PLAN; MINIMUM INSTALLATION AT BRIDGE ENDS
S-GR31-1	W-BEAM GUARDRAIL
S-GRC-1	GUARDRAIL CONNECTION TO BRIDGE ENDS OR BARRIER WALL
S-GRT-2	TYPE 38 GUARDRAIL TERMINAL
S-GRT-2P	EARTH PAD FOR TYPE 38 TERMINAL
S-GRA-3	GUARDRAIL ANCHOR FOR TYPE 21, 13, AND IN-LINE TERMINALS

TRAFFIC CONTROL APPURTENANCES

T-M-1	11-01-11	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS AND MARKING ABBREVIATIONS
T-M-2	01-15-13	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS
T-PBR-1	06-30-09	INTERCONNECTED PORTABLE BARRIER RAIL
T-RR-6	10-25-13	TYPICAL SIGNING AND MARKINGS AT PASSIVE RAILROAD HIGHWAY GRADE CROSSINGS
T-S-9	11-01-11	STANDARD LAYOUT GROUND MOUNTED SIGNS
T-S-11	06-06-11	DELINEATOR AND MILEPOST DETAILS
T-S-16	11-01-11	GROUND MOUNTED ROADSIDE SIGN AND DETAILS
T-S-16A	11-01-11	GROUND MOUNTED ROADSIDE SIGN PLACEMENT DETAILS
T-S-20	11-01-11	SIGN DETAILS
T-SG-1	11-01-11	WOOD POLE, DETAILS FOR SPAN MOUNTED SIGNALS
T-SG-2	07-29-04	LOOP LEAD-INS, CONDUIT AND PULL BOXES
T-SG-3	11-11-04	NOTES AND DETAILS OF INDUCTIVE LOOPS
T-SG-3A		ALTERNATE DETECTION DETAILS
T-SG-4		SPAN WIRE AND MESSENGER CABLE DETAILS
T-SG-5	12-04-13	CONTROLLER CABINET DETAILS
T-SG-7A	11-01-11	TYPICAL SIGNAL HEAD PLACEMENT
T-SG-8	12-04-13	STRAIN POLE DETAILS FOR SPAN MOUNTED SIGNALS
T-SG-9A	12-04-13	MISCELLANEOUS SIGNAL DETAILS
T-SG-12	11-01-11	TYPICAL WIRING FOR SIGNAL HEADS AND DETECTION LOOPS
T-WZ-10	04-02-12	ADVANCE ROAD WORK SIGNING ON HIGHWAYS AND FREEWAYS
T-WZ-11	03-13-09	ONE LANE CLOSURE DETAIL ON DIVIDED HIGHWAYS
T-WZ-16	03-13-09	LANE SHIFT ON DIVIDED HIGHWAYS AND FREEWAYS
T-WZ-32	10-29-13	TRAFFIC CONTROL PLAN SIGNAL LAYOUT FOR TRAFFIC SIGNAL AT TWO LANE BRIDGE RECONSTRUCTION SITE
T-WZ-33	05-27-98	TRAFFIC CONTROL PLAN FOR CLOSE INTERSECTION CONDITIONS USING TRAFFIC SIGNAL AT TWO LANE BRIDGE RECONSTRUCTION SITE
T-WZ-34	09-01-05	TRAFFIC CONTROL PLAN GENERAL NOTES FOR TRAFFIC SIGNAL AT TWO LANE BRIDGE RECONSTRUCTION SITE
T-WZ-35	04-02-12	TRAFFIC CONTROL PLAN PAY ITEM AND SIGN DETAILS FOR TRAFFIC SIGNAL AT TWO LANE BRIDGE RECONSTRUCTION SITE

EROSION PREVENTION AND SEDIMENT CONTROL

EC-STR-3B	08-01-12	SILT FENCE
EC-STR-3E	04-01-08	SILT FENCE FABRIC JOINING DETAILS
EC-STR-25	08-01-12	TEMPORARY CULVERT CROSSING, CONSTRUCTION EXIT, CONSTRUCTION FORD

BRIDGE APPURTENANCES

STD-1-1SS	05-01-14	BRIDGE RAILING SINGLE SLOPE CONCRETE PARAPET - 2006
STD-1-2SS		STEEL SLIDER PLATE ASSEMBLIES FOR SINGLE SLOPE CONCRETE AND BRIDGE DECK DRAIN DETAILS - 2007
STD-1-5	03-26-14	REINFORCED CONCRETE PAVEMENT AT BRIDGE ENDS - 1995
STD-2-1	11-01-10	BRIDGE MOUNTED INTERCONNECTED PORTABLE BARRIER RAIL - 2005
STD-4-1	04-08-05	STANDARD PRECAST PRESTRESSED BRIDGE DECK PANELS GENERAL DETAILS - 1992
STD-4-2	04-08-05	STANDARD PRECAST PRESTRESSED BRIDGE DECK PANELS DESIGN CRITERIA - 1992
STD-4-3	03-02-02	STANDARD PRECAST PRESTRESSED BRIDGE DECK PANELS GENERAL DETAILS - 1992
STD-4-4	06-10-96	STANDARD PRECAST PRESTRESSED BRIDGE DECK PANELS CONSTRUCTION DETAILS - 1992
STD-9-1	10-07-08	REINFORCING BAR SUPPORT DETAILS FOR CONCRETE SLABS
STD-10-1	04-08-05	MISCELLANEOUS ABUTMENT AND DRAINAGE DETAILS - 1971
SBR-2-124	01-04-96	DETAILS SHOWING REPLACEMENT OF EXISTING BRIDGERAIL SYSTEM WITH NEW JERSEY SHAPE CONCRETE PARAPET AND NEW 10'-2" ENDPPOST - 1988
SBR-2-125	11-05-01	DETAILS SHOWING REPLACEMENT OF EXISTING BRIDGERAIL SYSTEM WITH NEW JERSEY SHAPE CONCRETE PARAPET AND NEW 10'-2" ENDPPOST - 1988
SBR-2-126	01-04-96	DETAILS SHOWING REPLACEMENT OF EXISTING BRIDGERAIL SYSTEM WITH NEW JERSEY SHAPE CONCRETE PARAPET AND NEW 10'-2" ENDPPOST - 1988
SBR-2-136	11-05-01	STANDARD DRAWING FOR REPLACING EXISTING CONCRETE ENDPPOST AND GUARDRAIL AT EXISTING BRIDGE ENDS - 1992

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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

BRIDGE NO. 80-SR264-2.88
STATE ROUTE 264
OVER HICKMAN CREEK
SMITH COUNTY
2014

INDEX AND STANDARD DRAWINGS

TENNESSEE D.O.T.
DESIGN DIVISION

FILE NO.

TYPE	YEAR	PROJECT NO.	SHEET NO.
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PROJECT COMMITMENTS

COMMITMENT TO	SOURCE DIVISION	DESCRIPTION	STA./LOCATION
-----	ENVIRONMENTAL DIVISION, ENVIRONMENT	COMMITMENT ID: EDHZ001 - AN ASBESTOS-CONTAINING MATERIAL (ACM) SURVEY WAS CONDUCTED ON BRIDGE# 80SR2640005, SR-264 OVER HICKMAN CREEK, LM 2.88 (80-SR264-2.88). NO ACM WAS DETECTED. NO SPECIAL ACCOMMODATIONS FOR DEMOLITION AND WASTE DISPOSAL ARE ANTICIPATED FOR THIS BRIDGE AND THE MATERIAL CAN BE DEPOSITED IN A C&D LANDFILL. PLEASE NOTE THAT EVEN THOUGH THE SAMPLES WERE FOUND TO CONTAIN NO ASBESTOS, THE DEMOLITION CONTRACTOR IS REQUIRED TO SUBMIT THE NATIONAL EMISSION STANDARDS FOR HAZARDOUS AIR POLLUTANTS STANDARD 10-DAY NOTICE OF DEMOLITION TO THE TENNESSEE DIVISION OF AIR POLLUTION CONTROL.	LM 2.88

UNOFFICIAL SET
NOT FOR BIDDING



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PROJECT COMMITMENTS

BRIDGE NO. 80-SR264-2.88
STATE ROUTE 264
OVER HICKMAN CREEK
SMITH COUNTY
2014

ESTIMATED ROADWAY QUANTITIES

ITEM NO.	DESCRIPTION	UNIT	QUANTITY
① 105-01	CONSTRUCTION STAKES, LINES AND GRADES	LS	1
② 203-01	ROAD AND DRAINAGE EXCAVATION (UNCLASSIFIED)	C.Y.	55
③ 209-05	SEDIMENT REMOVAL	C.Y.	1
③ 209-08.02	TEMPORARY SILT FENCE (WITH BACKING)	L.F.	35
303-01	MINERAL AGGREGATE, TYPE A BASE, GRADING D	TON	40
307-01.08	ASPHALT CONCRETE MIX (PG64-22) (BPMB-HM) GRADING B-M2	TON	15
402-01	BITUMINOUS MATERIAL FOR PRIME COAT (PC)	TON	0.1
403-01	BITUMINOUS MATERIAL FOR TACK COAT (TC)	TON	0.4
411-01.10	ACS MIX (PG64-22) GRADING D	TON	55
④ 415-01.02	COLD PLANING BITUMINOUS PAVEMENT	S.Y.	840
705-01.01	GUARDRAIL AT BRIDGE ENDS	L.F.	81
705-02.02	SINGLE GUARDRAIL (TYPE 2)	L.F.	20
705-04.05	GUARDRAIL TERMINAL (TYPE IN-LINE)	EACH	2
705-04.07	TAN ENERGY ABSORBING TERM (NCHRP 350, TL3)	EACH	1
705-04.09	EARTH PAD FOR TYPE 38 GR END TREATMENT	EACH	1
⑤ 705-08.51	PORTABLE IMPACT ATTENUATOR (NCHRP 350, TL-3)	EACH	2
706-01	GUARDRAIL REMOVED	L.F.	385
⑥ 712-01	TRAFFIC CONTROL	LS	1
⑦ 712-02.02	INTERCONNECTED PORTABLE BARRIER RAIL	L.F.	100
⑧ 712-04.01	FLEXIBLE DRUMS (CHANNELIZING)	EACH	42
712-04.50	PORTABLE BARRIER RAIL DELINEATOR	EACH	10
712-05.03	WARNING LIGHTS (TYPE C)	EACH	10
⑨ 712-06	SIGNS (CONSTRUCTION)	S.F.	1,165
⑩ 712-06.10	NEW SIGNS (CONSTRUCTION)	S.F.	23
712-09.01	REMOVABLE PAVEMENT MARKING LINE	L.F.	660
712-09.04	REMOVABLE PAVEMENT MARKING (STOP LINE)	L.F.	48
713-15	REMOVAL OF SIGNS, POSTS AND FOOTINGS	LS	1
⑪ 713-16.01	CHANGEABLE MESSAGE SIGN	EACH	1
713-16.09	RAILROAD ADVANCE WARNING SIGN AND SUPPORT	EACH	2
716-05.01	PAINTED PAVEMENT MARKING (4" LINE)	L.M.	1,20
716-05.05	PAINTED PAVEMENT MARKING (STOP LINE)	L.F.	48
716-05.10	PAINTED PAVEMENT MARKING (RXR)	EACH	2
717-01	MOBILIZATION	LS	1
⑫ 730-40	TEMPORARY TRAFFIC SIGNAL SYSTEM	EACH	5
⑬ 801-01	SEEDING (WITH MULCH)	UNIT	1
⑭ 801-03	WATER (SEEDING AND SODDING)	M.G.	1

FOOTNOTES

- ① ALL DIMENSIONAL DETAILS SHOWN ON PLANS, INCLUDING ELEVATIONS, SHALL BE CHECKED BY THE CONTRACTOR TO ASSURE ACCURACY OF THE LAYOUT PRIOR TO CONSTRUCTION. ALL BRIDGE SUBSTRUCTURES SHALL BE CHECKED AS TO LOCATION, DIMENSIONAL LAYOUTS AND ELEVATIONS, BY MEANS OF TWO INDEPENDENT LAYOUT METHODS. ANY ERRORS AND APPARENT DISCREPANCIES FOUND IN PREVIOUS SURVEYS, OR IN EITHER THE SPECIFICATIONS OR SPECIAL PROVISIONS, SHALL BE CALLED TO THE ENGINEER'S ATTENTION BY THE CONTRACTOR IMMEDIATELY AND PRIOR TO PROCEEDING WITH WORK.
- ② INCLUDES COST OF EXCAVATION WITHIN THE 10'-0" LONG SECTION OF PAVEMENT TRANSITION OFF OF THE END OF THE PAVEMENT AT BRIDGE ENDS AT EACH END OF BRIDGE.
- ③ SEE SUBSECTION 209.07 OF THE STANDARD SPECIFICATIONS FOR MAINTENANCE REPLACEMENT.
- ④ DEPTH VARIES FROM 0" TO 1 1/4" IN ALL AREAS NECESSARY FOR CONSTRUCTION OF ASPHALT TRANSITIONS AND ANY PRIVATE DRIVE OR FIELD ENTRANCE TRANSITIONS WITHIN THE TAPER EXCEPT AREA OF PROPOSED PAVEMENT AT BRIDGE ENDS. REMOVE ASPHALT FULL DEPTH AT PROPOSED LOCATION OF PAVEMENT AT BRIDGE ENDS, FULL DEPTH REMOVAL SHALL BE PAID FOR UNDER ITEM NO. 202-03, REMOVAL OF RIGID PAVEMENT, SIDEWALK, ETC., S.Y.
- ⑤ THIS ITEM SHALL BE A PORTABLE ENERGY ABSORBING TERMINAL MEETING THE REQUIREMENTS OF NCHRP 350 FOR TEST LEVEL 3. EXAMPLES WOULD BE A QUAD-GUARD, A REACT 350 OR A TRACC. THE PAY ITEM WILL INCLUDE FURNISHING AND INSTALLING ALL COMPONENTS AS LISTED ON THE MANUFACTURERS BILL OF MATERIALS.
- ⑥ INCLUDES COST FOR REMOVAL OF EXISTING OR CONFLICTING MARKINGS.
- ⑦ COST INCLUDES 100 L.F. FOR PHASE I AND 100 L.F. FOR PHASE II RELOCATION.
- ⑧ INCLUDES RELOCATION AND INSTALLATION FOR EACH PHASE OF THE CONSTRUCTION SEQUENCE: PHASE I = 21 AND PHASE II = 42 ON STATE ROUTE 264.
- ⑨ BASED ON SECTION 712.10 OF STANDARD SPECIFICATIONS, 757 S.F. FOR WIDE LOAD DETOUR.
- ⑩ SEE SHEET NO. 2J FOR PLACEMENT OF NEW SIGNS.
- ⑪ COORDINATE WITH T.D.O.T. CONSTRUCTION DIVISION FOR LOCATION AND MESSAGE FOR CHANGEABLE MESSAGE SIGN.
- ⑫ THE COST ASSOCIATED WITH COORDINATING THE TRAFFIC SIGNAL PHASING AND TIMING SHALL BE INCLUDED IN THE UNIT PRICE.
- ⑬ ITEM SHALL BE BID AS A CONTINGENCY AND MAY BE INCREASED, DECREASED OR ELIMINATED AS DIRECTED BY THE ENGINEER.
- ⑭ INCLUDES 1 THOUSAND GALLONS FOR EROSION PREVENTION AND SEDIMENT CONTROL.

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BRIDGE NO. 80-SR264-2.88
STATE ROUTE 264
OVER HICKMAN CREEK
SMITH COUNTY
2014

ESTIMATED
ROADWAY
QUANTITIES

ROADWAY GENERAL NOTES

GRADING

- (1) ANY AREA THAT IS DISTURBED OUTSIDE LIMITS OF CONSTRUCTION DURING THE LIFE OF THIS PROJECT SHALL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE.
- (2) THE CONTRACTOR SHALL NOT DISPOSE OF ANY MATERIAL EITHER ON OR OFF STATE-OWNED R.O.W. IN A REGULATORY FLOODWAY AS DEFINED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY WITHOUT APPROVAL BY SAME. ALL MATERIAL SHALL BE DISPOSED OF IN UPLAND (NON-WETLAND) AREAS AND ABOVE ORDINARY HIGH WATER OF ANY ADJACENT WATERCOURSE. THIS DOES NOT ELIMINATE THE NEED TO OBTAIN ANY OTHER LICENSES OR PERMITS THAT MAY BE REQUIRED BY ANY OTHER FEDERAL, STATE OR LOCAL AGENCY.

SEEDING AND SODDING

- (3) ITEM NO. 801-01, SEEDING (WITH MULCH), SHALL BE USED WHERE EROSION CONTROL BLANKET OR SOD ARE NOT APPLIED.

GUARDRAIL

- (4) THE CONTRACTOR SHALL NOT REMOVE ANY SECTIONS OF EXISTING GUARDRAIL TO REWORK SHOULDERS OR FLATTEN SLOPES UNTIL THE ENGINEER CONCURS IN THE NECESSITY OF REMOVAL DUE TO CONSTRUCTION REQUIREMENTS AND THE APPROPRIATE WARNING DEVICES ARE INSTALLED. THE PROPOSED GUARDRAIL, INCLUDING ANY ANCHOR SYSTEM, SHALL BE INSTALLED QUICKLY TO MINIMIZE TRAFFIC EXPOSURE TO ANY HAZARD. NO PAYMENT WILL BE MADE FOR A SECTION OF PROPOSED GUARDRAIL, INCLUDING ANCHORS, UNTIL IT IS COMPLETELY IN PLACE.
- (5) IF ANY APPROACH END OF A SECTION OF GUARDRAIL OR BRIDGE RAIL MUST TEMPORARILY BE LEFT INCOMPLETE AND EXPOSED TO TRAFFIC, THE CONTRACTOR SHALL USE TWO (2) TEMPORARY BARRICADES OR DRUMS WITH TYPE "A" LIGHTS AND ROUNDED END ELEMENTS AS MINIMUM MEASURES TO PROTECT TRAFFIC FROM THE HAZARD OF AN EXPOSED END. ALL COST OF FURNISHING AND INSTALLING A TEMPORARY ROUNDED END ELEMENT SHALL BE INCLUDED IN THE COST OF THE PROPOSED GUARDRAIL.

DRAINAGE

- (6) THE CONTRACTOR SHALL SHAPE DITCHES TO THE SPECIFIED DESIGN. THIS WORK WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COST WILL BE INCLUDED IN THE COST OF OTHER ITEMS.

UTILITIES

- (7) THE LOCATIONS OF UTILITIES SHOWN WITHIN THESE PLANS ARE APPROXIMATE ONLY. EXACT LOCATIONS SHALL BE DETERMINED IN THE FIELD BY CONTACTING THE UTILITY COMPANIES INVOLVED. NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC., AT 1-800-351-1111 AS REQUIRED BY TCA 65-61-106 WILL BE REQUIRED.
- (8) UNLESS OTHERWISE NOTED, ALL UTILITY ADJUSTMENTS WILL BE PERFORMED BY THE UTILITY OR IT'S REPRESENTATIVE. THE CONTRACTOR AND UTILITY OWNERS WILL BE REQUIRED TO COOPERATE WITH EACH OTHER IN ORDER TO EXPEDITE THE WORK REQUIRED BY THIS CONTRACT. ON CONTRACTS WHERE CONSTRUCTION STAKES, LINES, AND GRADES ARE CONTRACT ITEMS, THE CONTRACTOR WILL BE REQUIRED TO PROVIDE RIGHT-OF-WAY OR SLOPE STAKES, DITCH OR STREAM BED GRADES, OR OTHER ESSENTIAL SURVEY STAKING TO PREVENT CONFLICTS WITH THE HIGHWAY CONSTRUCTION. FREQUENTLY, THIS WILL BE REQUIRED AS THE FIRST ITEM OF WORK AND AT ANY LOCATION ON THE PROJECT DIRECTED BY THE ENGINEER.
- (9) THE CONTRACTOR WILL PROVIDE ALL NECESSARY PROTECTIVE MEASURES TO SAFEGUARD EXISTING UTILITIES FROM DAMAGE DURING CONSTRUCTION OF THIS PROJECT. IN THE EVENT THAT SPECIAL EQUIPMENT IS REQUIRED TO WORK OVER AND AROUND THE UTILITIES, THE CONTRACTOR WILL BE REQUIRED TO FURNISH SUCH EQUIPMENT. THE COST OF PROTECTING UTILITIES FROM DAMAGE AND FURNISHING SPECIAL EQUIPMENT WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (10) THE CONTRACTOR SHALL NOTIFY EACH INDIVIDUAL UTILITY OWNER OF HIS PLAN OF OPERATION IN THE AREA OF THE UTILITIES. PRIOR TO COMMENCING WORK, THE CONTRACTOR SHALL CONTACT THE UTILITY OWNERS AND REQUEST THEM TO PROPERLY LOCATE THEIR RESPECTIVE UTILITY ON THE GROUND. THIS NOTIFICATION SHALL BE GIVEN AT LEAST THREE (3) BUSINESS DAYS PRIOR TO COMMENCEMENT OF OPERATIONS AROUND THE UTILITY IN ACCORDANCE WITH TCA 65-31-106.

MISCELLANEOUS

- (11) ALL DETOUR, ACCESS, SERVICE AND FRONTAGE ROADS SHALL BE CONSTRUCTED WITH A MINIMUM OF ONE (1) COURSE OF BASE MATERIAL BEFORE TRAFFIC IS INTERRUPTED ON EXISTING ROADS.
- (12) THE CONTRACTOR SHALL BE REQUIRED TO REMOVE AND RESET MAILBOXES WHERE AND AS DIRECTED BY THE ENGINEER.

- (13) NOTHING IN THE GENERAL NOTES OR SPECIAL PROVISIONS SHALL RELIEVE THE CONTRACTOR FROM HIS RESPONSIBILITIES TOWARD THE SAFETY AND CONVENIENCE OF THE GENERAL PUBLIC AND THE RESIDENTS ALONG THE PROPOSED CONSTRUCTION AREA.

PAVEMENT MARKINGS

- (14) PERMANENT PAVEMENT LINE MARKINGS SHALL BE 4" SPRAY THERMOPLASTIC (40 MIL) INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-13.06. SPRAY THERMO PVMT MRKNG (40 MIL) (4 IN LINE), L.M. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK AND THEN INSTALLING THE PERMANENT MARKING AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.
- (15) PERMANENT PAVEMENT LINE MARKINGS SHALL BE REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT, UNMARKED SECTIONS SHALL NOT BE ALLOWED. THESE MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-05.01 PAINTED PAVEMENT MARKING (4IN LINE), L.M.

DETOURS, LANE SHIFTS AND MEDIAN CROSS-OVERS

- (16) THE PAVEMENT MARKING ON THE MEDIAN CROSS-OVER FOR CENTERLINES AND EDGELINES WILL BE INSTALLED AND MAINTAINED TO THE SAME STANDARDS AS FOR PERMANENT MARKINGS ON THE MAIN ROADWAY. THESE MARKINGS SHALL BE IN PLACE PRIOR TO ALLOWING TRAFFIC ONTO THE PAVEMENT. THESE PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-05.01 PAINTED PAVEMENT MARKING (4" LINE), LIN. MI.

PAVING

- (17) THE CONTRACTOR SHALL BE REQUIRED TO COLD PLANE AND PAVE IN THE DIRECTION OF TRAFFIC.

RESURFACING

- (18) WHERE DIRECTED BY THE TDOT ENGINEER, THE CONTRACTOR SHALL BE REQUIRED TO SHAPE PUBLIC SIDE ROADS, BUSINESS ENTRANCES, AND PRIVATE DRIVES, AS WELL AS CLEANING OF EXISTING DRAINS BEFORE PLACING MATERIALS. ALL COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (19) ALL PUBLIC SIDE ROADS SHALL BE PAVED ONE PAVER WIDTH THROUGH THE INTERSECTION AS A MINIMUM. A SATISFACTORY TRANSITION FROM THE NEW PAVEMENT TO THE EXISTING GRADE OF THE INTERSECTING PUBLIC ROAD OR BUSINESS ENTRANCE SHALL BE PROVIDED. SHOULD THE PAVEMENT OF THE INTERSECTING PUBLIC ROAD BE DISTRESSED, THE RESURFACING WIDTH MAY BE INCREASED TO THE NORMAL RIGHT OF WAY LINE.
- (20) PRIVATE DRIVEWAYS, FIELD ENTRANCES, AND BUSINESS ENTRANCES WILL BE RESURFACED A PAVER WIDTH (LANE WIDTH) AS A MINIMUM. A PAVEMENT TAPER TO TRANSITION THE NEW PAVEMENT SHALL BE REQUIRED. IT SHALL BE BASED ON AN ADDITIONAL ONE FOOT OF WIDTH PER ONE INCH DEPTH OF PAVEMENT. IF THE SHOULDER IS NARROW ENOUGH THAT THE SUM OF THE SHOULDER AND THE TRANSITION ARE LESS THAN A PAVER WIDTH, THE TRANSITION SHALL OCCUR WITHIN THE PAVER WIDTH. IF THE SUM OF THE SHOULDER AND THE TRANSITION IS GREATER THAN A PAVER WIDTH (LANE WIDTH), THE TRANSITION SHALL OCCUR OUTSIDE OF THE PAVER WIDTH.
- (21) IN ALL CASES, THE LENGTH OF THE PAVEMENT TRANSITION, THE THICKNESS AND WIDTH OF THE RESURFACING AND ANY ADDITIONAL PAVEMENT MATERIALS SHALL BE AS DIRECTED BY THE TDOT ENGINEER.

CONSTRUCTION WORK ZONE AND TRAFFIC CONTROL NOTES

- (22) ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE WEEK BEFORE NEEDED, IF THE SIGN FACE IS FULLY COVERED.
- (23) IF THE CONTRACTOR MOVES OFF THE PROJECT, HE SHALL COVER OR REMOVE ALL UNNEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OR REMOVAL, COVERING, AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION) PER SQUARE FOOT.
- (24) A LONG TERM BUT SPORADIC USE WARNING SIGN, SUCH AS A FLAGGER SIGN, MAY REMAIN IN PLACE WHEN NOT REQUIRED PROVIDED THE SIGN FACE IS FULLY COVERED.
- (25) TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.

- (26) USE OF BARRICADES, PORTABLE BARRIER RAILS, VERTICAL PANELS, AND DRUMS SHALL BE LIMITED TO THE IMMEDIATE AREAS OF CONSTRUCTION WHERE A HAZARD IS PRESENT. THESE DEVICES SHALL NOT BE STORED ALONG THE ROADWAY WITHIN THIRTY (30) FEET OF THE EDGE OF THE TRAVELED WAY BEFORE OR AFTER USE UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL INCREASE TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. THESE DEVICES SHALL BE REMOVED FROM THE CONSTRUCTION WORK ZONE WHEN THE ENGINEER DETERMINES THEY ARE NO LONGER NEEDED. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.
- (27) THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY, WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHEN THE LANE IS OPEN TO TRAFFIC UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO PARK WITHIN THIRTY (30) FEET OF A OPEN TRAFFIC LANE AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCTIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.
- (28) ALL DETOUR AND CONSTRUCTION SIGNING SHALL BE IN STRICT ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

EROSION PREVENTION AND SEDIMENT CONTROL DISTURBED AREA

- (29) PRE-CONSTRUCTION VEGETATIVE GROUND COVER SHALL NOT BE DESTROYED, REMOVED OR DISTURBED (I.E. CLEARING AND GRUBBING INITIATED) MORE THAN 15 CALENDAR DAYS PRIOR TO GRADING OR EARTH MOVING ACTIVITIES UNLESS THE AREA IS SEEDED AND/OR MULCHED OR OTHER TEMPORARY COVER IS INSTALLED.
- (30) CLEARING, GRUBBING, AND OTHER DISTURBANCE TO RIPARIAN VEGETATION SHALL BE LIMITED TO THE MINIMUM NECESSARY FOR SLOPE CONSTRUCTION AND EQUIPMENT OPERATIONS. EXISTING VEGETATION SHOULD BE PRESERVED TO THE MAXIMUM EXTENT POSSIBLE. UNNECESSARY VEGETATION REMOVAL IS PROHIBITED.
- (31) ALL DISTURBED AREAS SHALL BE PROPERLY STABILIZED AS SOON AS PRACTICABLE. PRIORITY SHALL BE GIVEN TO FINISHING OPERATIONS AND PERMANENT EPSC MEASURES OVER TEMPORARY EPSC MEASURES ON ALL PROJECTS.

SEDIMENT CONTROL

- (32) EPSC MEASURES SHALL BE INSTALLED AND FUNCTIONAL PRIOR TO ANY EARTH MOVING OPERATIONS, AND SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD.
- (33) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT THE OFF-SITE MIGRATION OR DEPOSIT OF SEDIMENT ON ROADWAYS USED BY THE GENERAL PUBLIC. IF SEDIMENT ESCAPES THE CONSTRUCTION SITE, OFF-SITE ACCUMULATIONS OF SEDIMENT THAT HAVE NOT REACHED A STREAM MUST BE REMOVED AT A FREQUENCY SUFFICIENT TO MINIMIZE OFF-SITE IMPACTS (E.G., FUGITIVE SEDIMENT THAT HAS ESCAPED THE CONSTRUCTION SITE AND HAS COLLECTED IN A STREET MUST BE REMOVED SO THAT IT IS NOT SUBSEQUENTLY WASHED INTO STORM SEWERS AND STREAMS BY THE NEXT RAIN AND/OR SO THAT IT DOES NOT POSE A SAFETY HAZARD TO USERS OF PUBLIC STREETS). ARRANGEMENTS CONCERNING REMOVAL OF SEDIMENT ON ADJOINING PROPERTY MUST BE SETTLED WITH THE ADJOINING PROPERTY OWNER BEFORE REMOVAL OF SEDIMENT.
- (34) TEMPORARY EPSC MEASURES MAY BE REMOVED AT THE BEGINNING OF THE WORKDAY, BUT MUST BE REPLACED AT THE END OF THE WORKDAY.

TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	2014	80046-4215-04	2C

UNOFFICIAL SET

NOT FOR BIDDING



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

BRIDGE NO. 80-SR264-2.88
STATE ROUTE 264
OVER HICKMAN CREEK
SMITH COUNTY
2014

ROADWAY
GENERAL NOTES

ROADWAY GENERAL AND SPECIAL NOTES (CONT'D)

TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	2014	80046-4215-04	2D

TENNESSEE D.O.T. DESIGN DIVISION

FILE NO.

STREAM/WETLAND

- (35) SOIL MATERIALS MUST BE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. EPSC MEASURES TO PROTECT WATER QUALITY MUST BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD. APPROPRIATE EPSC MEASURES MUST BE INSTALLED ALONG THE BASE OF ALL FILLS AND CUTS, ON THE DOWNHILL SIDE OF STOCKPILED SOIL, AND ALONG STREAM BANKS IN CLEARED AREAS TO PREVENT SEDIMENT MIGRATION INTO STREAMS IN ACCORDANCE WITH TDOT STANDARDS. THEY MUST BE INSTALLED ON THE CONTOUR, ENTRENCHED AND STAKED, AND EXTEND THE WIDTH OF THE AREA TO BE CLEARED.
- (36) STREAM BEDS SHALL NOT BE USED AS TRANSPORTATION ROUTES FOR CONSTRUCTION EQUIPMENT. TEMPORARY CROSSING MUST BE LIMITED TO ONE POINT PER STREAM AND EPSC MEASURES MUST BE USED WHERE THE STREAM BANKS ARE DISTURBED. WHERE THE STREAMBED IS NOT COMPOSED OF BEDROCK, A PAD OF CLEAN ROCK MUST BE USED AT THE CROSSING POINT AND CULVERTED TO PREVENT THE IMPOUNDMENT OF WATER FLOW. CLEAN ROCK IS ROCK OF VARIOUS TYPE AND SIZE, DEPENDING UPON APPLICATION, WHICH CONTAINS NO FINES, SOILS, OR OTHER WASTES OR CONTAMINANTS. OTHER MATERIALS USED FOR ALL TEMPORARY FILLS MUST BE COMPLETELY REMOVED IN THEIR ENTIRETY AFTER THE WORK IS COMPLETED AND THE AFFECTED AREAS RETURNED TO THEIR PREEXISTING ELEVATION. ALL TEMPORARY CROSSINGS MUST BE CONSTRUCTED IN ACCORDANCE WITH STD.DWG.EC-STR-25 UNLESS SPECIFICALLY ADDRESSED IN THE EPSC PLANS. ALTERNATIVELY, PLACING A TEMPORARY BRIDGE (BAILEY BRIDGE OR EQUIVALENT, TIMBERS, ETC.) FROM TOP OF BANK TO TOP OF BANK OR THE APPROPRIATE USE OF BARGES AT THE CROSSING TO AVOID DISTURBANCE OF THE STREAMBED IS AN ACCEPTABLE OPTION.

INSPECTION, MAINTENANCE, REPAIR

- (37) EPSC CONTROLS WILL BE MAINTAINED IN ACCORDANCE WITH TDOT STANDARD DRAWINGS AND GOOD ENGINEERING PRACTICES.
- (38) INSPECTION, REPAIR, AND MAINTENANCE OF EPSC MEASURES/STRUCTURES IS TO BE PERFORMED ON A REGULAR BASIS. SEDIMENT SHALL BE REMOVED FROM SEDIMENT CONTROL STRUCTURES WHEN THE DESIGN CAPACITY HAS BEEN REDUCED BY FIFTY PERCENT (50%). DURING SEDIMENT REMOVAL, THE CONTRACTOR SHALL TAKE CARE TO ENSURE THAT STRUCTURAL COMPONENTS OF EPSC MEASURES ARE NOT DAMAGED AND THUS MADE INEFFECTIVE. IF DAMAGE DOES OCCUR, THE CONTRACTOR SHALL REPAIR THE STRUCTURES AT THE CONTRACTOR'S OWN EXPENSE.
- (39) SEDIMENT REMOVED FROM SEDIMENT CONTROL STRUCTURES SHALL BE PLACED AND BE TREATED IN A MANNER SO THAT THE SEDIMENT IS CONTAINED WITHIN THE PROJECT LIMITS AND DOES NOT MIGRATE INTO WATERS OF THE STATE/U.S. COST FOR THIS TREATMENT IS TO BE INCLUDED IN PRICE BID FOR ITEM NO. 209-05 SEDIMENT REMOVAL, C.Y.
- (40) THE CONTRACTOR SHALL INSTALL A RAIN GAUGE EVERY LINEAR MILE AT ALL SITES WHERE CLEARING, GRUBBING, EXCAVATION, GRADING, CUTTING OR FILLING IS BEING ACTIVELY PERFORMED, OR EXPOSED SOIL HAS NOT YET BEEN PERMANENTLY STABILIZED. IF THE PROJECT LENGTH IS LESS THAN ONE LINEAR MILE, ONE RAIN GAUGE SHALL BE INSTALLED AT THE CENTER OF THE PROJECT OR AS INDICATED BY THE TDOT EPSC INSPECTOR. THE CONTRACTOR SHALL ENSURE THAT EACH GAUGE IS MAINTAINED IN GOOD WORKING CONDITION. TDOT AND/OR THE CONTRACTOR SHALL RECORD DAILY PRECIPITATION AND FORECASTED PERCENTAGE OF PRECIPITATION IN DETAILED RECORDS OF RAINFALL EVENTS INCLUDING DATES, AMOUNTS OF RAINFALL PER GAUGE, THE ESTIMATED DURATION (OR STARTING AND ENDING TIMES), AND FORECASTED PERCENTAGE OF PRECIPITATION FOR THE PROJECT. THIS INFORMATION SHALL BE PROVIDED TO THE ENGINEER ON A MONTHLY BASIS. THE COST FOR THE RAIN GAUGES IS TO BE INCLUDED IN THE UNIT BID PRICES FOR OTHER ITEMS. RAIN GAUGES SHALL BE AS SPECIFIED IN THE APPROVED TDOT RAINFALL MONITORING PLAN.
- (41) INSPECTION OF EPSC MEASURES SHALL BE DONE AT LEAST TWICE PER CALENDAR WEEK AT LEAST 72 HOURS APART. A CALENDAR WEEK IS DEFINED AS SUNDAY THROUGH SATURDAY. QUALITY ASSURANCE/QUALITY CONTROL SITE ASSESSMENT OF EPSC SHALL BE PERFORMED PER THE TDOT ENVIRONMENTAL DIVISION'S COMPREHENSIVE INSPECTION OFFICE GUIDELINES.
- (42) OUTFALL POINTS SHALL BE INSPECTED TO ASCERTAIN WHETHER EPSC MEASURES ARE EFFECTIVE IN PREVENTING SIGNIFICANT IMPACTS TO SURROUNDING WATERS, WHERE DISCHARGE LOCATIONS ARE INACCESSIBLE, NEARBY DOWNSTREAM LOCATIONS SHALL BE INSPECTED. LOCATIONS WHERE VEHICLES ENTER AND EXIT THE SITE SHALL BE INSPECTED FOR EVIDENCE OF OFF-SITE ROADWAY SEDIMENT TRACKING.
- (43) UPON CONCLUSION OF THE INSPECTIONS, EPSC MEASURES FOUND TO BE INEFFECTIVE SHALL BE REPAIRED, REPLACED, OR MODIFIED BEFORE THE NEXT RAIN EVENT, IF POSSIBLE, BUT IN NO CASE MORE THAN 24 HOURS AFTER THE INSPECTION OR WHEN THE CONDITION IS IDENTIFIED. IF THE REPAIR, REPLACEMENT OR MODIFICATION IS NOT PRACTICAL WITHIN THE TIME FRAME, WRITTEN DOCUMENTATION MUST BE PROVIDED IN THE FIELD BOOK AND AN ESTIMATED REPAIR, REPLACEMENT OR MODIFICATION SCHEDULE SHALL BE DOCUMENTED WITHIN 24 HOURS AFTER IDENTIFICATION.

MATERIALS

- (44) WASTE AND BORROW AREAS SHALL BE LOCATED IN NON-WETLAND AREAS AND ABOVE THE 100-YEAR, FEDERAL EMERGENCY MANAGEMENT AGENCY FLOODPLAIN. BORROW AND WASTE DISPOSAL AREAS SHALL NOT AFFECT ANY WATERS OF THE STATE/U.S. UNLESS THESE AREAS ARE SPECIFICALLY COVERED BY AN ARAP, 404, OR NPDES PERMIT, OBTAINED SOLELY BY THE CONTRACTOR.

SWPPP, PERMITS, PLANS, RECORDS

- (45) THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR AND OBTAIN ANY NECESSARY ENVIRONMENTAL PERMITS OR APPROVALS, INCLUDING BUT NOT LIMITED TO TDEC ARAP/401, USACE SECTION 404, TVA SECTION 26A, AND TDEC NPDES PERMITS, FROM FEDERAL, STATE AND/OR LOCAL AGENCIES REGARDING THE OPERATION OF ANY PROJECT-DEDICATED ASPHALT AND/OR CONCRETE PLANTS.

- (46) ANY DISAGREEMENT BETWEEN THE PROJECT PLANS, THE PROJECT AS CONSTRUCTED, AND THE PERMIT(S) ISSUED FOR THE PROJECT, SHALL BE BROUGHT TO THE ATTENTION OF THE TDOT PROJECT ENGINEER, THE ENVIRONMENTAL DIVISION, DESIGN DIVISION, AND HEADQUARTERS CONSTRUCTION OFFICE SHALL BE CONTACTED IN THESE INSTANCES AND DECIDE WHICH HAS PRECEDENCE AND WHETHER PERMIT OR PLANS REVISIONS ARE NEEDED. IN GENERAL, PERMIT CONDITIONS WILL PREVAIL.
- (47) THE FOLLOWING INFORMATION SHALL BE MAINTAINED ON OR NEAR THE SITE: DATES THAT MAJOR GRADING ACTIVITIES OCCUR, DATES WHERE CONSTRUCTION ACTIVITIES TEMPORARILY OR PERMANENTLY CEASE ON A PORTION OF THE SITE, DATES WHEN STABILIZATION MEASURES ARE INITIATED, EPSC INSPECTION RECORDS, QUALITY ASSURANCE SITE ASSESSMENT RECORDS, PRECIPITATION RECORDS, SWPPP, PROJECT ENVIRONMENTAL PERMITS, AND A COPY OF THE PROJECT EPSC INSPECTOR'S TDEC LEVEL 1 CERTIFICATION.
- (48) ALL WATER QUALITY AND STORM WATER PERMITS, INCLUDING A COPY OF THE NOC WITH NPDES PERMIT TRACKING NUMBER AND THE LOCATION OF THE SWPPP, SHALL BE POSTED NEAR THE MAIN ENTRANCE OF THE CONSTRUCTION SITE ACCESSIBLE TO THE PUBLIC. THE NAME, COMPANY NAME, EMAIL ADDRESS, TELEPHONE NUMBER AND ADDRESS OF THE PROJECT SITE OWNER, OPERATOR, OR A LOCAL CONTACT PERSON WITH A BRIEF DESCRIPTION OF THE PROJECT SHALL ALSO BE POSTED. IF POSTING THIS INFORMATION NEAR A MAIN ENTRANCE IS INFEASIBLE, THE INFORMATION SHALL BE PLACED IN A PUBLICLY ACCESSIBLE LOCATION NEAR WHERE THE CONSTRUCTION IS ACTIVELY UNDERWAY AND MOVED AS NECESSARY. THIS LOCATION SHALL BE POSTED AT THE CONSTRUCTION SITE. ALL POSTINGS SHALL BE MAINTAINED IN LEGIBLE CONDITION.
- (49) IF A CHANGE IN PROJECT SCOPE OCCURS DURING CONSTRUCTION, INCLUDING VALUE ENGINEERING, THE ENVIRONMENTAL DIVISION SHALL BE CONTACTED TO DETERMINE WHETHER PERMIT REVISIONS OR MODIFICATIONS OF THE SWPPP ARE NEEDED. THE DESIGN DIVISION SHALL BE CONTACTED TO DETERMINE IF ANY PLAN REVISIONS ARE NEEDED.
- (50) PROJECT INSPECTORS AND SUPERVISORS (INCLUDING TDOT STAFF, CONSULTANTS AND CONTRACTOR STAFF) RESPONSIBLE FOR THE IMPLEMENTATION AND MAINTENANCE OF EPSC PLANS SHALL SUCCESSFULLY COMPLETE THE TDEC "LEVEL 1 - FUNDAMENTALS OF EROSION PREVENTION AND SEDIMENT CONTROL FOR CONSTRUCTION SITES" COURSE AND ANY REFRESHER COURSES AS REQUIRED TO MAINTAIN CERTIFICATION. A COPY OF CERTIFICATION RECORDS FOR THE COURSE SHALL BE KEPT ON SITE AND AVAILABLE UPON REQUEST.

LITTER, DEBRIS, WASTE, PETROLEUM

- (51) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT LITTER, CONSTRUCTION DEBRIS, AND CONSTRUCTION WASTES FROM ENTERING WATERS OF THE STATE/U.S. THESE MATERIALS WILL BE PICKED UP AND REMOVED FROM STORMWATER EXPOSURE PRIOR TO ANTICIPATED STORM EVENTS. AFTER USE, MATERIALS USED FOR EPSC WILL BE REMOVED FROM THE SITE.
- (52) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS TO ENSURE THAT PETROLEUM PRODUCTS OR OTHER CHEMICAL POLLUTANTS ARE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. ALL EQUIPMENT REFUELING, SERVICING, AND STAGING AREAS SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL LAWS, RULES, REGULATIONS, AND ORDINANCES, INCLUDING THOSE OF THE NATIONAL FIRE PROTECTION ASSOCIATION (NFPA). APPROPRIATE CONTAINMENT MEASURES FOR THESE AREAS SHALL BE USED. ALL SPILLS MUST BE REPORTED TO THE APPROPRIATE AGENCY, AND MEASURES SHALL BE TAKEN IMMEDIATELY TO PREVENT THE POLLUTION OF WATERS OF THE STATE/U.S., INCLUDING GROUNDWATER, SHOULD A SPILL OCCUR.

SPECIAL NOTES

- (53) NO LESS THAN SEVEN (7) DAYS PRIOR TO THE CLOSURE OF LANES, THE CONTRACTOR SHALL NOTIFY THE FOLLOWING INDIVIDUALS OR AGENCIES COMPLETELY DESCRIBING THE AFFECTED ROADS AND THE APPROXIMATE DURATION OF THE CONSTRUCTION; THESE PARTIES INCLUDE, BUT ARE NOT LIMITED TO: (1) SMITH COUNTY SHERIFF'S DEPARTMENT, (2) SMITH COUNTY FIRE DEPARTMENT, (3) LOCAL AMBULANCE SERVICE, (4) SMITH COUNTY SCHOOL SUPERINTENDENT, (5) LOCAL POSTAL SERVICE, (6) TDOT REGION 3 TRAFFIC ENGINEERING OFFICE.

UTILITIES

- (54) PRIOR TO SUBMITTING HIS BID, THE CONTRACTOR WILL BE SOLELY RESPONSIBLE FOR CONTACTING OWNERS OF ALL AFFECTED UTILITIES IN ORDER TO DETERMINE THE EXTENT TO WHICH UTILITY RELOCATIONS AND/OR ADJUSTMENTS WILL HAVE UPON THE SCHEDULE OF WORK FOR THE PROJECT. WHILE SOME WORK MAY BE REQUIRED "AROUND" UTILITY FACILITIES THAT WILL REMAIN IN PLACE, OTHER UTILITY FACILITIES MAY NEED TO BE ADJUSTED CONCURRENTLY WITH THE CONTRACTOR'S OPERATIONS. ADVANCE CLEAR CUTTING MAY BE REQUIRED BY THE ENGINEER AT ANY LOCATION WHERE CLEARING IS CALLED FOR IN THE SPECIFICATIONS AND CLEAR CUTTING IS NECESSARY FOR A UTILITY RELOCATION. ANY ADDITIONAL COST WILL BE INCLUDED IN OTHER ITEMS BID ON.

NASHVILLE & EASTERN RAILROAD NOTES

- (55) THE CONTRACTOR SHALL NOT, FOR ANY REASON, STORE CONSTRUCTION EQUIPMENT UPON NOR STORE OR DUMP WASTE CONSTRUCTION MATERIALS UPON THE RAILROAD'S RIGHTS-OF-WAY.
- (56) THE CONTRACTOR SHALL CONDUCT HIS WORK SO AS TO PROTECT THE RAILROAD'S TRACK FACILITIES AND PROPERTY FROM DAMAGE AND ANY WORK BY THE CONTRACTOR NEAR, UP TO AND ADJACENT TO OR WITHIN 25 FEET OF THE RAILROAD'S R.O.W WILL, AT THE DISCRETION OF THE RAILROAD, REQUIRE THE PRESENCE AND PERMISSION OF AN AUTHORIZED RAILROAD WATCHMAN OR FLAGGER.
- (57) SEE SPECIAL PROVISION 105C(SL) TO SET UP FLAGGING SERVICES AND SUBMIT FOR INSURANCE REQUIRED FOR WORK AT OR NEAR, UP TO AND ADJACENT TO, OR OTHERWISE WITHIN 25 FEET OF THE RAILROAD ON THIS PROJECT.
- (58) FOR ANY WORK DONE BY THE CONTRACTOR WITH CONSTRUCTION CRANES AND/OR WITH OTHER SMALLER EQUIPMENT HAVING LIFTING BOOMS OR WORK PLATFORM BOOMS, WHERE THE BOOM LENGTH OF ANY OF THESE TYPE EQUIPMENT IS SUCH WHERE A MECHANICAL FAILURE, ACCIDENT OR OTHERWISE HAS THE POTENTIAL TO FOUL THE RAILROAD'S TRACK OR RIGHT-OF-WAY, WILL REQUIRE, PER THE DISCRETION OF THE RAILROAD, THE PRESENCE AND PERMISSION OF A FULL TIME RAILROAD WATCHMAN OR FLAGGER.
- (59) THE CONTRACTOR SHALL COORDINATE HIS WORK AT, NEAR, UP TO AND ADJACENT TO, OR WITHIN 25 FEET OF THE RAILROAD'S TRACKS WITH THE FOLLOWING REPRESENTATIVES OF THE RAILROAD:

NASHVILLE & EASTERN RAILWAY:
MR. WILLIAM J. DRUNIC, PRESIDENT
NASHVILLE & EASTERN RAILROAD CORPORATION
5138 MAIN STREET
BATTEN SHIRE BLDG.
MANCHESTER CENTER, VT 05255
PHONE: (800) 722-8725 (EXT.#1)
FAX: (802) 362-1918

MR. CRAIG WADE, VICE PRES & GENERAL MANAGER
NASHVILLE & EASTERN RAILROAD CORPORATION
514 KNOXVILLE AVENUE
LEBANON, TN 37087
PHONE: (615) 444-1567
FAX: (615) 444-4682
CELL: (615) 445-5353

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BIDDING

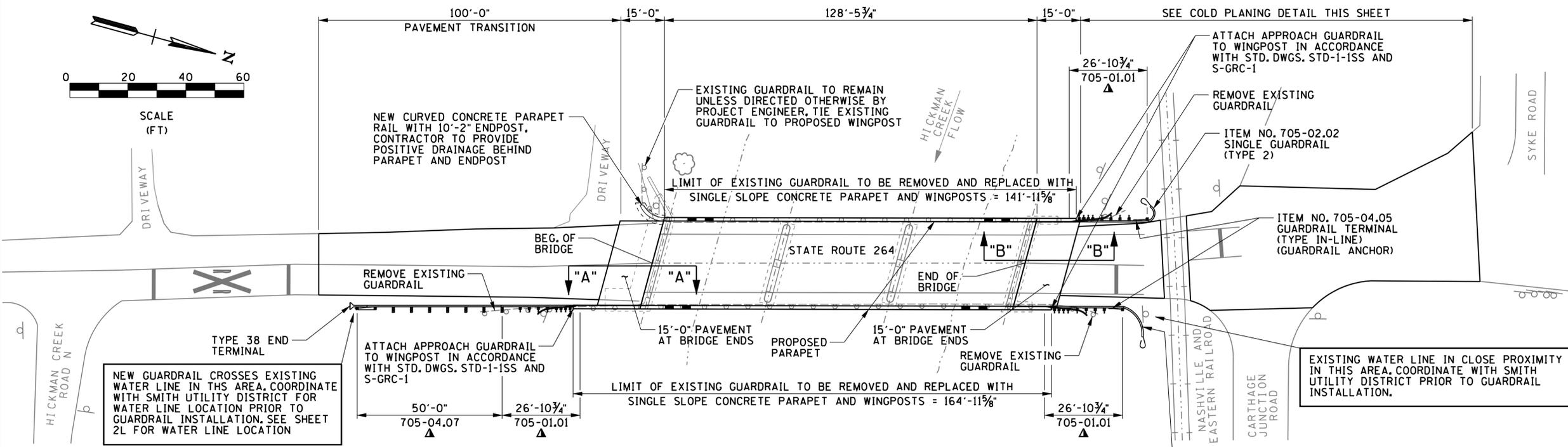
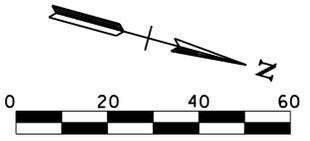


STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

BRIDGE NO. 80-SR264-2.88
STATE ROUTE 264
OVER HICKMAN CREEK
SMITH COUNTY
2014

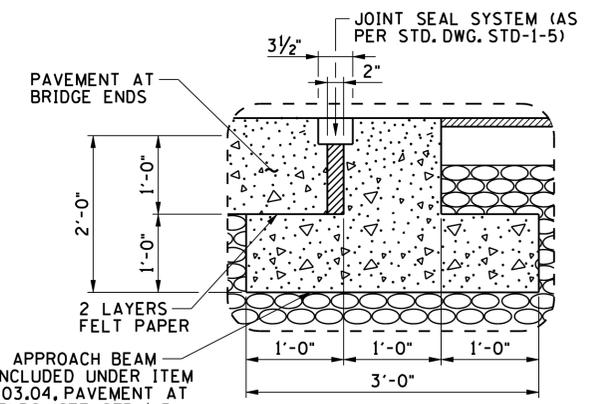
ROADWAY
GENERAL AND
SPECIAL NOTES

TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	2014	80046-4215-04	2E



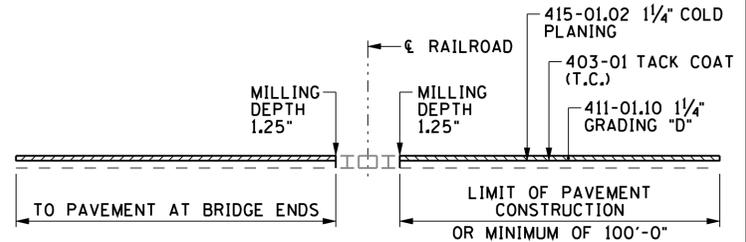
PLAN

- DENOTES: PROPOSED BRIDGE PARAPET DRAINS (8 TOTAL) SHALL BE IN ACCORDANCE WITH STD. DWG. STD-1-ISS (ITEM NO. 620-05.01)
- ▲ DENOTES: GUARDRAIL SHALL BE IN ACCORDANCE WITH STD. DWGS. S-PL-3, S-GRC-1, AND S-GRT-2
- NOTE: EXISTING GUARDRAIL THAT IS NOT TO BE RESET SHALL BE REMOVED AND RETURNED TO TDOT'S MAINTENANCE DEPARTMENT. COST FOR REMOVING EXISTING GUARDRAIL SHALL BE PAID FOR UNDER ITEM NO. 706-01, GUARDRAIL REMOVED, L.F.
- NOTE: SEE STD. DWGS. S-PL-2, S-PL-3, S-GR31-1, S-GRC-1, S-GRT-2, S-GRT-2P, AND S-GRA-3 FOR INSTALLATION DETAILS.



APPROACH BEAM DETAIL

(AT ABUTMENT NO. 1)
N.T.S.

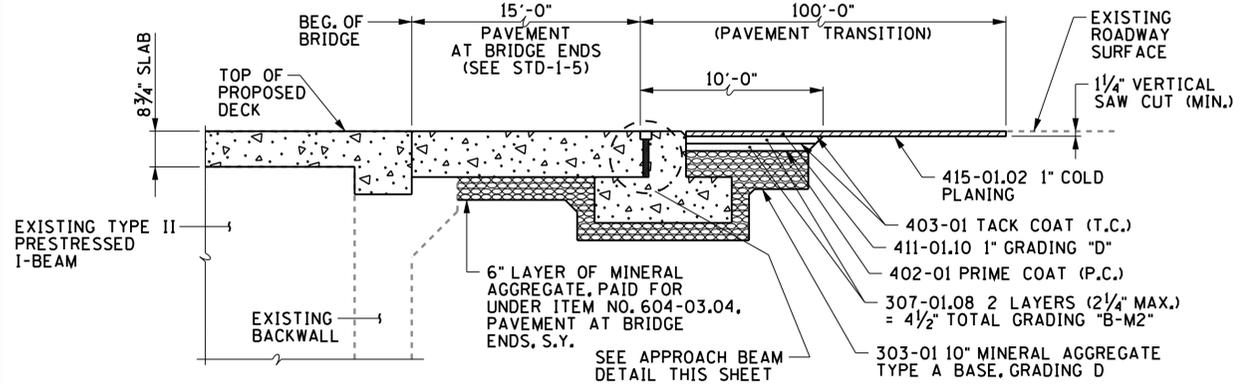


COLD PLANING DETAIL

(RAILROAD CROSSING 348935G (NERR))
N.T.S.

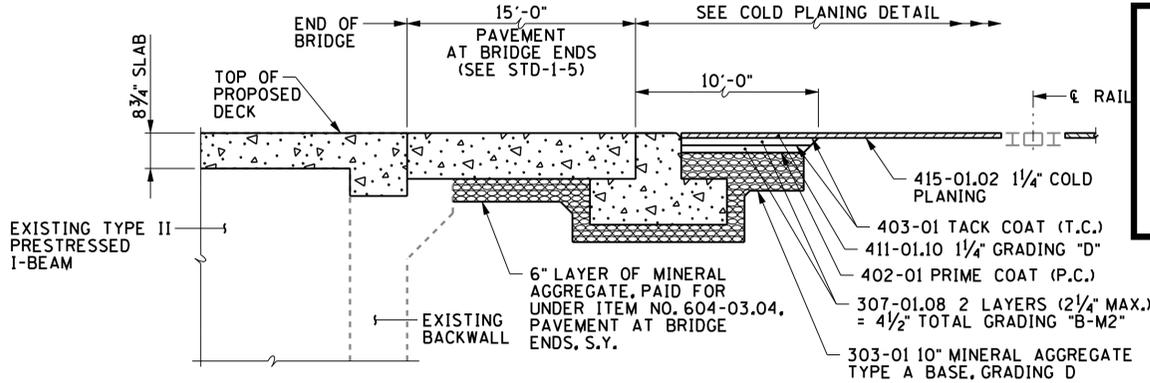
COLD PLANING AS CLOSE AS PRACTICAL OR AS DIRECTED BY THE RAILROAD TO THE FIELD SIDES OF THE RAILROAD CROSSING.

NOTE: TO CREATE A SMOOTH TRANSITION ACROSS THE RAILROAD'S CROSSING, THE SURFACE MIX SHALL BE CARRIED UP TO AND LEVEL WITH THE FIELD SIDES OF THE CROSSING, ON BOTH SIDES OF THE CROSSING.



SECTION "A-A"

(P.A.B.E AND PAVEMENT TRANSITION)
(AT ABUTMENT NO. 1)
N.T.S.



SECTION "B-B"

(P.A.B.E AND PAVEMENT TRANSITION)
(AT ABUTMENT NO. 2)
N.T.S.

UNOFFICIAL SET
NOT FOR BIDDING



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ROADWAY
DETAILS

BRIDGE NO. 80-SR264-2.88
STATE ROUTE 264
OVER HICKMAN CREEK
SMITH COUNTY
2014

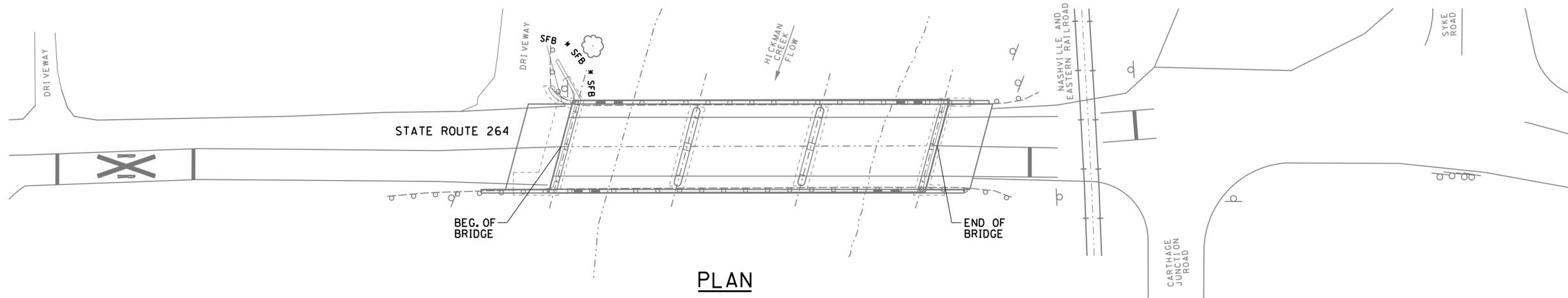
EROSION CONTROL QUANTITIES

ITEM NO.	DESCRIPTION	UNIT	QUANTITY
209-05	SEDIMENT REMOVAL	C.Y.	1
209-08.02	TEMPORARY SILT FENCE (WITH BACKING)	L.F.	35
801-01	SEEDING (WITH MULCH)	UNIT	1
801-03	WATER (SEEDING AND SODDING)	M.G.	1

EPSC LEGEND

•SFB•SFB•	SILT FENCE WITH BACKING (SFB)	EC-STR-3B
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TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	2014	80046-4215-04	2F



PLAN

- NOTE: SILT FENCE SHALL BE INSTALLED ON EXISTING GROUND CONTOURS WITH A 3' MINIMUM UPHILL TURN ON EACH END.
- NOTE: SEEDING (WITH MULCH) SHALL BE PLACED WITH A 4' MINIMUM WIDTH AFTER REMOVAL OF THE SILT FENCE.
- NOTE: ALL WORK AND EQUIPMENT TO REMAIN INSIDE EXISTING RIGHT OF WAY, WITH EXCEPTION TO ABUTMENT NO. 1 (LEFT SIDE).
- NOTE: SEE SHEET NO. 2B (ESTIMATED ROADWAY QUANTITIES) FOR FOOTNOTES TO EACH ITEM.

- NOTE: THE CONTRACTOR SHALL TAKE APPROPRIATE MEASURES BELOW THE BRIDGE DURING THE REMOVAL OF CONCRETE FROM SUBSTRUCTURES AND DECK TO PREVENT ANY DEBRIS FROM FALLING INTO THE CREEK. COST FOR CONTAINMENT WILL NOT BE PAID DIRECTLY BUT SHALL BE INCLUDED IN ITEMS BID ON.
- NOTE: THE CONTRACTOR SHALL REMOVE ANY DEBRIS THAT FALLS ONTO THE AREAS BELOW THE BRIDGE, AND ANY PAINT CHIPS SHALL BE HANDLED AND DISPOSED OF PER THE SPECIFICATIONS SET FORTH IN THE GENERAL NOTES.

EROSION PREVENTION AND SEDIMENT CONTROL SPECIAL NOTES

STREAM/WETLAND

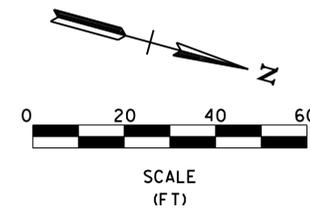
(1) A 30 FOOT NATURAL RIPARIAN BUFFER ZONE ADJACENT TO AND ON BOTH SIDES OF THE RECEIVING STREAM SHALL BE PRESERVED, TO THE MAXIMUM EXTENT PRACTICABLE, DURING CONSTRUCTION ACTIVITIES AT THE SITE. BUFFER ZONES ARE NOT SEDIMENT CONTROL MEASURES AND SHOULD NOT BE RELIED UPON AS PRIMARY SEDIMENT CONTROL MEASURES. THE RIPARIAN BUFFER ZONE SHALL BE ESTABLISHED BETWEEN THE TOP OF THE STREAM BANK AND THE DISTURBED CONSTRUCTION AREA. THE 30 FOOT CRITERION FOR THE WIDTH OF THE BUFFER ZONE CAN BE ESTABLISHED ON AN AVERAGE WIDTH BASIS AT A PROJECT, AS LONG AS THE MINIMUM WIDTH OF THE BUFFER ZONE IS MORE THAN 15 FEET AT ANY MEASURED LOCATION. EVERY ATTEMPT SHALL BE MADE FOR CONSTRUCTION ACTIVITIES NOT TO TAKE PLACE WITHIN THE BUFFER ZONES. BEST MANAGEMENT PRACTICES (BMPs) PROVIDING EQUIVALENT PROTECTION AS THE NATURAL RIPARIAN ZONE MAY BE USED. A JUSTIFICATION FOR USE AND DESIGN EQUIVALENCY SHALL BE DOCUMENTED WITHIN THE SWPPP. THE ENVIRONMENTAL AND ROADWAY DESIGN DIVISIONS SHALL REVIEW AND APPROVE THIS REVISION OF THE SWPPP BEFORE DISTURBANCE OF THE SITE PROCEEDS, UNLESS PREVIOUSLY EXEMPT IN THE NPDES CONSTRUCTION GENERAL PERMIT. WHERE ISSUED, ARAP/401 REQUIREMENTS WILL PREVAIL IF IN CONFLICT WITH THESE BUFFER ZONE REQUIREMENTS.

NPDES

- (2) NO WORK SHALL BE STARTED UNTIL THE CONTRACTOR'S PLAN FOR THE STAGING OF THEIR OPERATIONS, INCLUDING THE PLAN FOR STAGING OF TEMPORARY AND PERMANENT EPSC MEASURES, HAS BEEN ACCEPTED BY THE ENGINEER. THE CONTRACTOR'S EPSC PLAN SHALL INCORPORATE AND SUPPLEMENT, AS ACCEPTABLE, THE BASIC EPSC DEVICES ON THE EPSC PLAN, CONTAINED IN THE APPROVED SWPPP.
- (3) THE EPSC MEASURES AND/OR PLAN SHALL BE MODIFIED AS NECESSARY SO THAT THEY ARE EFFECTIVE AT ALL TIMES THROUGHOUT THE COURSE OF THE PROJECT.

- (4) THE ACCEPTED EPSC PLAN SHALL REQUIRE THAT EPSC MEASURES BE IN PLACE BEFORE CLEARING, GRUBBING, EXCAVATION, GRADING, CUTTING OR FILLING OCCURS, EXCEPT AS SUCH WORK MAY BE NECESSARY TO INSTALL EPSC MEASURES, INCLUDING WITHOUT LIMITATION AS FOLLOWS:
 - A. INITIAL CLEARING AND GRUBBING SHALL BE LIMITED TO THAT NECESSARY FOR THE INSTALLATION OF APPLICABLE EPSC MEASURES IN ACCORDANCE WITH THE ACCEPTED EPSC PLAN INCORPORATED INTO THE SWPPP.
 - B. NO OTHER CLEARING AND GRUBBING OPERATIONS SHALL BE STARTED BEFORE APPLICABLE EPSC MEASURES ARE IN PLACE IN ACCORDANCE WITH THE ACCEPTED EPSC PLAN INCORPORATED INTO THE SWPPP.
 - C. NO CULVERT OR BRIDGE CONSTRUCTION SHALL BE STARTED BEFORE APPLICABLE EPSC MEASURES ARE IN PLACE IN ACCORDANCE WITH THE ACCEPTED EPSC PLAN INCORPORATED INTO THE SWPPP.
 - D. NO GRADING, EXCAVATION, CUTTING, FILLING, OR OTHER EARTHWORK SHALL BE STARTED BEFORE EPSC MEASURES ARE IN PLACE IN ACCORDANCE WITH THE ACCEPTED EPSC PLAN INCORPORATED INTO THE SWPPP.
- (5) PERMANENT EPSC MEASURES SHALL BE INITIATED WITHIN 14 CALENDAR DAYS AFTER FINAL GRADING OF ANY SEQUENCE OR PHASE. TEMPORARY OR PERMANENT STABILIZATION SHALL BE INITIATED WITHIN 14 CALENDAR DAYS AFTER FINAL GRADING OR WHEN CONSTRUCTION ACTIVITIES ON A PORTION OF THE SITE ARE TEMPORARILY CEASED AND EARTH DISTURBING ACTIVITIES WILL NOT RESUME UNTIL AFTER 14 CALENDAR DAYS. PERMANENT STABILIZATION WITH PERENNIAL VEGETATION OR OTHER PERMANENTLY STABLE NON-ERODING SURFACE SHALL REPLACE ANY TEMPORARY MEASURES AS SOON AS PRACTICABLE. UNPACKED GRAVEL CONTAINING FINES (SILT AND CLAY SIZED PARTICLES) OR CRUSHER-RUN WILL NOT BE CONSIDERED A NON-ERODIBLE SURFACE.
- (6) EXCEPT AS OTHERWISE SPECIFIED, THERE ARE NO KNOWN SPECIAL ENVIRONMENTAL FACTORS PRESENT ON THIS PROJECT THAT INDICATE A NEED FOR SEASONAL LIMITATIONS ON THE CLEARING, GRUBBING, EXCAVATION, GRADING, CUTTING OR FILLING OPERATIONS OR ON THE TOTAL AREA OF EXPOSED SOIL.

UNOFFICIAL SET
NOT FOR BIDDING



BRIDGE NO. 80-SR264-2.88
STATE ROUTE 264
OVER HICKMAN CREEK
SMITH COUNTY
2014



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

EROSION CONTROL PLAN AND SPECIAL NOTES

TRAFFIC CONTROL QUANTITIES

ITEM NO.	DESCRIPTION	UNIT	QUANTITY	SIZE	M.U.T.C.D. NO.	REMARKS
705-08.51	PORTABLE IMPACT ATTENUATOR (NCHRP 350, TL-3)	EACH	2			
712-01	TRAFFIC CONTROL	LS	1			
712-02.02	INTERCONNECTED PORTABLE BARRIER RAIL	L.F.	100			
712-02.47	BRIDGE MOUNTED INTERCONNECTED PORTABLE BARRIER RAIL	L.F.	140			
712-04.01	FLEXIBLE DRUMS (CHANNELIZING)	EACH	42			
712-04.50	PORTABLE BARRIER RAIL DELINEATOR	EACH	9			
712-05.03	WARNING LIGHTS (TYPE C)	EACH	10			
712-06	SIGNS (CONSTRUCTION)	S.F.	1,165			
712-06.10	NEW SIGNS (CONSTRUCTION)	S.F.	23			
712-09.01	REMOVABLE PAVEMENT MARKING LINE	L.F.	660			
712-09.04	REMOVABLE PAVEMENT MARKING (STOP LINE)	L.F.	48			
713-16.01	CHANGEABLE MESSAGE SIGN	EACH	1			
713-16.09	RAILROAD ADVANCE WARNING SIGN AND SUPPORT	EACH	2			
716-05.01	PAINTED PAVEMENT MARKING (4" LINE)	L.M.	1.20			
716-05.05	PAINTED PAVEMENT MARKING (STOP LINE)	L.F.	48			
716-05.10	PAINTED PAVEMENT MARKING (RXR)	EACH	2			
717-01	MOBILIZATION	LS	1			
730-40	TEMPORARY TRAFFIC SIGNAL SYSTEM	EACH	1			

ITEM NO. 712-06 SIGNS (CONSTRUCTION)

	DESCRIPTION	QUANTITY	ITEM NO. 712-06 (S.F.)	SIZE	M.U.T.C.D. NO.	REMARKS
	END ROAD WORK	7	56	48" x 24"	G20-2A	
	STOP HERE ON RED	2	12	36" x 24"	R10-6	
	STAY IN LANE TO EXTEND GREEN	1	6	36" x 24"	R10-6(MOD.)	
	NO TURN ON RED	3	18	36" x 24"	R10-11A	
	LANE SHIFT RIGHT	1	9	36" x 36"	W1-4AR	
	SIGNAL AHEAD	5	45	36" x 36"	W3-3	
	BE PREPARED TO STOP	2	18	36" x 36"	W3-4	
	ROAD WORK AHEAD	5	45	36" x 36"	W20-1	
	ROAD WORK 500 FT	2	18	36" x 36"	W20-1	
	ROAD WORK 1000 FT	2	18	36" x 36"	W20-1	
	ROAD WORK 1500 FT	2	18	36" x 36"	W20-1	
	ROAD WORK 1/2 MILE	2	18	36" x 36"	W20-1	
	ONE LANE ROAD 1500 FT	2	18	36" x 36"	W20-4	
	FLAGGER LOGO SIGN	2	18	36" x 36"	W20-7A	
	MAX. XX MINUTE RED	5	70	42" x 48"	SPECIAL	
	MAINTAIN XX M.P.H. SPEED	2	21	42" x 36"	SPECIAL	
SHT 2H-TOTAL			408			
	NORTH	19	38	24" x 12"	M3-1	
	SOUTH	21	42	24" x 12"	M3-3	
	DETOUR	40	80	24" x 12"	M4-8	
	END DETOUR	2	6	24" x 18"	M4-8A	
	ADVANCE ARROW LT	2	5	21" x 15"	M5-1L	
	DIRECTIONAL ARROW LT	3	7	21" x 15"	M6-1L	
	DIRECTIONAL ARROW RT	5	11	21" x 15"	M6-1R	
	DIRECTION ARROW 45 RT	2	5	21" x 15"	M6-2R	
	DIRECTION ARROW 90	28	62	21" x 15"	M6-3	
	DETOUR 1000 FT	4	36	36" x 36"	W20-2	
	DETOUR 2000 FT	2	18	36" x 36"	W20-2	
	SPECIAL	35	315	36" x 36"	SP-1	
	SPECIAL	5	132	90" x 42"	SP-2	
DETOUR TOTAL			757			

NOTE: SEE SHEET NO. 2B (ESTIMATED ROADWAY QUANTITIES) FOR FOOTNOTES TO EACH ITEM.

NOTE: SEE SHEET NO. 2J (WIDE LOAD DETOUR) FOR SPECIAL NOTES AND APPROXIMATE SIGN PLACEMENT.

TRAFFIC CONTROL SPECIAL NOTES

- (1) THESE TRAFFIC CONTROL PLANS DO NOT RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY OF INSTALLING TRAFFIC CONTROL DEVICES IN ACCORDANCE WITH THE CURRENT EDITION OF THE "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES."
- (2) EACH PHASE OF THE SUGGESTED SEQUENCE OF CONSTRUCTION FOR WHICH THESE TRAFFIC CONTROL PLANS WERE DESIGNED, REQUIRES THE TIMELY COMPLETION OF THE PRECEDING PHASE. ANY VARIATIONS IN THE PROPOSED PHASING SHALL REQUIRE A REVIEW AND APPROVAL OF THE SIGNING AND TRAFFIC CONTROL DEVICES BY THE ENGINEER.
- (3) THE CONTRACTOR IS REQUIRED TO PROVIDE LANE SHIFTS WHERE NECESSARY TO ROUTE TRAFFIC AROUND CONSTRUCTION.
- (4) NO TRAFFIC SHALL BE DETOURED OR ROADWAY CLOSED, ABANDONED OR REMOVED WITHOUT THE PRIOR APPROVAL OF THE ENGINEER.
- (5) CONSTRUCTION SIGNING SHOWN IN THESE PLANS IS TO SERVE AS A GUIDE ONLY. OTHER SIGNS MAY BE REQUIRED DURING VARIOUS PHASES OF CONSTRUCTION.
- (6) PERMANENT SIGNS AND PERMANENT PAVEMENT MARKINGS SHALL BE IN PLACE BEFORE COMPLETED ROADWAYS ARE OPEN TO TRAFFIC.
- (7) TYPE "C" WARNING LIGHTS SHALL BE USED ON ALL CHANNELIZING DRUMS IN TAPERS ON STATE ROUTE 264.
- (8) THE CONTRACTOR SHALL MAINTAIN ALL TRAFFIC CONTROL DEVICES IN PROPER CONDITION THROUGHOUT THE DURATION OF THE PROJECT.
- (9) EXISTING SIGNS THAT CONFLICT WITH TEMPORARY TRAFFIC CONTROL SIGNING SHALL BE COVERED OR REMOVED AND STOCKPILED AS DIRECTED BY THE ENGINEER.
- (10) THE CONTRACTOR WILL BE REQUIRED TO HAVE A RESPONSIBLE PERSON ON CALL (WITHIN ONE HOUR), AT ALL TIMES, FOR TRAFFIC CONTROL DURING THE CONSTRUCTION OF THIS PROJECT.
- (11) AT ALL TIMES THE ROADWAY MUST BE MAINTAINED IN A CONDITION TO ALLOW THE PASSAGE OF EMERGENCY VEHICLES.
- (12) FOR TRAFFIC CONTROL DETAILS, REFER TO STD. DWG. NOS. T-WZ-10, T-WZ-11, T-WZ-16, AND T-RR-6 THROUGHOUT THE DURATION OF THE PROJECT.
- (13) IF ADDITIONAL SIGNS ARE DEEMED NECESSARY BY THE ENGINEER, THEY SHALL BE FURNISHED AND INSTALLED AT THE BID PRICE FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION), S.F.
- (14) ALL LOCAL EMERGENCY AGENCIES AND RESIDENCES WITHIN THE IMMEDIATE AREA PROJECT AREA SHALL BE NOTIFIED NOT LESS THAN 48 HOURS IN ADVANCE OF BRIDGE CONSTRUCTION WHICH MAY AFFECT ACCESS TO THESE AREAS.
- (15) NO LESS THAN SEVEN (7) DAYS PRIOR TO THE CLOSURE OF LANES, THE CONTRACTOR SHALL NOTIFY THE FOLLOWING INDIVIDUALS OR AGENCIES COMPLETELY DESCRIBING THE AFFECTED ROADS AND THE APPROXIMATE DURATION OF THE CONSTRUCTION; THESE PARTIES INCLUDE, BUT ARE NOT LIMITED TO: (1) SMITH COUNTY SHERIFF'S DEPARTMENT, (2) SMITH COUNTY FIRE DEPARTMENT, (3) LOCAL AMBULANCE SERVICE, (4) SMITH COUNTY SCHOOL SUPERINTENDENT, (5) LOCAL POST OFFICE, (6) TDOT REGION 3 TRAFFIC ENGINEERING OFFICE.
- (16) BEFORE OPENING THE LANE SHIFTS TO TRAFFIC, THE TRANSITIONAL MARKINGS ON THE EXISTING ROADWAY MUST BE IN PLACE. THESE MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-05.01, PAINTED PAVEMENT MARKING (4" LINE), L.M. ALL EXISTING MARKINGS IN THE AREA OF THESE TRANSITIONAL MARKINGS SHALL BE OBLITERATED TO ELIMINATE CONFLICTING MARKINGS. REMOVAL OF THE EXISTING CONFLICTING MARKINGS WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COST WILL BE INCLUDED IN ITEM NO. 712-01, TRAFFIC CONTROL, LS.

ITEM NO. 712-06.10 NEW SIGNS (CONSTRUCTION)

	DESCRIPTION	QUANTITY	ITEM NO. 712-06.10 (S.F.)	SIZE	M.U.T.C.D. NO.	REMARKS
	YIELD	2	8	36"x36"x36"	R1-2	
	RAILROAD TRACKS	2	6	48" x 9"	R15-1	
	RR XING LT AT SIDEROAD	1	9	36" x 36"	W10-3L	
SHT 2J-TOTAL			23			

TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	2014	80046-4215-04	26

UNOFFICIAL SET

NOT FOR BIDDING

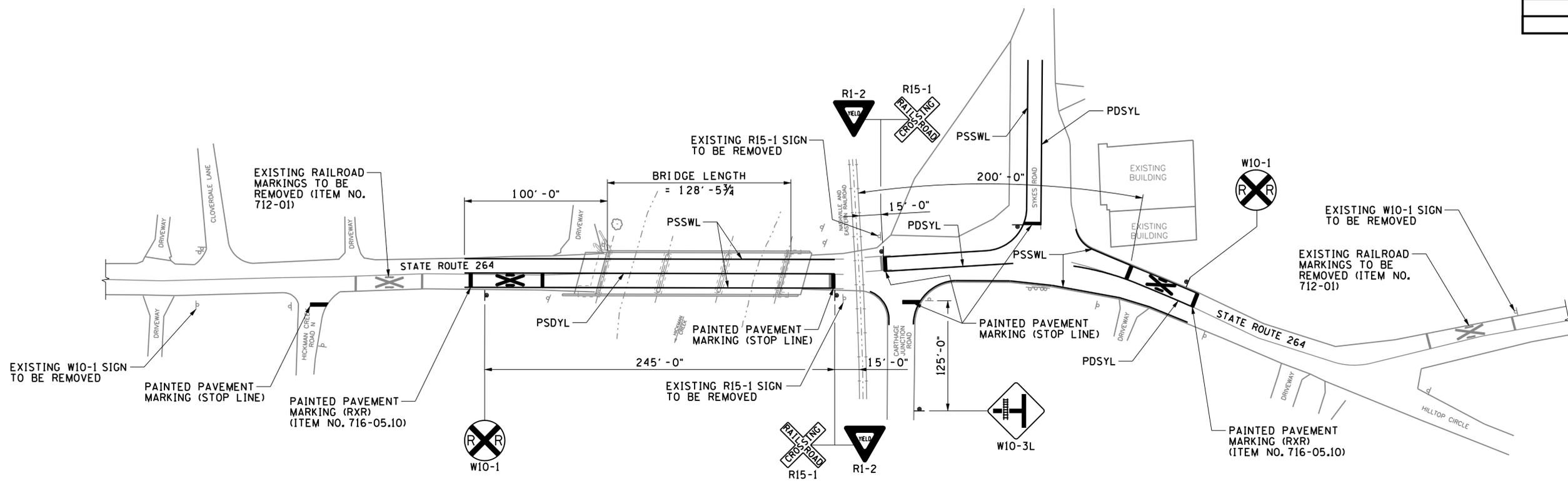


STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL
SPECIAL NOTES
AND
QUANTITIES

BRIDGE NO. 80-SR264-2.88
STATE ROUTE 264
OVER HICKMAN CREEK
SMITH COUNTY
2014

TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	2014	80046-4215-04	2J



FINAL STRIPING

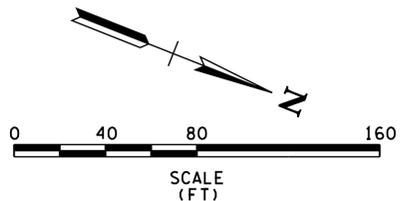
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NOT FOR BIDDING



TRAFFIC CONTROL LEGEND	
	SIGN (CONSTRUCTION)
	PAINTED SINGLE SOLID WHITE LINE
	PAINTED DOUBLE SOLID YELLOW LINE

SPECIAL NOTES

1. FINAL STRIPING AND SIGNING SHALL BE IN ACCORDANCE WITH STD. DWG. T-RR-6.
2. REMOVAL OF EXISTING SIGNS SHALL BE PAID FOR UNDER ITEM NO. 713-15, REMOVAL OF SIGNS, POSTS, AND FOOTINGS, LUMP SUM. NEW SIGNS SHALL BE PAID FOR UNDER ITEM NO. 712-06.10, NEW SIGNS (CONSTRUCTION), PER S.F.
3. AFTER COMPLETION OF PHASE II CONSTRUCTION, ANY EXISTING PAVEMENT MARKINGS SHALL BE REMOVED BEFORE PLACEMENT OF FINAL STRIPING.

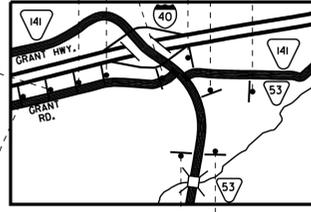
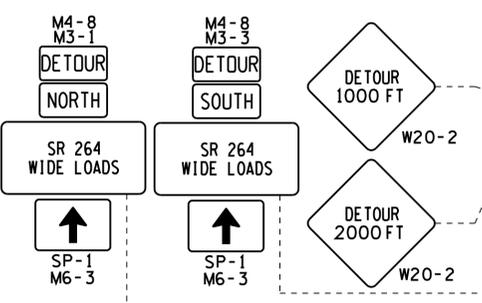
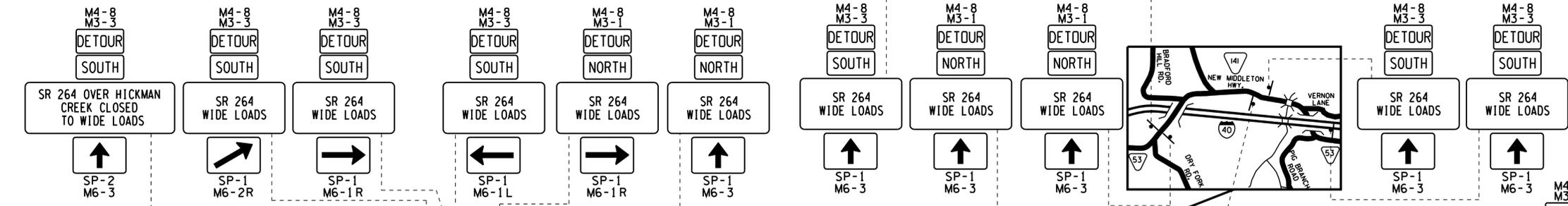


BRIDGE NO. 80-SR264-2.88
STATE ROUTE 264
OVER HICKMAN CREEK
SMITH COUNTY
2014

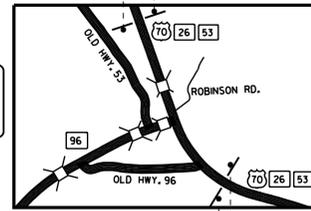
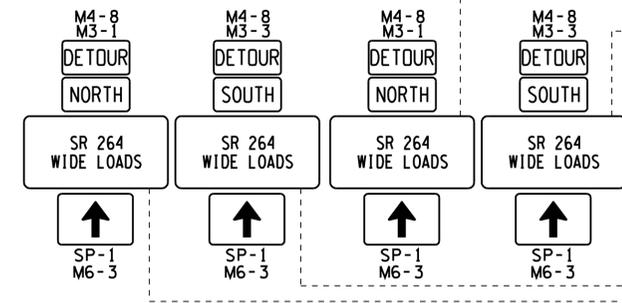
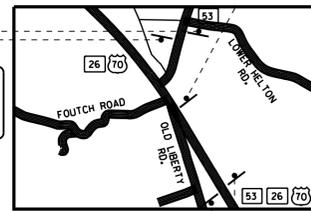
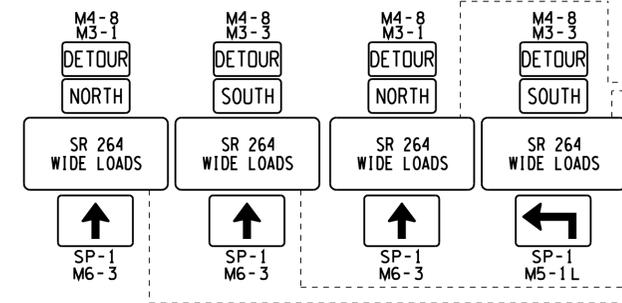
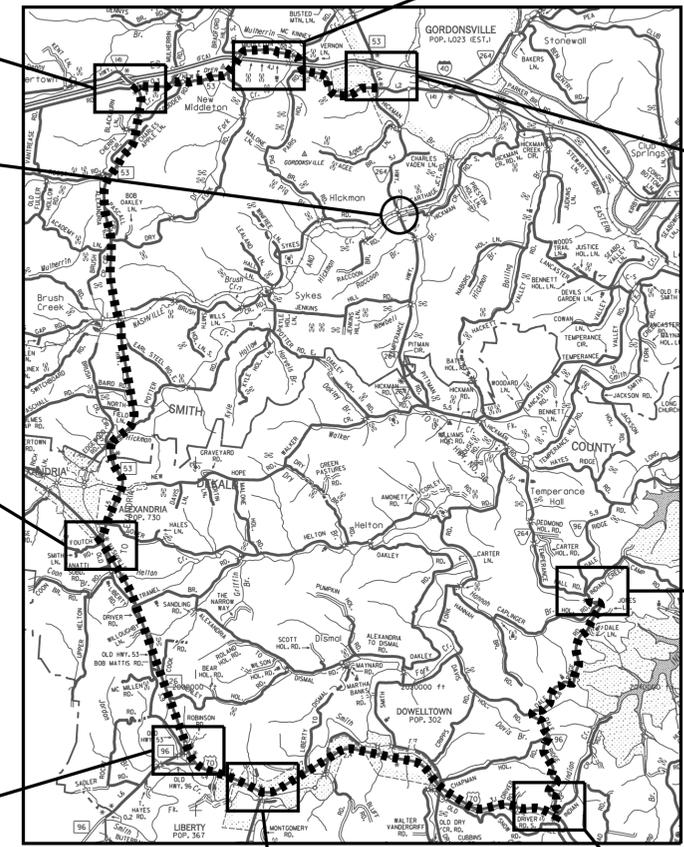
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
**TRAFFIC CONTROL PLAN
FINAL STRIPING**

TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	2014	80046-4215-04	2K

TOTAL DETOUR DISTANCE: 28 MILES

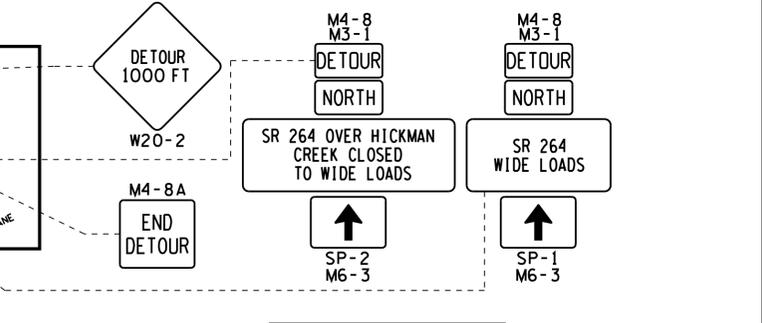
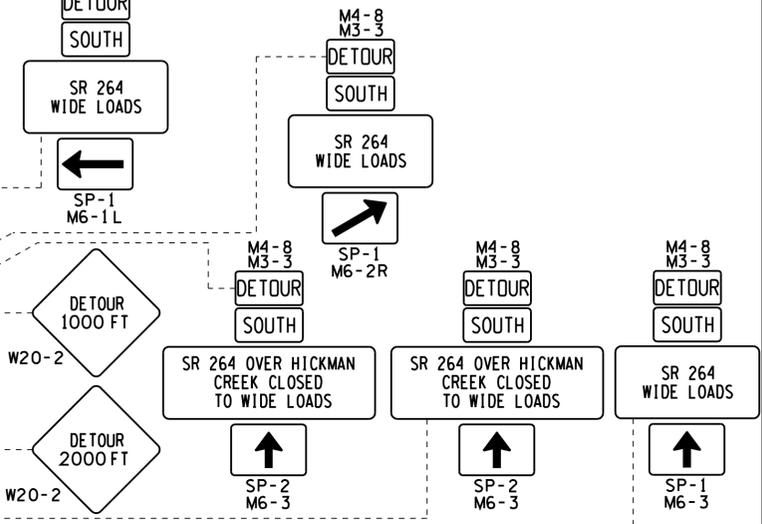
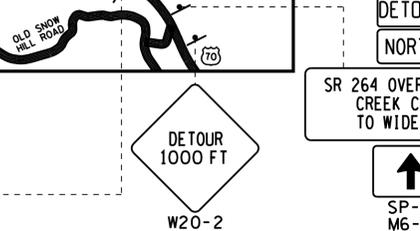
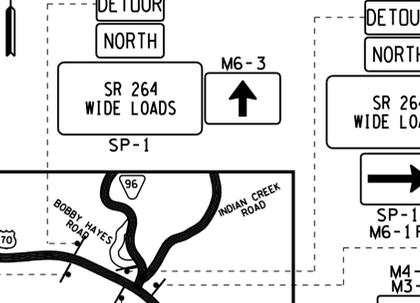
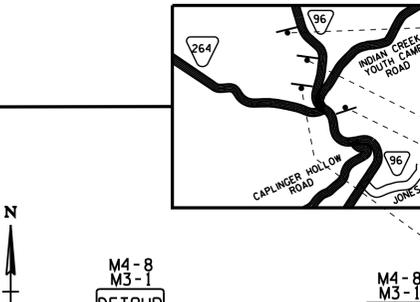
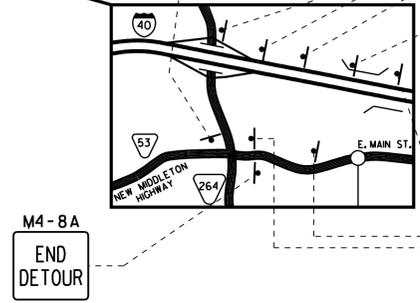
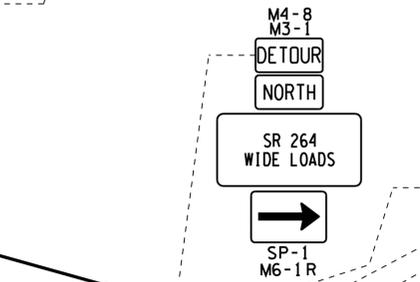
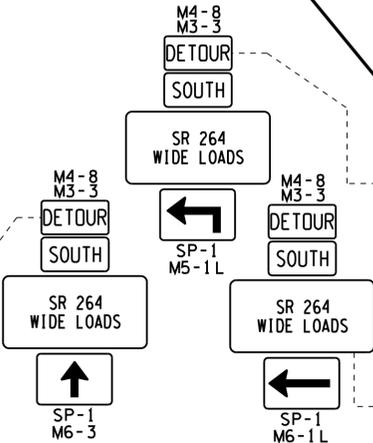
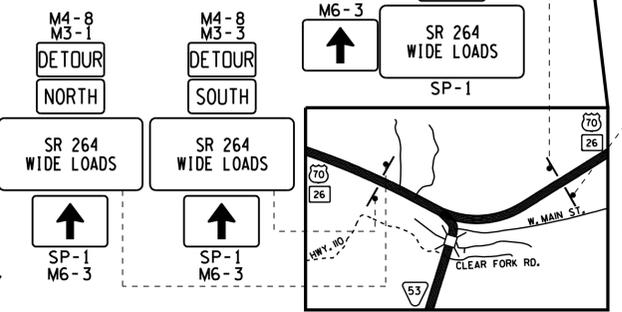


PROJECT LOCATION



SPECIAL NOTES

1. THESE TRAFFIC CONTROL PLANS DO NOT RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY OF INSTALLING TRAFFIC CONTROL DEVICES IN ACCORDANCE WITH THE CURRENT EDITION OF THE "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES."
2. CONSTRUCTION SIGNING SHOWN IN THESE PLANS IS TO SERVE AS A GUIDE ONLY. OTHER SIGNS MAY BE REQUIRED DURING VARIOUS PHASES OF CONSTRUCTION.
3. CONTRACTOR IS RESPONSIBLE FOR LOCATING UTILITIES PRIOR TO INSTALLING POST MOUNTED SIGNS.
4. CONTRACTOR SHALL RETURN GROUND TO ORIGINAL DISPOSITION UPON SIGN REMOVAL.
5. CONTRACTOR SHALL TEMPORARILY COVER ANY EXISTING SIGNS THAT MAY CONFLICT WITH THE TEMPORARY DETOUR SIGNS.



UNOFFICIAL SET
NOT FOR BIDDING

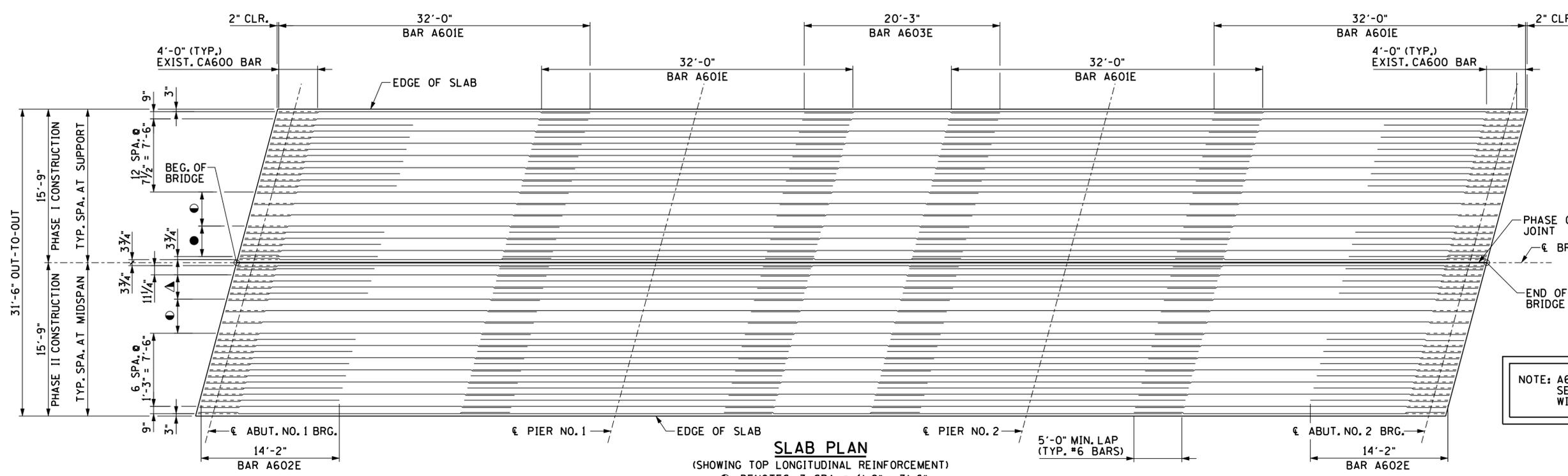


BRIDGE NO. 80-SR264-2.88
STATE ROUTE 264
OVER HICKMAN CREEK
SMITH COUNTY
2014

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL
WIDE LOAD DETOUR

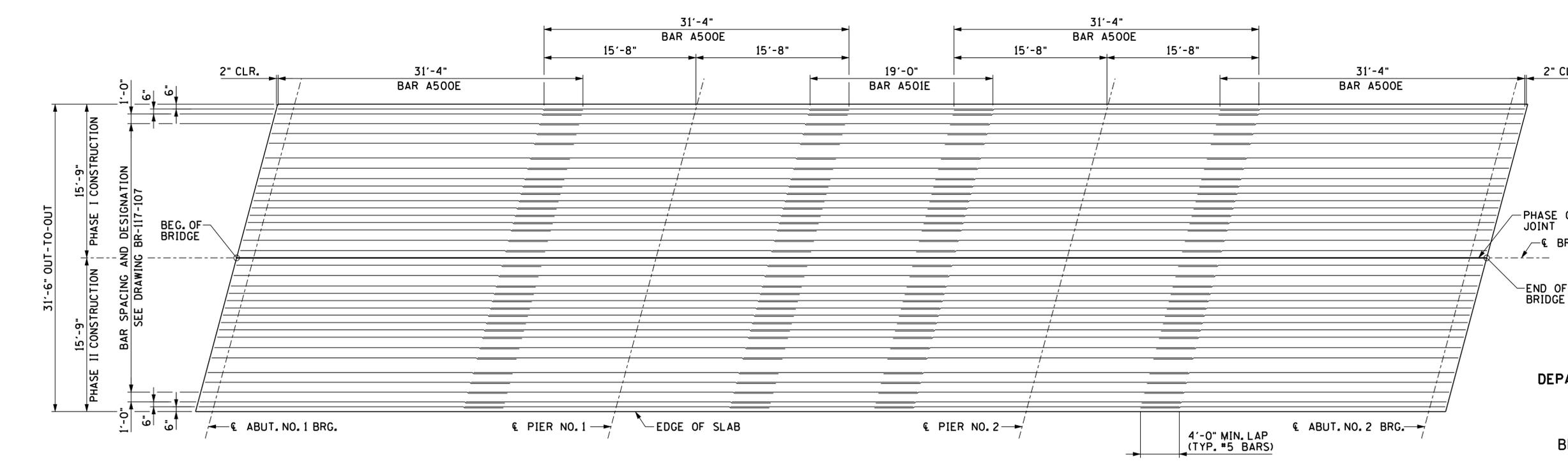
CONST. NO.			
PROJECT NO.	YEAR	SHEET NO.	
80046-4215-04	2014		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



SLAB PLAN
(SHOWING TOP LONGITUDINAL REINFORCEMENT)

- DENOTES: 3 SPA. @ 1'-2" = 3'-6"
- DENOTES: 2 SPA. @ 1'-3" = 2'-6"
- ▲ DENOTES: 5 SPA. @ 7/2' = 3'-1/2"

NOTE: A604E BAR NOT SHOWN FOR CLARITY. SEE BR-117-107 FOR A604E BAR TO SPLICE WITH TOP LONGITUDINAL REINFORCEMENT.



SLAB PLAN
(SHOWING BOTTOM LONGITUDINAL REINFORCEMENT)

UNOFFICIAL SET
NOT FOR BIDDING

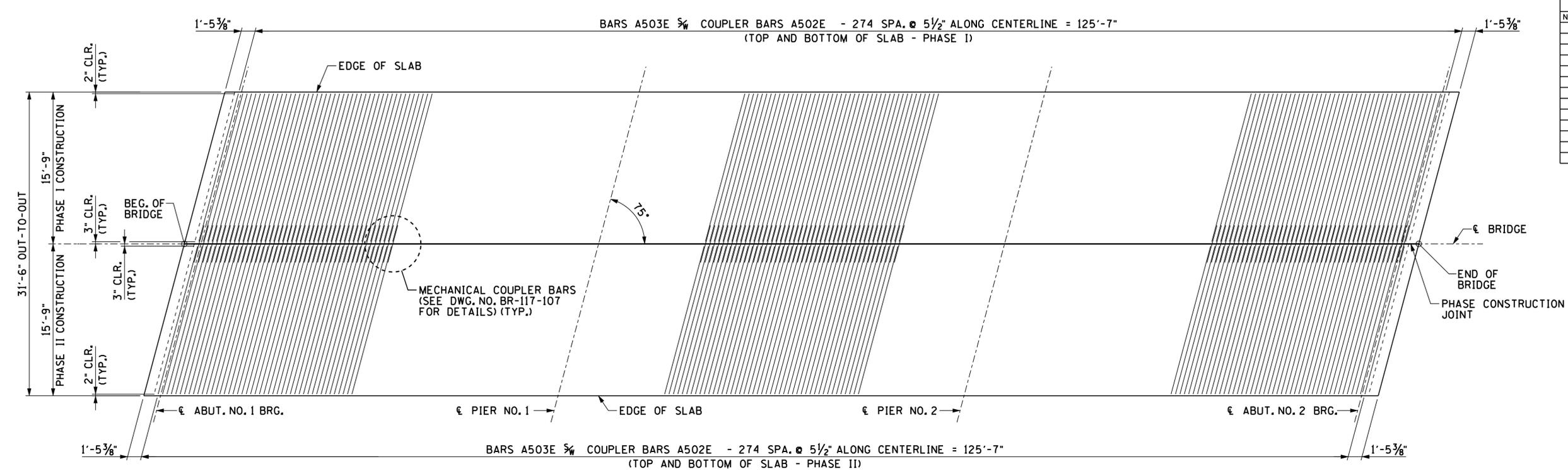


STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
SUPERSTRUCTURE
DETAILS
BRIDGE NO. 80-SR264-2.88
STATE ROUTE 264
OVER HICKMAN CREEK
SMITH COUNTY
2014

DESIGNED BY DAVID THOMPSON
DRAWN BY ANGELA MOORE
SUPERVISED BY DARRELL JAMES
CHECKED BY JAMIE GILLESPIE



CONST. NO.			
PROJECT NO.	YEAR	SHEET NO.	
80046-4215-04	2014		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



SLAB PLAN
(SHOWING TOP AND BOTTOM TRANSVERSE REINFORCEMENT)

UNOFFICIAL SET
NOT FOR BIDDING



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
SUPERSTRUCTURE DETAILS
BRIDGE NO. 80-SR264-2.88
STATE ROUTE 264
OVER HICKMAN CREEK
SMITH COUNTY
2014

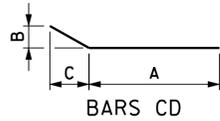
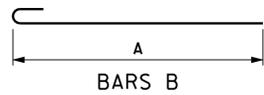
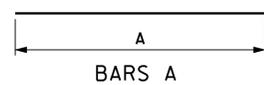
DESIGNED BY DAVID THOMPSON DATE _____
DRAWN BY ANGELA MOORE DATE _____
SUPERVISED BY DARRELL JAMES DATE _____
CHECKED BY JAMIE GILLESPIE DATE _____



CONST. NO. _____			
PROJECT NO.	YEAR	SHEET NO.	
80046-4215-04	2014		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

PHASE I								
SUPERSTRUCTURE - EPOXY								
BAR	LOCATION	SIZE	NO. REQ'D.	BENDING DIMENSIONS				LENGTH
				A	B	C	D	
A500E	SLAB (BOTTOM)	5	68	31'-4"				31'-4"
A501E	SLAB (BOTTOM)	5	17	19'-0"				19'-0"
A502E	COUPLER BARS (SLAB)	5	550	3'-0"				3'-0"
A503E	SLAB (TOP AND BOTTOM)	5	550	15'-11"				15'-11"
A601E	SLAB (TOP)	6	56	32'-0"				32'-0"
A602E	SLAB (TOP)	6	18	14'-2"				14'-2"
A603E	SLAB (TOP)	6	14	20'-3"				20'-3"
A604E	DECK AND P.A.B.E.	6	46	8'-8"				8'-8"
B570E	PARAPET SUPPORT	5	386	2'-8"				3'-3"
ABUTMENT NO. 1 - EPOXY								
CD600E	ROADWAY BRACKET AT P.A.B.E.	6	15	1'-4"	1'-9"	1'-0"		3'-4"
ABUTMENT NO. 2 - EPOXY								
CD600E	ROADWAY BRACKET AT P.A.B.E.	6	15	1'-4"	1'-9"	1'-0"		3'-4"

PHASE II								
SUPERSTRUCTURE - EPOXY								
BAR	LOCATION	SIZE	NO. REQ'D.	BENDING DIMENSIONS				LENGTH
				A	B	C	D	
A500E	SLAB (BOTTOM)	5	68	31'-4"				31'-4"
A501E	SLAB (BOTTOM)	5	17	19'-0"				19'-0"
A502E	COUPLER BARS (SLAB)	5	550	3'-0"				3'-0"
A503E	SLAB (TOP AND BOTTOM)	5	550	15'-11"				15'-11"
A601E	SLAB (TOP)	6	56	32'-0"				32'-0"
A602E	SLAB (TOP)	6	18	14'-2"				14'-2"
A603E	SLAB (TOP)	6	14	20'-3"				20'-3"
A604E	DECK AND P.A.B.E.	6	46	8'-8"				8'-8"
B570E	PARAPET SUPPORT	5	386	2'-8"				3'-3"
ABUTMENT NO. 1 - EPOXY								
CD600E	ROADWAY BRACKET AT P.A.B.E.	6	15	1'-4"	1'-9"	1'-0"		3'-4"
ABUTMENT NO. 2 - EPOXY								
CD600E	ROADWAY BRACKET AT P.A.B.E.	6	15	1'-4"	1'-9"	1'-0"		3'-4"



UNOFFICIAL SET
 NOT FOR BIDDING



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

BILL OF STEEL

BRIDGE NO. 80-SR264-2.88
STATE ROUTE 264
OVER HICKMAN CREEK
SMITH COUNTY
2014

DESIGNED BY DAVID THOMPSON DATE _____
 DRAWN BY ANGELA MOORE DATE _____
 SUPERVISED BY DARRELL JAMES DATE _____
 CHECKED BY JAMIE GILLESPIE DATE _____

