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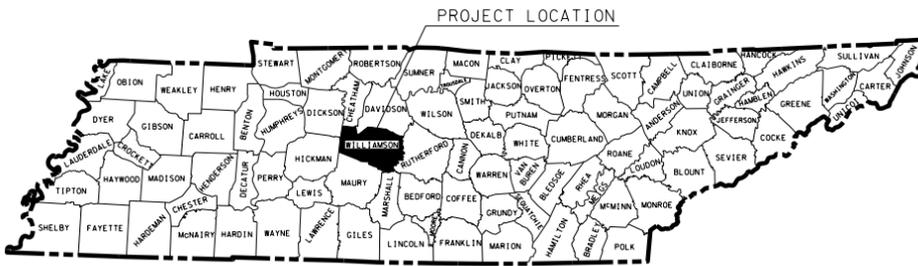
"NO PROJECT COMMITMENTS SHEET INCLUDED IN THIS SET OF PLANS."  
 "NO UTILITY SHEETS"

STATE OF TENNESSEE  
 DEPARTMENT OF TRANSPORTATION  
 BUREAU OF ENGINEERING

TENN.	YEAR	SHEET NO.
	2015	1
FED. AID PROJ. NO.	STP/HSIP-96(42)	
STATE PROJ. NO.	94011-3253-94	
STATE PROJ. NO.	94011-8253-14	

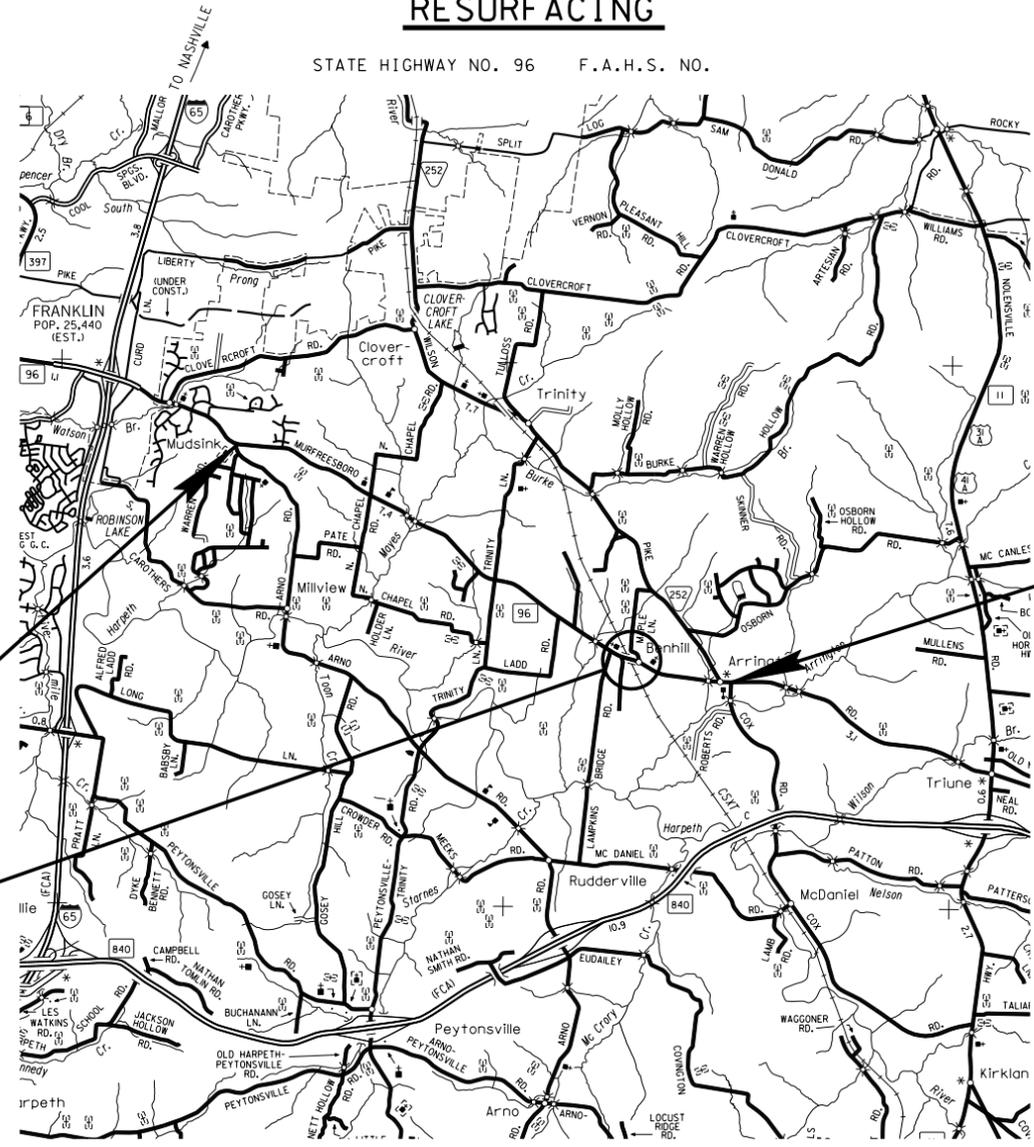
**WILLIAMSON COUNTY**

STATE ROUTE NO. 96  
 FROM ARNO ROAD (L.M. 14.66)  
 TO COX ROAD (L.M. 20.62)  
**RESURFACING**



**STANDARD ROADWAY DRAWINGS**

DWG. NO.	REV.	DESCRIPTION
<b>ROADWAY DESIGN STANDARDS</b>		
RD-A-1	12-18-99	STANDARD ABBREVIATIONS
RD-L-1	10-26-94	STANDARD LEGEND
<b>SAFETY APPURTENANCES AND FENCE</b>		
S-GRT-2	11-03-14	TYPE 38 GUARDRAIL TERMINAL
<b>TRAFFIC CONTROL APPURTENANCES</b>		
T-FAB-1	05-27-97	FLASHING YELLOW ARROW BOARD
T-M-1	07-24-14	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS AND MARKING ABBREVIATIONS
T-M-2	07-24-14	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS
T-M-3	07-24-14	MARKING STANDARDS FOR TRAFFIC ISLANDS, MEDIANS & PAVED SHOULDERS ON CONVENTIONAL ROADS
T-M-4	07-24-14	STANDARD INTERSECTION PAVEMENT MARKINGS
T-S-9	06-10-14	STANDARD LAYOUT GROUND MOUNTED SIGNS
T-S-10	04-04-12	STANDARD MOUNTING DETAILS FLAT SHEET SIGNS ALUMINUM-STEEL DESIGN
T-S-16	06-05-14	GROUND MOUNTED ROADSIDE SIGN AND DETAILS
T-S-16A		GROUND MOUNTED ROADSIDE SIGN PLACEMENT DETAILS
T-SG-2	07-29-04	LOOP LEAD-INS, CONDUIT AND PULL BOXES
T-SG-3	11-11-04	STANDARD NOTES AND DETAILS OF INDUCTIVE LOOPS
T-WZ-10	04-02-12	ADVANCE ROAD WORK SIGNING ON HIGHWAYS AND FREEWAYS



STATE HIGHWAY NO. 96 F.A.H.S. NO.

SCALE: 1" = 1 MILE



EXCLUSION	
DESCRIPTION	LENGTH (FT.)
C.S.X. BRIDGE (L.M. 19.47)	159'
<b>TOTAL =</b>	<b>159'</b>

94011-3253-94  
 94011-8253-14  
 END PROJ. NO. STP/HSIP-96(42)  
 L.M. 20.62

94011-3253-94  
 94011-8253-14  
 BEGIN PROJ. NO. STP/HSIP-96(42)  
 L.M. 14.66

**C.S.X. OVERHEAD CROSSING**  
 #350363H (94SR0960023)

**SPECIAL NOTES**

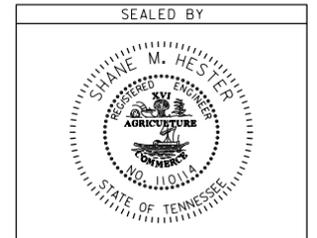
PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED JANUARY 1, 2015 AND ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS AND IN THE PROPOSAL CONTRACT.

TRANS. PROJ. SPEC. SUPVR. 2 BILLY BINION  
 DESIGNER DARRELL GRAY CHECKED BY RUSTY BOGUSKIE  
 P.E. NO. 98034-4299-04 (Design)  
 PIN NO. 120928.00

PROJECT LENGTH **5.93 MILES**  
 TOTAL LANE MILES RESURFACED **13.4 MILES**

**UNOFFICIAL SET**  
 NOT FOR BIDDING



APPROVED: Paul D. Degges  
 PAUL D. DEGGES, CHIEF ENGINEER

DATE: \_\_\_\_\_  
 APPROVED: John Schroer  
 JOHN SCHROER, COMMISSIONER

TRAFFIC DATA	
ADT (2015)	14,650
V	45-55 MPH

U.S. DEPARTMENT OF TRANSPORTATION  
 FEDERAL HIGHWAY ADMINISTRATION  
 APPROVED: \_\_\_\_\_  
 DIVISION ADMINISTRATOR DATE

13-JAN-2015 11:47 \\j03wfol1\001-state-tn-us\03shored\SURVEY\DESIGN\Resurfacing Plans\2015\WilliamsonSR96 DG\01-001\TheSheet.dgn

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2015	STP/HSIP-96(42)	2

ESTIMATED ROADWAY QUANTITIES					
ITEM NO.	DESCRIPTION	UNIT	QUANTITY		TOTAL QUANTITY
			94011-8253-14	94011-3253-94	
1	303-01 MINERAL AGGREGATE, TYPE A BASE, GRADING D	TON		75	75
2	307-01.08 ASPHALT CONCRETE MIX (PG64-22) (BPMB-HM) GRADING B-M2	TON	500		500
	403-01 BITUMINOUS MATERIAL FOR TACK COAT (TC)	TON	58		58
	403-05.01 BITUMINOUS MATERIAL (FOG SEAL) SHOULDER	TON	1.5		1.5
3,4,5	411-02.10 ACS MIX(PG70-22) GRADING D	TON	8844		8844
	415-01.01 COLD PLANING BITUMINOUS PAVEMENT	TON	8762		8762
	705-04.07 TAN ENERGY ABSORBING TERM (NCHRP 350, TL3)	EACH		3	3
	706-01 GUARDRAIL REMOVED	L.F.		150	150
6	712-01 TRAFFIC CONTROL	LS	1		1
	712-04.01 FLEXIBLE DRUMS (CHANNELIZING)	EACH	55		55
	712-05.01 WARNING LIGHTS (TYPE A)	EACH	4		4
7	712-06 SIGNS (CONSTRUCTION)	S.F.	668		668
	712-08.03 ARROW BOARD (TYPE C)	EACH	1		1
8	713-16.20 SIGNS (12"X36" OBJECT MARKER W/POST )	EACH		16	16
9	713-16.50 REMOVE AND REPLACE SIGN (STOP W/POST)	EACH		2	2
	716-01.21 SNWPLWBLE PVMT MRKRS (BI-DIR)(1 COLOR)	EACH		523	523
	716-01.22 SNWPLWBLE PVMT MRKRS (MONO-DIR)(1 COLOR)	EACH		50	50
10	716-02.04 PLASTIC PAVEMENT MARKING(CHANNELIZATION STRIPING)	S.Y.	130		130
10	716-02.05 PLASTIC PAVEMENT MARKING (STOP LINE)	L.F.	84		84
10	716-02.06 PLASTIC PAVEMENT MARKING (TURN LANE ARROW)	EACH	19		19
10	716-03.03 PLASTIC WORD PAVEMENT MARKING (STOP AHEAD)	EACH	2		2
11	716-05.01 PAINTED PAVEMENT MARKING (4" LINE)	L.M.		19	19
12	716-12.02 ENHANCED FLATLINE THERMO PVMT MRKNG (6IN LINE)	L.M.		12	12
	716-12.05 ENHANCED FLATLINE THERMO PVMT MRKNG (6IN DOTTED LINE)	L.F.		155	155
13	716-13.02 SPRAY THERMO PVMT MRKNG (60 mil) (6IN LINE)	L.M.		12	12
	717-01 MOBILIZATION	LS	1		1
	730-14.02 SAW SLOT	L.F.	3400		3400
	730-14.03 LOOP WIRE	L.F.	7300		7300

**FOOTNOTES**

- TO BE USED AS DIRECTED BY THE ENGINEER FOR GUARDRAIL ANCHORS.
- TO BE USED AS DIRECTED BY THE ENGINEER FOR SPOT LEVELING.
- FOR RIDEABILITY SEE SPECIAL PROVISIONS 411C.
- FOR MATERIAL TRANSFER DEVICE SEE SPECIAL PROVISIONS 407G.
- INCLUDES 182 TON FOR COUNTY ROADS, BUSINESS ENTRANCES, PRIVATE DRIVES AND FIELD ENTRANCES.
- THE CONTRACTOR SHALL COMPLY WITH SECTION 712 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION REGARDING TEMPORARY TRAFFIC CONTROL AND THE CURRENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- IN THE EVENT THAT A CONSTRUCTION AND/OR REGULATORY SIGN IS TEMPORAIRILY DESIGNATED NOT IN USE DURING THE CONSTRUCTION PHASE OF A PROJECT, THE CONTRACTOR SHALL CHOOSE A SIGN COVERING METHOD FROM A DESIGNATED LIST OF METHODS ACCEPTED BY THE DEPARTMENT. SAID SIGN COVERING METHOD SHALL BE SELECTED FROM QUALIFIED PRODUCTS LIST 10, SECTION D OR APPROVED BY THE DIVISION OF MATERIALS AND TEST AND SHALL BE USED TO COVER THESE SIGNS. NO OTHER METHOD FOR SIGN COVERING WILL BE ALLOWED. TEMPORARY SIGN COVERINGS SHALL NOT BE PAID FOR DIRECTLY BUT SHALL BE INCLUDED IN THE PRICE BID FOR ITEM NO. 712-06 SIGNS (CONSTRUCTION).
- INCLUDES ALL COST TO ADD 12"X36" OBJECT MARKERS (OM3-L & OM3-R) W/ POST (8 LEFT & 8 RIGHT) AT BRIDGE AND CULVERT ENDS.
- INCLUDES ALL COST TO REMOVE 2 EXISTING STOP SIGNS MOUNTED IN BARRELS AND REPLACE WITH 2 NEW STOP SIGNS W/ POST GROUND MOUNTED.
- THE CONTRACTOR MAY ELECT TO SUBSTITUTE PREFORMED PLASTIC FOR THERMOPLASTIC. PREFORMED PLASTIC SHALL BE PAID FOR AT THE SAME UNIT PRICE AS BID FOR THERMOPLASTIC.
- FOR TEMPORARY PAVEMENT MARKING ON INTERMEDIATE LAYERS.
- FOR CENTERLINE PAVEMENT MARKINGS ON FINAL SURFACE (TWO LANE SECTION IS BASED ON 8 % PASS, 18 % NO PASS ONE SIDE, AND 74 % NO PASS BOTH SIDES) AND EDGE LINE FROM L.M.14.66 TO L.M. 15.11.
- FOR EDGE LINE PAVEMENT MARKINGS ON FINAL SURFACE FROM L.M. 15.11 TO L.M. 20.62.

**THERE ARE NO UTILITY ADJUSTMENTS ON THIS PROJECT**

COLD PLANE					
LOCATION	LENGTH MILE	WIDTH FEET	DEPTH INCH	TOTAL TON	
A L.M. 14.66 TO L.M. 15.04	0.38	60'	1.25"	877.8	
B L.M. 15.04 TO L.M. 15.11	0.07	64'	1.25"	172.48	
C L.M. 15.11 TO L.M. 15.28	0.17	38'	1.25"	248.71	
D L.M. 15.28 TO L.M. 15.36	0.08	38'	1.25"	117.04	
E L.M. 15.36 TO L.M. 16.20	0.84	38'	1.25"	1228.92	
F L.M. 16.20 TO L.M. 16.62	0.42	44'	1.25"	711.48	
G L.M. 16.62 TO L.M. 19.37	2.75	32'	1.25"	3388	
H L.M. 19.37 TO L.M. 19.40	0.03	36'	1.25"	41.58	
I L.M. 19.40 TO L.M. 20.62	1.22	32'	1.25"	1465.94	
<b>TOTAL</b>				<b>8251.95</b>	

\* OMIT 159' BRIDGE @ L.M.19.47

GUARDRAIL						
LOCATION (L.M.)	SIDE		GUARDRAIL AT BRIDGE 705-01.01 (L.F.)	GUARDRAIL SINGLE 705-02.02 (L.F.)	ANCHOR TYPE 38 705-04.07 (EACH)	GUARDRAIL REMOVED 706-01 (L.F.)
	LT	RT				
15.485	X				1	50
19.179	X				1	50
19.539	X				1	50
			<b>0</b>	<b>0</b>	<b>3</b>	<b>150</b>

SIGNS (CONSTRUCTION) 712-06				
QUANTITY	M.U.T.C.D. NO.	DESCRIPTION	SIZE	712-06 S.F.
2	G20-1	ROAD WORK NEXT 6 MILES	64 x 24	22
2	G20-2	END ROAD WORK	48 x 24	16
2	W20-1	ROAD WORK 1500'	48 x 48	32
2	W20-1	ROAD WORK 1000'	48 x 48	32
2	W20-1	ROAD WORK 500'	48 x 48	32
16	W20-1	ROAD WORK AHEAD	48 x 48	256
2	W20-4	ONE LANE ROAD 1000' - PORTABLE	36 x 36	18
2	W4-2R	LANE CLOSED MERGE RIGHT	48 x 48	32
2	W4-2L	LANE CLOSED MERGE LEFT	48 x 48	32
2	W20-5R	RIGHT LANE CLOSED AHEAD	48 x 48	32
2	W20-5L	LEFT LANE CLOSED AHEAD	48 x 48	32
2	W20-7a	FLAGGER SYMBOL - PORTABLE	36 x 36	18
2	W16-2	SUPPLEMENTAL PLATE	24 x 18	6
2	W21-2	FRESH OIL - PORTABLE	36 x 36	18
2	W21-5	SHOULDER WORK - PORTABLE	36 x 36	18
2	W8-9a	SHOULDER DROP-OFF - PORTABLE	36 x 36	18
6	W8-11	UNEVEN LANES - PORTABLE	36 x 36	54
<b>TOTAL S.F.</b>				<b>668</b>

OTHER SIGNS, AS DIRECTED BY THE ENGINEER MAY BE REQUIRED DURING VARIOUS PHASES OF CONSTRUCTION AND WILL BE MEASURED AND PAID FOR AT THE UNIT BID PRICE. SEE CURRENT M.U.T.C.D. FOR STANDARDS AND TYPICAL APPLICATIONS.

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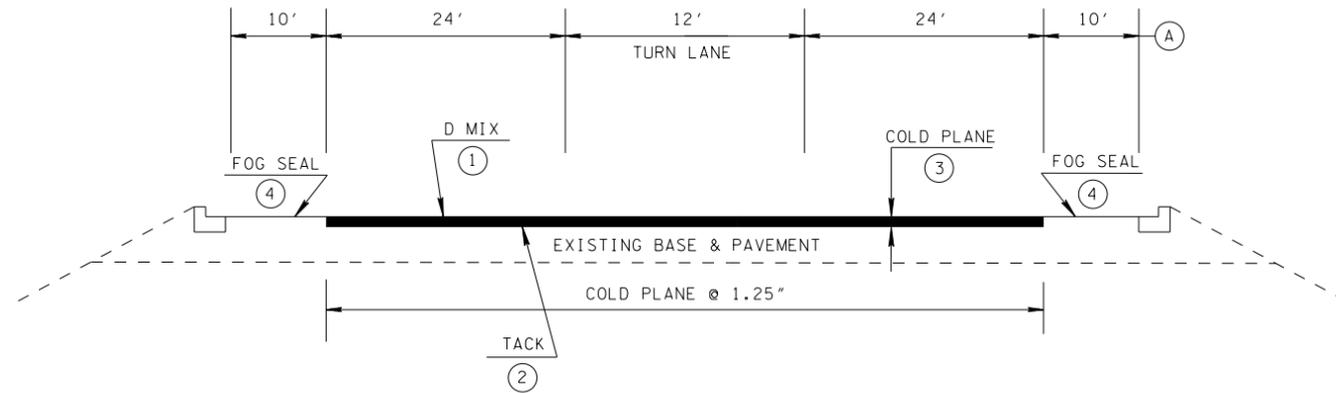
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STATE OF TENNESSEE DEPARTMENT OF TRANSPORTATION

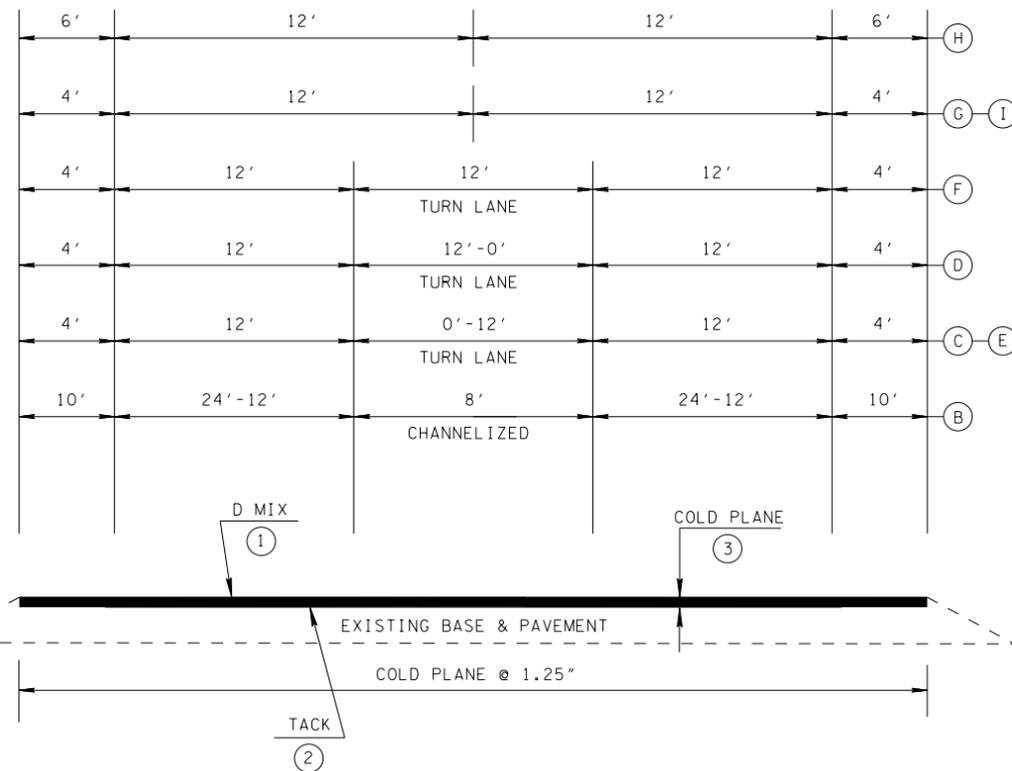
**ESTIMATED ROADWAY QUANTITIES**

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2015	STP/HSIP-96(42)	2A



**TYPICAL SECTIONS**

(A) L.M. 14.66 TO L.M. 15.04



**TYPICAL SECTIONS**

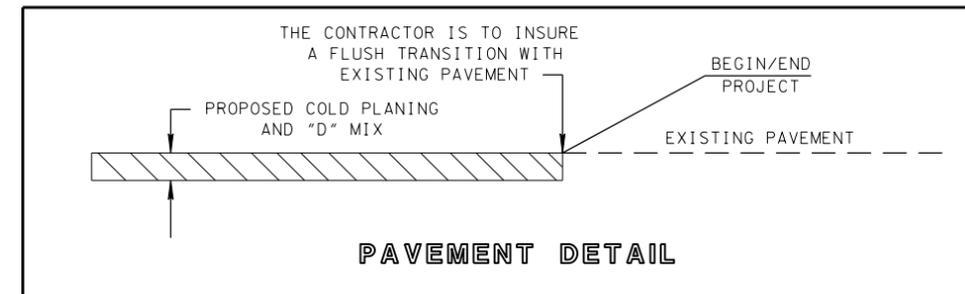
- (B) L.M. 15.04 TO L.M. 15.11
- (C) L.M. 15.11 TO L.M. 15.28
- (D) L.M. 15.28 TO L.M. 15.36
- (E) L.M. 15.36 TO L.M. 16.20
- (F) L.M. 16.20 TO L.M. 16.62
- (G) L.M. 16.62 TO L.M. 19.37
- (H) L.M. 19.37 TO L.M. 19.40
- (I) L.M. 19.40 TO L.M. 19.47
- (I) L.M. 19.47 TO L.M. 19.50 - 159' BRIDGE EXCLUSION
- (I) L.M. 19.50 TO L.M. 20.62

PAVING SCHEDULE	
1	GRADING "D" SURFACE @ 1.25" THICK (APPROX 132.5 LBS/SY) 411-02.10 ASC MIX (PG70-22) GRADING D
2	TACK COAT @ 0.10 GAL/SY (MILLING - COLD PLANE) 403-01 BITUMINOUS MATERIAL FOR TACK COAT (TC)
3	COLD PLANING @ 1.25" (APPROX 131.25 LB/SY) 415-01.01 COLD PLANING OF BITUMINOUS PAVEMENT
4	FOG SEAL @ 0.5 X 0.15 GAL/SY 403-05.01 BITUMINOUS MATERIAL (FOG SEAL) SHOULDER

**SPECIAL PAVING NOTE**

ALL SHOULDERS WILL BE DEGRADED / CLIPPED AND CLEANED OF ALL VEGETATION, GRASS, AND DEBRIS. SEE SPECIAL NOTES. ALL COST TO BE INCLUDED IN OTHER ITEMS OF WORK.

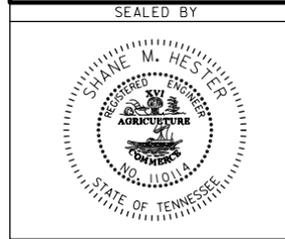
BRIDGE NOTES			
LOCATION	BRIDGE NUMBER	LENGTH (FT.)	RECOMMENDATION
L.M. 16.72	94SR0960017	28.5	COLD PLANE 1.25" OF THE EXISTING ASPHALT AND REPLACE WITH 1.25" NEW ASPHALT
L.M. 16.76	94SR0960019	88.5	COLD PLANE 1.25" OF THE EXISTING ASPHALT AND REPLACE WITH 1.25" NEW ASPHALT
L.M. 19.07	94SR0960021	28.5	COLD PLANE 1.25" OF THE EXISTING ASPHALT AND REPLACE WITH 1.25" NEW ASPHALT
L.M. 19.47	94SR0960023	159	LEAVE AS IS



Test Performed by TDOT Road Profiler									Average Roughness Pre-Construction HC-IRI Number Per Mile
Test Limits		Direction and Measured Roughness ( HC-IRI Number Per Mile )							
From	To	NSEW	*Lane	Pre Ride Number	Payment Table	Max. # for Final Ride	Min. Required % Improvement	Percent Pay	
0.00	1.00	E	1	81	Table 2	56.7	30	100	86.0
1.00	2.00	E	1	76	Table 1	55.0		100	
2.00	3.00	E	1	78	Table 1	55.0		100	
3.00	4.00	E	1	97	Table 2	67.9	30	100	
4.00	5.00	E	1	90	Table 2	63.0	30	100	
5.00	5.94	E	1	74	Table 1	55.0		100	
0.00	1.00	W	1	93	Table 2	65.1	30	100	REMARKS
1.00	2.00	W	1	95	Table 2	66.5	30	100	
2.00	3.00	W	1	78	Table 1	55.0		100	
3.00	4.00	W	1	82	Table 2	57.4	30	100	
4.00	5.00	W	1	88	Table 2	61.6	30	100	
5.00	5.95	W	1	100	Table 2	70.0	30	100	

\*Lanes to be Left to Right each Direction - 1,2,3,4, etc. with 1 being the Inside Lane

**UNOFFICIAL SET NOT FOR BIDDING**



STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

**TYPICAL SECTIONS AND PAVEMENT SCHEDULE**

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2015	STP/HS1P-96(42)	2B

## GENERAL NOTES

### GUARDRAIL

- (1) THE PROPOSED GUARDRAIL, INCLUDING ANY ANCHOR SYSTEM, SHALL BE INSTALLED QUICKLY TO MINIMIZE TRAFFIC EXPOSURE TO ANY HAZARD. NO PAYMENT WILL BE MADE FOR A SECTION OF PROPOSED GUARDRAIL, INCLUDING ANCHORS, UNTIL IT IS COMPLETE IN PLACE.

### MISCELLANEOUS

- (2) THE CONTRACTOR SHALL BE REQUIRED TO REMOVE AND RESET MAILBOXES WHERE AND AS DIRECTED BY THE ENGINEER.
- (3) NOTHING IN THE GENERAL NOTES OR SPECIAL PROVISIONS SHALL RELIEVE THE CONTRACTOR FROM HIS RESPONSIBILITIES TOWARD THE SAFETY AND CONVENIENCE OF THE GENERAL PUBLIC AND THE RESIDENTS ALONG THE PROPOSED CONSTRUCTION AREA

### PAVEMENT MARKINGS

#### TEMPORARY PAVEMENT MARKING ON INTERMEDIATE LAYERS

- (4) TEMPORARY PAVEMENT LINE MARKINGS ON INTERMEDIATE LAYERS OF PAVEMENT SHALL BE REFLECTIVE TAPE OR REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT, UNMARKED SECTIONS SHALL NOT BE ALLOWED. THESE MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-05.01, PAINTED PAVEMENT MARKING (4" LINE), L.M.

#### FINAL PAVEMENT MARKING IF 6" ENHANCED FLATLINE THERMOPLASTIC IS USED

- (5) PERMANENT PAVEMENT LINE MARKINGS SHALL BE 6" ENHANCED FLATLINE THERMOPLASTIC INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-12.02, ENHANCED FLATLINE THERMO PVMT MRKNG (6IN LINE), L.M. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.

#### FINAL PAVEMENT MARKING IF 6" SPRAY THERMOPLASTIC (60 mil) IS USED

- (6) PERMANENT PAVEMENT LINE MARKINGS SHALL BE 6" SPRAY THERMOPLASTIC (60 mil) INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-13.02, SPRAY THERMO PVMT MRKNG (60 mil) (6IN LINE), L.M. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.

### PAVEMENT

#### PAVING

- (7) THE CONTRACTOR SHALL BE REQUIRED TO COLD PLANE AND PAVE IN THE DIRECTION OF TRAFFIC.
- (8) THE CONTRACTOR SHALL ATTACH A DEVICE TO THE SCREED OF THE PAVER SUCH THAT MATERIAL IS CONFINED AT THE END GATE AND EXTRUDES THE ASPHALT MATERIAL IN SUCH A WAY THAT RESULTS IN A CONSOLIDATED WEDGE-SHAPE PAVEMENT EDGE OF APPROXIMATELY 25 TO 30 DEGREES AS IT LEAVES THE PAVER (MEASURED FROM A LINE PARALLEL TO THE PAVEMENT SURFACE.) THE DEVICE SHALL MEET THE REQUIREMENTS THAT ARE CURRENTLY SET FORTH IN SPECIAL PROVISION 407SE.

### RESURFACING

- (9) WHERE DIRECTED BY THE TDOT ENGINEER, THE CONTRACTOR SHALL BE REQUIRED TO SHAPE PUBLIC SIDE ROADS, BUSINESS ENTRANCES, AND PRIVATE DRIVES, AS WELL AS CLEANING OF EXISTING DRAINS BEFORE PLACING MATERIALS. ALL COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (10) ALL PUBLIC SIDE ROADS SHALL BE PAVED ONE PAVER WIDTH THROUGH THE INTERSECTION AS A MINIMUM. A SATISFACTORY TRANSITION FROM THE NEW PAVEMENT TO THE EXISTING GRADE OF THE INTERSECTING PUBLIC ROAD OR BUSINESS ENTRANCE SHALL BE PROVIDED. SHOULD THE PAVEMENT OF THE INTERSECTING PUBLIC ROAD BE DISTRESSED, THE RESURFACING WIDTH MAY BE INCREASED TO THE NORMAL RIGHT OF WAY LINE.
- (11) PRIVATE DRIVEWAYS, FIELD ENTRANCES, AND BUSINESS ENTRANCES WILL BE RESURFACED A PAVER WIDTH (LANE WIDTH) AS A MINIMUM. A PAVEMENT TAPER TO TRANSITION THE NEW PAVEMENT SHALL BE REQUIRED, IT SHALL BE BASED ON AN ADDITIONAL ONE FOOT OF WIDTH PER ONE INCH DEPTH OF PAVEMENT. IF THE SHOULDER IS NARROW ENOUGH THAT THE SUM OF THE SHOULDER AND THE TRANSITION ARE LESS THAN A PAVER WIDTH, THE TRANSITION SHALL OCCUR WITHIN THE PAVER WIDTH. IF THE SUM OF THE SHOULDER AND THE TRANSITION IS GREATER THAN A PAVER WIDTH (LANE WIDTH), THE TRANSITION SHALL OCCUR OUTSIDE OF THE PAVER WIDTH.
- (12) ON CURB AND GUTTER SECTIONS, PUBLIC ROAD INTERSECTIONS SHALL BE RESURFACED TO THE END OF RADIUS. A SATISFACTORY TRANSITION FROM THE NEW PAVEMENT TO THE EXISTING GRADE OF THE INTERSECTING PUBLIC ROAD SHALL BE PROVIDED.
- (13) ON URBAN TYPICAL SECTIONS, (CURB AND GUTTER), RESIDENTIAL DRIVEWAYS AND BUSINESS ENTRANCES SHALL HAVE A MINIMUM WIDTH OF MATERIAL NOT LESS THAN ONE FOOT USED IN THE TRANSITION TO FEATHER THE PAVEMENT EDGE.
- (14) IN ALL CASES, THE LENGTH OF THE PAVEMENT TRANSITION, THE THICKNESS AND WIDTH OF THE RESURFACING AND ANY ADDITIONAL PAVEMENT MATERIALS SHALL BE AS DIRECTED BY THE TDOT ENGINEER.

### SIGNING

- (15) AFTER THE SIGN LOCATIONS HAVE BEEN STAKED, BUT PRIOR TO ORDERING ANY MATERIAL FOR THE SUPPORTS, THERE SHALL BE A FIELD INSPECTION AND APPROVAL BY THE REGIONAL CONSTRUCTION OFFICE.
- (16) THE LENGTHS OF ALL SIGN SUPPORTS SHOWN ON THE SIGN SCHEDULE ARE APPROXIMATE AND ARE FOR ESTIMATING PURPOSES ONLY. THE CONTRACTOR SHALL VERIFY ALL SUPPORT LENGTHS AT THE SITE PRIOR TO ERECTION.
- (17) THE LETTERS, DIGITS, ARROWS, BORDERS, AND ALPHABET ACCESSORIES ON ALL FLAT SHEET SIGNS SHALL BE APPLIED BY SILK SCREENING PROCESS.

### SIGNALIZATION

- (18) IF RESURFACING IS INCLUDED IN THE PROJECT, SIGNAL DETECTION LOOPS SHALL BE INSTALLED BEFORE THE FINAL SURFACE IS APPLIED.
- (19) THE PROJECT ENGINEER SHALL NOTIFY THE LOCAL GOVERNMENTAL AGENCY RESPONSIBLE FOR TRAFFIC CONTROL MAINTENANCE AT LEAST ONE DAY IN ADVANCE OF THE COLD PLANING ACTIVITY AT SIGNALIZED INTERSECTIONS WHERE DETECTOR LOOPS ARE ON THE PAVEMENT. THE MAINTAINING AGENCY WILL THEN BE RESPONSIBLE FOR DISCONNECTING THE LOOP DETECTORS AND MAKING ANY NECESSARY TIMING ADJUSTMENTS IN THE SIGNAL CONTROLLER PRIOR TO THE CONSTRUCTION.
- (20) LOOPS SHALL BE INSTALLED IN THE LEVELING COURSE IF A LEVELING COURSE IS PROVIDED.
- (21) LOOP REPLACEMENT SHALL BE IN ACCORDANCE WITH SECTION 730 OF THE STANDARD SPECIFICATIONS.

### CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

- (22) ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE WEEK BEFORE NEEDED, IF THE SIGN FACE IS FULLY COVERED.
- (23) IF THE CONTRACTOR MOVES OFF THE PROJECT, HE SHALL COVER OR REMOVE ALL UNNEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OF REMOVAL, COVERING, AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO 712-06, SIGNS (CONSTRUCTION) PER SQUARE FOOT.
- (24) A LONG TERM BUT SPORADIC USE WARNING SIGN, SUCH AS A FLAGGER SIGN, MAY REMAIN IN PLACE WHEN NOT REQUIRED PROVIDED THE SIGN FACE IS FULLY COVERED.
- (25) TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.
- (26) USE OF BARRICADES, PORTABLE BARRIER RAILS, VERTICAL PANELS, AND DRUMS SHALL BE LIMITED TO THE IMMEDIATE AREAS OF CONSTRUCTION WHERE A HAZARD IS PRESENT. THESE DEVICES SHALL NOT BE STORED ALONG THE ROADWAY WITHIN THIRTY (30) FEET OF THE EDGE OF THE TRAVELED WAY BEFORE OR AFTER USE UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL INCREASE TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. THESE DEVICES SHALL BE REMOVED FROM THE CONSTRUCTION WORK ZONE WHEN THE ENGINEER DETERMINES THEY ARE NO LONGER NEEDED. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.
- (27) THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY, WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHEN THE LANE IS OPEN TO TRAFFIC UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO PARK WITHIN THIRTY (30) FEET OF AN OPEN TRAFFIC LANE AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE.. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.

### EROSION PREVENTION AND SEDIMENT CONTROL

#### LITTER, DEBRIS, WASTE, PETROLEUM

- (28) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT LITTER, CONSTRUCTION DEBRIS, AND CONSTRUCTION WASTES FROM ENTERING WATERS OF THE STATE/U.S. THESE MATERIALS WILL BE PICKED UP AND REMOVED FROM STORMWATER EXPOSURE PRIOR TO ANTICIPATED STORM EVENTS. AFTER USE, MATERIALS USED FOR EPSC WILL BE REMOVED FROM THE SITE.
- (29) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS TO ENSURE THAT PETROLEUM PRODUCTS OR OTHER CHEMICAL POLLUTANTS ARE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. ALL EQUIPMENT REFUELING, SERVICING, AND STAGING AREAS SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL LAWS, RULES, REGULATIONS, AND ORDINANCES, INCLUDING THOSE OF THE NATIONAL FIRE PROTECTION ASSOCIATION (NFPA). APPROPRIATE CONTAINMENT MEASURES FOR THESE AREAS SHALL BE USED. ALL SPILLS MUST BE REPORTED TO THE APPROPRIATE AGENCY, AND MEASURES SHALL BE TAKEN IMMEDIATELY TO PREVENT THE POLLUTION OF WATERS OF THE STATE/U.S., INCLUDING GROUNDWATER, SHOULD A SPILL OCCUR.

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**GENERAL  
NOTES**

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2015	STP/HS1P-96(42)	2C

## SPECIAL NOTES

### PAVEMENT EDGE DROP-OFF TRAFFIC CONTROL NOTES

- (1) DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES OR TRAFFIC LANE AND SHOULDER WHERE THE TRAFFIC LANE IS BEING USED BY TRAFFIC, CAUSED BY BASE, PAVING OR RESURFACING:
1. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 0.75 INCH AND NOT EXCEEDING 2 INCHES:
    - a. WARNING SIGNS, UNEVEN PAVEMENT (W8-11) AND/OR SHOULDER DROP-OFF (W8-9A), SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.
    - b. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY ADDED PAVEMENT SHALL BE ELIMINATED WITHIN THREE WORKDAYS.
    - c. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY COLD PLANING SHALL BE ELIMINATED WITHIN THREE WORKDAYS.
    - d. WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE TRAFFIC LANE BEING UTILIZED BY TRAFFIC AND SHOULDER THE DIFFERENCE IN ELEVATION SHALL BE ELIMINATED WITHIN SEVEN WORKDAYS AFTER THE CONDITION IS CREATED.

### MISCELLANEOUS

- (2) THE CONTRACTOR WILL BE REQUIRED TO PERFORM THE FOLLOWING WORK AND INCLUDE ALL COSTS THEREOF IN THE PRICES BID FOR OTHER ITEMS OF CONSTRUCTION:
1. REPAIR OF SHOULDERS AND DITCHES THROUGHOUT THE LENGTH OF THE PROJECT. DEGRASS ALL SHOULDERS, MATERIAL SHALL BE PICKED UP AND REMOVED.
  2. CLEANING AND STRAIGHTENING ENDS OF EXISTING SIDEDRAINS AND CROSSDRAINS WHERE DIRECTED BY THE ENGINEER.
- (3) THE CONTRACTOR SHALL TAKE EXTREME CARE WHEN WORKING AROUND EXISTING GUARDRAIL, HIGHWAY SIGNS, OR ANY DEPARTMENT OF TRANSPORTATION STRUCTURE TO AVOID DAMAGE DURING THE COURSE OF CONSTRUCTION. ANY DAMAGE WILL BE REPLACED OR REPAIRED AT HIS OWN EXPENSE.

### PAVEMENT MARKINGS

- (4) THE CONTRACTOR WILL BE REQUIRED TO ESTABLISH AND LOCATE ALL NO-PASSING ZONES. THE ESTABLISHMENT OF NO-PASSING ZONES IS TO BE IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, 2009, PART III, MARKING. ALL NO-PASSING ZONES WILL BE APPROVED BY THE ENGINEER WITH ADVICE FROM THE REGIONAL TRAFFIC ENGINEER PRIOR TO THE MARKING APPLICATION.
- (5) THE CONTRACTOR IS TO PROVIDE THE LAYOUT OF ALL EXISTING PAVEMENT MARKINGS BEFORE BEGINNING THE RESURFACING PROJECT. EXISTING MARKINGS SHALL BE REFERENCED AS NEEDED TO ENSURE THAT ALL NEW MARKINGS ARE REPLACED IN THEIR CORRESPONDING LOCATIONS UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

### PAVEMENT

#### PAVING

- (6) SURFACE TO BE CROWNED AS DIRECTED BY THE ENGINEER.
- (7) ALL STRUCTURES (DRAINAGE STRUCTURES, MANHOLES, UTILITY BOXES, ETC.) SHALL HAVE A FINISHED GRADE COMPATIBLE WITH THE FINISHED GRADE OF THE ROADWAY.

### RESURFACING

- (8) DURING COLD PLANING OPERATION, A MINIMUM OF ONE LANE OF TRAFFIC IN EACH DIRECTION WILL REMAIN OPEN.
- (9) AFTER ANY COLD PLANING OPERATION, TEMPORARY STRIPING WILL BE REQUIRED BEFORE RETURNING TRAFFIC ONTO EXCAVATED ROADWAY.

### CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

- (10) THE CONSTRUCTION SIGNS, FLAGMEN AND OTHER TRAFFIC CONTROL DEVICES WILL BE IN ACCORDANCE WITH THE CURRENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- (11) ONE ADVANCE ROAD CONSTRUCTION AHEAD SIGN IS TO BE PLACED ON ALL SIDE ROADS PRIOR TO THEIR INTERSECTION WITH THE PROPOSED PROJECT. LOCATION TO BE DETERMINED IN THE FIELD BY THE ENGINEER OR CONTRACTORS SUPERINTENDENT.

### EROSION PREVENTION AND SEDIMENT CONTROL

#### FOR GUARDRAIL TERMINALS

- (12) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS BEFORE CONSTRUCTING EARTH PADS FOR THE TYPE 38 TERMINALS TO ENSURE THAT ENVIRONMENTAL FEATURES (STREAMS, WETLANDS, OR SPRINGS) ARE NOT IMPACTED DURING CONSTRUCTION. IF THE CONTRACTOR / TDOT INSPECTOR IS UNSURE WHETHER THE ENVIRONMENTAL FEATURES ARE STREAMS, SPRINGS OR WETLANDS, THE INSPECTOR SHALL CONTACT THE TDOT ENVIRONMENTAL DIVISION ECOLOGY OR PERMITS SECTION SO AN ACCURATE DETERMINATION CAN BE ASSESSED.
- (13) DURING CONSTRUCTION OF THE EARTH PADS FOR THE TYPE 38 GUARDRAIL TERMINALS, EXISTING VEGETATION WILL BE REQUIRED TO BE REMOVED FOR THE PROPOSED PAD AND SLOPES. IF DURING THE VEGETATION REMOVAL, ANY TREES GREATER THAN 3 INCHES IN DIAMETER ARE REQUIRED TO BE REMOVED, CONTACT THE TDOT ENVIRONMENTAL DIVISION ECOLOGY SECTION IMMEDIATELY. DUE TO THE RECENT MODIFICATION OF THE USFWS STANCE REGARDING THE INDIANA BAT NO TREES > 3" DBH SHOULD BE CUT WITHOUT CLEARANCE FROM THE TDOT ENVIRONMENTAL DIVISION ECOLOGY SECTION.

#### NPDES

- (14) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS, BEFORE CLEANING OUT OR REMOVAL/REPLACEMENT OF DITCHES, SIDE DRAINS, AND CROSS DRAINS, TO ENSURE THAT THE DRAINAGE FEATURES / WATERCOURSES ARE NOT STREAMS OR WETLANDS. IF THE CONTRACTOR / TDOT INSPECTOR IS UNSURE WHETHER THE DRAINAGE FEATURES ARE STREAMS OR WETLANDS, THE INSPECTOR SHALL CONTACT THE TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION TO OBTAIN THE APPROPRIATE PERMITS.
- (15) DUE TO THE TYPE OF WORK IT IS OUR UNDERSTANDING THAT THE PROJECT WILL NOT DISTURB MORE THAN ONE ACRE. IF THE DISTURBED AREA IS MORE THAN ONE ACRE, PLEASE CONTACT THE TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION BECAUSE A NPDES PERMIT WILL BE REQUIRED.

#### WORK RESTRICTIONS

- (16) PAVING WITH "D" MIX TO BE APPLIED DURING DAYLIGHT HOURS ONLY.
- (17) DAYTIME WORK ONLY 9 AM TO 3 PM.

### RAILROAD CROSSING NOTES

- (18) THE CONTRACTOR SHALL CONDUCT HIS WORK SO AS TO PROTECT THE RAILROAD'S TRACKS AND PROPERTIES FROM ANY DAMAGE. THE WORK SHALL BE DONE IN ACCORDANCE WITH REGULATIONS STIPULATED BY CSXT SO AS TO MAINTAIN CLEARANCE AND NOT INTERRUPT TRAFFIC IN ANY MANNER.
- (19) THE CONTRACTOR SHALL NOT, FOR ANY REASON, STORE CONSTRUCTION EQUIPMENT UPON NOR STORE OR DUMP WASTE CONSTRUCTION MATERIALS UPON THE RAILROAD'S RIGHT-OF-WAY.
- (20) BRIDGE OVER CSXT RAILROAD @ L.M. 19.47 (350363H) TO BE EXCLUDED FROM RESURFACING. HOWEVER, PAVEMENT MARKING WILL BE APPLIED TO BRIDGE SURFACE.
- (21) THE CONTRACTOR SHALL USE EXTREME CARE AND TAKE ANY MEASURE NECESSARY TO PREVENT DEBRIS FROM FALLING ON TO THE RAILROAD'S RIGHT OF WAY. THE COST OF REMOVING AND DISPOSING OF ANY DEBRIS, AND THE COMPLETE AND FULL PROTECTION OF THE CONTRACTOR'S WORK AREA FOR PAVEMENT MARKING INSTALLATION AND/OR RE-STRIPING SHALL BE INCLUDED IN UNIT ITEMS BID ON.
- (22) FOR ANY PAVEMENT MARKING INSTALLATION AND/OR RE-STRIPING OVER THE RAILROAD, IT IS ANTICIPATED THAT RAILROAD FLAGGING SERVICES MAY BE REQUIRED. FOR ANY OTHER ASPECT OF THE CONTRACTOR'S WORK ON THE BRIDGE DECK OVER THE RAILROAD'S RIGHT OF WAY, FLAGGING WILL BE AT THE RAILROAD'S DISCRETION.
- (23) SEE AGREEMENT/STATE CONTRACT SPECIAL PROVISION 105C(R) REGARDING INSURANCE AND RAILROAD FLAGGING SERVICES REQUIREMENTS, FOR WORK OVER AND/OR ADJACENT TO THE RAILROAD'S RIGHT-OF-WAY.

#### FOR CSX RAILROAD:

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