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SEE SHEET IA

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
BUREAU OF ENGINEERING

**MARSHALL COUNTY**

STATE ROUTE 50 OVER BIG ROCK CREEK  
BRIDGE NO. 59-SR50-6.57

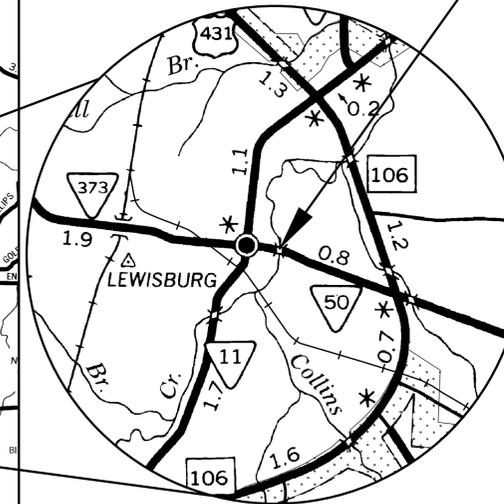
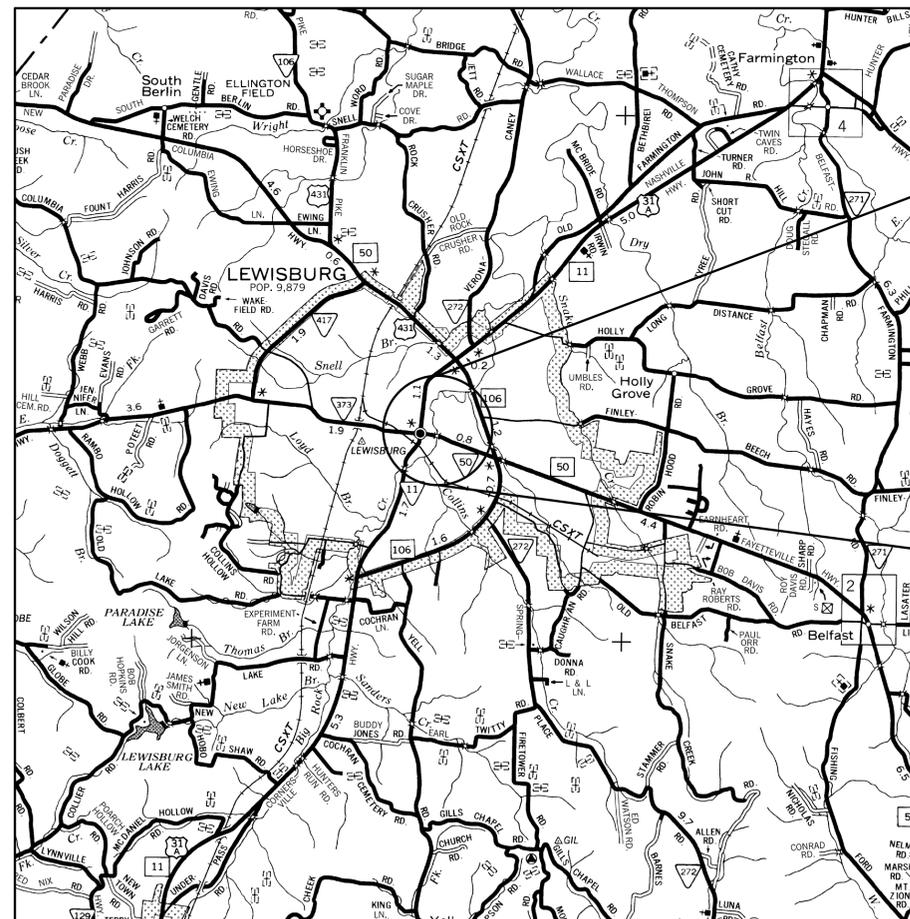
TENN.	YEAR	SHEET NO.
	2015	1
FED. AID PROJ. NO.		
STATE PROJ. NO.	59006-4219-04	

REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

PROJECT NO. 59006-4219-04



**BRIDGE REPAIR**



BRIDGE NO. 59-SR50-6.57  
OVER  
BIG ROCK CREEK

**UNOFFICIAL SET**  
NOT FOR BIDDING

**SPECIAL NOTES**

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED JANUARY 1, 2015 AND ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS AND IN THE PROPOSAL CONTRACT.

TDOT C.E. MANAGER 1 OR  
TDOT DESIGN MANAGER 1 BRIAN EGLI

TDOT PROJECT MANAGER TERRY MACKIE

DESIGNED BY JAMES + ASSOCIATES, INC.

DESIGNER DAVID THOMPSON, P.E. CHECKED BY JAMIE GILLESPIE, P.E.

P.E. NO. 59006-4219-04

PIN NO. 121386.00

SCALE: 1" = 5,280'

TRAFFIC DATA	
ADT (2015)	6,410
ADT (2035)	7,050
DHV (2035)	705
D	65 - 35
T (ADT)	2%
T (DHV)	1%
V	30 MPH

ROADWAY LENGTH 0.067 MILES  
BRIDGE LENGTH 0.033 MILES  
PROJECT LENGTH 0.100 MILES

APPROVED: Paul D. Degges  
PAUL D. DEGGES, CHIEF ENGINEER

DATE: \_\_\_\_\_

APPROVED: John Schroer  
JOHN SCHROER, COMMISSIONER

U.S. DEPARTMENT OF TRANSPORTATION  
FEDERAL HIGHWAY ADMINISTRATION

APPROVED: \_\_\_\_\_  
DIVISION ADMINISTRATOR DATE

\*\*\*\*\*SYTIME\*\*\*\*\*  
\*\*\*\*\*DGN5PEC\*\*\*\*\*

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## ROADWAY DESIGN STANDARDS

DWG. NO.	REVISION DATE	DESCRIPTION
RD-A-1	12-18-99	STANDARD ABBREVIATIONS
RD-L-1	10-26-94	STANDARD LEGEND
RD-L-2	09-05-01	STANDARD LEGEND FOR UTILITY INSTALLATIONS
RD-L-3	04-15-04	STANDARD LEGEND FOR SIGNALIZATION AND LIGHTING
RD-L-4	04-15-04	STANDARD LEGEND FOR SIGNALIZATION AND LIGHTING
RD-L-5	05-01-08	STANDARD LEGEND FOR EROSION PREVENTION AND SEDIMENT CONTROL
RD-L-6	03-30-10	STANDARD LEGEND FOR EROSION PREVENTION AND SEDIMENT CONTROL
RD-L-7	05-24-12	STANDARD LEGEND FOR EROSION PREVENTION AND SEDIMENT CONTROL

## CULVERTS AND ENDWALLS

D-PB-1	01-02-13	STANDARD DETAILS FOR CONCRETE PIPE INSTALLATION
D-PB-2	01-29-14	STANDARD DETAILS FOR FLEXIBLE PIPE INSTALLATION

## ROADWAY AND PAVEMENT APPEARANCES

RP-H-3	01-30-15	CURB RAMP AND TRUNCATED DOME SURFACE DETAIL
RP-H-5	01-30-15	PARALLEL CURB RAMP
RP-H-7	01-30-15	PERPENDICULAR CURB RAMP TYPE 1
RP-H-8	06-04-13	PERPENDICULAR CURB RAMP TYPE 2
RP-H-9	01-30-15	PARALLEL CURB RAMP TYPE 3 AND 4
RP-S-7	06-04-13	DETAILS FOR STANDARD CONCRETE SIDEWALKS

## TRAFFIC CONTROL APPURTENANCES

T-M-1	07-24-14	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS AND MARKING ABBREVIATIONS
T-S-7	02-12-91	HIGHWAY SHIELDS USED ON INTERSTATE AND U.S. NUMBERED ROUTES
T-S-8	07-15-91	HIGHWAY SHIELDS USED ON STATE NUMBERED ROUTES AND ARROWS
T-S-10	04-04-12	STANDARD MOUNTING DETAILS FLAT SHEET SIGNS ALUMINUM-STEEL DESIGN
T-S-16	06-05-14	GROUND MOUNTED ROADSIDE SIGN AND DETAILS
T-S-17	07-19-13	STANDARD GROUND MOUNTED SIGN USING PERFORATED/KNOCKOUT SQUARE TUBE
T-S-18	02-14-14	END OF ROADWAY AND DEAD END SIGNS, METAL BARRICADES (TYPE III) AND WORK ZONE SPEED SIGNS
T-S-19	07-19-13	STANDARD STEEL SIGN SUPPORTS
T-S-20	11-01-11	SIGN DETAILS

## EROSION PREVENTION AND SEDIMENT CONTROL

EC-STR-3C	08-01-12	SILT FENCE WITH WIRE BACKING
EC-STR-3E	04-01-08	SILT FENCE FABRIC JOINING DETAILS
EC-STR-19	04-01-08	CATCH BASIN PROTECTION
EC-STR-25	08-01-12	TEMPORARY CULVERT CROSSING, CONSTRUCTION EXIT, CONSTRUCTION FORD
EC-STR-37	06-10-14	SEDIMENT TUBE

## LIGHTING AND UTILITY POLE APPURTENANCES

T-L-1	12-04-13	STANDARD LIGHTING FOUNDATION DETAILS
T-L-1SA	09-11-13	STANDARD LIGHTING DETAILS FOR SINGLE ARM SUPPORTS
T-L-3	04-15-96	STANDARD LIGHTING DETAILS PULL BOXES
T-L-4	05-25-11	STANDARD LIGHTING DETAILS CONDUIT, CABLE INSTALLATION

## BRIDGE APPURTENANCES (NEW STRUCTURES)

STD-1-1SS	05-01-14	BRIDGE RAILING SINGLE SLOPE CONCRETE PARAPET - 2006
STD-1-2SS		STEEL SLIDER PLATE ASSEMBLIES FOR SINGLE SLOPE CONCRETE AND BRIDGE DECK DRAIN DETAILS - 2007
STD-1-5	03-26-14	REINFORCED CONCRETE PAVEMENT AT BRIDGE ENDS - 1995
STD-1-6	04-28-97	BRIDGE END DRAIN W/PAVEMENT AT BRIDGE ENDS - 1993
STD-1-7	08-24-11	BRIDGE END DRAIN W/PAVEMENT AT BRIDGE ENDS - 1993
STD-1-8	05-01-95	BRIDGE END DRAIN 2' X 8'-7" W/PABE - 1993
STD-4-1	04-08-05	STANDARD PRECAST PRESTRESSED BRIDGE DECK PANELS GENERAL DETAILS - 1992
STD-4-2	04-08-05	STANDARD PRECAST PRESTRESSED BRIDGE DECK PANELS DESIGN CRITERIA - 1992
STD-4-3	03-02-02	STANDARD PRECAST PRESTRESSED BRIDGE DECK PANELS GENERAL DETAILS - 1992
STD-4-4	06-10-96	STANDARD PRECAST PRESTRESSED BRIDGE DECK PANELS CONSTRUCTION DETAILS - 1992
STD-6-1	11-01-10	STANDARD SEISMIC DETAILS
STD-8-2SS		SINGLE SLOPE PARAPET STANDARD LIGHT SUPPORT DETAILS
STD-10-1	04-08-05	MISCELLANEOUS ABUTMENT AND DRAINAGE DETAILS - 1971
STD-14-3	10-15-08	STANDARD DETAILS FOR PRESTRESSED BOX BEAMS

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STATE ROUTE 50  
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MARSHALL COUNTY  
2015

INDEX AND STANDARD DRAWINGS





## ESTIMATED ROADWAY QUANTITIES

ITEM NO.	DESCRIPTION	UNIT	QUANTITY
① 105-01	CONSTRUCTION STAKES, LINES AND GRADES	LS	1
202-03	REMOVAL OF RIGID PAVEMENT, SIDEWALK, ETC.	S.Y.	18
② 202-03.01	REMOVAL OF ASPHALT PAVEMENT	S.Y.	80
203-01	ROAD AND DRAINAGE EXCAVATION (UNCLASSIFIED)	C.Y.	32
④③ 209-05	SEDIMENT REMOVAL	C.Y.	3
④③ 209-08.02	TEMPORARY SILT FENCE (WITH BACKING)	L.F.	42
④③ 209-40.30	CATCH BASIN PROTECTION (TYPE A)	EACH	4
303-01	MINERAL AGGREGATE, TYPE A BASE, GRADING D	TON	36
307-01.01	ASPHALT CONCRETE MIX (PG64-22) (BPMB-HM) GRADING A	TON	14
307-01.08	ASPHALT CONCRETE MIX (PG64-22) (BPMB-HM) GRADING B-M2	TON	9
402-01	BITUMINOUS MATERIAL FOR PRIME COAT (PC)	TON	1
403-01	BITUMINOUS MATERIAL FOR TACK COAT (TC)	TON	1
411-01.10	ACS MIX (PG64-22) GRADING D	TON	101
415-01.02	COLD PLANING BITUMINOUS PAVEMENT	S.Y.	1,440
⑤ 604-10.07	CONCRETE REMOVAL	LS	1
⑦⑥ 610-07.03	18-INCH PIPE DRAIN (BRIDGE DRAIN)	L.F.	70
611-01.20	ADJUSTMENT OF EXISTING MANHOLE	EACH	1
611-09.01	ADJUSTMENT OF EXISTING CATCHBASIN	EACH	4
⑥ 709-01.01	RUBBLE STONE RIP-RAP	C.Y.	3
⑧ 712-01	TRAFFIC CONTROL	LS	1
712-05.01	WARNING LIGHTS (TYPE A)	EACH	4
⑨ 712-06	SIGNS (CONSTRUCTION)	S.F.	1,100
712-07.03	TEMPORARY BARRICADES (TYPE III)	L.F.	106
713-15.02	REMOVAL AND RELOCATION OF SIGN AND SUPPORT	EACH	1
713-16.20	SIGNS (OM3-L)	EACH	2
713-16.21	SIGNS (OM3-R)	EACH	2
⑩ 716-01.21	SNWPLWBLE PVMT MRKRS (BI-DIR) (1 COLOR)	EACH	8
⑪ 716-13.01	SPRAY THERMO PVMT MRKNG (60 MIL) (4" LINE)	L.M.	0.40
717-01	MOBILIZATION	LS	1
④ 740-11.01	TEMPORARY SEDIMENT TUBE (8 INCH) (STRAW WATTLES)	L.F.	157
④⑫ 801-01	SEEDING (WITH MULCH)	UNIT	1
④⑬ 801-03	WATER (SEEDING AND SODDING)	M.G.	1

### FOOTNOTES

- ① ALL DIMENSIONAL DETAILS SHOWN ON PLANS, INCLUDING ELEVATIONS, SHALL BE CHECKED BY THE CONTRACTOR TO ASSURE ACCURACY OF THE LAYOUT PRIOR TO CONSTRUCTION. ALL BRIDGE SUBSTRUCTURES SHALL BE CHECKED AS TO LOCATION, DIMENSIONAL LAYOUTS AND ELEVATIONS, BY MEANS OF TWO INDEPENDENT LAYOUT METHODS. ANY ERRORS AND APPARENT DISCREPANCIES FOUND IN PREVIOUS SURVEYS, OR IN EITHER THE SPECIFICATIONS OR SPECIAL PROVISIONS, SHALL BE CALLED TO THE ENGINEER'S ATTENTION BY THE CONTRACTOR IMMEDIATELY AND PRIOR TO PROCEEDING WITH WORK.
- ② UNIT PRICE INCLUDES REMOVAL OF ALL MATERIALS NECESSARY TO INSTALL 9'-0" PAVEMENT REINFORCEMENT SECTION AT BRIDGE ENDS.
- ③ SEDIMENT SHALL BE REMOVED FROM BEHIND THE SILT FENCE WITH WIRE BACKING, DEWATERING STRUCTURE, CATCH BASIN PROTECTION OR SEDIMENT TUBE WHEN IT HAS ACCUMULATED TO ONE-HALF THE ORIGINAL HEIGHT OF THE STRUCTURE AND PAID FOR UNDER ITEM NO. 209-05, SEDIMENT REMOVAL, C.Y.
- ④ SEE SUBSECTION 209.07 OF THE STANDARD SPECIFICATIONS FOR MAINTENANCE REPLACEMENT. ALL QUANTITIES ARE TO BE USED AS DIRECTED BY THE ENGINEER.
- ⑤ PARTIAL RETAINING WALL REMOVAL FOR ADA RAMP CURBS AND CORING FOR 18" BRIDGE DRAIN.
- ⑥ STANDARD DRAWING STD-1-7 IS TO BE USED FOR BURIAL OF THE OUTLET PIPE AND FOR END TREATMENT DETAILS.
- ⑦ COST OF CLASS B BEDDING MATERIAL TO BE INCLUDED IN UNIT PRICE.
- ⑧ INCLUDES COST FOR REMOVAL OF EXISTING OR CONFLICTING PAVEMENT MARKINGS.
- ⑨ BASED ON SECTION 712.10 OF STANDARD SPECIFICATIONS, 272 S.F. INCLUDED FOR LOCAL DETOUR AND 828 S.F. FOR WIDE LOAD DETOUR.
- ⑩ SPRMS LOCATED ALONG CENTERLINES OF 2 DIRECTION ROADWAYS (YELLOW STRIPE) SHALL BE PAID FOR UNDER ITEM NO. 716-01.21 "BI-DIRECTIONAL SNOWPLOWABLE RAISED PAVEMENT MARKERS (REFLECTOR ON BOTH SIDES), PER EACH. LENSES SHALL BE YELLOW. REFER TO T-M-1 STANDARD DRAWING FOR DETAILS. TWO LANE ROADS WILL NORMALLY REQUIRE BI-DIRECTIONAL SNOWPLOWABLE RAISED PAVEMENT MARKERS.
- ⑪ THE CONTRACTOR MAY ELECT TO SUBSTITUTE PREFORMED PLASTIC FOR THERMOPLASTIC. PREFORMED PLASTIC SHALL BE PAID FOR AT THE SAME UNIT PRICE AS BID FOR THERMOPLASTIC.
- ⑫ ITEM MAY BE INCREASED, DECREASED OR ELIMINATED AS DIRECTED BY THE ENGINEER.
- ⑬ INCLUDES 1 THOUSAND GALLONS FOR EROSION PREVENTION AND SEDIMENT CONTROL.

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BRIDGE NO. 59-SR50-6.57  
STATE ROUTE 50  
OVER BIG ROCK CREEK  
MARSHALL COUNTY  
2015

ESTIMATED  
ROADWAY  
QUANTITIES

# ROADWAY GENERAL NOTES

## GRADING

- (1) ANY AREA THAT IS DISTURBED OUTSIDE LIMITS OF CONSTRUCTION DURING THE LIFE OF THIS PROJECT SHALL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE.
- (2) THE CONTRACTOR SHALL NOT DISPOSE OF ANY MATERIAL EITHER ON OR OFF STATE-OWNED R.O.W. IN A REGULATORY FLOODWAY AS DEFINED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY WITHOUT APPROVAL BY SAME. ALL MATERIAL SHALL BE DISPOSED OF IN UPLAND (NON-WETLAND) AREAS AND ABOVE ORDINARY HIGH WATER OF ANY ADJACENT WATERCOURSE. THIS DOES NOT ELIMINATE THE NEED TO OBTAIN ANY OTHER LICENSES OR PERMITS THAT MAY BE REQUIRED BY ANY OTHER FEDERAL, STATE OR LOCAL AGENCY.

## SEEDING AND SODDING

- (3) ITEM NO. 801-01, SEEDING (WITH MULCH), SHALL BE USED WHERE EROSION CONTROL BLANKET OR SOD ARE NOT APPLIED.

## DRAINAGE

- (4) THE CONTRACTOR SHALL SHAPE DITCHES TO THE SPECIFIED DESIGN. THIS WORK WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COST WILL BE INCLUDED IN THE COST OF OTHER ITEMS.
- (5) EXCAVATION FOR THE BRIDGE END DRAIN WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT WILL BE INCLUDED IN THE PRICE BID PER LINEAR FOOT OF PIPE (PIPE CULVERTS, STORM SEWERS, CONDUITS, ALL OTHER CULVERTS AND MINOR STRUCTURES).

## UTILITIES

- (6) THE LOCATIONS OF UTILITIES SHOWN WITHIN THESE PLANS ARE APPROXIMATE ONLY. EXACT LOCATIONS SHALL BE DETERMINED IN THE FIELD BY CONTACTING THE UTILITY COMPANIES INVOLVED. NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC., AT 1-800-351-1111 AS REQUIRED BY TCA 65-31-106 WILL BE REQUIRED.
- (7) UNLESS OTHERWISE NOTED, ALL UTILITY ADJUSTMENTS WILL BE PERFORMED BY THE UTILITY OR IT'S REPRESENTATIVE. THE CONTRACTOR AND UTILITY OWNERS WILL BE REQUIRED TO COOPERATE WITH EACH OTHER IN ORDER TO EXPEDITE THE WORK REQUIRED BY THIS CONTRACT. ON CONTRACTS WHERE CONSTRUCTION STAKES, LINES, AND GRADES ARE CONTRACT ITEMS, THE CONTRACTOR WILL BE REQUIRED TO PROVIDE RIGHT-OF-WAY OR SLOPE STAKES, DITCH OR STREAM BED GRADES, OR OTHER ESSENTIAL SURVEY STAKING TO PREVENT CONFLICTS WITH THE HIGHWAY CONSTRUCTION. FREQUENTLY, THIS WILL BE REQUIRED AS THE FIRST ITEM OF WORK AND AT ANY LOCATION ON THE PROJECT DIRECTED BY THE ENGINEER.
- (8) THE CONTRACTOR WILL PROVIDE ALL NECESSARY PROTECTIVE MEASURES TO SAFEGUARD EXISTING UTILITIES FROM DAMAGE DURING CONSTRUCTION OF THIS PROJECT. IN THE EVENT THAT SPECIAL EQUIPMENT IS REQUIRED TO WORK OVER AND AROUND THE UTILITIES, THE CONTRACTOR WILL BE REQUIRED TO FURNISH SUCH EQUIPMENT. THE COST OF PROTECTING UTILITIES FROM DAMAGE AND FURNISHING SPECIAL EQUIPMENT WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (9) PRIOR TO SUBMITTING HIS BID, THE CONTRACTOR WILL BE RESPONSIBLE FOR CONTACTING OWNERS OF ALL AFFECTED UTILITIES IN ORDER TO DETERMINE THE EXTENT TO WHICH UTILITY RELOCATIONS AND/OR ADJUSTMENTS WILL HAVE UPON THE SCHEDULE OF WORK FOR THE PROJECT. WHILE SOME WORK MAY BE REQUIRED AROUND UTILITY FACILITIES THAT WILL REMAIN IN PLACE, OTHER UTILITY FACILITIES MAY NEED TO BE ADJUSTED CONCURRENTLY WITH THE CONTRACTOR'S OPERATIONS. ADVANCE CLEAR CUTTING MAY BE REQUIRED BY THE ENGINEER AT ANY LOCATION WHERE CLEARING IS CALLED FOR IN THE SPECIFICATIONS AND CLEAR CUTTING IS NECESSARY FOR A UTILITY RELOCATION. ANY ADDITIONAL COST WILL BE INCLUDED IN THE UNIT PRICE BID FOR THE CLEARING ITEM SPECIFIED IN THE PLANS.
- (10) THE CONTRACTOR SHALL NOTIFY EACH INDIVIDUAL UTILITY OWNER OF HIS PLAN OF OPERATION IN THE AREA OF THE UTILITIES. PRIOR TO COMMENCING WORK, THE CONTRACTOR SHALL CONTACT THE UTILITY OWNERS AND REQUEST THEM TO PROPERLY LOCATE THEIR RESPECTIVE UTILITY ON THE GROUND. THIS NOTIFICATION SHALL BE GIVEN AT LEAST THREE (3) BUSINESS DAYS PRIOR TO COMMENCEMENT OF OPERATIONS AROUND THE UTILITY IN ACCORDANCE WITH TCA 65-31-106.

## MISCELLANEOUS

- (11) ALL DETOUR, ACCESS, SERVICE AND FRONTAGE ROADS SHALL BE CONSTRUCTED WITH A MINIMUM OF ONE (1) COURSE OF BASE MATERIAL BEFORE TRAFFIC IS INTERRUPTED ON EXISTING ROADS.
- (12) THE CONTRACTOR SHALL BE REQUIRED TO REMOVE AND RESET MAILBOXES WHERE AND AS DIRECTED BY THE ENGINEER.
- (13) NOTHING IN THE GENERAL NOTES OR SPECIAL PROVISIONS SHALL RELIEVE THE CONTRACTOR FROM HIS RESPONSIBILITIES TOWARD THE SAFETY AND CONVENIENCE OF THE GENERAL PUBLIC AND THE RESIDENTS ALONG THE PROPOSED CONSTRUCTION AREA.

## ROAD CLOSURE

- (14) NO LESS THAN SEVEN (7) DAYS PRIOR TO THE CLOSURE OF THE ROAD, THE CONTRACTOR SHALL NOTIFY THE FOLLOWING INDIVIDUALS OR AGENCIES COMPLETELY DESCRIBING THE AFFECTED ROADS AND THE APPROXIMATE DURATION OF THE CONSTRUCTION: THESE PARTIES INCLUDE, BUT ARE NOT LIMITED TO: (1) LOCAL LAW ENFORCEMENT OFFICE, (2) LOCAL FIRE DEPARTMENT, (3) AMBULANCE SERVICE, (4) LOCAL SCHOOL SUPERINTENDENT, (5) UNITED STATES POSTAL SERVICE, AND (6) LOCAL ROAD SUPERINTENDENT.

## PAVEMENT MARKINGS

- (15) PERMANENT PAVEMENT LINE MARKINGS SHALL BE 4 INCH SPRAY THERMOPLASTIC (60 MIL) INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-13.01, SPRAY THERMO PVMT MRKNG (60 MIL) (4IN LINE), L.M. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.

## PAVING

- (16) THE CONTRACTOR SHALL BE REQUIRED TO COLD PLANE AND PAVE IN THE DIRECTION OF TRAFFIC.

## RESURFACING

- (17) WHERE DIRECTED BY THE TDOE ENGINEER, THE CONTRACTOR SHALL BE REQUIRED TO SHAPE PUBLIC SIDE ROADS, BUSINESS ENTRANCES, AND PRIVATE DRIVES, AS WELL AS CLEANING OF EXISTING DRAINS BEFORE PLACING MATERIALS. ALL COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (18) ALL PUBLIC SIDE ROADS SHALL BE PAVED ONE PAVEMENT WIDTH THROUGH THE INTERSECTION AS A MINIMUM. A SATISFACTORY TRANSITION FROM THE NEW PAVEMENT TO THE EXISTING GRADE OF THE INTERSECTING PUBLIC ROAD OR BUSINESS ENTRANCE SHALL BE PROVIDED. SHOULD THE PAVEMENT OF THE INTERSECTING PUBLIC ROAD BE DISTRESSED, THE RESURFACING WIDTH MAY BE INCREASED TO THE NORMAL RIGHT OF WAY LINE.
- (19) PRIVATE DRIVEWAYS, FIELD ENTRANCES, AND BUSINESS ENTRANCES WILL BE RESURFACED A PAVEMENT WIDTH (LANE WIDTH) AS A MINIMUM. A PAVEMENT TAPER TO TRANSITION THE NEW PAVEMENT SHALL BE REQUIRED, IT SHALL BE BASED ON AN ADDITIONAL ONE FOOT OF WIDTH PER ONE INCH DEPTH OF PAVEMENT. IF THE SHOULDER IS NARROW ENOUGH THAT THE SUM OF THE SHOULDER AND THE TRANSITION ARE LESS THAN A PAVEMENT WIDTH, THE TRANSITION SHALL OCCUR WITHIN THE PAVEMENT WIDTH. IF THE SUM OF THE SHOULDER AND THE TRANSITION IS GREATER THAN A PAVEMENT WIDTH (LANE WIDTH), THE TRANSITION SHALL OCCUR OUTSIDE OF THE PAVEMENT WIDTH.
- (20) IN ALL CASES, THE LENGTH OF THE PAVEMENT TRANSITION, THE THICKNESS AND WIDTH OF THE RESURFACING AND ANY ADDITIONAL PAVEMENT MATERIALS SHALL BE AS DIRECTED BY THE TDOE ENGINEER.

## CONSTRUCTION WORK ZONE AND TRAFFIC CONTROL

- (21) ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE WEEK BEFORE NEEDED, IF THE SIGN FACE IS FULLY COVERED.
- (22) IF THE CONTRACTOR MOVES OFF THE PROJECT, HE SHALL COVER OR REMOVE ALL UNNEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OF REMOVAL, COVERING, AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION) PER SQUARE FOOT.
- (23) A LONG TERM BUT SPORADIC USE WARNING SIGN, SUCH AS A FLAGGER SIGN, MAY REMAIN IN PLACE WHEN NOT REQUIRED PROVIDED THE SIGN FACE IS FULLY COVERED.
- (24) TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.
- (25) USE OF BARRICADES, PORTABLE BARRIER RAILS, VERTICAL PANELS, AND DRUMS SHALL BE LIMITED TO THE IMMEDIATE AREAS OF CONSTRUCTION WHERE A HAZARD IS PRESENT. THESE DEVICES SHALL NOT BE STORED ALONG THE ROADWAY WITHIN THIRTY (30) FEET OF THE EDGE OF THE TRAVELED WAY BEFORE OR AFTER USE UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL INCREASE TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. THESE DEVICES SHALL BE REMOVED FROM THE CONSTRUCTION WORK ZONE WHEN THE ENGINEER DETERMINES THEY ARE NO LONGER NEEDED. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.

- (26) THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY, WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHEN THE LANE IS OPEN TO TRAFFIC UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO PARK WITHIN THIRTY (30) FEET OF A OPEN TRAFFIC LANE AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.

- (27) ALL DETOUR AND CONSTRUCTION SIGNING SHALL BE IN STRICT ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

## LIGHTING

- (28) INSTALLATION AND MATERIALS SHALL COMPLY WITH SECTIONS 714 AND 917 OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION DATED JANUARY 1, 2015 AND WITH THE LATEST REVISIONS TO THE NATIONAL ELECTRIC CODE, NFPA 70.
- (29) ALL WIRING SHALL BE CONCEALED UNDERGROUND IN 2-INCH SCHEDULE 40 PVC RIGID CONDUIT.
- (30) THE GROUND WIRE SHALL BE RUN INSIDE CONDUIT WITHIN STRUCTURES, SHALL BE COLORED GREEN AND HAVE THW INSULATION.
- (31) EXISTING FOUNDATIONS TO BE REMOVED A MINIMUM OF SIX INCHES BELOW GRADE.
- (32) ALL INCIDENTAL EQUIPMENT AND MATERIAL REQUIRED FOR THE SUCCESSFUL EXECUTION OF THIS WORK SHALL BE FURNISHED IN 714 ITEMS WHETHER SPECIFICALLY NOTED OR NOT.
- (33) LIGHT STANDARDS SHALL BE ROUND TAPERED POLES. LENGTH SHALL BE DETERMINED BY REQUIRED MOUNTING HEIGHT.
- (34) STANDARDS SHALL BE DESIGNED IN ACCORDANCE WITH THE REQUIREMENTS OF THE LATEST EDITION OF THE STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORT FOR HIGHWAY SIGNS, LUMINARIES AND TRAFFIC SIGNALS PUBLISHED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS.
- (35) STANDARDS SHALL BE DESIGNED FOR 80-MPH WIND PRESSURE AND SHALL SUPPORT A 62-POUND LUMINAIRE ON A 15-FOOT ARM.
- (36) ALL NEW ROADWAY LIGHT STANDARDS SHALL BE MOUNTED ON BASES WITH ACCESS DOOR, TRANSFORMER BASES SHALL MEET AASHTO SPECIFICATIONS AND HAVE FHWA APPROVAL. STANDARDS SHALL BE ALUMINUM WITH TRANSFORMER BASES.
- (37) BRACKET ARMS SHALL BE ROUND TAPERED TRUSS TYPE WITH STRAP MOUNTING AND LENGTHS AS SCHEDULED.
- (38) BRACKET ARM UPSWEEP SHALL BE THE SAME FOR ALL LIGHT STANDARDS OF THE SAME TYPE.

## EROSION PREVENTION AND SEDIMENT CONTROL DISTURBED AREA

- (39) AREAS TO BE UNDISTURBED SHALL BE CLEARLY MARKED IN THE FIELD BEFORE CONSTRUCTION ACTIVITIES BEGIN. THE TOTAL AREA TO BE DISTURBED NOT INCLUDING EXCLUSIVE BORROW/WASTE AREAS AND STAGING AREAS IS 0.095 ACRES. IF THE TOTAL DISTURBED AREA FOR THE PROJECT SITE (INCLUDING STAGING AND EXCLUSIVE WASTE/BORROW AREAS) INCREASES TO MORE THAN 1 ACRE, THEN THE NATURAL RESOURCES OFFICE OF THE ENVIRONMENTAL DIVISION MUST BE NOTIFIED SO THAT A SWPPP CAN BE PREPARED.
- (40) PRE-CONSTRUCTION VEGETATIVE GROUND COVER SHALL NOT BE DESTROYED, REMOVED OR DISTURBED (I.E. CLEARING AND GRUBBING INITIATED) MORE THAN 15 CALENDAR DAYS (10 CALENDAR DAYS FOR SITES WITH AN ACTIVE ARAP) PRIOR TO GRADING OR EARTH MOVING ACTIVITIES UNLESS THE AREA IS MULCHED, SEEDED WITH MULCH, OR OTHER TEMPORARY COVER IS INSTALLED.
- (41) CLEARING, GRUBBING, AND OTHER DISTURBANCE TO RIPARIAN VEGETATION SHALL BE LIMITED TO THE MINIMUM NECESSARY FOR SLOPE CONSTRUCTION AND EQUIPMENT OPERATIONS. EXISTING VEGETATION SHOULD BE PRESERVED TO THE MAXIMUM EXTENT POSSIBLE. UNNECESSARY VEGETATION REMOVAL IS PROHIBITED.
- (42) ALL DISTURBED AREAS SHALL BE PROPERLY STABILIZED AS SOON AS PRACTICABLE. PRIORITY SHALL BE GIVEN TO FINISHING OPERATIONS AND PERMANENT EPSC MEASURES OVER TEMPORARY EPSC MEASURES ON ALL PROJECTS.
- (43) CONSTRUCTION SHALL BE SEQUENCED AND STAGED TO MINIMIZE THE EXPOSURE TIME OF GRADED OR DENUDED SOIL AREAS, PRESERVE TOPSOIL, AND MINIMIZE SOIL COMPACTION.

TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	2015	59006-4219-04	2C

UNOFFICIAL  
SET  
  
NOT FOR  
BIDDING

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

BRIDGE NO. 59-SR50-6.57  
STATE ROUTE 50  
OVER BIG ROCK CREEK  
MARSHALL COUNTY  
2015

ROADWAY  
GENERAL NOTES

# ROADWAY GENERAL NOTES (CONT'D)

## EROSION PREVENTION AND SEDIMENT CONTROL DISTURBED AREA (CONT'D)

(44) IF OFFSITE BORROW AND WASTE AREAS BECOME NECESSARY DURING THE LIFE OF THE PROJECT, THIS SUPPORT ACTIVITY SHALL BE ADDRESSED PER THE TDOT WASTE AND BORROW MANUAL.

### SEDIMENT CONTROL

(45) EPSC MEASURES SHALL BE INSTALLED AND FUNCTIONAL PRIOR TO ANY EARTH MOVING OPERATIONS, AND SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD.  
 (46) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT THE OFF-SITE MIGRATION OR DEPOSIT OF SEDIMENT ON ROADWAYS USED BY THE GENERAL PUBLIC. IF SEDIMENT ESCAPES THE CONSTRUCTION SITE, OFF-SITE ACCUMULATIONS OF SEDIMENT THAT HAVE NOT REACHED A STREAM MUST BE REMOVED AT A FREQUENCY SUFFICIENT TO MINIMIZE OFF-SITE IMPACTS (E.G., FUGITIVE SEDIMENT THAT HAS ESCAPED THE CONSTRUCTION SITE AND HAS COLLECTED IN A STREET MUST BE REMOVED SO THAT IT IS NOT SUBSEQUENTLY WASHED INTO STORM SEWERS AND STREAMS BY THE NEXT RAIN AND/OR SO THAT IT DOES NOT POSE A SAFETY HAZARD TO USERS OF PUBLIC STREETS). ARRANGEMENTS CONCERNING REMOVAL OF SEDIMENT ON ADJOINING PROPERTY MUST BE SETTLED WITH THE ADJOINING PROPERTY OWNER BEFORE REMOVAL OF SEDIMENT.

(47) WATER PUMPED FROM WORK AREAS AND EXCAVATION MUST BE HELD IN SETTLING BASINS OR TREATED BY FILTRATION OR CHEMICAL TREATMENT PRIOR TO ITS DISCHARGE INTO SURFACE WATERS. ALL PHYSICAL AND/OR CHEMICAL TREATMENT WILL BE APPLIED IN ACCORDANCE WITH MANUFACTURER'S GUIDELINES AND FULLY DESCRIBED IN THE EPSC PLANS. WATER MUST BE HELD IN SETTLING BASINS UNTIL AT LEAST AS CLEAR AS THE RECEIVING WATERS. SETTLING BASINS SHALL NOT BE LOCATED CLOSER THAN 20 FEET FROM THE TOP BANK OF A STREAM. SETTLING BASINS AND SEDIMENT TRAPS SHALL BE PROPERLY DESIGNED ACCORDING TO THE SIZE OF THE DRAINAGE AREAS OR VOLUME OF WATER TO BE TREATED. TREATED WATER MUST BE DISCHARGED THROUGH A PIPE OR WELL-VEGETATED OR LINED CHANNEL, SO THAT THE DISCHARGE DOES NOT CAUSE EROSION OR SEDIMENT TRANSPORT. DISCHARGES FROM BASINS AND IMPOUNDMENTS SHALL UTILIZE OUTLET STRUCTURES THAT ONLY WITHDRAW WATER FROM NEAR THE SURFACE OF THE BASIN OR IMPOUNDMENT. DISCHARGES MUST NOT CAUSE AN OBJECTIONABLE COLOR CONTRAST WITH THE RECEIVING STREAM.

(48) CHECK DAMS SHALL BE USED WHERE RUNOFF IS CONCENTRATED. CLEAN ROCK, BRUSH, GABION, OR SANDBAG CHECK DAMS SHALL BE PROPERLY CONSTRUCTED TO REDUCE VELOCITY AND CONTROL EROSION.

(49) DELAYING PLANTING OF PERMANENT COVER VEGETATION UNTIL WINTER MONTHS OR DRY MONTHS SHOULD BE AVOIDED, IF POSSIBLE.

(50) OFFSITE VEHICLE TRACKING OF SEDIMENTS AND THE GENERATION OF DUST SHALL BE MINIMIZED. A STABILIZED CONSTRUCTION ACCESS (A POINT OF ENTRANCE/EXIT TO THE CONSTRUCTION PROJECT) SHALL BE PROVIDED, AS NEEDED, TO REDUCE THE TRACKING OF MUD AND DIRT ONTO PUBLIC ROADS BY CONSTRUCTION VEHICLES.

(51) TEMPORARY EPSC MEASURES MAY BE REMOVED AT THE BEGINNING OF THE WORKDAY, BUT MUST BE REPLACED AT THE END OF THE WORKDAY.

(52) ALL PHYSICAL AND/OR CHEMICAL TREATMENT WILL BE APPLIED IN ACCORDANCE WITH MANUFACTURER'S GUIDELINES AND DESCRIBED ON THE EPSC PLANS FOR ALL PROJECTS REQUIRING ADDITIONAL PHYSICAL OR CHEMICAL TREATMENT OF STORMWATER RUNOFF.

### STREAM/WETLAND

(53) SOIL MATERIALS MUST BE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. EPSC MEASURES TO PROTECT WATER QUALITY MUST BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD. APPROPRIATE EPSC MEASURES MUST BE INSTALLED ALONG THE BASE OF ALL FILLS AND CUTS, ON THE DOWNHILL SIDE OF STOCKPILED SOIL, AND ALONG STREAM BANKS IN CLEARED AREAS TO PREVENT SEDIMENT MIGRATION INTO STREAMS IN ACCORDANCE WITH TDOT STANDARDS. THEY MUST BE INSTALLED ON THE CONTOUR, ENTRENCHED AND STAKED, AND EXTEND THE WIDTH OF THE AREA TO BE CLEARED.

(54) INSTREAM EPSC DEVICES REQUIRE THE ENVIRONMENTAL DIVISION'S PERMITS SECTION REVIEW AND MUST BE PROCESSED BY THE PERMITS SECTION TO OBTAIN TDEC, USACE, AND TVA PERMITS.

(55) THE OPERATION OF EQUIPMENT IN WATERS OF THE STATE/U.S., INCLUDING WETLANDS, SHALL BE ONLY AS SHOWN ON THE PROJECT PLANS AND/OR AS SO SPECIFIED IN THE ARAP/401, SECTION 404 PERMITS(S) AND/OR TVA26(A), IF APPLICABLE. ANY ADDITIONAL PERMITS REQUIRED BY THE CONTRACTOR'S METHOD OF OPERATION SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO OBTAIN, AFTER RECEIVING THE APPROVAL OF TDOT ENVIRONMENTAL DIVISION.

(56) STREAM BEDS SHALL NOT BE USED AS TRANSPORTATION ROUTES FOR CONSTRUCTION EQUIPMENT. TEMPORARY CROSSING MUST BE LIMITED TO ONE POINT PER STREAM AND EPSC MEASURES MUST BE USED WHERE THE STREAM BANKS ARE DISTURBED. WHERE THE STREAMBED IS NOT COMPOSED OF BEDROCK, A PAD OF CLEAN ROCK MUST BE USED AT THE CROSSING POINT AND CULVERTED TO PREVENT THE IMPOUNDMENT OF WATER FLOW. CLEAN ROCK IS ROCK OF VARIOUS TYPE AND SIZE, DEPENDING UPON APPLICATION, WHICH CONTAINS NO FINES, SOILS, OR OTHER WASTES OR CONTAMINANTS. OTHER MATERIALS USED FOR ALL TEMPORARY FILLS MUST BE COMPLETELY REMOVED IN THEIR ENTIRETY AFTER THE WORK IS COMPLETED AND THE AFFECTED AREAS RETURNED TO THEIR PREEXISTING ELEVATION. ALL TEMPORARY CROSSINGS MUST BE CONSTRUCTED IN ACCORDANCE WITH STD. DWG. EC-STR-25 UNLESS SPECIFICALLY ADDRESSED IN THE EPSC PLANS. ALTERNATIVELY, PLACING A TEMPORARY BRIDGE (BAILEY BRIDGE OR EQUIVALENT, TIMBERS, ETC.) FROM TOP OF BANK TO TOP OF BANK OR THE APPROPRIATE USE OF BARGES AT THE CROSSING TO AVOID DISTURBANCE OF THE STREAMBED IS AN ACCEPTABLE OPTION.

(57) HEAVY EQUIPMENT WORKING IN WETLANDS MUST BE PLACED ON MATS, OR OTHER MEASURES MUST BE TAKEN TO MINIMIZE SOIL DISTURBANCE UNLESS SPECIFICALLY ADDRESSED IN THE EPSC PLANS. ANY MATS AND OTHER MEASURES USED FOR HEAVY EQUIPMENT MUST BE REMOVED IN THEIR ENTIRETY AFTER THE WORK IS COMPLETED.

(58) WETLANDS SHALL NOT BE USED AS EQUIPMENT STORAGE, STAGING, OR TRANSPORTATION AREAS, UNLESS PROVIDED FOR IN THE PLANS.

### SPECIES

(59) NO ACTIVITY MAY SUBSTANTIALLY DISRUPT THE MOVEMENT OF THOSE SPECIES OF AQUATIC LIFE INDIGENOUS TO THE WATER BODY, INCLUDING THOSE SPECIES THAT NORMALLY MIGRATE THROUGH THE AREA. THE SWPPP SHALL BE MODIFIED TO INCLUDE EPSC MEASURES TO PREVENT NEGATIVE IMPACTS TO LEGALLY PROTECTED STATE OR FEDERAL FAUNA OR FLORA OR AS INDICATED IN THE ECOLOGICAL STUDIES OR ON THE PERMITS(S).

### INSPECTION, MAINTENANCE, REPAIR

(60) EPSC CONTROLS WILL BE INSTALLED AND MAINTAINED IN ACCORDANCE WITH TDOT STANDARD DRAWINGS AND GOOD ENGINEERING PRACTICES.

(61) INSPECTION, REPAIR, AND MAINTENANCE OF EPSC MEASURES/STRUCTURES IS TO BE PERFORMED ON A REGULAR BASIS. SEDIMENT SHALL BE REMOVED FROM SEDIMENT CONTROL STRUCTURES WHEN THE DESIGN CAPACITY HAS BEEN REDUCED BY FIFTY PERCENT (50%). DURING SEDIMENT REMOVAL, THE CONTRACTOR SHALL TAKE CARE TO ENSURE THAT STRUCTURAL COMPONENTS OF EPSC MEASURES ARE NOT DAMAGED AND THUS MADE INEFFECTIVE. IF DAMAGE DOES OCCUR, THE CONTRACTOR SHALL REPAIR THE STRUCTURES AT THE CONTRACTOR'S OWN EXPENSE.

(62) SEDIMENT REMOVED FROM SEDIMENT CONTROL STRUCTURES SHALL BE PLACED AND BE TREATED IN A MANNER SO THAT THE SEDIMENT IS CONTAINED WITHIN THE PROJECT LIMITS AND DOES NOT MIGRATE INTO WATERS OF THE STATE/U.S. COST FOR THIS TREATMENT IS TO BE INCLUDED IN PRICE BID FOR ITEM NO. 209-05 SEDIMENT REMOVAL, C.Y.

(63) DISCHARGE POINTS SHALL BE INSPECTED TO ASCERTAIN WHETHER EPSC MEASURES ARE EFFECTIVE IN PREVENTING SIGNIFICANT IMPACTS TO SURROUNDING WATERS. WHERE DISCHARGE LOCATIONS ARE INACCESSIBLE, NEARBY DOWNSTREAM LOCATIONS SHALL BE INSPECTED. LOCATIONS WHERE VEHICLES ENTER AND EXIT THE SITE SHALL BE INSPECTED FOR EVIDENCE OF OFF-SITE ROADWAY SEDIMENT TRACKING.

(64) THE TDOT PROJECT SUPERVISOR (OR THEIR DESIGNEE) AND THE CONTRACTOR'S SITE SUPERINTENDENT ARE RESPONSIBLE FOR INSPECTIONS, MAINTENANCE AND REPAIR ACTIVITIES ARE THE RESPONSIBILITY OF THE CONTRACTOR.

(65) EPSC MEASURES FOUND TO BE INEFFECTIVE SHALL BE REPAIRED, REPLACED, OR MODIFIED BEFORE THE NEXT RAIN EVENT, IF POSSIBLE.

### MATERIALS AND STAGING

(66) WASTE AND BORROW AREAS SHALL BE LOCATED IN NON-WETLAND AREAS AND ABOVE THE 100-YEAR, FEDERAL EMERGENCY MANAGEMENT AGENCY FLOODPLAIN. BORROW AND WASTE DISPOSAL AREAS SHALL NOT AFFECT ANY WATERS OF THE STATE/U.S. UNLESS THESE AREAS ARE SPECIFICALLY COVERED BY AN ARAP, 404, OR NPDES PERMIT, OBTAINED SOLELY BY THE CONTRACTOR.

(67) THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR AND OBTAIN ANY NECESSARY ENVIRONMENTAL PERMITS OR APPROVALS, INCLUDING BUT NOT LIMITED TO ARCHAEOLOGY, ECOLOGY, HISTORICAL, HAZARDOUS MATERIALS, AIR AND NOISE, TDEC ARAP/401, USACE SECTION 404, TVA SECTION 26A, AND TDEC NPDES PERMITS, FROM FEDERAL, STATE AND/OR LOCAL AGENCIES REGARDING ANY STAGING AREAS TO BE USED. ANY SUCH PERMITS SHALL BE SUPPLIED TO THE TDOT PROJECT RESPONSIBLE PARTY PRIOR TO THE USE OF THE PERMITTED AREAS.

### PERMITS, PLANS, RECORDS

(68) THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR AND OBTAIN ANY NECESSARY ENVIRONMENTAL PERMITS OR APPROVALS, INCLUDING BUT NOT LIMITED TO TDEC ARAP/401, USACE SECTION 404, TVA SECTION 26A, AND TDEC NPDES PERMITS, FROM FEDERAL, STATE AND/OR LOCAL AGENCIES REGARDING THE OPERATION OF ANY PROJECT-DEDICATED ASPHALT AND/OR CONCRETE PLANTS.

(69) ANY DISAGREEMENT BETWEEN THE PROJECT PLANS, THE PROJECT AS CONSTRUCTED, AND THE PERMITS(S) ISSUED FOR THE PROJECT, SHALL BE BROUGHT TO THE ATTENTION OF THE TDOT PROJECT ENGINEER, THE ENVIRONMENTAL DIVISION, ROADWAY DESIGN DIVISION, AND HEADQUARTERS CONSTRUCTION OFFICE SHALL BE CONTACTED IN THESE INSTANCES AND DECIDE WHICH HAS PRECEDENCE AND WHETHER PERMIT OR PLANS REVISIONS ARE NEEDED. IN GENERAL, PERMIT CONDITIONS WILL PREVAIL.

(70) ALL PROJECT RELATED ENVIRONMENTAL PERMITS SHALL BE MAINTAINED AT OR NEAR THE PROJECT SITE.

(71) IF A CHANGE IN PROJECT SCOPE OCCURS DURING CONSTRUCTION, INCLUDING VALUE ENGINEERING, THE ENVIRONMENTAL DIVISION SHALL BE CONTACTED TO DETERMINE WHETHER PERMIT REVISIONS ARE NEEDED. THE ROADWAY DESIGN DIVISION SHALL BE CONTACTED TO DETERMINE IF ANY PLAN REVISIONS ARE NEEDED.

(72) THE EPSC PLAN SHALL BE UPDATED WHENEVER EPSC INSPECTIONS INDICATE, OR WHERE STATE OR FEDERAL OFFICIALS DETERMINE EPSC MEASURES ARE PROVING INEFFECTIVE IN ELIMINATING OR SIGNIFICANTLY MINIMIZING POLLUTANT SOURCES OR ARE OTHERWISE NOT ACHIEVING THE GENERAL OBJECTIVES OF CONTROLLING POLLUTANTS IN STORM WATER DISCHARGES ASSOCIATED WITH THE CONSTRUCTION ACTIVITY.

(73) THE TDOT PROJECT RESPONSIBLE PARTY (INCLUDING TDOT STAFF, CONSULTANTS AND CONTRACTOR STAFF) RESPONSIBLE FOR THE IMPLEMENTATION AND MAINTENANCE OF EPSC PLANS SHALL SUCCESSFULLY COMPLETE THE TDEC "LEVEL 1 - FUNDAMENTALS OF EROSION PREVENTION AND SEDIMENT CONTROL FOR CONSTRUCTION SITES" COURSE AND ANY REFRESHER COURSES AS REQUIRED TO MAINTAIN CERTIFICATION. A COPY OF CERTIFICATION RECORDS FOR THE COURSES SHALL BE KEPT ON SITE AND AVAILABLE UPON REQUEST.

(74) THE EPSC PLAN IS TO SERVE AS AN INITIAL GUIDE FOR SITE PERSONNEL AS THE CONSTRUCTION PROCESS DEVELOPS. IT MUST BE AMENDED, MODIFIED, AND UPDATED WHENEVER A CHANGE IN THE DESIGN OR CONSTRUCTION OF THE PROJECT OCCURS. THE PHASES DEPICTED IN THE EPSC PLANS MAY NOT COINCIDE WITH THE ACTUAL PHASES OF CONSTRUCTION ESTABLISHED BY THE CONTRACTOR DURING CONSTRUCTION. THUS MODIFICATIONS WILL BE REQUIRED TO ENSURE THE EPSC PLAN IS MAINTAINED TO DEPICT CURRENT SITE CONDITIONS. IT SHOULD BE MAINTAINED SUCH THAT IT WILL ALWAYS REFLECT THE MEASURES THAT ARE INSTALLED DURING THE VARIOUS STAGES OF CONSTRUCTION. IT IS IMPRACTICAL TO DETERMINE ALL THE INTERMEDIATE PHASES OF CONSTRUCTION THAT WILL OCCUR. THUS THESE DOCUMENTS WILL HAVE TO BE UPDATED THROUGHOUT THE LIFE OF THE CONSTRUCTION PROJECT.

### LITTER, DEBRIS, WASTE, PETROLEUM

(75) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT LITTER, CONSTRUCTION DEBRIS, AND CONSTRUCTION WASTES FROM ENTERING WATERS OF THE STATE/U.S. THESE MATERIALS WILL BE PICKED UP AND REMOVED FROM STORMWATER EXPOSURE PRIOR TO ANTICIPATED STORM EVENTS. AFTER USE, MATERIALS USED FOR EPSC WILL BE REMOVED FROM THE SITE.

(76) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS TO ENSURE THAT PETROLEUM PRODUCTS OR OTHER CHEMICAL POLLUTANTS ARE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. ALL EQUIPMENT REFUELING, SERVICING, AND STAGING AREAS SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL LAWS, RULES, REGULATIONS, AND ORDINANCES, INCLUDING THOSE OF THE NATIONAL FIRE PROTECTION ASSOCIATION (NFPA). APPROPRIATE CONTAINMENT MEASURES FOR THESE AREAS SHALL BE USED. ALL SPILLS MUST BE REPORTED TO THE APPROPRIATE AGENCY, AND MEASURES SHALL BE TAKEN IMMEDIATELY TO PREVENT THE POLLUTION OF WATERS OF THE STATE/U.S., INCLUDING GROUNDWATER, SHOULD A SPILL OCCUR.

(77) IF PORTABLE SANITARY FACILITIES ARE PROVIDED ON CONSTRUCTION SITES, SANITARY WASTE WILL BE COLLECTED FROM THE PORTABLE UNITS IN A TIMELY MANNER BY A LICENSED WASTE MANAGEMENT CONTRACTOR OR AS REQUIRED BY ANY LOCAL REGULATIONS. THE CONTRACTOR WILL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF SANITARY WASTE.

(78) ALL HAZARDOUS WASTE MATERIALS WILL BE DISPOSED OF IN A MANNER WHICH IS COMPLIANT WITH LOCAL OR STATE REGULATIONS. SITE PERSONNEL WILL BE INSTRUCTED IN THESE PRACTICES, AND THE INDIVIDUAL DESIGNATED AS THE CONTRACTOR'S ON-SITE REPRESENTATIVE WILL BE RESPONSIBLE FOR SEEING THAT THESE PRACTICES ARE FOLLOWED. THE CONTRACTOR WILL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF HAZARDOUS MATERIAL.

(79) WASTE MATERIAL (EARTH, ROCK, ASPHALT, CONCRETE, ETC.) NOT REQUIRED FOR THE CONSTRUCTION OF THE PROJECT WILL BE DISPOSED OF BY THE CONTRACTOR. THE CONTRACTOR WILL OBTAIN ANY AND ALL NECESSARY PERMITS INCLUDING, BUT NOT LIMITED TO NPDES, AQUATIC RESOURCES ALTERATION PERMIT(S) CORPS OF ENGINEERS SECTION 404 PERMITS, AND TVA SECTION 26A PERMITS TO DISPOSE OF WASTE MATERIALS.

### SPILL PREVENTION, MANAGEMENT AND NOTIFICATION

(80) ONLY NEEDED PRODUCTS WILL BE STORED ON-SITE BY THE CONTRACTOR. THE CONTRACTOR WILL STORE ALL MATERIALS UNDER COVER AND IN APPROPRIATE CONTAINERS. PRODUCTS MUST BE STORED IN ORIGINAL CONTAINERS AND LABELED. MATERIAL MIXING WILL BE CONDUCTED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS. THE CONTRACTOR'S SITE SUPERINTENDENT WILL INSPECT MATERIALS STORAGE AREAS REGULARLY TO ENSURE PROPER USE AND DISPOSAL.

(81) WHEN POSSIBLE, ALL PRODUCTS WILL BE USED COMPLETELY BEFORE PROPERLY DISPOSING OF THE CONTAINER OFF SITE. THE MANUFACTURER'S DIRECTIONS FOR DISPOSAL OF MATERIALS AND CONTAINERS WILL BE FOLLOWED.

(82) WHEEL WASH WATER WILL BE COLLECTED AND ALLOWED TO SETTLE OUT SUSPENDED SOLIDS PRIOR TO DISCHARGE. WHEEL WASH WATER WILL NOT BE DISCHARGED DIRECTLY INTO ANY STORMWATER SYSTEM OR STORMWATER TREATMENT SYSTEM.

(83) ALL ON-SITE VEHICLES WILL BE MONITORED FOR LEAKS AND RECEIVE MAINTENANCE TO REDUCE THE CHANCE OF LEAKAGE.

UNOFFICIAL  
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2015

ROADWAY  
GENERAL  
NOTES

TENNESSEE D.O.T.  
 DESIGN DIVISION  
 FILE NO.

## ROADWAY GENERAL AND SPECIAL NOTES (CONT'D)

### EROSION PREVENTION AND SEDIMENT CONTROL SPILL PREVENTION, MANAGEMENT AND NOTIFICATION (CONT'D)

- (84) FERTILIZERS WILL BE APPLIED ONLY IN THE AMOUNTS SPECIFIED BY TDOT. ONCE APPLIED, FERTILIZERS WILL BE WORKED INTO THE SOIL TO LIMIT THE EXPOSURE TO STORMWATER.
- (85) ALL PAINT CONTAINERS WILL BE TIGHTLY SEALED AND STORED WHEN NOT REQUIRED FOR USE. EXCESS PAINT WILL BE DISPOSED OF ACCORDING TO THE MANUFACTURER'S INSTRUCTIONS AND APPLICABLE STATE AND LOCAL REGULATIONS.
- (86) CONTRACTORS WILL PROVIDE DESIGNATED TRUCK WASHOUT AREAS ON THE SITE. THESE AREAS MUST BE SELF CONTAINED AND NOT CONNECTED TO ANY STORMWATER OUTLET OF THE SITE. UPON COMPLETION OF CONSTRUCTION WASHOUT AREAS WILL BE PROPERLY STABILIZED. WASH DOWN OR WASTE DISCHARGE OF CONCRETE TRUCKS WILL NOT BE PERMITTED ON-SITE UNLESS PROPER SETTLEMENT AREAS HAVE BEEN PROVIDED IN ACCORDANCE WITH BOTH STATE AND FEDERAL REGULATIONS.
- (87) APPROPRIATE CLEANUP MATERIALS AND EQUIPMENT WILL BE MAINTAINED BY THE CONTRACTOR IN THE MATERIALS STORAGE AREA ON-SITE AND UNDER COVER. SPILL RESPONSE EQUIPMENT WILL BE INSPECTED AND MAINTAINED BY THE CONTRACTOR AS NECESSARY TO REPLACE ANY MATERIALS USED IN SPILL RESPONSE ACTIVITIES.
- (88) ALL SPILLS WILL BE CLEANED IMMEDIATELY AFTER DISCOVERY AND THE MATERIALS DISPOSED OF PROPERLY. THE SPILL AREA WILL BE KEPT WELL VENTILATED AND PERSONNEL WILL WEAR APPROPRIATE PROTECTIVE CLOTHING TO PREVENT INJURY FROM CONTACT WITH A HAZARDOUS SUBSTANCE.
- (89) THE CONTRACTOR'S SITE SUPERINTENDENT WILL BE THE SPILL PREVENTION AND CLEANUP COORDINATOR. THE CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT THE SITE SUPERINTENDENT HAS HAD APPROPRIATE TRAINING FOR HAZARDOUS MATERIALS HANDLING, SPILL MANAGEMENT, AND CLEANUP.
- (90) IF OIL SHEEN IS OBSERVED ON SURFACE WATER (E.G. SETTLING PONDS, DETENTION PONDS, SWALES), ACTION WILL BE TAKEN IMMEDIATELY TO REMOVE THE MATERIAL CAUSING THE SHEEN. THE CONTRACTOR WILL USE APPROPRIATE MATERIALS TO CONTAIN AND ABSORB THE SPILL. THE SOURCE OF THE OIL SHEEN WILL ALSO BE IDENTIFIED AND REMOVED OR REPAIRED AS NECESSARY TO PREVENT FURTHER RELEASES.
- (91) WHERE A RELEASE CONTAINING A HAZARDOUS SUBSTANCE IN AN AMOUNT EQUAL TO OR IN EXCESS OF A REPORTABLE QUANTITY ESTABLISHED UNDER EITHER 40 CFR 117 OR 40 CFR 302 OCCURS DURING A 24 HOUR PERIOD, SEE THE LATEST TENNESSEE GENERAL PERMIT NO. TNRI00000 STORMWATER DISCHARGES FROM CONSTRUCTION ACTIVITIES SECTION 5.1 FOR REPORTING REQUIREMENTS.

## SPECIAL NOTES

### DEMOLITION, REPAIR, OR REHABILITATION OF BRIDGES

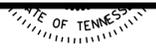
- (92) THE CONTRACTOR SHALL VERIFY THAT AN ASBESTOS SURVEY HAS BEEN COMPLETED PRIOR TO ANY DEMOLITION, REPAIR OR REHABILITATIONS ACTIVITIES (NOT INCLUDING ASPHALT MILLING OR OVERLAY).
- (93) ASBESTOS-CONTAINING MATERIALS (ACM) ABATEMENT IS THE RESPONSIBILITY OF THE CONTRACTOR AND SHALL BE COMPLETED PRIOR TO ANY DEMOLITION, REPAIR OR REHABILITATION OF BRIDGE(S). ABATEMENT SHOULD BE ACCOMPLISHED PER SP202ACM SPECIAL PROVISION REGARDING REMOVAL OF ASBESTOS-CONTAINING MATERIALS. STATE OF TENNESSEE ASBESTOS ACCREDITATION REQUIREMENTS (TCA 1200-01-20) MANDATE THAT ACM ABATEMENT WORK BE PERFORMED BY AN ACCREDITED FIRM (CONTRACTOR) USING ACCREDITED ABATEMENT WORKERS AND SUPERVISORS.
- (94) THE CONTRACTOR SHALL BE RESPONSIBLE FOR SUBMITTING A NOTICE TO THE TDEC, DIVISION OF AIR POLLUTION CONTROL TEN (10) DAYS IN ADVANCE OF ANY ACM ABATEMENT, DEMOLITION, OR MAJOR REPAIR INVOLVING THE REMOVAL/REPLACEMENT OF A STRUCTURAL COMPONENT.

### ENVIRONMENTAL

- (95) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION COMPREHENSIVE INSPECTION OFFICE SHALL BE INVITED TO ALL PRE-CONSTRUCTION MEETINGS.

TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	2015	59006-4219-04	2E

**UNOFFICIAL  
 SET**  
  
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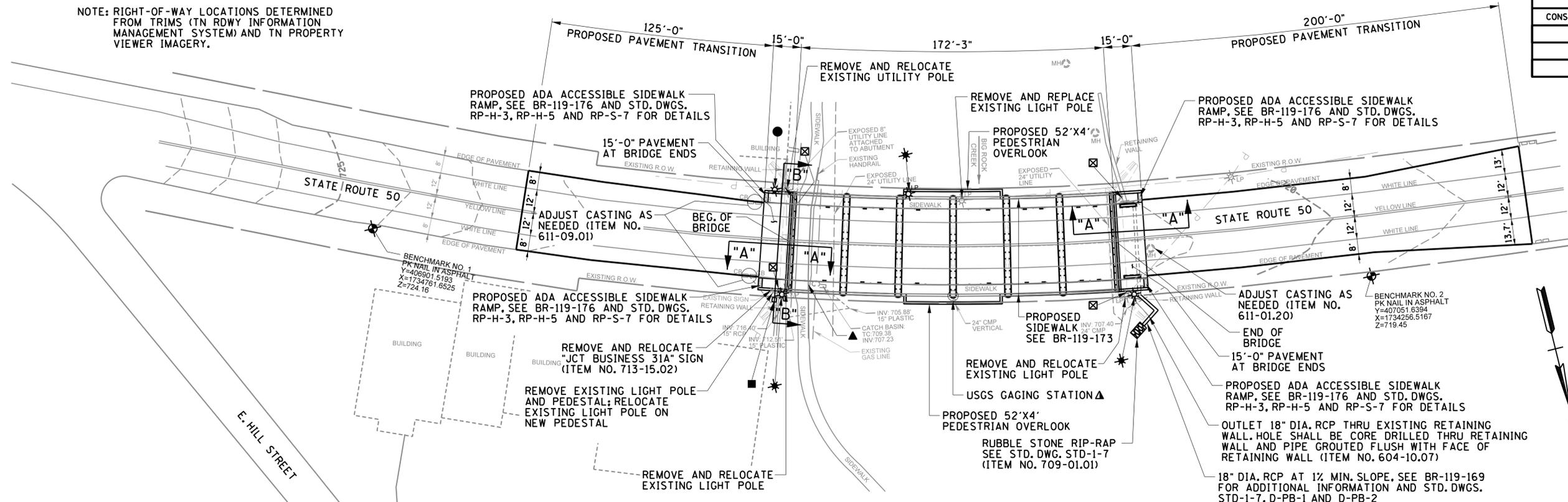
STATE OF TENNESSEE  
 DEPARTMENT OF TRANSPORTATION

BRIDGE NO. 59-SR50-6.57  
 STATE ROUTE 50  
 OVER BIG ROCK CREEK  
 MARSHALL COUNTY  
 2015

ROADWAY  
 GENERAL AND  
 SPECIAL NOTES

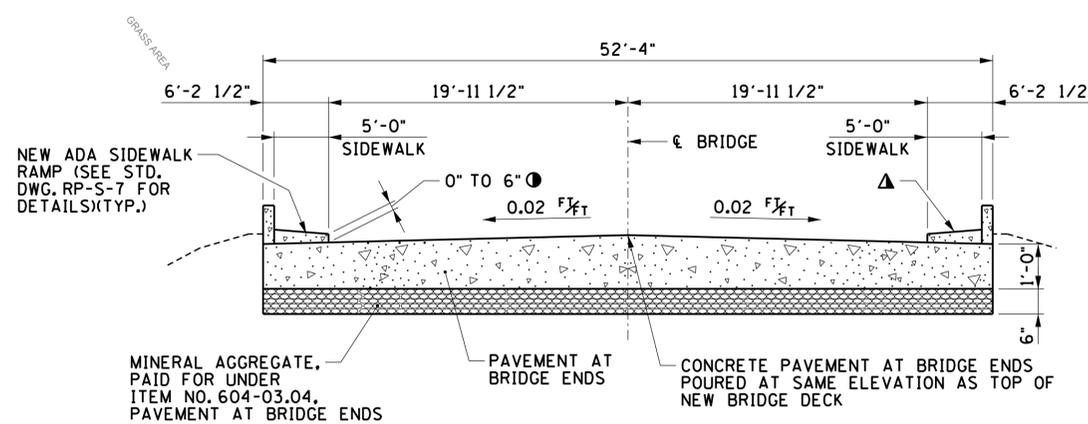
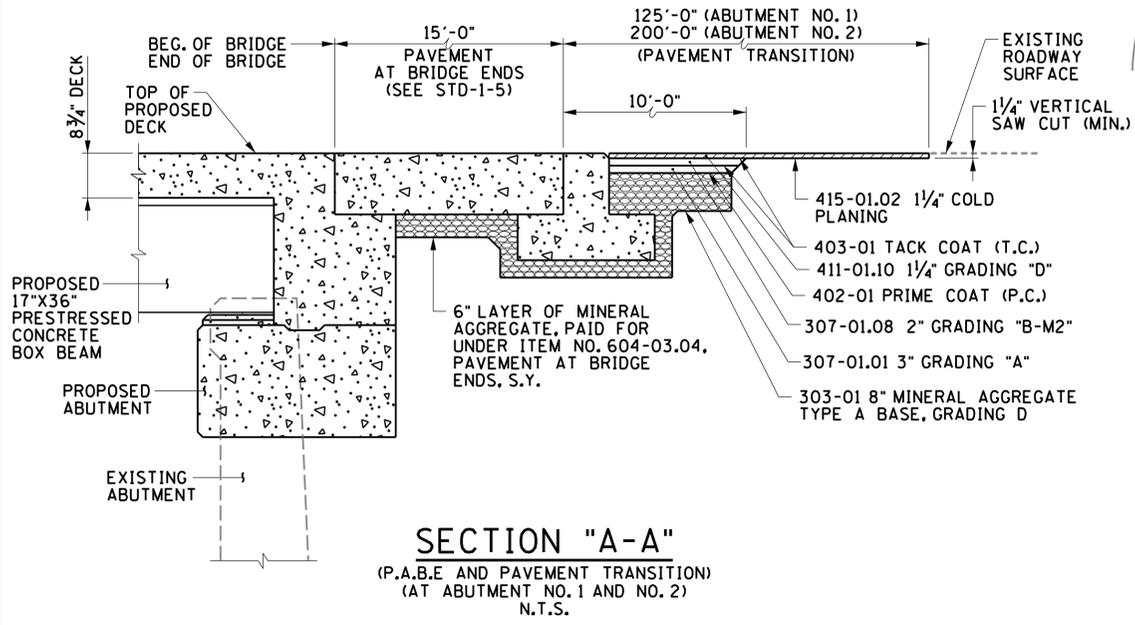
TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	2015	59006-4219-04	2F

NOTE: RIGHT-OF-WAY LOCATIONS DETERMINED FROM TRIMS (TN RDWY INFORMATION MANAGEMENT SYSTEM) AND TN PROPERTY VIEWER IMAGERY.

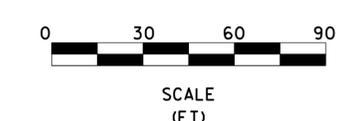


PLAN

- DENOTES: BRIDGE END DRAIN (2'x8'-7") ITEM NO. 604-03.04, FOR DETAILS SEE STD. DWG. STD-1-6, STD-1-7, AND STD-1-8 AND BR-119-169
- DENOTES: BRIDGE DECK DRAIN LOCATIONS AT 1/3 POINT OF SPANS, SEE STD-1-2SS FOR DETAILS, COST SHALL BE INCLUDED IN ITEM NO. 604-03.09, CLASS D CONCRETE (BRIDGE DECK)
- ▲ DENOTES: CONTRACTOR SHALL NOT DISTURB EXISTING HANDRAIL, CONTRACTOR TO REPAIR IF DAMAGED
- △ DENOTES: REMOVE USGS GAGING STATION ON THE NORTH END OF PIER NO. 3 (ITEM NO. 604-10.90)
- \* DENOTES: PROPOSED LOCATION OF NEW LIGHT POLE. FINAL LOCATION WILL BE DETERMINED IN THE FIELD WITH UTILITY OWNER AND TDOT ENGINEER
- DENOTES: PROPOSED LOCATION OF NEW POWER POLE. FINAL LOCATION WILL BE DETERMINED IN THE FIELD WITH UTILITY OWNER AND TDOT ENGINEER.
- DENOTES: PROPOSED LOCATION OF EXISTING LIGHT POLE AND NEW PEDESTAL. FINAL LOCATION WILL BE DETERMINED IN THE FIELD WITH OWNER AND TDOT ENGINEER.
- ⊠ DENOTES: REMOVE PORTION OF EXISTING RETAINING WALL, AS REQUIRED.



- ▲ DENOTES: NEW ADA SIDEWALK RAMP, SEE DWG. NO. BR-119-176 FOR DETAILS AND SEE STD. DWGS. RP-H-3, RP-H-5 AND RP-S-7 FOR HANDICAP RAMP DETAILS
- DENOTES: ADA SIDEWALK RAMP VARIES FROM 0" TO 6" THICK AND SHALL BE POURED MONOLITHICALLY WITH PAVEMENT AT BRIDGE ENDS



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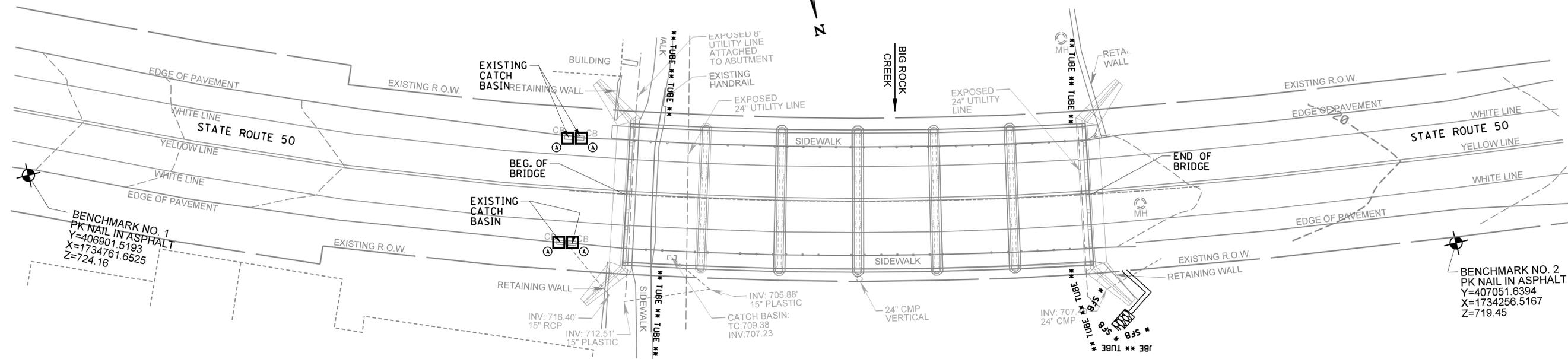
UNOFFICIAL SET  
NOT FOR BIDDING

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

ROADWAY DETAILS

EPSC LEGEND		
•SFB•SFB•	SILT FENCE WITH WIRE BACKING (SFB)	EC-STR-3C
□ <sub>A</sub>	CATCH BASIN PROTECTION (TYPE A)	EC-STR-19
•TUBE••TUBE•	SEDIMENT TUBE	EC-STR-37

TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	2015	59006-4219-04	26



**PLAN**

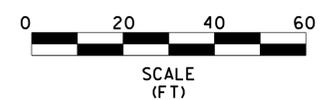
AREA OF DISTURBANCE = 4,138 S.F. (0.095 ACRES)

- NOTE: SILT FENCE SHALL BE INSTALLED ON EXISTING GROUND CONTOURS WITH A 3' MINIMUM UPHILL TURN ON EACH END.
- NOTE: SEEDING (WITH MULCH) SHALL BE PLACED WITH A 4' MINIMUM WIDTH AFTER REMOVAL OF THE SILT FENCE.
- NOTE: ALL WORK AND EQUIPMENT TO REMAIN INSIDE EXISTING RIGHT OF WAY.
- NOTE: SEE SHEET NO. 2B (ESTIMATED ROADWAY QUANTITIES) FOR FOOTNOTES TO EACH ITEM.

- NOTE: THE CONTRACTOR SHALL TAKE APPROPRIATE MEASURES BELOW THE BRIDGE DURING THE REMOVAL OF CONCRETE FROM SUBSTRUCTURES AND DECK TO PREVENT ANY DEBRIS FROM FALLING INTO THE WATER. COST FOR CONTAINMENT WILL NOT BE PAID DIRECTLY BUT SHALL BE INCLUDED IN ITEMS BID ON.
- NOTE: THE CONTRACTOR SHALL REMOVE ANY DEBRIS THAT FALLS ONTO THE AREAS BELOW THE BRIDGE, AND ANY PAINT CHIPS SHALL BE HANDLED AND DISPOSED OF PER THE SPECIFICATIONS SET FORTH IN THE GENERAL NOTES.
- NOTE: RIGHT-OF-WAY LOCATIONS DETERMINED FROM TRIMS (TN RDWY INFORMATION MANAGEMENT SYSTEM) AND TN PROPERTY VIEWER IMAGERY.

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EROSION CONTROL QUANTITIES			
ITEM NO.	DESCRIPTION	UNIT	QUANTITY
209-05	SEDIMENT REMOVAL	C.Y.	3
209-08.02	TEMPORARY SILT FENCE (WITH BACKING)	L.F.	42
209-40.30	CATCH BASIN PROTECTION (TYPE A)	EACH	4
740-11.01	TEMPORARY SEDIMENT TUBE (8 INCH) (STRAW WATTLES)	L.F.	157
801-01	SEEDING (WITH MULCH)	UNIT	1
801-03	WATER (SEEDING AND SODDING)	M.G.	1



BRIDGE NO. 59-SR50-6.57  
STATE ROUTE 50  
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2015

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

**EROSION CONTROL PLAN AND SPECIAL NOTES**

**TRAFFIC CONTROL QUANTITIES**

ITEM NO.	DESCRIPTION	UNIT	QUANTITY	SIZE	M.U.T.C.D. NO.	REMARKS
712-01	TRAFFIC CONTROL	LS	1			
712-05.01	WARNING LIGHTS (TYPE A)	EACH	4			
712-06	SIGNS (CONSTRUCTION)	S.F.	1,100			
712-07.03	TEMPORARY BARRICADES (TYPE III)	L.F.	106			
713-16.20	SIGNS (OM3-L)	EACH	2			
713-16.21	SIGNS (OM3-R)	EACH	2			
716-01.21	SNWPLWBLE PVMT MRKRS (BI-DIR)(1 COLOR)	EACH	8			
716-13.01	SPRAY THERMO PVMT MRKNG (60 MIL) (4IN LINE)	L.M.	0.40			
717-01	MOBILIZATION	LS	1			

NOTE: SEE SHEET NO. 2B (ESTIMATED ROADWAY QUANTITIES) FOR FOOTNOTES TO EACH ITEM.

**ITEM NO. 712-06 SIGNS (CONSTRUCTION)**

	DESCRIPTION	QUANTITY	ITEM NO. 712-06 (S.F.)	SIZE	M.U.T.C.D. NO.	REMARKS
	DETOUR	6	12	24" x 12"	M4-8	
	END DETOUR	2	6	24" x 18"	M4-8A	
	DETOUR LEFT	8	40	30" x 24"	M4-9L	
	DETOUR RIGHT	4	20	30" x 24"	M4-9R	
	DIRECTIONAL ARROW 45 LT	1	3	21" x 15"	M6-2L	
	DIRECTIONAL ARROW 45 RT	2	6	21" x 15"	M6-2R	
	DIRECTION ARROW STRAIGHT	3	9	21" x 15"	M6-3	
	SIDEWALK CLOSED	4	16	30" x 18"	R9-9	
	ROAD CLOSED	2	20	48" x 30"	R11-2	
	SR 50 EAST	13	91	52" x 18"	SP-1	
	SR 50 WEST	7	49	52" x 18"	SP-2	
<b>SHT 2J-TOTAL</b>			<b>272</b>			
	EAST	22	44	24" x 12"	M3-2	
	WEST	14	28	24" x 12"	M3-4	
	DETOUR	36	72	24" x 12"	M4-8	
	END DETOUR	5	15	24" x 18"	M4-8A	
	DIRECTIONAL ARROW LT	3	9	21" x 15"	M6-1L	
	DIRECTIONAL ARROW RT	1	3	21" x 15"	M6-1R	
	DIRECTIONAL ARROW 45 RT	4	12	21" x 15"	M6-2R	
	DIRECTION ARROW STRAIGHT	28	84	21" x 15"	M6-3	
	DETOUR AHEAD	5	45	36" x 36"	W20-2	
	DETOUR 1000 FT	5	45	36" x 36"	W20-2	
	SR 50 WIDE LOADS	31	341	52" x 30"	SP-3	
	SR 50 OVER BIG ROCK CREEK CLOSED TO WIDE LOADS	5	130	87" x 42"	SP-4	
<b>SHT 2K-TOTAL</b>			<b>828</b>			

NOTE: SEE SHEET NO. 2J (LOCAL DETOUR) AND SHEET NO. 2K (WIDE LOAD DETOUR) FOR SPECIAL NOTES AND APPROXIMATE SIGN PLACEMENT.

**TRAFFIC CONTROL SPECIAL NOTES**

- (1) THESE TRAFFIC CONTROL PLANS DO NOT RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY OF INSTALLING TRAFFIC CONTROL DEVICES IN ACCORDANCE WITH THE CURRENT EDITION OF THE "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES."
- (2) NO TRAFFIC SHALL BE DETOURED OR ROADWAY CLOSED, ABANDONED OR REMOVED WITHOUT THE PRIOR APPROVAL OF THE ENGINEER.
- (3) CONSTRUCTION SIGNING SHOWN IN THESE PLANS IS TO SERVE AS A GUIDE ONLY. OTHER SIGNS MAY BE REQUIRED DURING VARIOUS PHASES OF CONSTRUCTION.
- (4) PERMANENT SIGNS AND PERMANENT PAVEMENT MARKINGS SHALL BE IN PLACE BEFORE COMPLETED ROADWAYS ARE OPEN TO TRAFFIC.
- (5) THE CONTRACTOR SHALL MAINTAIN ALL TRAFFIC CONTROL DEVICES IN PROPER CONDITION THROUGHOUT THE DURATION OF THE PROJECT.
- (6) EXISTING SIGNS THAT CONFLICT WITH TEMPORARY TRAFFIC CONTROL SIGNING SHALL BE COVERED OR REMOVED AND STOCKPILED AS DIRECTED BY THE ENGINEER.
- (7) THE CONTRACTOR WILL BE REQUIRED TO HAVE A RESPONSIBLE PERSON ON CALL (WITHIN ONE HOUR), AT ALL TIMES, FOR TRAFFIC CONTROL DURING THE CONSTRUCTION OF THIS PROJECT.
- (8) AT ALL TIMES THE DETOUR MUST BE MAINTAINED IN A CONDITION TO ALLOW THE PASSAGE OF EMERGENCY VEHICLES.
- (9) IF ADDITIONAL SIGNS ARE DEEMED NECESSARY BY THE ENGINEER, THEY SHALL BE FURNISHED AND INSTALLED AT THE BID PRICE FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION), S.F.
- (10) ALL LOCAL EMERGENCY AGENCIES AND RESIDENCES WITHIN THE IMMEDIATE AREA PROJECT AREA SHALL BE NOTIFIED NOT LESS THAN 48 HOURS IN ADVANCE OF BRIDGE CONSTRUCTION WHICH MAY AFFECT ACCESS TO THESE AREAS.
- (11) NO LESS THAN SEVEN (7) DAYS PRIOR TO THE CLOSURE OF LANES, THE CONTRACTOR SHALL NOTIFY THE FOLLOWING INDIVIDUALS OR AGENCIES COMPLETELY DESCRIBING THE AFFECTED ROADS AND THE APPROXIMATE DURATION OF THE CONSTRUCTION; THESE PARTIES INCLUDE, BUT ARE NOT LIMITED TO: (1) MARSHALL COUNTY SHERIFF'S DEPARTMENT, (2) MARSHALL COUNTY FIRE DEPARTMENT, (3) LOCAL AMBULANCE SERVICE, (4) MARSHALL COUNTY SCHOOL SUPERINTENDENT, (5) LOCAL POST OFFICE, (6) TDOT REGION 3 TRAFFIC ENGINEERING OFFICE.
- (12) SP-1, SP-2, SP-3 AND SP-4 ARE CONSIDERED SPECIAL SIGNS WHICH WILL BE STEEL, ORANGE WITH BLACK LETTERS.

TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	2015	59006-4219-04	2H

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STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

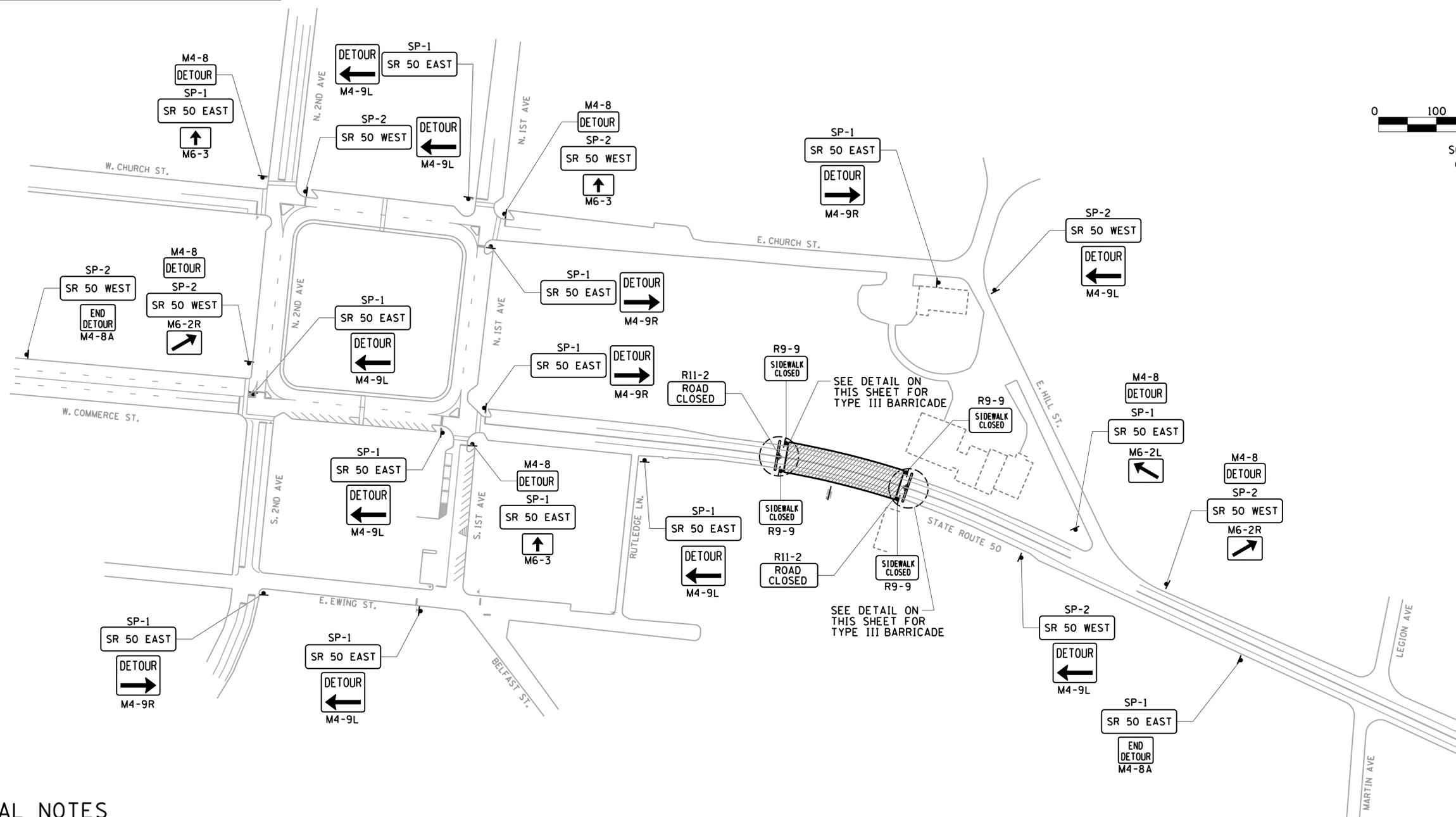
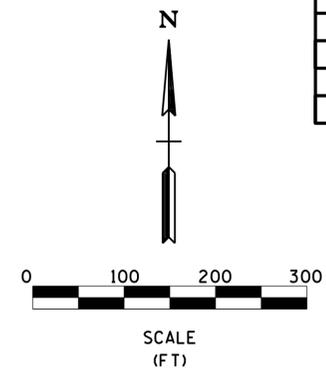
BRIDGE NO. 59-SR50-6.57  
STATE ROUTE 50  
OVER BIG ROCK CREEK  
MARSHALL COUNTY  
2015

TRAFFIC CONTROL  
SPECIAL NOTES  
AND  
QUANTITIES

### TRAFFIC CONTROL LEGEND

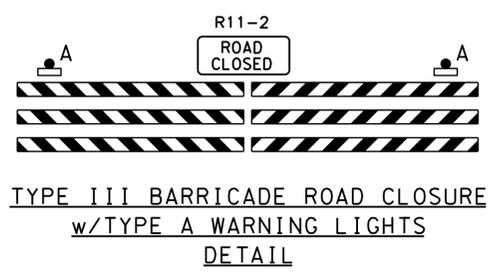
-  SIGN (CONSTRUCTION)
-  WORK ZONE

TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	2015	59006-4219-04	2J



### SPECIAL NOTES

1. THESE TRAFFIC CONTROL PLANS DO NOT RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY OF INSTALLING TRAFFIC CONTROL DEVICES IN ACCORDANCE WITH THE CURRENT EDITION OF THE "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES."
2. CONSTRUCTION SIGNING SHOWN IN THESE PLANS IS TO SERVE AS A GUIDE ONLY. OTHER SIGNS MAY BE REQUIRED DURING VARIOUS PHASES OF CONSTRUCTION.
3. CONTRACTOR IS RESPONSIBLE FOR LOCATING UTILITIES PRIOR TO INSTALLING POST MOUNTED SIGNS.
4. CONTRACTOR SHALL RETURN GROUND TO ORIGINAL DISPOSITION UPON SIGN REMOVAL.
5. CONTRACTOR SHALL TEMPORARILY COVER ANY EXISTING SIGNS THAT MAY CONFLICT WITH THE TEMPORARY DETOUR SIGNS.
6. SP-1 AND SP-2 ARE CONSIDERED SPECIAL SIGNS WHICH WILL BE STEEL, ORANGE WITH BLACK LETTERS.



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STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

BRIDGE NO. 59-SR50-6.57  
STATE ROUTE 50  
OVER BIG ROCK CREEK  
MARSHALL COUNTY  
2015

TRAFFIC CONTROL PLAN  
LOCAL DETOUR  
(STATE ROUTE 50)

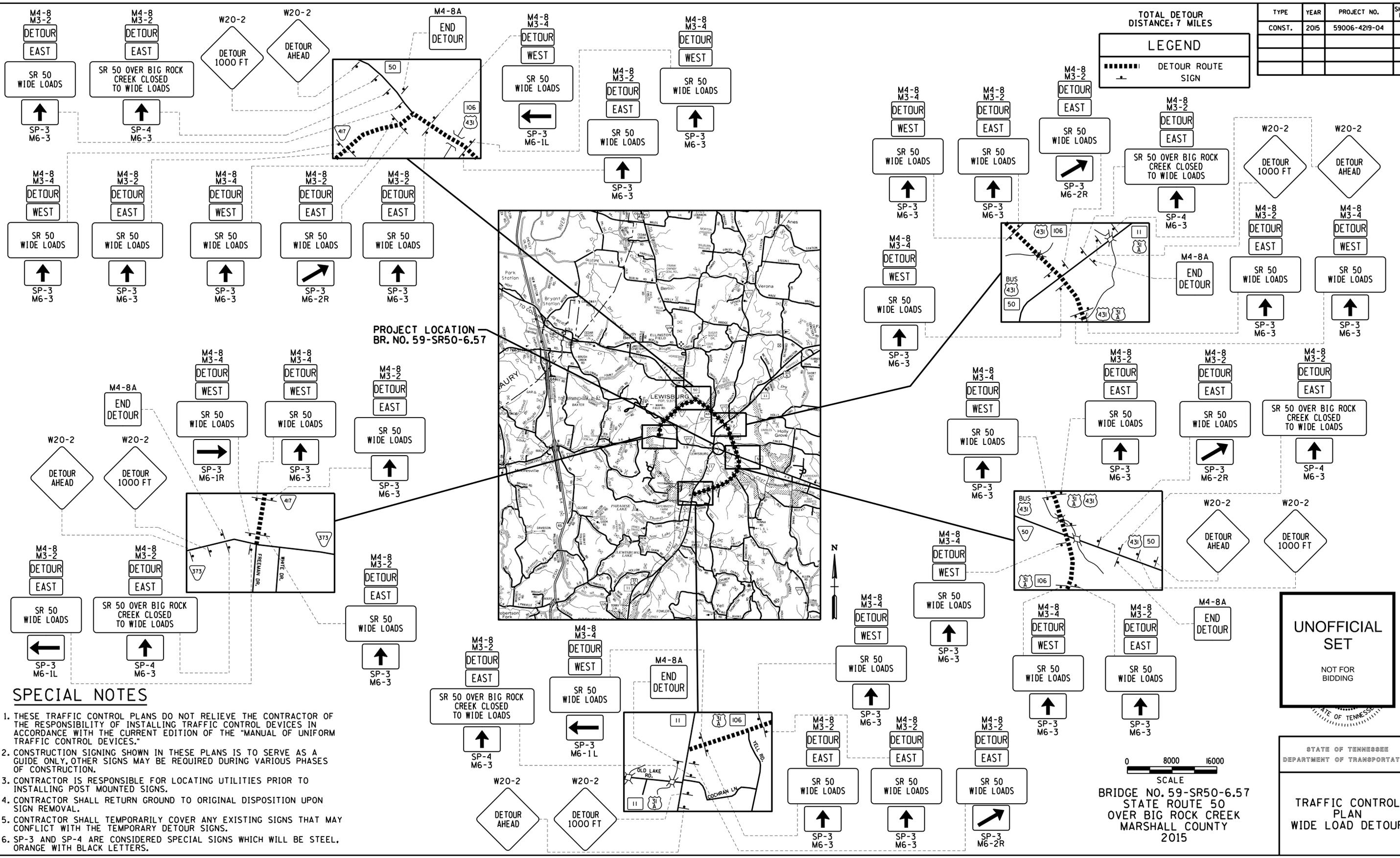
TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	2015	59006-4219-04	2K

TOTAL DETOUR DISTANCE: 7 MILES

**LEGEND**

----- DETOUR ROUTE

▲ DETOUR SIGN



PROJECT LOCATION  
BR. NO. 59-SR50-6.57

**SPECIAL NOTES**

1. THESE TRAFFIC CONTROL PLANS DO NOT RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY OF INSTALLING TRAFFIC CONTROL DEVICES IN ACCORDANCE WITH THE CURRENT EDITION OF THE "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES."
2. CONSTRUCTION SIGNING SHOWN IN THESE PLANS IS TO SERVE AS A GUIDE ONLY, OTHER SIGNS MAY BE REQUIRED DURING VARIOUS PHASES OF CONSTRUCTION.
3. CONTRACTOR IS RESPONSIBLE FOR LOCATING UTILITIES PRIOR TO INSTALLING POST MOUNTED SIGNS.
4. CONTRACTOR SHALL RETURN GROUND TO ORIGINAL DISPOSITION UPON SIGN REMOVAL.
5. CONTRACTOR SHALL TEMPORARILY COVER ANY EXISTING SIGNS THAT MAY CONFLICT WITH THE TEMPORARY DETOUR SIGNS.
6. SP-3 AND SP-4 ARE CONSIDERED SPECIAL SIGNS WHICH WILL BE STEEL, ORANGE WITH BLACK LETTERS.

**UNOFFICIAL SET**  
NOT FOR BIDDING

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SCALE  
BRIDGE NO. 59-SR50-6.57  
STATE ROUTE 50  
OVER BIG ROCK CREEK  
MARSHALL COUNTY  
2015

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
**TRAFFIC CONTROL PLAN**  
WIDE LOAD DETOUR

TENNESSEE D.O.T. DESIGN DIVISION

**REQUIRED LOCATION REQUEST INFORMATION**

NAME OF CALLER  
 TELEPHONE NUMBER  
 BEST TIME TO CALL  
 COUNTY  
 TOWN  
 STREET ADDRESS  
 START DATE START TIME  
 TYPE OF WORK  
 BLASTING ?  
 WORK BEING DONE BY  
 WORK BEING DONE FOR

CALL THREE WORKING DAYS BEFORE YOU DIG  
 1-800-351-1111  
 IN NASHVILLE : 366-1987



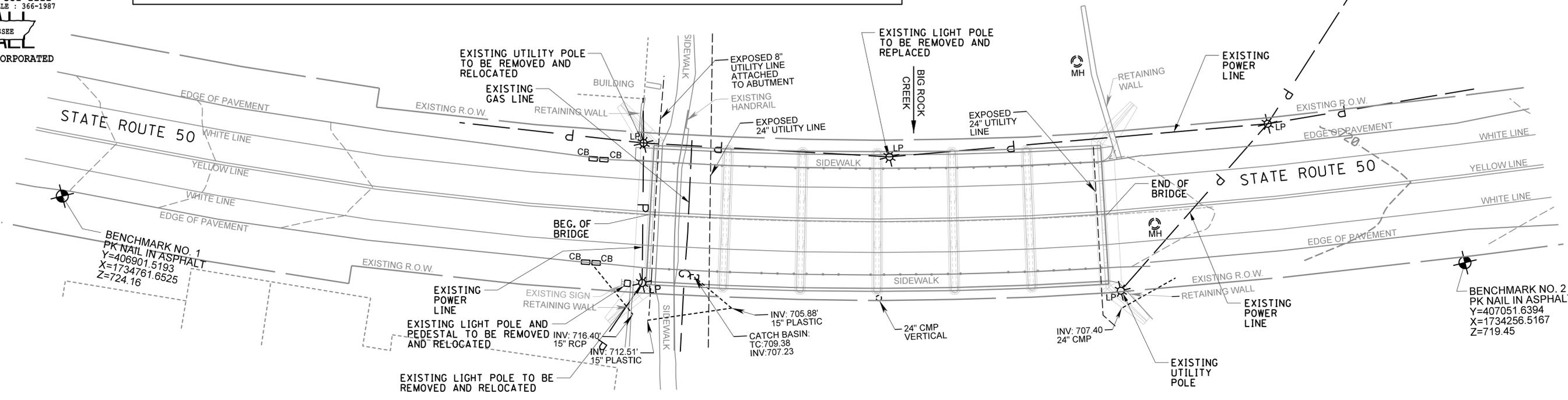
SYSTEM INCORPORATED

**UTILITY CONTACTS**

ATT 116 SOUTH CANON AVE MURFREESBORO, TN 37129 KENNETH KORNEGAY KK4096@ATT.COM O: 615-848-2082 C: 615-631-7221	DUCK RIVER ELECTRIC MEMBERSHIP CORPORATION 1411 MADISON ST. SHELBYVILLE, TN 37160 BLAKE BUTLER BBUTLER@DREMC.COM O: 931-684-4621 EXT. 5840	LEWISBURG NATURAL GAS DEPT. 505 NORTH ELLINGTON PARKWAY LEWISBURG, TN 37091 LEROY MULLIS LEROY-LGD@TNWEB.COM O: 931-703-2699	MARSHALL COUNTY BOARD PUBLIC UTILITIES 624 W. COMMERCE LEWISBURG, TN 37091 JESSE T. WHALEY MARSHALLCOUNTY@BELLSOUTH.NET O: 931-359-6831 C: 931-993-8042
CHARTER COMMUNICATIONS MID-SOUTH 215 INDUSTRIAL BLVD. TULAHOMA, TN 37388 RICHARD RIDDLE RICHARD.RIDDLE@CHARTER.COM O: 931-461-4315 C: 931-224-8787	LEWISBURG ELECTRIC SYSTEM 599 WEST ELLINGTON PWKY LEWISBURG, TN 37091 DOUG FAGAN DAFAGAN.LES@ATT.NET O: 931-359-2544 F: 931-359-6759	LEWISBURG WATER AND WASTEWATER 100 WATER STREET LEWISBURG, TN 37091 KENNETH CARR KENNETH@LEWISBURGWATER.ORG O: 931-359-6831 C: 931-993-8042	UNITED TELEPHONE CO. 120 TAYLOR STREET CHAPEL HILL, TN 37034 DOUG PRUITT DOUGPRUITT@OUTOFFICE.NET O: 931-364-4326 C: 615-804-3358

PORTIONS OF THIS DRAWING DEPICTS EXISTING CONDITIONS. LIMITS OF PROPOSED BRIDGE CONSTRUCTION MAY VARY FROM EXISTING CONDITIONS.

TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	2015	59006-4219-04	2L



**PLAN**

LEGEND	
<b>MAPPING SYMBOLS AND CODES</b>	
	CATCH BASIN (CB)
	CATCH BASIN DOUBLE (DCB)
	CATCH BASIN TRIPLE (TCB)
	ELECTRIC JUNCTION BOX (EJB)
	ELECTRIC METER (EM)
	ELECTRIC TRANSFORMER (ETF)
	FIBER OPTIC MARKER (FOM)
	FIRE HYDRANT (FH)
	FLOOD LIGHT (FLT)
	FLAG POLE (FP)
	GAS METER (GM)
	GUY POLE (GP)
	GAS VALVE (GV)
	GUY WIRE (GW)
	IRON PIN NEW (IR(N))
	IRON PIN OLD (IR(O))
	IRON PIPE OLD (IP(O))
	IRON SPIKE OLD (IS(O))
	LIGHT STANDARD METAL/WOOD (LS/LP)
	MAIL BOX (MB)
	MAN HOLE (MH)
	MONUMENT CONCRETE OLD (MON(O))
	MONUMENT CONCRETE NEW (MON(N))
	P. K. NAIL OLD (PK(O))
	P. K. NAIL NEW (PK(N))
	LIGHT POLE WITH POWER (PP)
	LIGHT POLE WITH TELEPHONE (T)
	POWER MAN HOLE (PMH)
	POWER POLE (P)
	POWER AND TELEPHONE POLE (P/T)
	RIGHT-OF-WAY MONUMENT (RWM)
	SPRINKLER HEAD/VALVE (SPH, SPV)
	SANITARY MAN HOLE (SSMH)
	SIGN (SN)
	STEAM MAN HOLE (STMH)
	STORM WATER MAN HOLE (SWMH)
	TELEPHONE POLE (T)
	TREE
	WATER METER (WM)
	WATER VALVE (WV)

LINE STYLES	
	CABLE LINE
	CENTER LINE
	CONCRETE AREA
	EDGE OF WOODS LINE
	FENCE LINE
	GAS LINE
	OVERHEAD POWER LINE
	OVERHEAD TELEPHONE LINE
	OVERHEAD POWER AND TELEPHONE LINE
	PROPERTY LINE
	SANITARY SEWER
	STORM WATER
	FIBER OPTIC CABLE
	UNDERGROUND FIBER OPTIC CABLE
	UNDERGROUND POWER LINE
	UNDERGROUND POWER AND TELEPHONE LINE
	UNDERGROUND TELEPHONE LINE
	WATER LINE

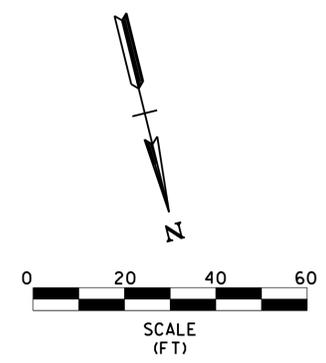
NOTE: RIGHT-OF-WAY LOCATIONS DETERMINED FROM TRIMS (TN RDWY INFORMATION MANAGEMENT SYSTEM) AND TN PROPERTY VIEWER IMAGERY.

NOTE: UTILITY INFORMATION SHOWN WAS DERIVED FROM FIELD MEASUREMENT AND OBSERVATION. THE CONTRACTOR SHALL FIELD VERIFY ALL EXISTING UTILITY LOCATIONS AND SERVICES PRIOR TO ROADWAY AND BRIDGE CONSTRUCTION.

NOTE: CONTRACTOR SHALL USE EXTREME CAUTION WHEN OPERATING IN ANY AREA WHERE UTILITIES ARE PRESENT AND AVOID WORKING IN THESE AREAS AS MUCH AS PRACTICALLY POSSIBLE. DAMAGES TO ANY UTILITIES ARE TO BE REPAIRED BY THE CONTRACTOR AT HIS OWN EXPENSE.

NOTE: THE CONTRACTOR SHALL VERIFY THE LOCATION OF EXISTING POLES, GUY WIRES AND ROADWAY SIGNS PRIOR TO COMMENCEMENT OF CONSTRUCTION. THE CONTRACTOR SHALL COORDINATE THE MOVEMENT OF SUCH SIGNS, POLES AND GUY WIRES WITH THEIR RESPECTIVE OWNERS. THE CONTRACTOR SHALL RE-SET EXISTING SIGNAGE IN ITS ORIGINAL LOCATION. THE COST OF RE-SETTING SIGNAGE WILL NOT BE PAID FOR DIRECTLY BUT WILL BE INCLUDED IN OTHER ITEMS OF CONSTRUCTION.

NOTE: CONTRACTOR TO COORDINATE WORK ON EXISTING LIGHT POLES WITH ELECTRIC UTILITY OWNER.



BRIDGE NO. 59-SR50-6.57  
 STATE ROUTE 50  
 OVER BIG ROCK CREEK  
 MARSHALL COUNTY  
 2015

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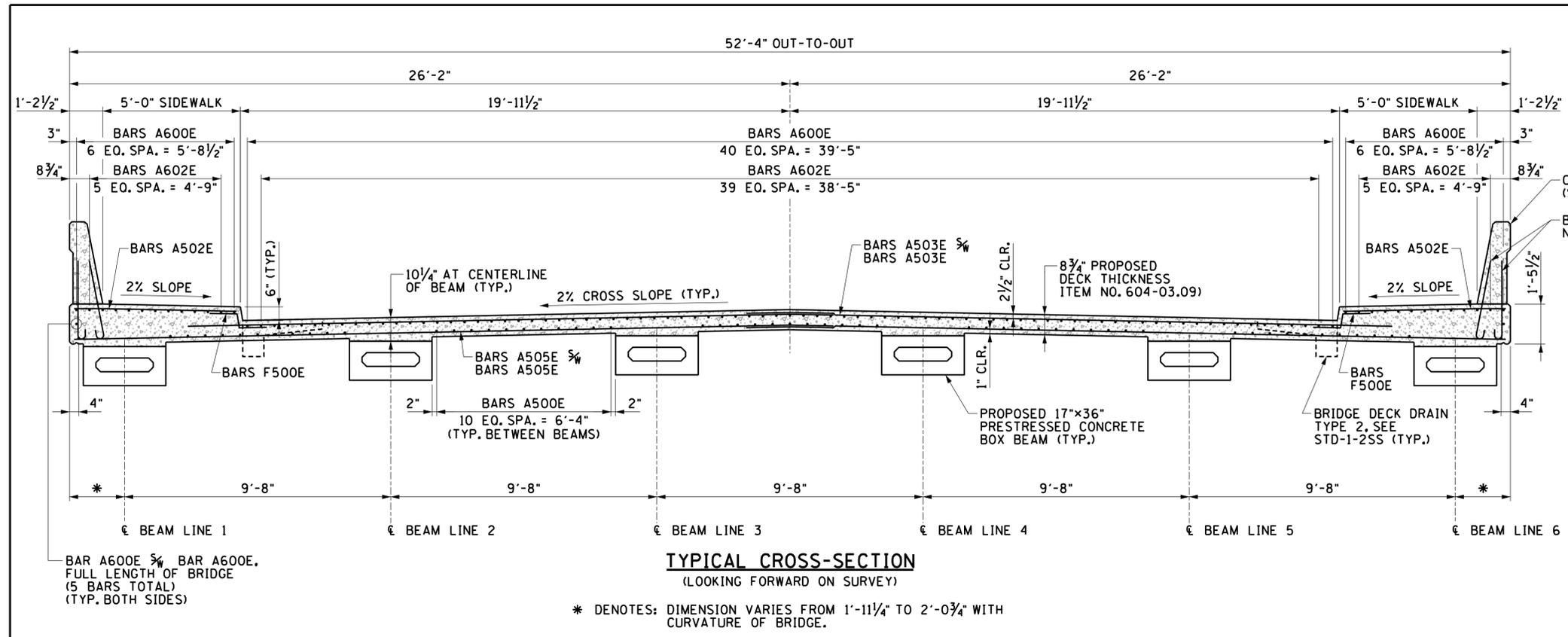
STATE OF TENNESSEE  
 DEPARTMENT OF TRANSPORTATION

UTILITIES

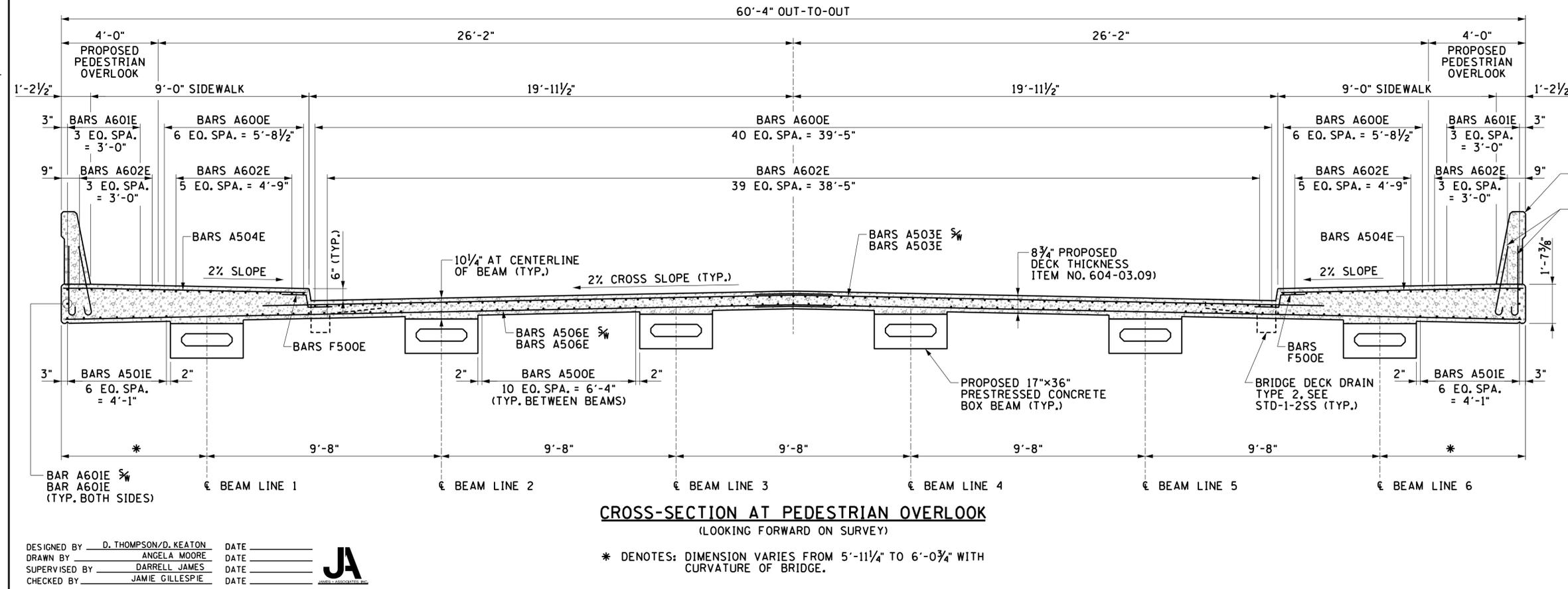




CONST. NO.			
PROJECT NO.	YEAR	SHEET NO.	
59006-4219-04	2015		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



- NOTES:**
1. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE STABILITY OF THE STRUCTURE DURING SLAB REPLACEMENT. DESIGN CALCULATION AND DETAILS OF TEMPORARY SUPPORT SYSTEM OR FALSEWORK REQUIRED SHALL BE SUBMITTED TO THE ENGINEER FOR REVIEW AND SHALL MEET WITH THE FULL SATISFACTION OF THE ENGINEER BEFORE ANY DEMOLITION IS BEGUN. COST OF STABILIZING THE STRUCTURE SHALL BE INCLUDED IN ITEM NO. 602-10.05, BRACING REPAIRS, L.S.
  2. FOR STANDARD REINFORCING DETAILS OF PARAPET, SEE STD-14-3.
  3. WHEN POURING SLAB, PROVISIONS SHALL BE MADE FOR SETTING REINFORCING STEEL FOR THE PARAPET. THE PARAPET SHALL NOT BE POURED UNTIL THE SLAB IS POURED AND CURED.
  4. THE COST OF CONCRETE REQUIRED FOR THE NEW BRIDGE DECK SLAB SHALL BE INCLUDED IN THE PRICE BID FOR ITEM NO. 604-03.09, CLASS D CONCRETE (BRIDGE DECK), C.Y.
  5. ALL REINFORCING STEEL FOR THE NEW CONCRETE SLAB SHALL BE EPOXY COATED. REINFORCING STEEL TO BE INCLUDED IN THE PRICE BID FOR ITEM NO. 604-02.03, EPOXY COATED REINFORCING STEEL, LBS.
  6. THE COST OF REMOVING AND DISPOSING OF THE EXISTING CONCRETE AND REINFORCING STEEL, WITHIN THE LIMITS SHOWN, SHALL BE INCLUDED IN ITEM NO. 202-04.01, REMOVAL OF STRUCTURES, L.S.



ESTIMATED QUANTITIES		
CLASS "D" CONCRETE (BRIDGE DECK) (C.Y.)	EPOXY COATED REINFORCING STEEL (BRIDGES) (LBS.)	STEEL BAR REINFORCEMENT (BRIDGES) (LBS.)
364	86,022	1,909

**UNOFFICIAL SET**  
NOT FOR BIDDING

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

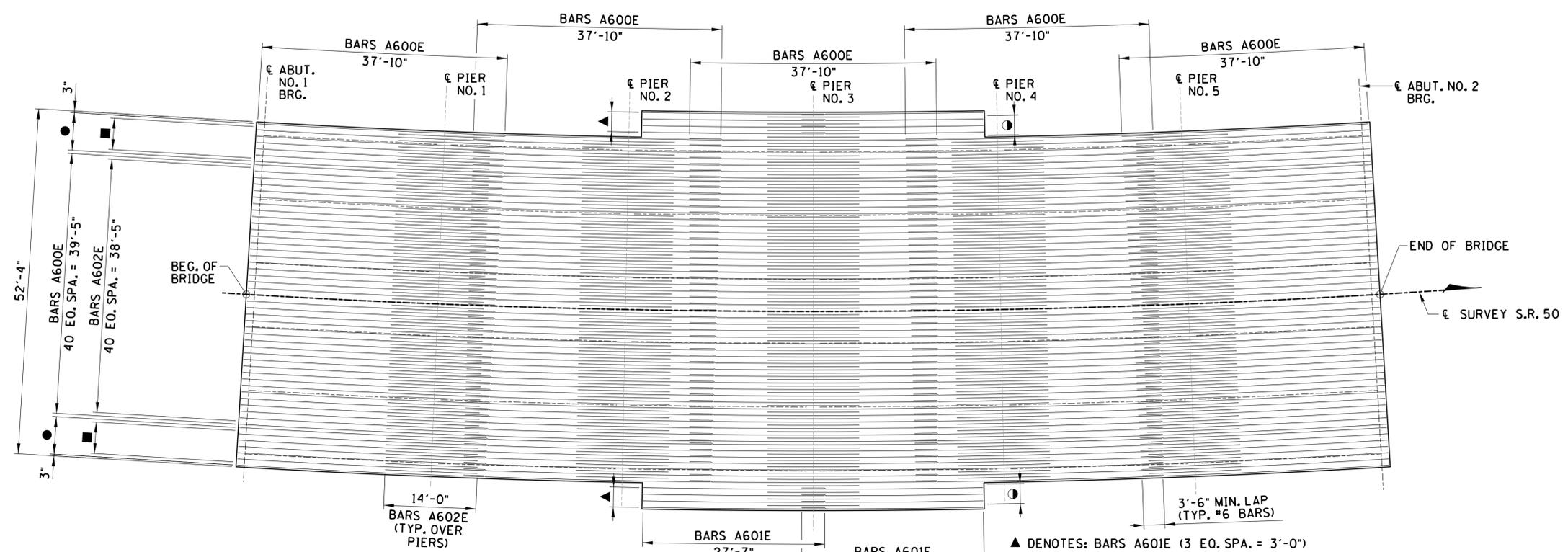
SUPERSTRUCTURE

BRIDGE NO. 59-SR50-6.57  
SR50 OVER BIG ROCK CREEK  
MARSHALL COUNTY  
2015

DESIGNED BY D. THOMPSON/D. KEATON  
DRAWN BY ANGELA MOORE  
SUPERVISED BY DARRELL JAMES  
CHECKED BY JAMIE GILLESPIE

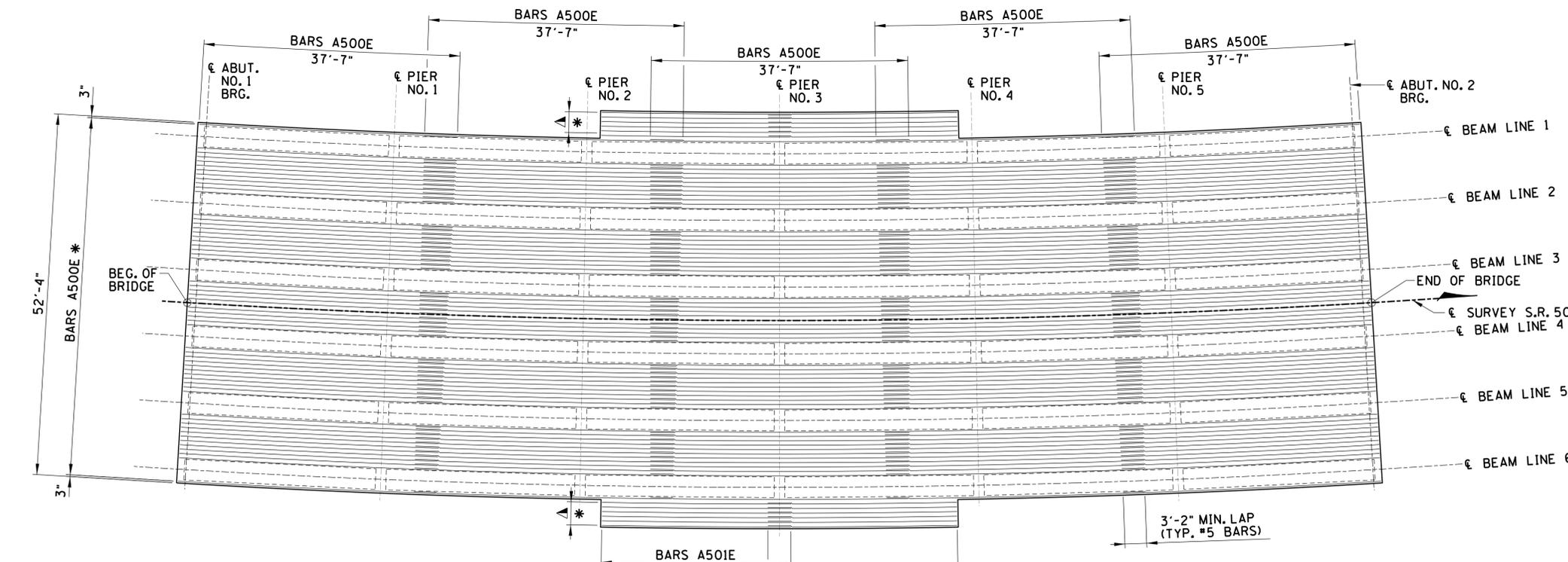


CONST. NO.			
PROJECT NO.	YEAR	SHEET NO.	
59006-4219-04	2015		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



**SLAB PLAN**  
(SHOWING TOP LONGITUDINAL REINFORCEMENT)

- ▲ DENOTES: BARS A601E (3 EQ. SPA. = 3'-0")
- DENOTES: BARS A602E (5 EQ. SPA. = 4'-9")
- DENOTES: BARS A600E (6 EQ. SPA. = 5'-8 1/2")
- DENOTES: BARS A602E (3 EQ. SPA. = 3'-0")



**SLAB PLAN**  
(SHOWING BOTTOM LONGITUDINAL REINFORCEMENT)

- \* DENOTES: SEE SHEET BR-119-173 FOR BAR SPACING.
- ▲ DENOTES: BARS A501E 3/4 BARS A501E

**UNOFFICIAL SET**  
NOT FOR BIDDING

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

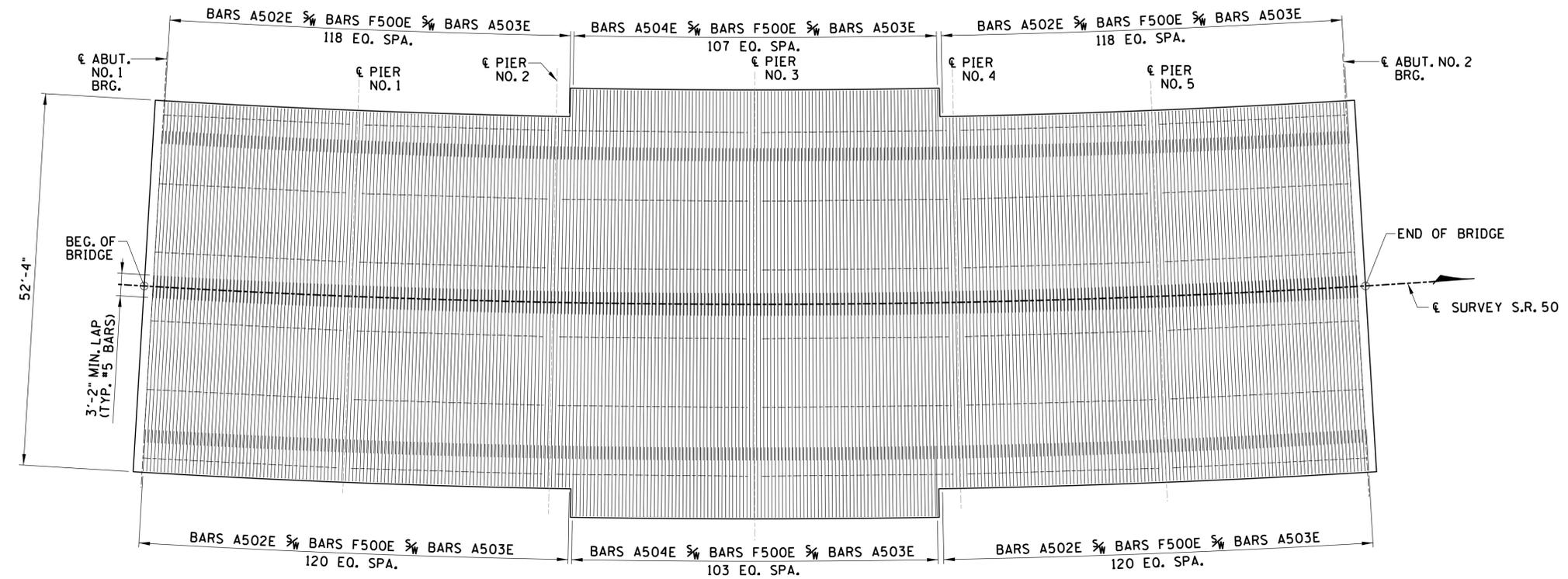
SUPERSTRUCTURE DETAILS

BRIDGE NO. 59-SR50-6.57  
SR50 OVER BIG ROCK CREEK  
MARSHALL COUNTY  
2015

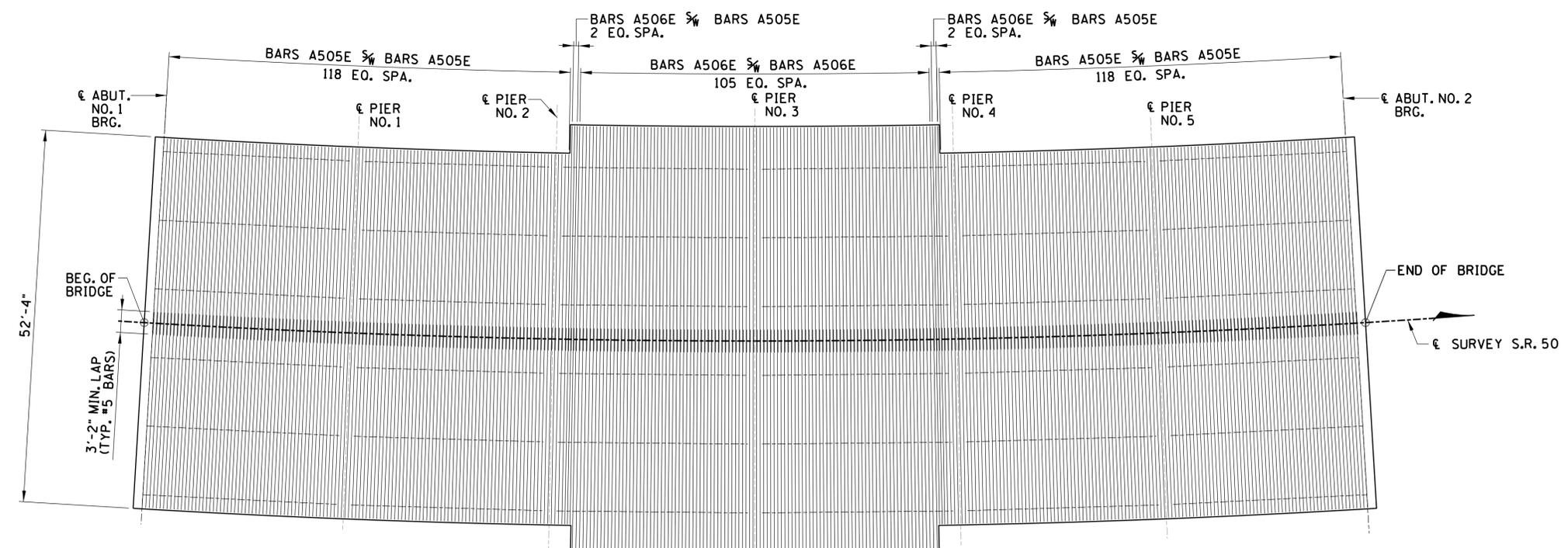
DESIGNED BY D. THOMPSON/D. KEATON  
DRAWN BY ANGELA MOORE  
SUPERVISED BY DARRELL JAMES  
CHECKED BY JAMIE GILLESPIE



CONST. NO.			
PROJECT NO.	YEAR	SHEET NO.	
59006-4219-04	2015		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



**SLAB PLAN**  
(SHOWING TOP TRANSVERSE REINFORCEMENT)



**SLAB PLAN**  
(SHOWING BOTTOM TRANSVERSE REINFORCEMENT)

DESIGNED BY D. THOMPSON/D. KEATON DATE \_\_\_\_\_  
 DRAWN BY ANGELA MOORE DATE \_\_\_\_\_  
 SUPERVISED BY DARRELL JAMES DATE \_\_\_\_\_  
 CHECKED BY JAMIE GILLESPIE DATE \_\_\_\_\_



**UNOFFICIAL SET**  
 NOT FOR BIDDING

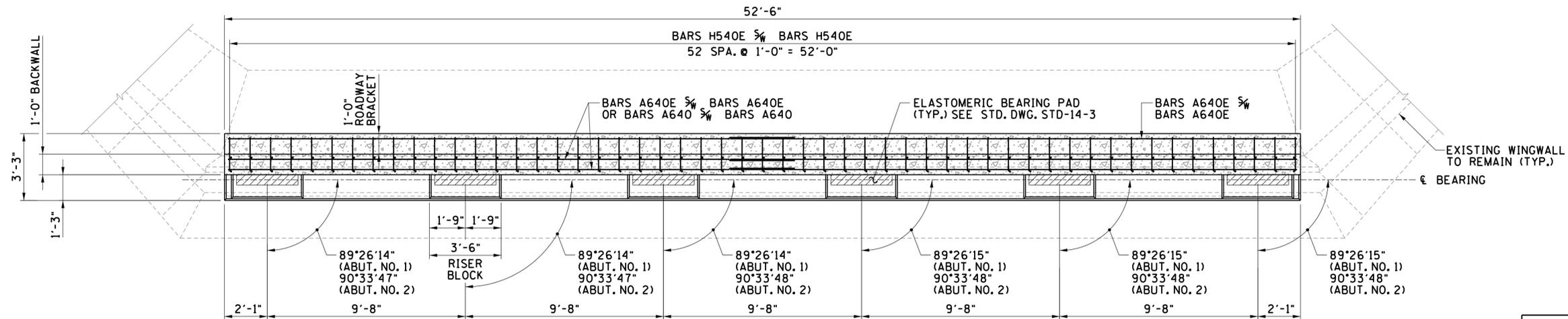
STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

SUPERSTRUCTURE DETAILS

BRIDGE NO. 59-SR50-6.57  
SR50 OVER BIG ROCK CREEK  
MARSHALL COUNTY  
2015



CONST. NO.			
PROJECT NO.	YEAR	SHEET NO.	
59006-4219-04	2015		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



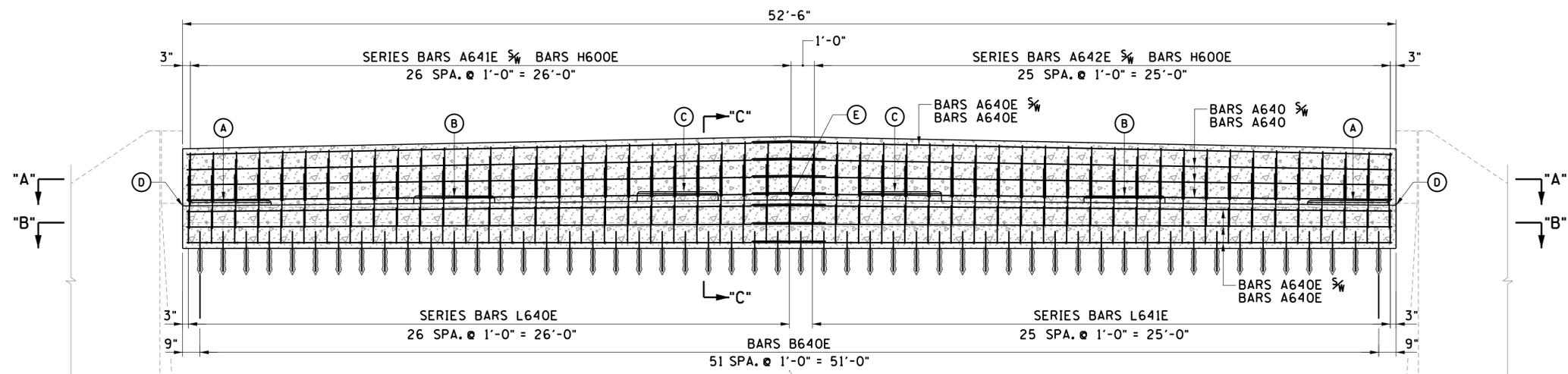
SECTION "A-A"

ABUTMENT NO. 1 ELEVATIONS	
LOCATION	ELEVATION
A	719.02
B	719.21
C	719.40
D	718.82
E	719.08

ABUTMENT NO. 2 ELEVATIONS	
LOCATION	ELEVATION
A	719.02
B	719.21
C	719.40
D	718.82
E	719.08

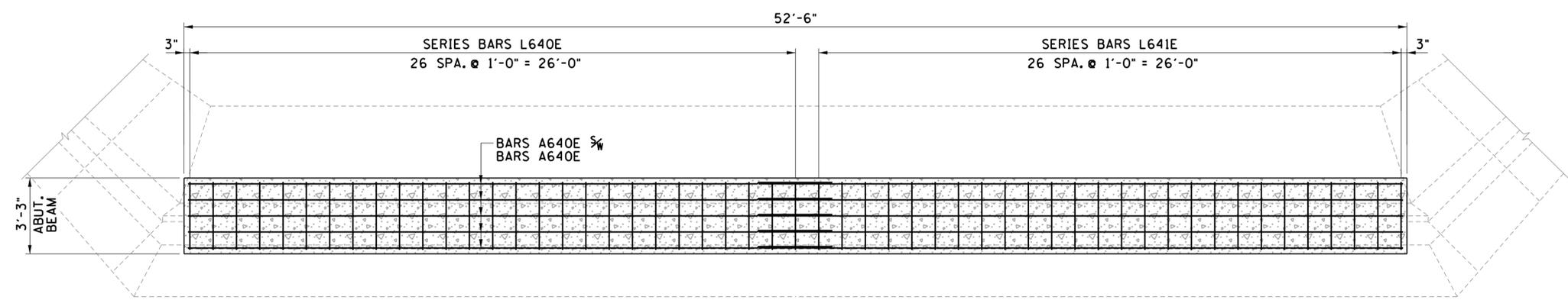
NOTE: ELEVATIONS A, B AND C REPRESENT CONCRETE BEAM SEAT ELEVATION BEFORE 1/2" BEARING PAD INSTALLATION.

NOTE: DECORATIVE MURAL ON ABUTMENT NO. 1 SHALL NOT BE DISTURBED.



ELEVATION

(LOOKING BACK ON SURVEY - ABUTMENT NO. 1)  
(LOOKING FORWARD ON SURVEY - ABUTMENT NO. 2)  
(SEE BR-119-178 FOR SECTION "C-C")



SECTION "B-B"

UNOFFICIAL SET  
NOT FOR BIDDING

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

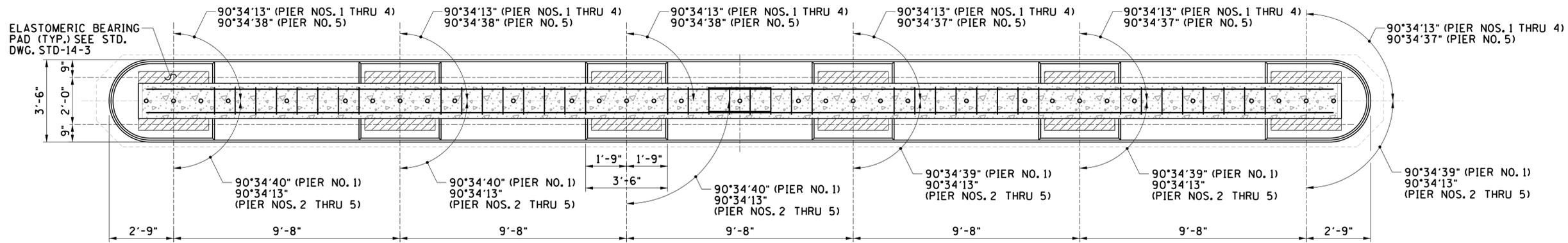
ABUTMENT NO. 1 AND ABUTMENT NO. 2

BRIDGE NO. 59-SR50-6.57  
SR50 OVER BIG ROCK CREEK  
MARSHALL COUNTY  
2015

DESIGNED BY D. THOMPSON/D. KEATON  
DRAWN BY ANGELA MOORE  
SUPERVISED BY DARRELL JAMES  
CHECKED BY JAMIE GILLESPIE

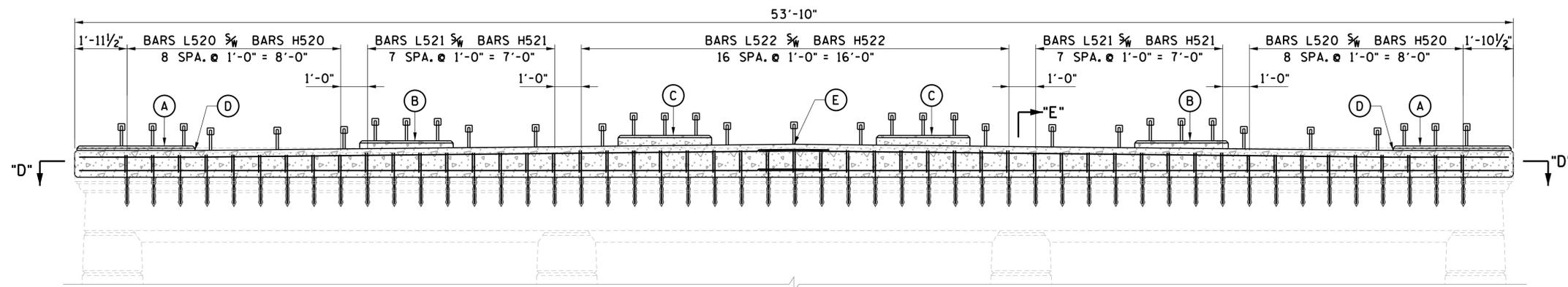




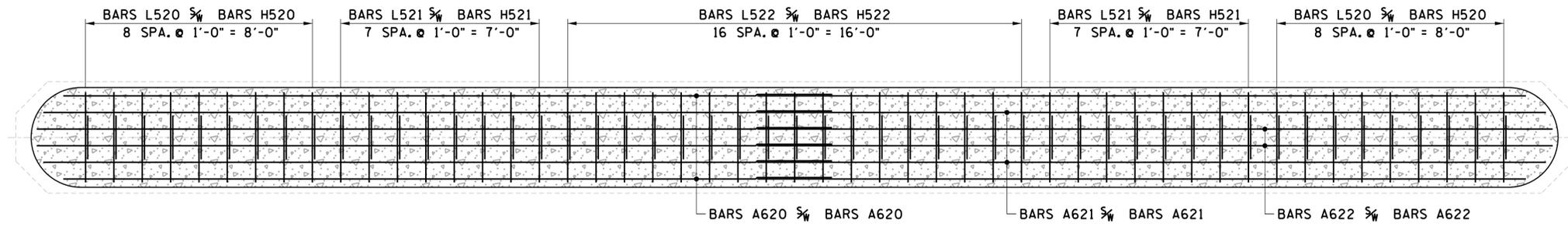


**PIER - PLAN VIEW**  
(LOOKING FORWARD ON SURVEY)

CONST. NO.			
PROJECT NO.	YEAR	SHEET NO.	
59006-4219-04	2015		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



**ELEVATION**  
(SEE BR-119-180 FOR SECTION "E-E")



**SECTION "D-D"**

	LOCATION				
	A	B	C	D	E
PIER NO. 1 (SPAN 1)	719.24	719.43	719.63	719.07	719.30
PIER NO. 1 (SPAN 2)	719.25	719.45	719.64	719.07	719.30
PIER NO. 2 (SPAN 2)	719.38	719.58	719.77	719.21	719.44
PIER NO. 2 (SPAN 3)	719.39	719.58	719.78	719.21	719.44
PIER NO. 3 (SPAN 3)	719.44	719.63	719.82	719.27	719.50

NOTE: ELEVATIONS A, B AND C REPRESENT CONCRETE BEAM SEAT ELEVATION BEFORE 1/2" BEARING PAD INSTALLATION.

	LOCATION				
	A	B	C	D	E
PIER NO. 3 (SPAN 4)	719.44	719.63	719.82	719.27	719.50
PIER NO. 4 (SPAN 4)	719.39	719.58	719.78	719.21	719.44
PIER NO. 4 (SPAN 5)	719.38	719.58	719.77	719.21	719.44
PIER NO. 5 (SPAN 5)	719.25	719.45	719.64	719.07	719.30
PIER NO. 5 (SPAN 6)	719.24	719.43	719.63	719.07	719.30

NOTE: ELEVATIONS A, B AND C REPRESENT CONCRETE BEAM SEAT ELEVATION BEFORE 1/2" BEARING PAD INSTALLATION.

DESIGNED BY D. THOMPSON/D. KEATON DATE \_\_\_\_\_  
 DRAWN BY ANGELA MOORE DATE \_\_\_\_\_  
 SUPERVISED BY DARRELL JAMES DATE \_\_\_\_\_  
 CHECKED BY JAMIE GILLESPIE DATE \_\_\_\_\_



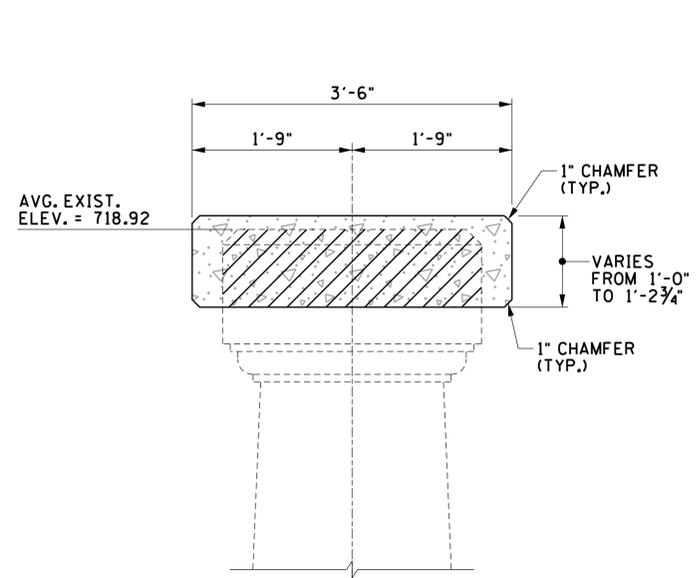
**UNOFFICIAL SET**  
NOT FOR BIDDING

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

PIER NOS. 1 THRU 5

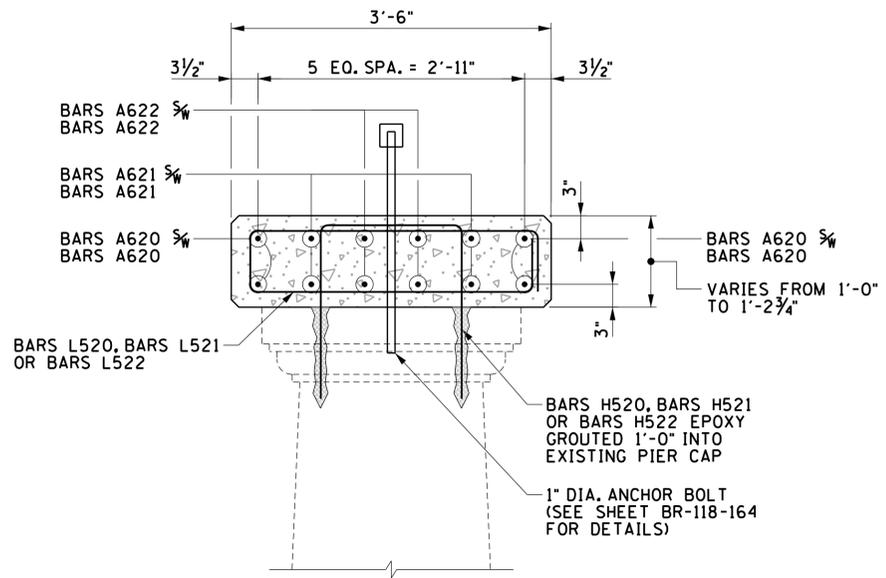
BRIDGE NO. 59-SR50-6.57  
SR50 OVER BIG ROCK CREEK  
MARSHALL COUNTY  
2015

CONST. NO.			
PROJECT NO.	YEAR	SHEET NO.	
59006-4219-04	2015		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



**SECTION "E-E"**  
(NOT TO SCALE)

//// DENOTES: AREA OF PIER CAP TO BE REMOVED AND INCLUDED IN ITEM NO. 202-04.01. (SEE BR-119-172 FOR LIMITS OF DEMOLITION)



**SECTION "E-E"**  
(SHOWING REINFORCING STEEL)  
(NOT TO SCALE)

**ESTIMATED QUANTITIES**

	CLASS "A" CONCRETE (BRIDGE) (C.Y.)	STEEL BAR REINFORCEMENT (BRIDGES) (LBS.)
PIER NO. 1	7.6	1,720
PIER NO. 2	7.6	1,720
PIER NO. 3	7.6	1,720
PIER NO. 4	7.6	1,720
PIER NO. 5	7.6	1,720

- NOTES:**
- COST OF REINFORCING STEEL FOR NEW PIER CAP SHALL BE INCLUDED IN ITEM NO. 604-03.02, STEEL BAR REINFORCEMENT (BRIDGES), LBS.
  - THE COST OF REMOVING CONCRETE SHALL BE INCLUDED IN ITEM NO. 202-04.01, REMOVAL OF STRUCTURES, L.S.
  - CONCRETE REQUIRED TO REBUILD DEMOLISHED AREAS SHALL BE INCLUDED IN ITEM NO. 604-03.01, CLASS "A" CONCRETE (BRIDGES). EXISTING REINFORCING STEEL SHALL BE CLEANED AND REUSED. MODIFICATION OF REINFORCING STEEL SHALL BE INCLUDED IN ITEM NO. 604-03.02, STEEL BAR REINFORCEMENT (BRIDGES), LBS.
  - ELASTOMERIC PADS SHALL BE IN PLACE A MINIMUM OF ONE DAY BEFORE BEING DISTURBED BY SETTING BEAMS. PLACE RUBBER BONDING CEMENT IN SUCH A WAY THAT VISIBLE CONCRETE SURFACES WILL NOT BE STAINED.
  - THE COST OF FORMING, LABOR, CONCRETE, AND ALL MISCELLANEOUS MATERIAL (EXCEPT REINFORCING STEEL) NECESSARY TO REPAIR THE PIER AND POUR NEW PIER CAP SHALL BE INCLUDED IN ITEM NO. 604-03.01, CLASS "A" CONCRETE (BRIDGES), C.Y.
  - THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE STABILITY OF THE STRUCTURE DURING BEAM AND SLAB REPLACEMENT. DESIGN CALCULATION AND DETAILS OF TEMPORARY SUPPORT SYSTEM OR FALSEWORK REQUIRED SHALL BE SUBMITTED TO THE ENGINEER FOR REVIEW AND SHALL MEET WITH THE FULL SATISFACTION OF THE ENGINEER BEFORE ANY DEMOLITION IS BEGUN. COST OF STABILIZING THE STRUCTURE SHALL BE INCLUDED IN ITEM NO. 602-10.05, BRACING REPAIRS, L.S.
  - SEE DEMOLITION DETAILS FOR AREAS TO BE REMOVED AND ADDITIONAL NOTES REGARDING EXISTING REINFORCING STEEL.
  - CONTRACTOR SHALL SPLICE NEW REINFORCING STEEL WITH EXISTING REINFORCING STEEL WHERE APPLICABLE.
  - DIAPHRAGM SHALL BE POURED CONCURRENTLY WITH THE SLAB USING CLASS "D" CONCRETE.

DESIGNED BY D. THOMPSON/D. KEATON DATE \_\_\_\_\_  
 DRAWN BY ANGELA MOORE DATE \_\_\_\_\_  
 SUPERVISED BY DARRELL JAMES DATE \_\_\_\_\_  
 CHECKED BY JAMIE GILLESPIE DATE \_\_\_\_\_



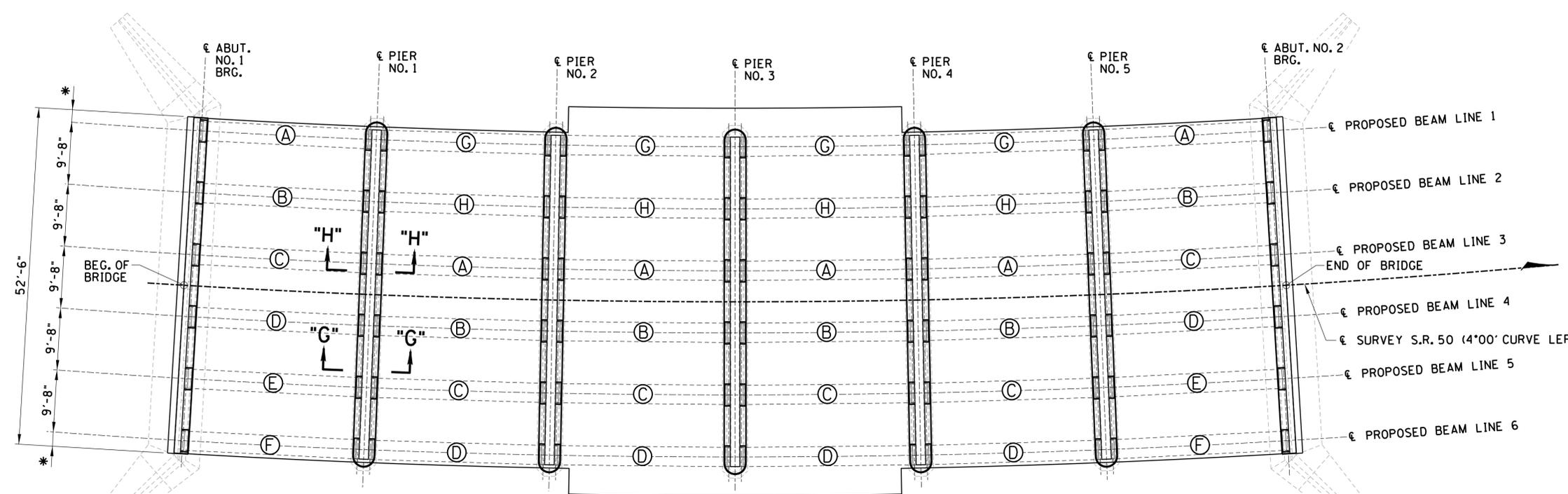
**UNOFFICIAL SET**  
NOT FOR BIDDING

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

PIER DETAILS

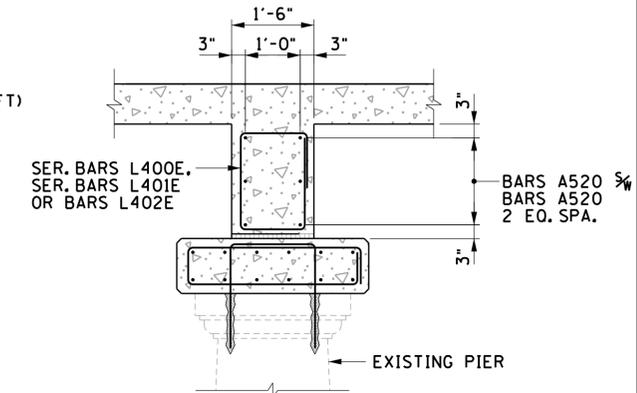
BRIDGE NO. 59-SR50-6.57  
SR50 OVER BIG ROCK CREEK  
MARSHALL COUNTY  
2015

CONST. NO.			
PROJECT NO.	YEAR	SHEET NO.	
59006-4219-04	2015		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

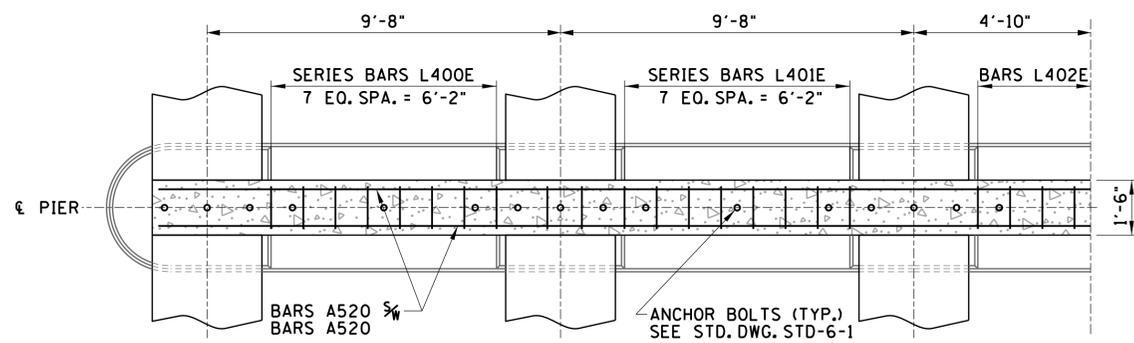


**FRAMING PLAN**  
(NOT TO SCALE)

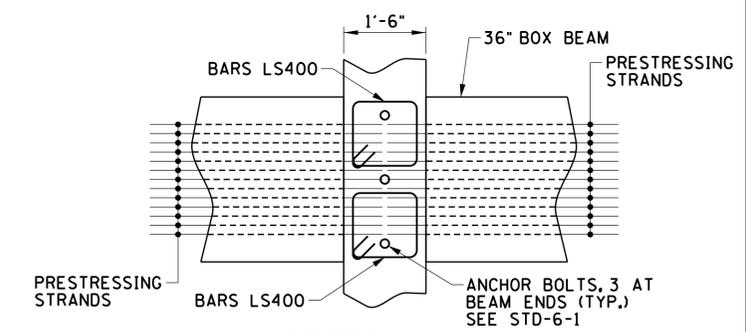
NOTE: DIAPHRAGM SHALL BE POURED CONCURRENTLY WITH THE SLAB USING CLASS "D" CONCRETE.  
 ○ DENOTES: BEAM DESIGNATION, SEE PRESTRESSED CONCRETE BOX BEAM DETAILS ON BR-119-182.  
 \* DENOTES: DIMENSION VARIES WITH CURVATURE OF BRIDGE.



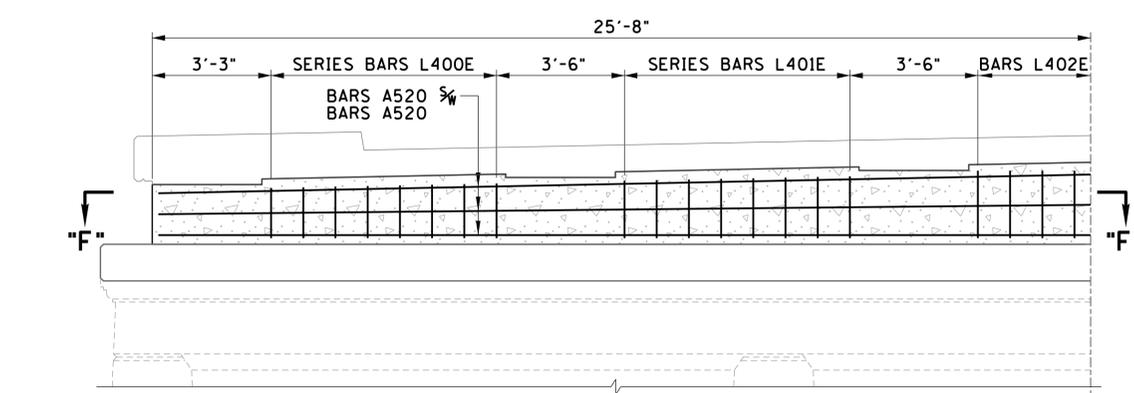
**SECTION "H-H"**  
(NOT TO SCALE)



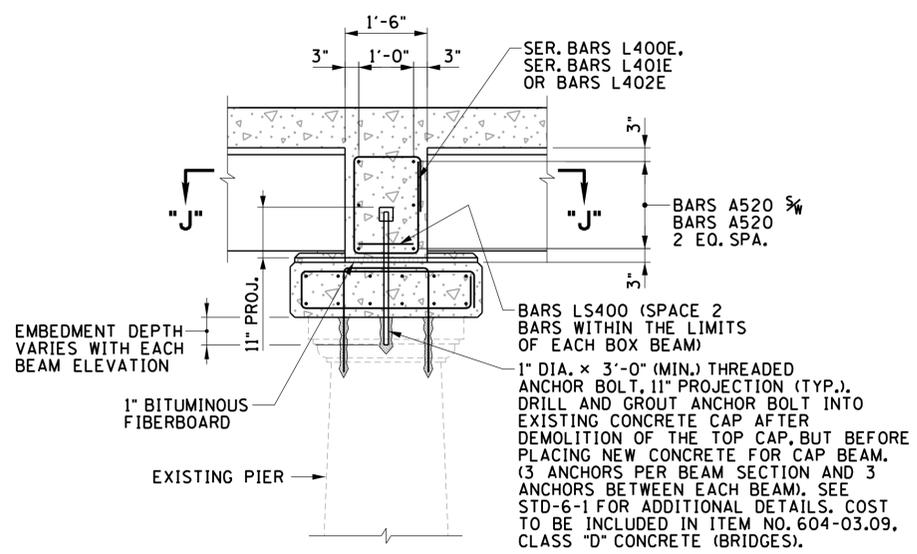
**SECTION "F-F"**  
(TYPICAL PART PLAN OF DIAPHRAGM)  
(NOT TO SCALE)



**SECTION "J-J"**  
(NOT TO SCALE)



**HALF ELEVATION OF DIAPHRAGM**  
(NOT TO SCALE)



**SECTION "G-G"**  
(NOT TO SCALE)

UNOFFICIAL  
SET  
NOT FOR  
BIDDING

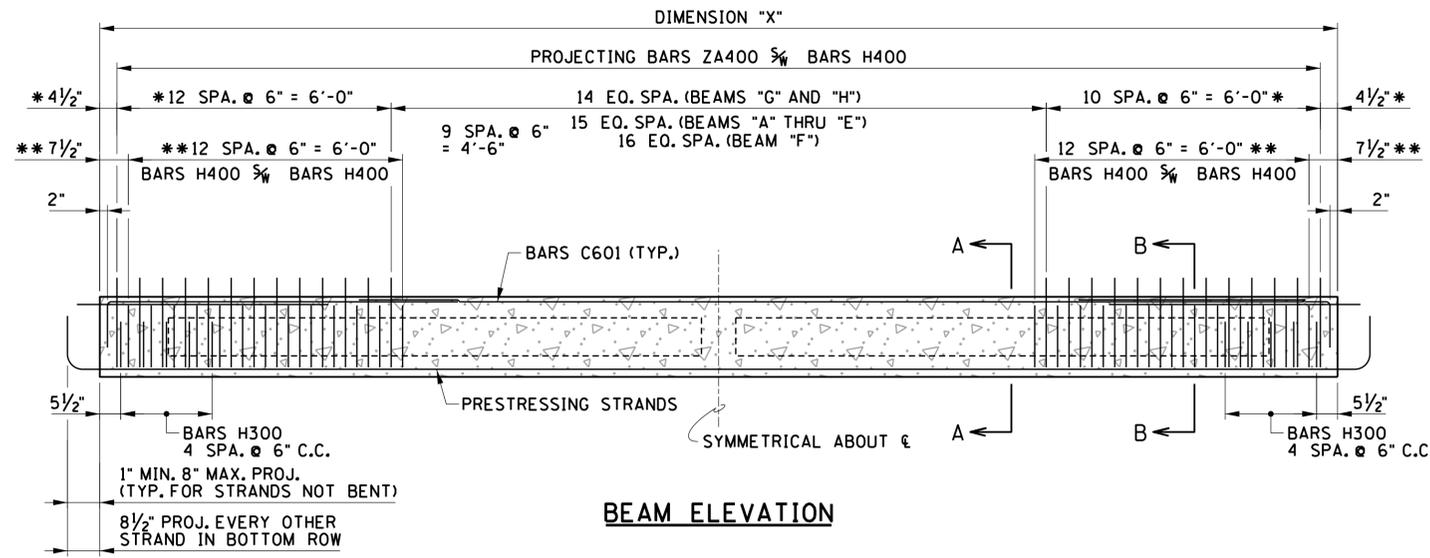
DEPARTMENT OF TRANSPORTATION

FRAMING PLAN

BRIDGE NO. 59-SR50-6.57  
SR50 OVER BIG ROCK CREEK  
MARSHALL COUNTY  
2015

DESIGNED BY D. THOMPSON/D. KEATON  
 DRAWN BY ANGELA MOORE  
 SUPERVISED BY DARRELL JAMES  
 CHECKED BY JAMIE GILLESPIE





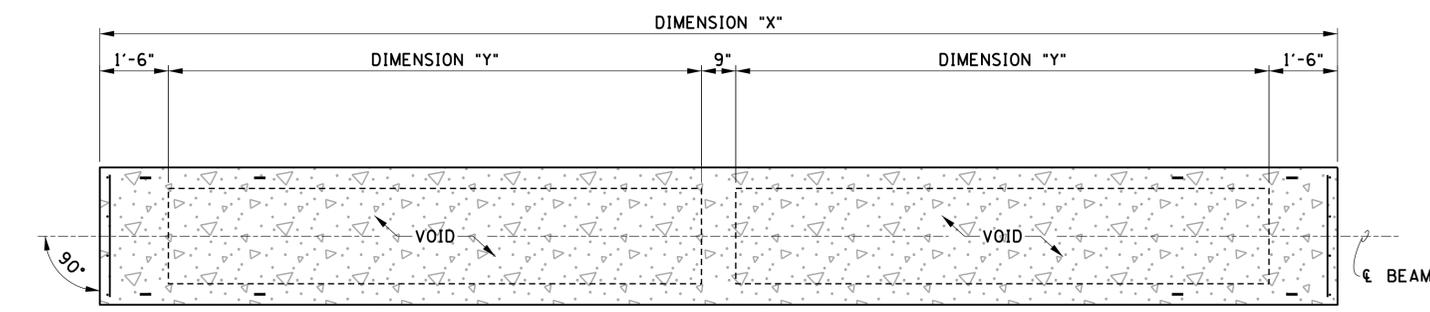
**BEAM ELEVATION**

BEAM TYPE	DIMENSION	
	"X"	"Y"
"A"	26'-10 <sup>3</sup> / <sub>8</sub> "	11'-6 <sup>1</sup> / <sub>16</sub> "
"B"	27'-0 <sup>1</sup> / <sub>16</sub> "	11'-7 <sup>13</sup> / <sub>16</sub> "
"C"	27'-3"	11'-9"
"D"	27'-5 <sup>5</sup> / <sub>8</sub> "	11'-10 <sup>5</sup> / <sub>8</sub> "
"E"	27'-7 <sup>5</sup> / <sub>8</sub> "	11'-11 <sup>5</sup> / <sub>16</sub> "
"F"	27'-10"	12'-0 <sup>1</sup> / <sub>2</sub> "
"G"	26'-5 <sup>5</sup> / <sub>8</sub> "	11'-4 <sup>1</sup> / <sub>16</sub> "
"H"	26'-8 <sup>3</sup> / <sub>16</sub> "	11'-5 <sup>5</sup> / <sub>16</sub> "

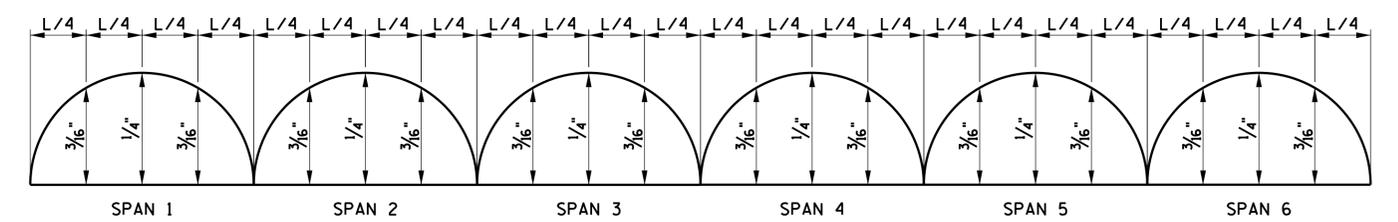
**BEAM NOTES:**

- \* DENOTES: PROJECTING BARS
- \*\* DENOTES: NON-PROJECTING BARS
- NOTE: 2'-0" MIN. SPLICE NO. 5 BARS (TYP.)
- NOTE: SEE STD-14-3 FOR BOX BEAM STANDARD DETAILS, NOTES AND REINFORCING. THE CONCRETE FOR THIS CONSTRUCTION SHALL BE OF SUCH PROPERTIES AS TO ATTAIN A COMPRESSIVE STRENGTH OF NOT LESS THAN 4,000 PSI AT THE AGE OF 28 DAYS AND STRESS TRANSFER SHALL NOT BE MADE TO THE BRIDGE MEMBER UNTIL THE TEST SPECIMENS INDICATE THAT THE CONCRETE HAS REACHED A COMPRESSIVE STRENGTH OF AT LEAST 5,000 PSI. SEE GENERAL NOTES (SHEET 2A) FOR CONCRETE FINISHING NOTE.

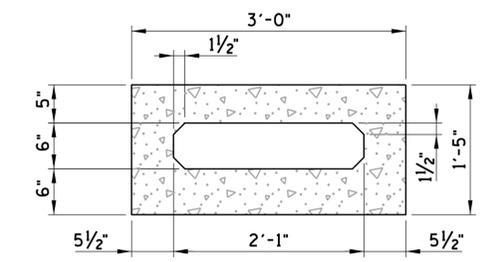
CONST. NO.			
PROJECT NO.	YEAR	SHEET NO.	
59006-4219-04	2015		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



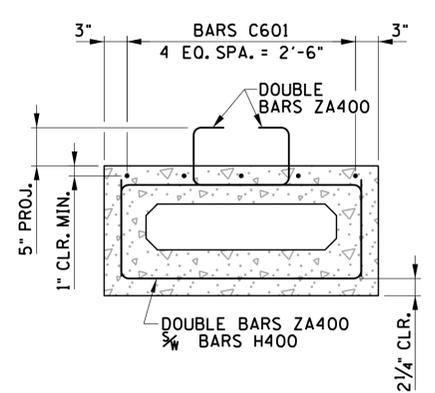
**BEAM PLAN VIEW**



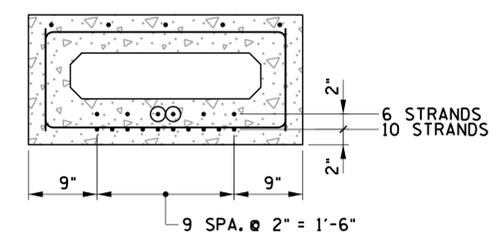
**DEAD LOAD CORRECTION CURVE**  
NOTE: THIS CURVE IS FOR SLAB DEAD LOAD AND ALL DEAD LOADS THAT ARE APPLIED AFTER THE SLAB IS IN PLACE.



**SECTION SHOWING PROPERTIES**



**SECTION "A-A"**



**SECTION "B-B"**  
(SHOWING PRESTRESSING STRANDS @ 16 TOTAL STRANDS)  
⊙ DENOTES: BOND BREAK 6'-0" FROM END OF BEAM.

**UNOFFICIAL SET**  
NOT FOR BIDDING

DEPARTMENT OF TRANSPORTATION  
**PRESTRESSED CONCRETE BOX BEAM DETAILS**

BRIDGE NO. 59-SR50-6.57  
SR50 OVER BIG ROCK CREEK  
MARSHALL COUNTY  
2015

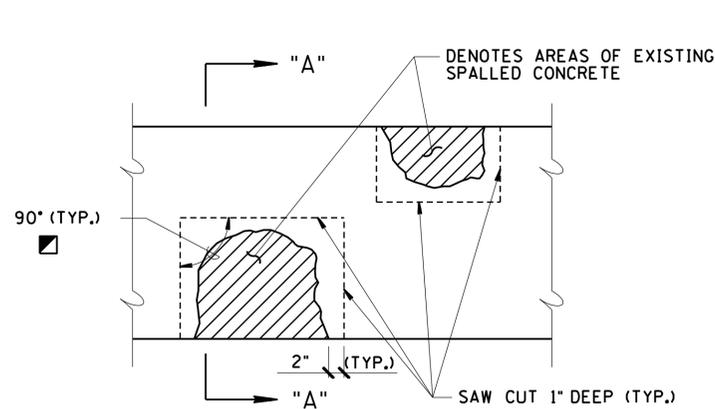
DESIGNED BY D. THOMPSON/D. KEATON DATE \_\_\_\_\_  
DRAWN BY ANGELA MOORE DATE \_\_\_\_\_  
SUPERVISED BY DARRELL JAMES DATE \_\_\_\_\_  
CHECKED BY JAMIE GILLESPIE DATE \_\_\_\_\_









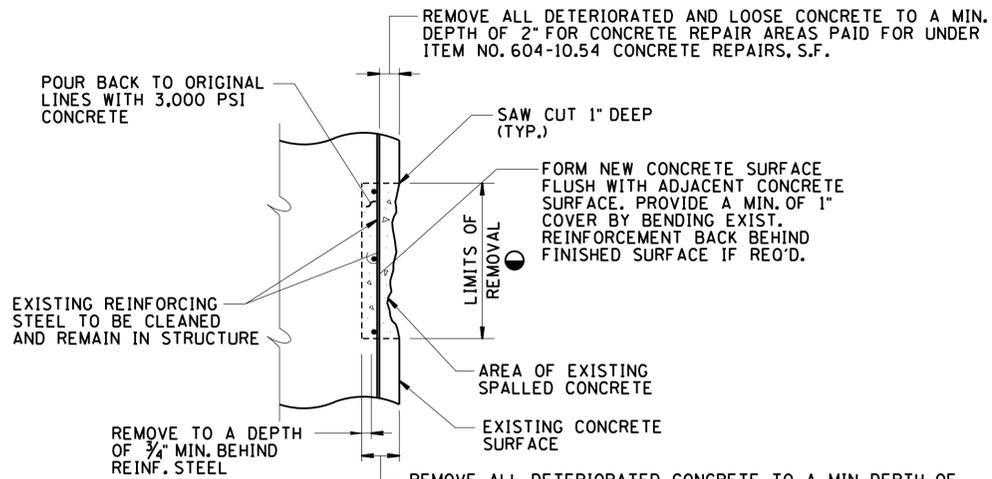


**DETAIL SHOWING AREAS OF EXISTING SPALLED CONCRETE SURFACES TO BE REMOVED AND REPAIRED**

- DENOTES: LIMITS AND LOCATION OF REPAIRS DESIGNATED ON THE CONCRETE REPAIR SHEETS.
- ▣ DENOTES: SAW CUT EXISTING CONCRETE SURFACES SO AS TO OBTAIN SQUARED CORNERS.

NOTE: THE COST OF REMOVING PORTIONS OF THE EXISTING CONCRETE WITHIN THE LIMITS SHOWN, SAW CUTTING, COMPLETELY CLEANING EXISTING REINFORCING STEEL, CONCRETE, FORMING, LABOR AND ALL MISCELLANEOUS MATERIAL, INCLUDING REINFORCING STEEL, NECESSARY TO COMPLETE THE REPAIRS AS SHOWN SHALL BE INCLUDED IN PRICE BID FOR ITEM NO. 604-10.05, CONCRETE, S.F. OR ITEM NO. 604-10.54, CONCRETE REPAIRS, S.F.

1. THE ENGINEER SHALL DESIGNATE ALL CONCRETE REPAIR AREAS IN THE FIELD. QUANTITIES GIVEN ARE APPROXIMATE. ITEM NO. 604-10.05 AND ITEM NO. 604-10.54 MAY BE INCREASED, DECREASED, OR ELIMINATED BY THE ENGINEER.
2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE STABILITY OF THE STRUCTURE DURING CONSTRUCTION. DESIGN CALCULATION AND DETAILS OF TEMPORARY SUPPORT SYSTEM OR FALSEWORK REQUIRED SHALL BE SUBMITTED TO THE ENGINEER FOR REVIEW AND SHALL MEET WITH THE FULL SATISFACTION OF THE ENGINEER BEFORE ANY DEMOLITION IS BEGUN. COST OF STABILIZING THE STRUCTURE SHALL BE INCLUDED IN ITEM NO. 602-10.05, BRACING REPAIRS, L.S..
3. THE ENGINEER SHALL HAVE THE OPTION OF DESIGNATING A SPALL AREA TO BE REPAIRED UNDER ITEM NO. 604-10.05 OR ITEM NO. 604-10.54. PATCHING MATERIAL FOR ITEM NO. 604-10.05 SHALL BE 3,000 PSI CONCRETE. PATCHING MATERIAL FOR ITEM NO. 604-10.54 SHALL BE A POLYMER MODIFIED CEMENTITIOUS STRUCTURAL PATCHING MATERIAL. SEE QUALIFIED PRODUCTS LIST 13, SECTION B.6.
4. EXTREME CARE SHALL BE TAKEN WHEN REMOVING THE EXISTING SPALLED CONCRETE SO AS NOT TO DAMAGE THE EXISTING REINFORCING STEEL. ALL EXPOSED REINFORCING STEEL SHALL RECEIVE A COMPLETE CLEANING TO REMOVE ALL RUST. ALL EXISTING REINFORCEMENT SHALL REMAIN IN PLACE. ALL WORK MUST MEET WITH THE FULL APPROVAL OF THE ENGINEER.
5. POWER DRIVEN HAND TOOLS USED FOR REMOVAL OF UNSOUND CONCRETE ARE SUBJECT TO THE FOLLOWING RESTRICTIONS:
  - A. PNEUMATIC HAMMERS HEAVIER THAN 35 LB. CLASS SHALL NOT BE USED.
  - B. CHIPPING HAMMERS OF THE 15 LB. CLASS SHALL BE USED TO REMOVE CONCRETE FROM BEHIND REINFORCING STEEL AND BEAM END REPAIRS.
6. EXTREME CARE SHALL BE TAKEN WHEN REMOVING EXISTING CONCRETE FROM OVER THE BENT CAPS SO AS NOT TO DAMAGE ANY REINFORCING STEEL TO REMAIN. ALL EXPOSED REINFORCING STEEL SHALL BE COMPLETELY CLEANED TO THE SATISFACTION OF THE ENGINEER.



**SECTION "A-A"**

BRIDGE NO. 59-SR50-6.57	
ESTIMATED QUANTITIES	
<b>ABUTMENT NO. 1</b>	
ITEM NO. 604-10.05 CONCRETE S.F.	ITEM NO. 604-10.54 CONCRETE REPAIRS S.F.
0	0
<b>PIER NO. 1</b>	
ITEM NO. 604-10.05 CONCRETE S.F.	ITEM NO. 604-10.54 CONCRETE REPAIRS S.F.
20	65
<b>PIER NO. 2</b>	
ITEM NO. 604-10.05 CONCRETE S.F.	ITEM NO. 604-10.54 CONCRETE REPAIRS S.F.
25	43
<b>PIER NO. 3</b>	
ITEM NO. 604-10.05 CONCRETE S.F.	ITEM NO. 604-10.54 CONCRETE REPAIRS S.F.
70	157
<b>PIER NO. 4</b>	
ITEM NO. 604-10.05 CONCRETE S.F.	ITEM NO. 604-10.54 CONCRETE REPAIRS S.F.
65	129
<b>PIER NO. 5</b>	
ITEM NO. 604-10.05 CONCRETE S.F.	ITEM NO. 604-10.54 CONCRETE REPAIRS S.F.
0	1
<b>ABUTMENT NO. 2</b>	
ITEM NO. 604-10.05 CONCRETE S.F.	ITEM NO. 604-10.54 CONCRETE REPAIRS S.F.
0	0
TOTAL FOR ITEM NO. 604-10.05 CONCRETE S.F.	TOTAL FOR ITEM NO. 604-10.54 CONCRETE REPAIRS S.F.
180	395

CONST. NO.			
PROJECT NO.	YEAR	SHEET NO.	
59006-4219-04	2015		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

**UNOFFICIAL SET**  
  
 NOT FOR BIDDING

DEPARTMENT OF TRANSPORTATION

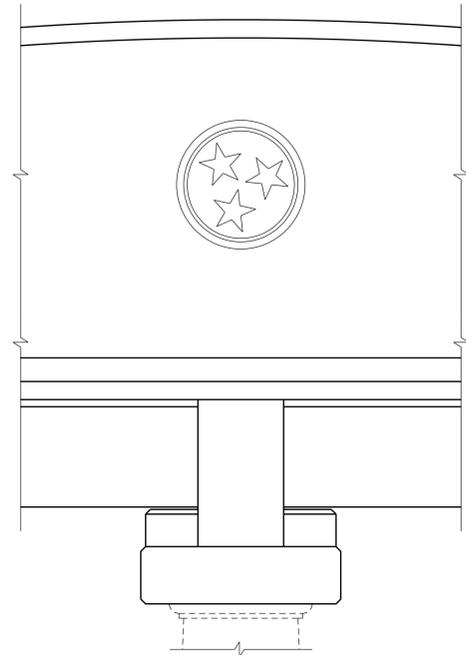
CONCRETE REPAIR DETAILS

BRIDGE NO. 59-SR50-6.57  
SR50 OVER BIG ROCK CREEK  
MARSHALL COUNTY  
2015

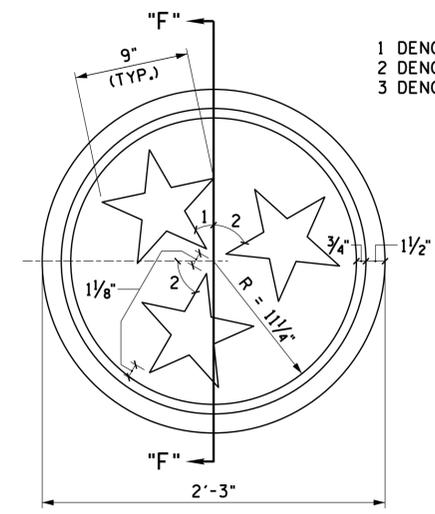
DESIGNED BY D. THOMPSON/D. KEATON DATE \_\_\_\_\_  
 DRAWN BY ANGELA MOORE DATE \_\_\_\_\_  
 SUPERVISED BY DARRELL JAMES DATE \_\_\_\_\_  
 CHECKED BY JAMIE GILLESPIE DATE \_\_\_\_\_



CONST. NO.			
PROJECT NO.	YEAR	SHEET NO.	
59006-4219-04	2015		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

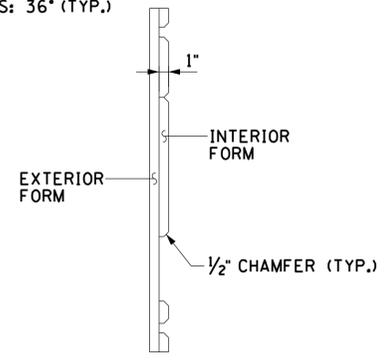


**BRIDGE RAIL TREATMENT**

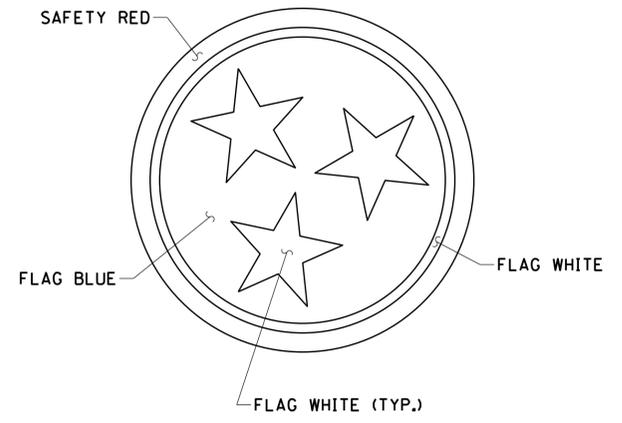


**ARCHITECTURAL TREATMENT  
3 STAR SYMBOL FORM DETAIL**

- 1 DENOTES: 30°
- 2 DENOTES: 60°
- 3 DENOTES: 36° (TYP.)



**SECTION "F-F"**



**PAINTING DETAIL  
LOCATION OF COLORS**

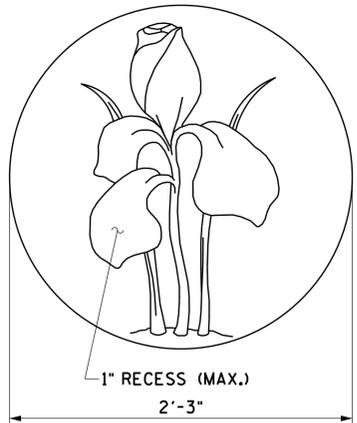
**NOTES:**

STATE SYMBOL TO BE "EMBOSSSED" INTO THE CONCRETE.  
 STATE SYMBOL IS TO BE PAINTED.  
 IRIS AND CHRYSANTHEMUM SHALL BE "EMBOSSSED" INTO THE CONCRETE AND PAINTED. FINAL DESIGN AND COLORS SHALL BE APPROVED BY THE CITY OF LEWISBURG.

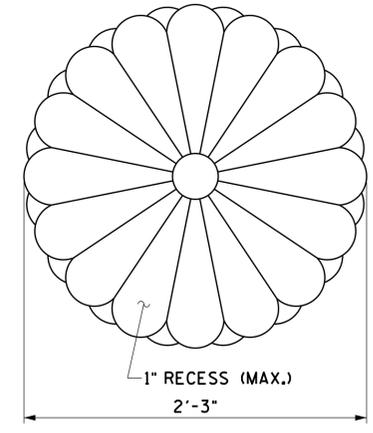
**PAINTING STATE SEAL:**

THE STATE SEAL SHALL BE PAINTED WITH THE FOLLOWING PAINTS OR APPROVED EQUAL. IF THE CONTRACTOR ELECTS USE OF AN APPROVED EQUAL PAINT FROM A DIFFERENT COMPANY, IT SHALL BE A MARINE ACRYLIC GLOSS PAINT WITH A SIMILAR COLOR AND PAINT PROPERTIES. A COLOR SAMPLE SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL. THE COST OF THE PREPARATION OF SURFACES, PAINT, SEALER, SEALING THE PAINTED SURFACES, AND ALL LABOR REQUIRED FOR PAINTING THE STATE SEAL SHALL BE INCLUDED IN ITEM NO. 604-04.41.

- RED SHERWIN WILLIAMS SHER-CRYL HPA HIGH PERFORMANCE ACRYLIC GLOSS "SAFETY RED"  
B66 R300  
6403-31922
- WHITE SHERWIN WILLIAMS SHER-CRYL HPA HIGH PERFORMANCE ACRYLIC GLOSS "FLAG WHITE"  
B66 T304  
6401-74215
- BLUE SHERWIN WILLIAMS SHER-CRYL HPA HIGH PERFORMANCE ACRYLIC GLOSS "FLAG BLUE"  
B66 T304  
6401-74215
- CLEAR COAT WATERBORNE ACRYLIC CLEAR COAT CLEAR GLOSS  
B66 C375  
6403-60111



**IRIS DETAIL**



**CHRYSANTHEMUM DETAIL**

**UNOFFICIAL  
SET**

NOT FOR  
BIDDING

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

DECORATIVE SURFACE FINISH

BRIDGE NO. 59-SR50-6.57  
SR50 OVER BIG ROCK CREEK  
MARSHALL COUNTY  
2015

DESIGNED BY D. THOMPSON/D. KEATON DATE \_\_\_\_\_  
 DRAWN BY ANGELA MOORE DATE \_\_\_\_\_  
 SUPERVISED BY DARRELL JAMES DATE \_\_\_\_\_  
 CHECKED BY JAMIE GILLESPIE DATE \_\_\_\_\_



