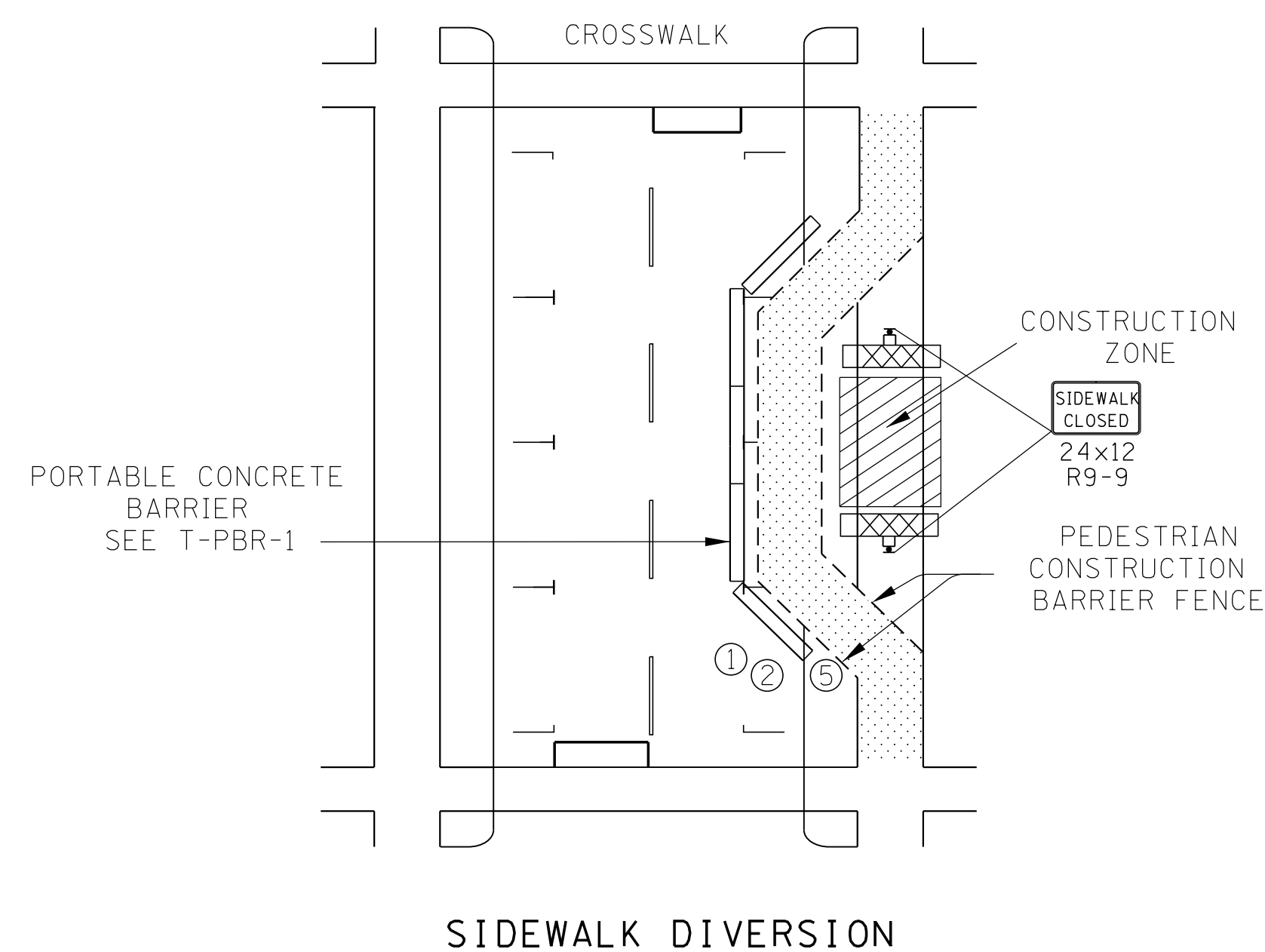
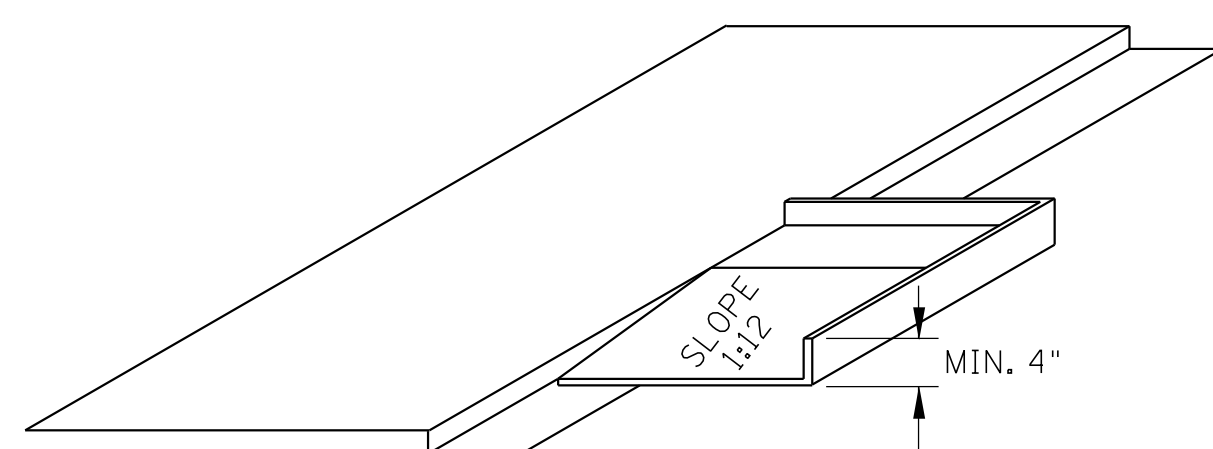


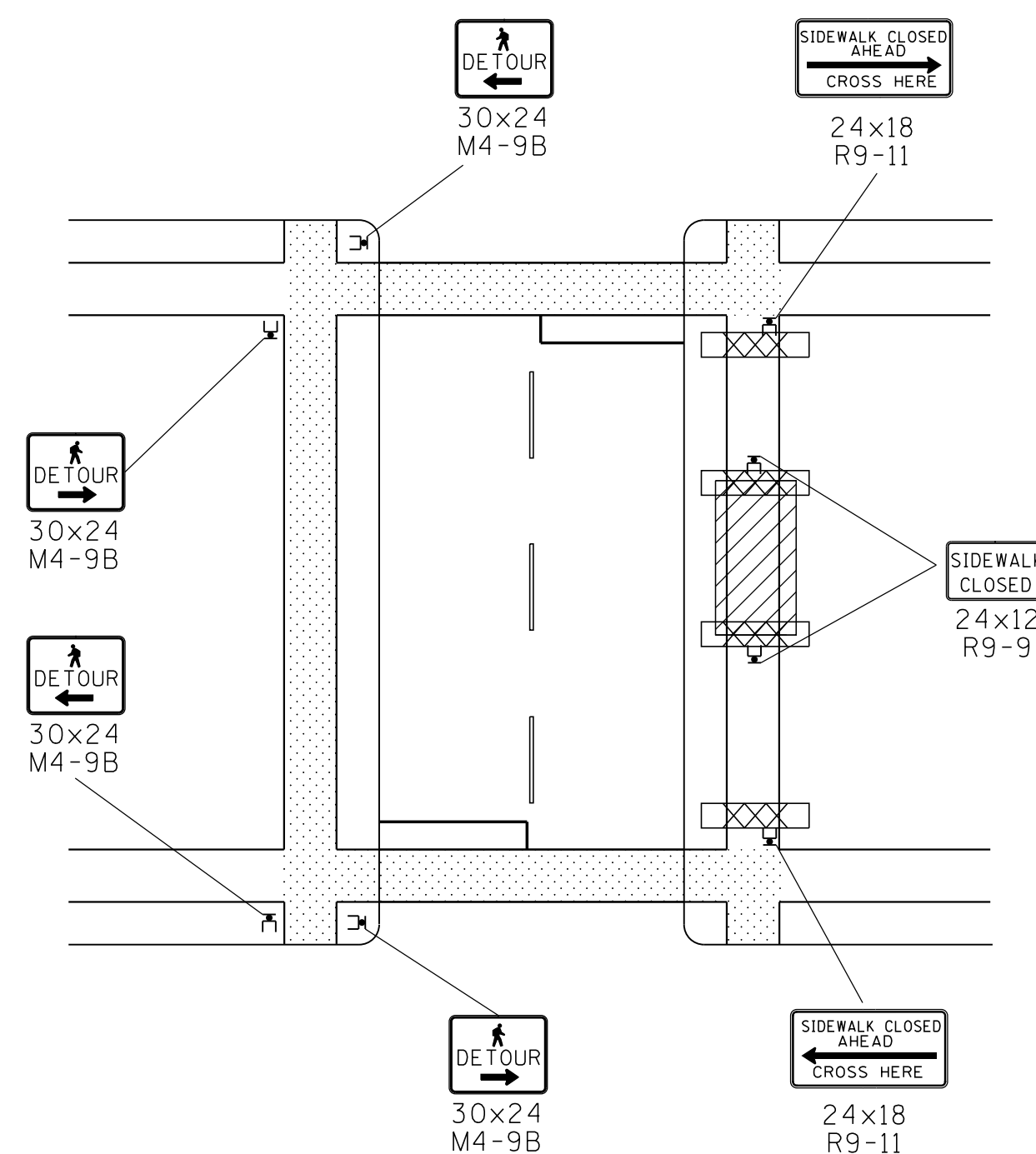
REV. 6-30-14: ADDED ITEM NUMBER FOR PEDESTRIAN CONSTRUCTION BARRIER FENCE.
 REV. 10-10-16: UPDATED GENERAL NOTE (E) FOR SIDEWALK DIVERSION.



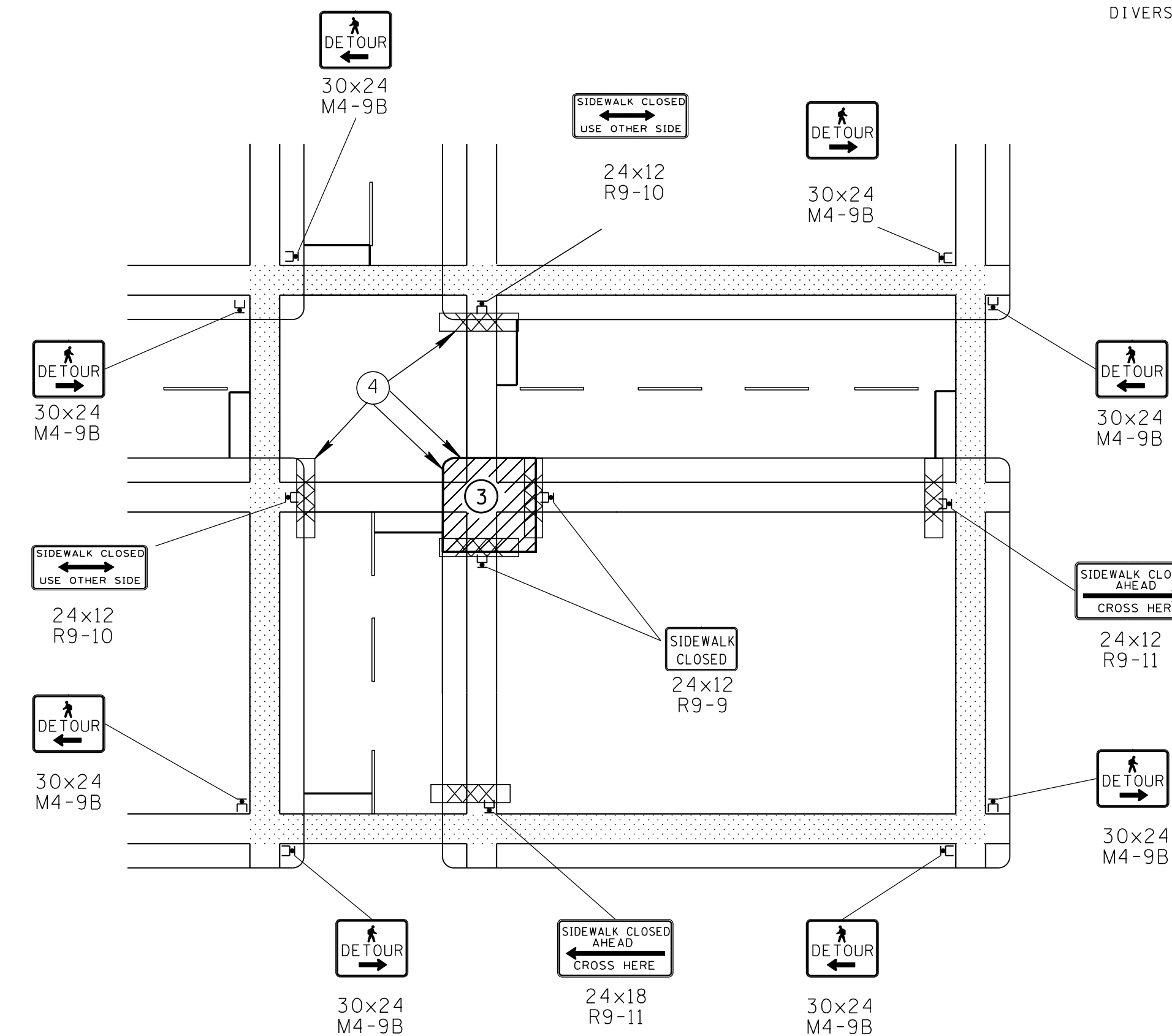
SIDEWALK DIVERSION



PLYWOOD CURB RAMP DETAIL



SIDEWALK CLOSURE, MIDBLOCK



SIDEWALK CLOSURE, CORNER

GENERAL NOTES FOR SIDEWALK DIVERSION

- (A) SIDEWALK DIVERSION MAY BE USED ON ROADS WITH ON STREET PARKING LANES ADJACENT TO THE SIDEWALK CLOSURE.
- (B) THE PEDESTRIAN WALKWAY SHALL BE AT LEAST 5' WIDE.
- (C) TEMPORARY FACILITIES SHALL BE COMPLIANT WITH THE CURRENT VERSION OF THE AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES (ADAAG).
- (D) DIVERSIONS MUST BE CLEARLY IDENTIFIED, PROTECTED FROM TRAFFIC AND FREE FROM HAZARDS.
- (E) PEDESTRIAN CONSTRUCTION BARRIER FENCE SHALL BE CONTINUOUS THROUGHOUT THE LENGTH OF THE DIVERSION WITH A DETECTABLE EDGING WITH A BOTTOM NO HIGHER THAN 2" ABOVE THE SURFACE AND A TOP NO LOWER THAN 32" ABOVE THE SURFACE. THE PEDESTRIAN CHANNELIZATION DEVICE SHALL BE ORANGE. HIGH VISIBILITY FENCE, PEDESTRIAN, RAIL, AND CHAIN LINK FENCE ARE ACCEPTABLE. COST OF FENCE TO BE PAID UNDER ITEM NUMBER:
 707-11.01 PEDESTRIAN CONSTRUCTION BARRIER FENCE PER L.F.
- (F) CROSSING THE DIVERSION PATH BY CONSTRUCTION VEHICLES SHOULD BE AVOIDED, WHEN NECESSARY, IT SHALL BE CONTROLLED BY FLAGGER.
- (G) TRAFFIC CONTROL DEVICES FOR VEHICULAR TRAFFIC ARE NOT SHOWN BUT ARE REQUIRED FOR CLOSING THE LANE.
- (H) A SMOOTH, HARD, CONTINUOUS AND RIDEABLE SURFACE SHALL BE PROVIDED THROUGHOUT THE LENGTH OF THE DIVERSION.
- (I) THE COST OF MAINTAINING PEDESTRIAN DIVERSION, INCLUDING CURB RAMPS IF NEEDED, SHALL NOT BE PAID DIRECTLY BUT PAID FOR IN THE COST OF OTHER ITEMS.

FOOTNOTES

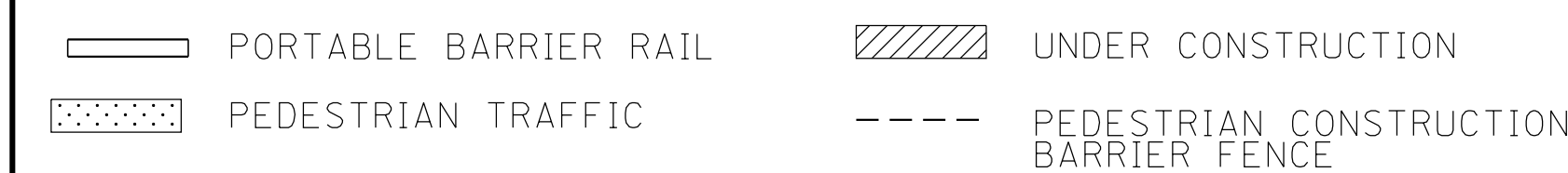
- (1) IF PARKING STALLS ARE USED FOR DIVERSION, CHANNELIZING DEVICES MAY BE SUBSTITUTED FOR PORTABLE BARRIER RAILS IF PORTABLE BARRIER RAILS ARE DEEMED UNNECESSARY BY ENGINEERING JUDGEMENT.
- (2) IF DIVERSION REQUIRES A LANE CLOSURE SEE T-WZ-SERIES FOR FURTHER INFORMATION.
- (3) LIMIT WORK TO ONE CORNER AT A TIME TO MINIMIZE DISRUPTION TO PEDESTRIAN TRAFFIC.
- (4) PEDESTRIAN TRAFFIC SIGNAL DISPLAYS CONTROLLING CLOSED CROSSWALKS SHALL BE COVERED.
- (5) IN AREAS WHERE THE ROUTE CROSSES GRASSY TERRAIN OR ELEVATION CHANGES, PLYWOOD MAY BE USED WITH A HIGHLIGHTED BEVEL AT THE JOINT.

GENERAL NOTES FOR SIDEWALK CLOSURE

- (A) TRAFFIC CONTROL DEVICES FOR VEHICULAR TRAFFIC ARE NOT SHOWN BUT MAY BE REQUIRED TO CONTROL VEHICLES THROUGH WORK ZONE.
- (B) SIGNS R9-9, R9-10 AND R9-11 TO BE ATTACHED TO TYPE III BARRICADE. ALL OTHER SIGNS SHOWN ON THIS PLAN MAY BE PLACED ON PORTABLE SUPPORTS.
- (C) MINIMIZE PEDESTRIAN OUT-OF-DIRECTION TRAVEL. IT IS NOT ACCEPTABLE TO REQUIRE PEDESTRIANS TO RETRACE THEIR PATH TO FIND A SAFE CROSSING.
- (D) DETOUR SHALL BE DETECTABLE AND INCLUDE ACCESSIBILITY FEATURES CONSISTENT WITH THE FEATURES PRESENT IN THE EXISTING FACILITY.
- (E) BARRICADES SHALL BE PLACED ACROSS THE FULL WIDTH OF THE CLOSED SIDEWALK.
- (F) WORK SHALL BE EXPEDITED TO MINIMIZE IMPACTS TO BUSINESS CAUSED BY THE SIDEWALK CLOSURE.

MINOR REVISION -- FHWA APPROVAL NOT REQUIRED.

LEGEND



STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

SIDEWALK TRAFFIC CONTROL

2-29-12 T-WZ-55

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